

MAINE TURNPIKE AUTHORITY

ADDENDUM NO. 2

CONTRACT 2023.01

**PAVEMENT REHABILITATION
AND DRAINAGE IMPROVEMENTS
MM 88.6 TO MM 98.0**

The following changes are made to the CONTRACT DOCUMENTS:

PROPOSAL SHEETS:

Remove P-2 thru P-6 and replace with attached.

SPECIFICATIONS:

Remove SP-12, SP-37, SP-47, SP-53 and SP-83 and replace with attached.

PLANS:

Remove Plan Sheet 2 and 7 and replace with attached.

Add 103 ORT barrier plan sheet to end of plan sheets.

PRE-BID MEETING MINUTES:

Attached.

QUESTIONS:

Q1: Is the table in section 401.165 Longitudinal Joint Density accurate with 0% payment for any core that falls below 88.4%?

A1: See SP-37 Revised Addendum #2.

Q2: Will the Authority allow a 7' mill on the 8' shoulder sections?

A2: See SP-12 Revised Addendum #2.

Q3: Will the Authority allow the 8' shoulder to be paved independently?

A3: No.

Notes: The above items shall be considered as part of the bid submittal.

The total number of pages included with this addendum is 21

All bidders are requested to acknowledge the receipt of the Addendum No. 2 by signing below and faxing this sheet to Nate Carll, Purchasing Department, MTA at 207-871-7739. Bidders are also required to acknowledge receipt of this Addendum No. 2 on Page P-7 of the bid package.

Business Name

Print Name and Title

Signature

Date

February 2, 2023

Very truly yours,
MAINE TURNPIKE AUTHORITY

Nathaniel Carll
Purchasing Manager
Maine Turnpike Authority

**SCHEDULE OF BID PRICES
CONTRACT NO. 2023.01
Pavement Rehabilitation Drainage Improvements
MM 88.6 to MM 98.0**

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
202.202	Removing Pavement Surface - Mainline	Square Yard	342,000				
202.2026	Removing Pavement Surface - Drainage Paths	Square Foot	2,500				
202.2031	Pavement Butt Joints - 103 ORT	Linear Foot	4,880				
202.205	Rumble Strips	Each	135,000				
203.20	Common Excavation	Cubic Yard	550				
203.25	Granular Borrow	Cubic Yard	350				
304.10	Aggregate Subbase Course - Gravel	Cubic Yard	105				
304.14	Aggregate Base Course - Type A	Cubic Yard	45				
403.2081	Hot Mix Asphalt, 12.5mm (Polymer Modified)- RAP	Ton	34,000				
403.2084	Hot Mix Asphalt, 12.5mm Nominal Maximum Size(sidewalks, drives, islands & incidentals)	Ton	150				
403.209	Hot Mix Asphalt 9.5mm Nominal Maximum Size(sidewalks, drives, islands & incidentals)	Ton	175				

CARRIED FORWARD:

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
BROUGHT FORWARD:							
403.211	Hot Mix Asphalt 9.5mm (shimming)	Ton	200				
409.152	Bituminous Tack Coat Trackless - Applied	Gallon	22,600				
419.30	Full Depth Bituminous Sawcut	Linear Foot	1,900				
424.323	Asphalt Rubber Mastic Crack Sealant	Pound	49,000				
459.061	Bituminous Concrete Waterway - Type II	Each	5				
470.08	Berm Dropoff Correction - Grindings	Ton	3,120				
470.081	Berm Correction	Linear Foot	50,000				
603.179	18 inch Culvert Pipe - Option III	Linear Foot	78				
603.199	24 inch Culvert Pipe - Option III	Linear Foot	171				
603.209	30 inch Culvert Pipe - Option III	Linear Foot	120				
603.219	36 inch Culvert Pipe - Option III	Linear Foot	36				
603.28	Concrete Collar	Each	19				

CARRIED FORWARD:

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
BROUGHT FORWARD:							
604.184	Rebuild Catch Basin to Grade - Type II	Each	22				
604.186	Rebuild Catch Basin to Grade - Type IV	Each	5				
606.132	31" W-Beam Guardrail - Mid-Way Splice (7' Steel Posts, 8" Offset Blocks, Double Faced)	Linear Foot	2,005				
610.08	Plain Rip Rap	Cubic Yard	225				
610.18	Stone Ditch Protection	Cubic Yard	150				
613.319	Erosion Control Blanket	Square Yard	2,000				
615.07	Loam	Cubic Yard	245				
618.1401	Seeding Method Number 2, Plan Quantity	Unit	15				
619.1201	Mulch, Plan Quantity	Unit	15				
619.1202	Temporary Mulch	Lump Sum	1				
620.58	Erosion Control Geotextile	Square Yard	900				
627.73	Temporary Pavement Marking Line Tape	Linear Foot	7,300				

CARRIED FORWARD:

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
BROUGHT FORWARD:							
627.78	Temporary Pavement Marking Line	Linear Foot	291,200				
627.812	Temporary Raised Pavement Markers	Each	19,410				
627.94	Pavement Marking Tape	Linear Foot	3,650				
629.05	Hand Labor, Straight Time	Hour	30				
631.10	Air Compressor (including operator)	Hour	10				
631.11	Air Tool (including operator)	Hour	10				
631.12	All Purpose Excavator (including operator)	Hour	20				
631.172	Truck - Large (including operator)	Hour	40				
631.18	Chain Saw Rental (including operator)	Hour	20				
631.36	Foreman	Hour	20				
652.30	Flashing Arrow	Each	4				
652.35	Construction Signs	Square Foot	4,134				

CARRIED FORWARD:

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
BROUGHT FORWARD:							
652.3611	Maintenance of Traffic Control Devices	Lump Sum	1				
652.411	Portable - Changeable Message Sign	Each	2				
652.45	Truck Mounted Attenuator	Cal. Day	175				
652.452	Automated Trailer Mounted Speed Limit Sign	Each	4				
656.50	Baled Hay, in place	Each	50				
656.632	30 inch Temporary Silt Fence	Linear Foot	400				
659.10	Mobilization	Lump Sum	1				
TOTAL:							

sensor, contact or otherwise, shall not be permitted. A copy of the automation operations manual shall be provided to the resident upon request. The equipment shall also have an effective means for removing excess material from the surface and preventing flying material in compliance with Subsections 105.2.5 Compliance with Health and Safety Laws and 105.2.6 Convenience of the Public, of the Specification.

The rotary drum on the machine shall be a minimum of 12 feet in width **for the travel lanes and wide enough to mill the shoulders in a single pass** and utilize carbide or diamond tipped tools at a maximum 8mm tooth spacing pattern and a minimum triple wrap configuration. The difference in height from the top of any ridge to the bottom of the groove adjacent to that ridge shall not exceed $\frac{1}{4}$ inch. The cutting tools on the rotary drum shall be continually maintained and shall be replaced as warranted to provide a uniform milled pavement texture. The forward operating speed shall be limited to a maximum speed of 50 feet per minute (fpm). The limited speed is not to be calculated on an average basis over time but shall be the actual limitation at any moment during the milling operation.

The track pads that the machine travel on shall all be of a uniform thickness equal to or exceeding the manufacturers recommendations. A copy of the manufacturers' recommendations shall be provided to the resident upon request.

The Contractor shall locate, identify and remove all objects in the pavement through the work area that would be detrimental to the milling machine.

The Contractor shall be responsible for the layout of the longitudinal centerline along the crown line. The contractor shall layout the site prior to any milling. Layout shall be achieved by physical measurements obtained every 50' along the length to be milled from a fixed reference point. The contractor shall transfer the measurements to the pavement surface every 50' and apply a paint mark at each location. The marks shall then be connected by a smoothed string line and subsequent paint marks applied along the string at no greater than 10' intervals. The Resident will inspect the layout line before milling activities may begin.

The finished milled surface will be inspected before being accepted, and any deviations in the profile exceeding $\frac{3}{8}$ inch under a 16 foot string line or straightedge placed parallel to the centerline will be corrected. Any deviations in the cross slope that exceed $\frac{3}{8}$ inch under a 12 foot string line or straightedge placed transversely to the centerline will be corrected. In no case shall the cross slope in a single lane width be inverted resulting in a depression as measured transverse to the direction of travel. Any cross slope inversions or depressions shall be corrected by spot shimming the area with HMA as directed by the resident prior to installing any leveling or wearing course. Any areas requiring corrections will be subject to the same acceptable surface tolerances. These corrections shall be done with no additional expense to the Authority. Excess material that becomes bonded to the milled surface shall be removed to the Resident's satisfaction before the area is accepted.

No additional course shall be constructed on a course until the density of the sample has been established and approved.

The minimum density of the completed pavement shall be 92.0 percent of the theoretical maximum density obtained. Two consecutive failing tests shall result in production shut down. Prior to resuming paving operations, the contractor quality control unit shall satisfy the Authority that the paving operation will produce joint densities in compliance with the Specifications.

The pavement will be accepted for joint density on a subplot basis. A subplot will consist of 500 tons. The number of cores per day will be computed as one for every 500 tons plus one for any portion that does not equal 500 tons or as directed by the Resident. There shall be a minimum of one subplot per day per JMF.

Each subplot will be evaluated separately and full or partial payment will be made based on the results of tests performed on the cores.

Payment reduction will be applied to each subplot that has a density lower than 92.0% as outlined below.

PERCENT COMPACTION	PERCENT PAY
92.0 or greater	100
91.9 to 90.0	95
89.9 to 88.5	90
88.4 or less	75

401.17 Joints

The Contractor shall construct wearing course transverse and longitudinal joints in such a manner that minimum tolerances shown in Subsection 401.101, Surface Tolerances, are met when measured with a straightedge.

The paver shall always maintain a uniform head of HMA during the joint construction.

The HMA shall be free of segregation and meet temperature requirements outlined in Subsection 401.04. Transverse joints of the wearing course shall be straight and neatly trimmed. The Contractor may form a vertical face exposing the full depth of the course by inserting a header, by breaking the bond with the underlying course, or by cutting back with hand tools.

When required by Special Provision Section 403, Mainline Longitudinal joints shall be constructed as notched-wedge joint and constructed in a manner that will best ensure joint integrity.

12.5 mm PGAB CONTENT	
% PGAB	% PAYMENT
JMF Aim ± 0.4	100
JMF Aim + 0.5 , - 0.5 , < 5.1	95
JMF Aim + 0.6 , - 0.6 , < 5.0	85
JMF Aim + 0.7 , - 0.7 , < 4.9	75
JMF Aim + 0.8 , - 0.8 , ≤ 4.8 , > 6.4	50
<u>Note:</u> PGAB content is based on samples tested at the plant for each 500 Ton subplot	

Gradation	
Sieve Size	% Deduction
% Passing #4 and larger sieves	2
% Passing #8 and #16 sieves	3
% Passing #30, #50, and #100 sieves	2
% Passing #200 sieve	3
<u>Note:</u> Gradation is based on samples tested at the plant for each 500 Ton subplot	

As an example of payment reduction, if a subplot of 500 tons of 12.5mm was tested and found to have 96 percent TMD compaction, 5.8 percent air voids and asphalt content of 5.19 percent, the payment reduction would be as follows:

$$\begin{array}{rcl}
 500 \text{ tons} \times 1.00 & = & 500 \text{ tons payment} = 0 \text{ tons reduction (compaction)} \\
 500 \text{ tons} \times 0.95 & = & 475 \text{ tons payment} = 25 \text{ tons reduction (voids)} \\
 500 \text{ tons} \times 0.95 & = & 475 \text{ tons payment} = 25 \text{ tons reduction (asphalt content)}
 \end{array}$$

$$\text{Payment} = 500 \text{ tons} - (0 + 25 + 25) = 450 \text{ tons.}$$

401.22 Basis of Payment

The Authority will pay for the work, in place and accepted, in accordance with the applicable sections of this Section, for each type of HMA specified.

The Authority will pay for the work specified in Subsection 401.11, for the HMA used, except that cleaning objectionable material from the pavement and furnishing and applying bituminous material to joints and contact surfaces is incidental.

Payment for this work under the appropriate pay items shall be full compensation for all labor, equipment, materials, and incidentals necessary to meet all related Contract requirements, including design of the JMF, implementation of the QCP, obtaining core samples, transporting cores and samples, filling core holes, applying specified material to joints, and providing testing facilities and equipment.

SPECIAL PROVISION

SECTION 403

HOT MIX ASPHALT PAVEMENT

Course	HMA Grading	Item Number	Total Thickness	No. of Layers	Complimentary Notes
--------	-------------	-------------	-----------------	---------------	---------------------

Mainline Mill and Fill/Overlay

Wearing	12.5 mm	403.2081	1.75"	1	A,D,E,F,G,H,I,J,K
---------	---------	----------	-------	---	-------------------

Slip Ramp S.B. 92.8

Wearing	12.5 mm	403.2084	2.0"	1	C,I
Binder	12.5 mm	403.2084	2.0"	1	C,I

Spot Shims/Delaminated Areas/Incidentals (As Directed by the Resident)

103 ORT Pavement Wedge

Shim	9.5 mm	403.211	variable	1	C,I
Incidentals	12.5 mm	403.2084	variable	1	C,I
103 ORT	9.5 mm	403.209	variable	1	C,I

COMPLEMENTARY NOTES

- A. The required PGAB for this mixture shall be **64E-28**.
- B. RAP may not be used.
- C. The Maine DOT will conduct the job mix verification. The aggregate qualities shall meet the design traffic level of 3 to <10 million ESALS for mix placed under this contract. Minimum and Maximum PGAB content limits from 401.21 shall not apply.
- D. The MTA will conduct the job mix verification. The aggregate qualities shall meet the design traffic level of 10 to <30 million ESALS for mix placed under this contract. The design verification, Quality Control, and Acceptance tests for this mix will be performed at **75 gyrations**. (N design)
- E. A material transfer vehicle (MTV) shall be used for the placement of Hot Mix Asphalt wearing surface on all roadways including acceleration and deceleration lanes and all ramps.

SPECIAL PROVISION

SECTION 652

MAINTENANCE OF TRAFFIC

(Specific Project Maintenance of Traffic Requirements)

This Specification describes the specific project maintenance of traffic requirements for this Project.

There are no lane closure restrictions for this project where the Contractor is responsible for maintenance of traffic.

The following shall dictate at what rate speed shall be reduced when work is being performed.

50 MPH Speed Limit – for all Paving and Milling Operations

60 MPH Speed Limit – for all other Operations

Temporary Concrete Barrier will only be allowed when there is an active work area behind it. Set up of the temporary concrete barrier shall occur just prior to the start of work and shall be removed as soon as the work is completed or as otherwise approved by the Resident.

A Spotter shall be required at the front and rear of the paving operation on the mainline or as approved by the Resident and shall not be measured for payment. All spotters shall be equipped with handheld radios and spare batteries. The spotters will be required to move and maintain channelization devices during the mobile paving operation.

652.62 Patrol Vehicle

The Contractor shall provide one traffic control vehicle(s) dedicated for traffic control only, with traffic coordinator(s) to be used for erecting, maintaining and dismantling lane closures as directed by the Resident. The traffic control vehicle(s) shall provide continuous patrolling (24-hours/seven days a week) when lane closures are installed (during non-work and work hours) to replace any and all damaged traffic control devices (arrow boards, variable message signs, barrels, signs, etc.). The traffic coordinator(s) shall report any and all disabled motorists, accidents or other

ESTIMATED QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY		PIPE TOTAL
			TOTAL	Cross over Removal	
202.202	Removing Pavement Surface - Mainline	SY	342,000		
202.2026	Removing Pavement Surface - Drainage Paths	SF	2,500		
202.2031	Pavement Butt Joints - 103 ORT	LF	4,880		
202.205	Rumble Strips	EA	135,000		
203.20	Common Excavation	CY	550		
203.25	Granular Borrow	CY	350		
304.10	Aggregate Subbase Course - Gravel	CY	105		
304.14	Aggregate Base Course - Type A	CY	45		
403.2081	Hot Mix Asphalt, 12.5mm (Polymer Modified)- RAP	Ton	34,000		
403.2084	Hot Mix Asphalt, 12.5mm Nominal Maximum Size(sidewalks, drives, islands & incidentals)	Ton	150		
403.209	Hot Mix Asphalt 9.5mm Nominal Maximum Size(sidewalks, drives, islands & incidentals)	Ton	175		
403.211	Hot Mix Asphalt 9.5mm (shimming)	Ton	200		
409.152	Bituminous Tack Coat Trackless - Applied	GAL	22,600		
419.30	Full Depth Bituminous Sawcut	LF		1,900	
424.32	Asphalt Rubber Mastic Crack Sealant	LB	49,000		
459.061	Bituminous Concrete Waterway - Type II	EA	5		
470.08	Berm Dropoff Correction - Grindings	Ton	3,120		
470.081	Berm Correction	LF	50,000		
603.179	18 inch Culvert Pipe - Option III	LF			78
603.199	24 inch Culvert Pipe - Option III	LF			171
603.209	30 inch Culvert Pipe - Option III	LF			120
603.219	36 inch Culvert Pipe - Option III	LF			36
603.28	Concrete Collar	EA			19
604.184	Rebuild Catch Basin to Grade - Type II	EA	22		
604.186	Rebuild Catch Basin to Grade - Type IV	EA	4	1	
606.132	31" W-Beam Guardrail - Mid-Way Splice (7" Steel Posts, 8" Offset Blocks, Double Faced)	LF		2,005	
610.08	Plain Rip Rap	CY			225
610.18	Stone Ditch Protection	CY			150
613.319	Erosion Control Blanket	SY		2,000	
615.07	Loam	CY		245	
618.1401	Seeding Method Number 2, Plan Quantity	UNIT		15	
619.1201	Mulch, Plan Quantity	UNIT		15	
619.1202	Temporary Mulch	LS	1		
620.58	Erosion Control Geotextile	SY	900		
627.730	Temporary Pavement Marking Line Tape	LF	7,300		
627.780	Temporary Pavement Marking Line	LF	291,200		
627.812	Temporary Raised Pavement Markers	EA	19,410		
627.94	Pavement Marking Tape	LF	3,650		
629.05	Hand Labor, Straight Time	HR	30		
631.10	Air Compressor (including operator)	HR	10		
631.11	Air Tool (including operator)	HR	10		
631.12	All Purpose Excavator (including operator)	HR	20		
631.172	Truck - Large (including operator)	HR	40		
631.18	Chain Saw Rental (including operator)	HR	20		
631.36	Foreman	HR	20		
652.30	Flashing Arrow	EA	4		
652.35	Construction Signs	SF	4,134		
652.3611	Maintenance of Traffic Control Devices	LS	1		
652.411	Portable - Changeable Message Sign	EA	2		
652.45	Truck Mounted Attenuator	CD	175		
652.452	Automated Trailer Mounted Speed Limit Sign	EA	4		
656.50	Baled Hay, in place	EA	50		
656.632	30 inch Temporary Silt Fence	LF	400		
659.100	Mobilization	LS	1		

GENERAL NOTES

- EXISTING UTILITIES ON THESE PLANS WERE COMPILED FROM EXISTING PLANS AND VARIOUS OTHER SOURCES. LOCATIONS ARE NOT GUARANTEED TO BE ACCURATE NOR IS IT GUARANTEED THAT ALL UTILITIES ARE SHOWN. NO SEPARATE OR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR DUE TO ANY VARIANCE BETWEEN THE DATA SHOWN ON THE PLANS AND THE ACTUAL FIELD CONDITIONS ENCOUNTERED, NO WORK SHALL BE STARTED UNTIL THE OWNERS OF THE VARIOUS UTILITIES ARE NOTIFIED BY THE CONTRACTOR OF THE PROPOSED CONSTRUCTION. THE CONTRACTOR IS ALSO REQUIRED TO CALL DIG SAFE AT 1-888-344-7233 AT LEAST 72 HOURS PRIOR TO THE START OF THE WORK.
- THE CONTRACTOR SHALL NOTIFY ALL NON-MEMBERS THROUGH WWW.OKtoDIG.COM OR AS OTHERWISE REQUIRED BY THE MAINE PUBLIC UTILITIES COMMISSION. ALL PROPOSED SIGN LOCATIONS AND EXCAVATION LOCATIONS SHALL BE MARKED AT THE NOTIFICATION TIME. NO EXCAVATION SHALL BE PERMITTED UNTIL THE AUTHORITY HAS LOCATED AND MARKED ITS UNDERGROUND UTILITIES. THE RESIDENT ENGINEER SHALL BE PROVIDED AN ELECTRONIC COPY OF TALL DIG SAFE TICKETS WITHIN 24 HOURS OF THEIR RELEASE FOR PROJECT NOTIFICATIONS AND 3RD PARTY UTILITY LOCATER COORDINATION.
- THE CONTRACTOR SHALL NOTIFY THE RESIDENT 10 CALENDAR DAYS PRIOR TO SUBMITTING A UTILITY LOCATE REQUEST THROUGH DIG SAFE SO THAT THE RESIDENT CAN ARRANGE FOR MAINE TURNPIKE UNDERGROUND UTILITY LOCATION. ALL PROPOSED SIGN LOCATIONS AND EXCAVATION LOCATIONS SHALL BE MARKED AT THE NOTIFICATION TIME.
- FOLLOWING THE COMPLETION OF THE INITIAL UTILITY LOCATE THE CONTRACTOR WILL GPS LOCATE ALL UTILITIES WITHIN THE PROJECT LIMITS AND PROVIDE A COPY OF THE DIG SAFE RECORDS TO THE AUTHORITY. THE CONTRACTOR, ACTING AS THE AUTHORITY'S THIRD PARTY LOCATOR SHALL BE RESPONSIBLE FOR REMARKING ALL MAINE TURNPIKE FACILITIES WHEN A DIG SAFE UTILITY LOCATE IS CALL FOR IN THE PROJECT. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

DRAINAGE NOTES

- NO EXISTING DRAINAGE SHALL BE ABANDONED, REMOVED OR PLUGGED WITHOUT APPROVAL OF THE RESIDENT.

EROSION CONTROL

- ADDITIONAL MEASURES MAY BE PROPOSED BY THE CONTRACTOR DUE TO SITE OR WEATHER CONDITIONS. THE RESIDENT MAY DIRECT THE CONTRACTOR TO IMPLEMENT ADDITIONAL MEASURES, ANY ADDITIONAL MEASURES APPROVED BY THE RESIDENT WILL BE MEASURED FOR PAYMENT.
- ALL TEMPORARY EROSION CONTROL DEVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE MAINE DEPARTMENT OF TRANSPORTATION BEST MANAGEMENT PRACTICES.

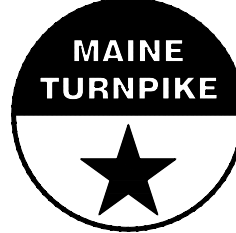
GUARDRAIL INSTALLATION AND MODIFICATION NOTES

- THE CONTRACTOR SHALL FOLLOW ALL MANUFACTURER'S INSTALLATION INSTRUCTIONS FOR THE INSTALLATION AND/OR MODIFICATION OF ANY AND ALL GUARDRAIL COMPONENTS TO BE INSTALLED UNDER THIS CONTRACT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER IF THE CONTRACTOR DISCOVERS CONFLICTS OR IRREGULARITIES OF ANY SORT BETWEEN THE MANUFACTURER'S INSTALLATION INSTRUCTIONS AND THESE CONTRACT DOCUMENTS.
- THE CONTRACTOR IS REQUIRED TO HAVE AN APPROVED CRASH END TREATMENT ON ALL GUARDRAIL ENDS IN ALL WORK AREAS AT THE END OF EACH DAY. THE CONTRACTOR SHALL ENSURE THAT ENOUGH TIME EXISTS IN THE WORK DAY TO COMPLETE ALL MODIFICATIONS AND/OR INSTALLATIONS TO END TERMINALS. THE CONTRACTOR SHALL ENSURE THAT ALL PARTS AND MANPOWER ARE ON-SITE PRIOR TO UNDERTAKING ANY GUARDRAIL MODIFICATIONS.
- GUARDRAIL HEIGHT SHALL BE INSTALLED OR ADJUSTED TO 31"+ 0", -1/2" HEIGHT. GUARDRAIL HEIGHT SHALL BE MEASURED FROM THE EDGE OF PAVEMENT WHEN WITHIN 2 FEET. IF GUARDRAIL IS OFFSET FROM EDGE OF PAVEMENT GREATER THAN 2 FEET, THEN HEIGHT OF RAIL SHALL BE MEASURED AT GROUND ELEVATION ADJACENT TO EACH POST BELOW THE RAIL TO THE TOP OF GUARDRAIL AND AFTER FINAL GRADING OF SHOULDER.
- THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL CUT BOLTS, DISCARDED PARTS, TRASH, PAVEMENT DEBRIS, ETC., AFTER COMPLETING GUARDRAIL MODIFICATIONS AND/OR INSTALLATIONS.
- CONNECTIONS FOR PROPOSED RAIL TO EXISTING RAIL SHALL BE INCIDENTAL TO THE INSTALLATION OF PROPOSED GUARDRAIL.

Scale: NOT TO SCALE

No.	Revision	By	Date
1	Addendum #2	JRL	2/2/23

Designed by:

		By	Date	By	Date
		JRL	01/13/23	JED	01/13/23
Designed	JRL	01/13/23	Checked	JED	01/13/23
Drawn	BMB	01/13/23	In Charge of	SRT	01/13/23

MAINE TURNPIKE AUTHORITY
 2360 Congress Street
 Portland, ME 04102
 TEL (207) 871-7771
 FAX (207) 879-5567

	<h2>THE GOLD STAR MEMORIAL HIGHWAY</h2>
	<p>MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.</p>

<p>PAVEMENT REHABILITATION</p> <p>ESTIMATED QUANTITIES AND GENERAL NOTES</p>
<p>SHEET NUMBER: C-1</p>
<p>CONTRACT: 2023.01</p>

K:\Contracts\2023 Construction Project Files\2023.01 Paving_MW 88.6-98.0 Project Development\GDR\CAD print date 2.1.2023

DIMENSIONS FOR SLOPE OF 2:1

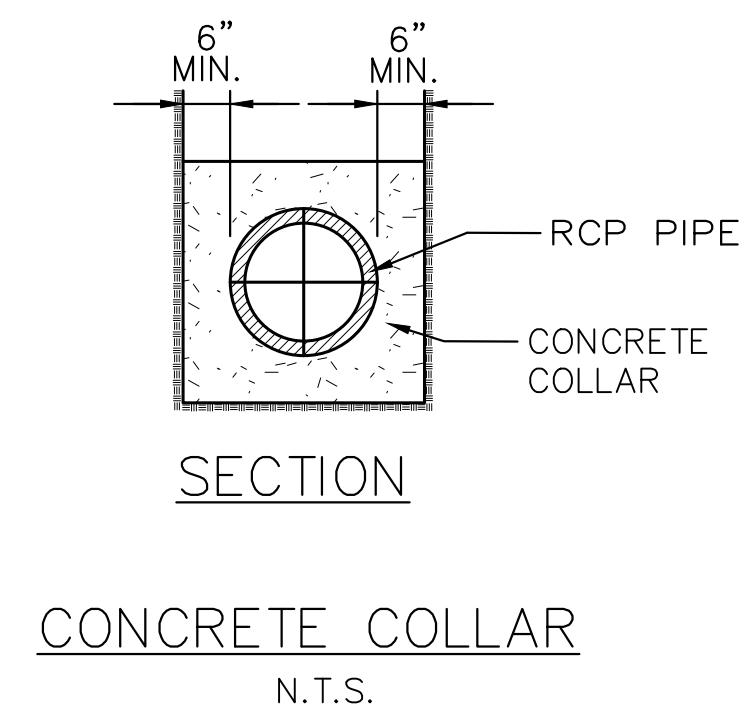
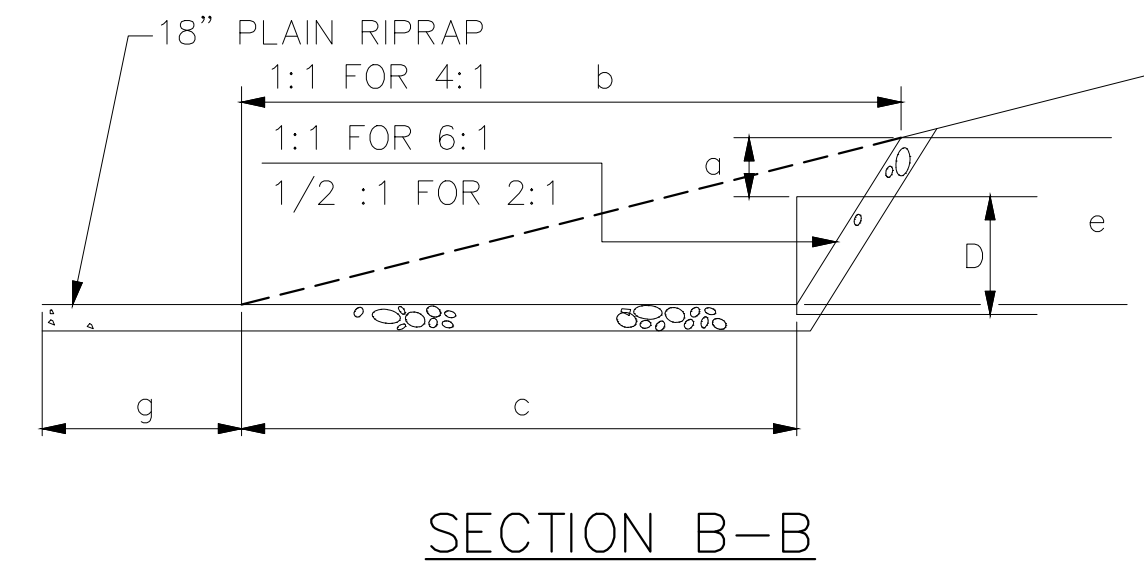
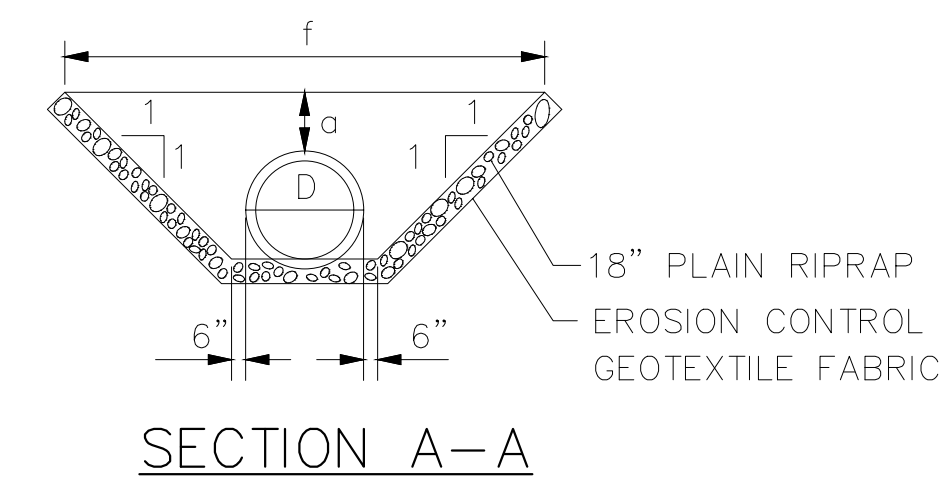
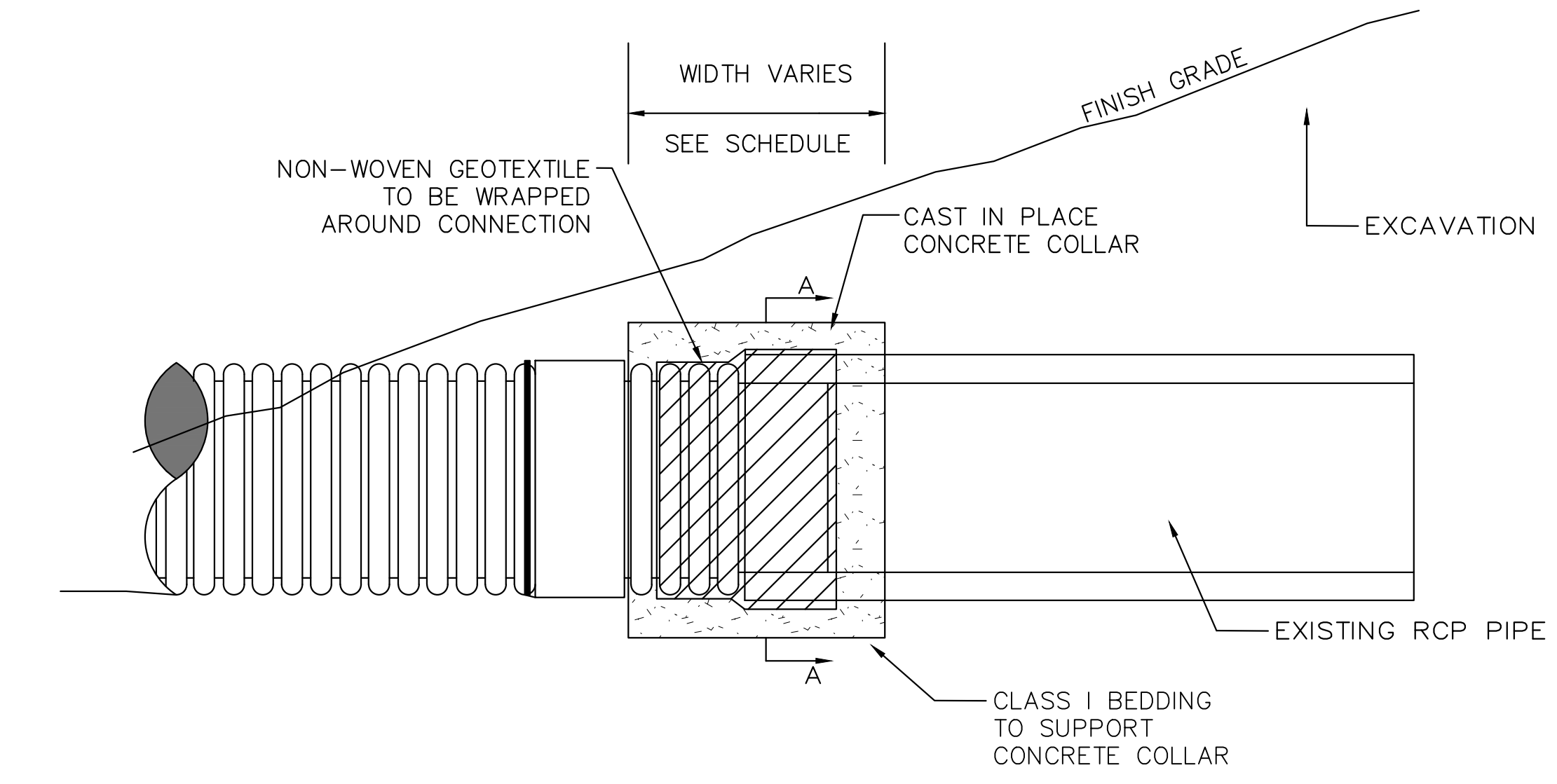
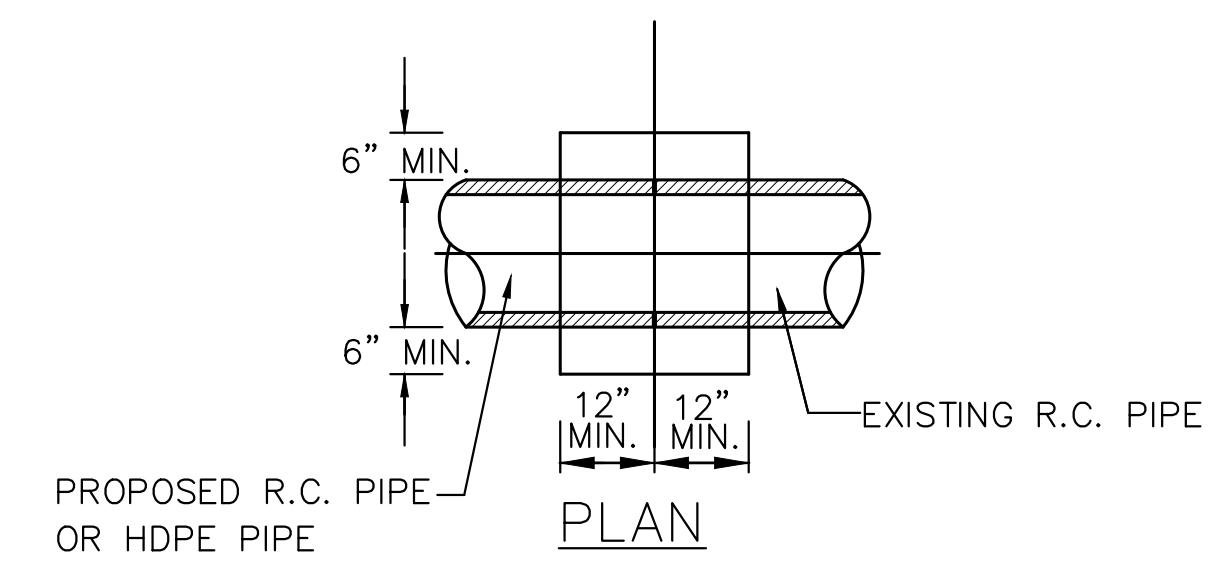
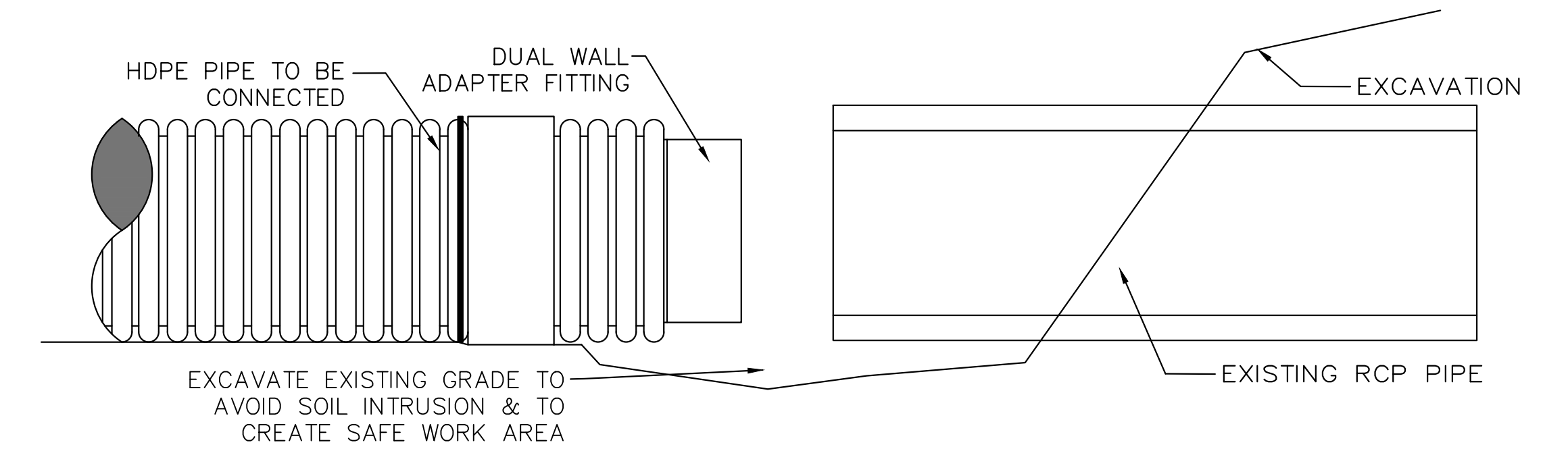
D	A (FT)	B (FT)	C (FT)	E (FT)	F (FT)	G (FT)	STONE DEPTH (FT)	STONE (CY)
12"	1.00	4.00	3.00	2.00	6.00	1.00	1.50	1.30
15"	1.00	4.50	3.37	2.25	6.75	1.63	1.50	1.70
18"	1.00	5.00	3.75	2.50	7.50	2.25	1.50	2.09
24"	1.00	6.00	4.50	3.00	9.00	3.50	1.50	3.12
30"	1.00	7.00	5.25	3.50	10.50	4.75	1.50	4.33

DIMENSIONS FOR SLOPE OF 6:1

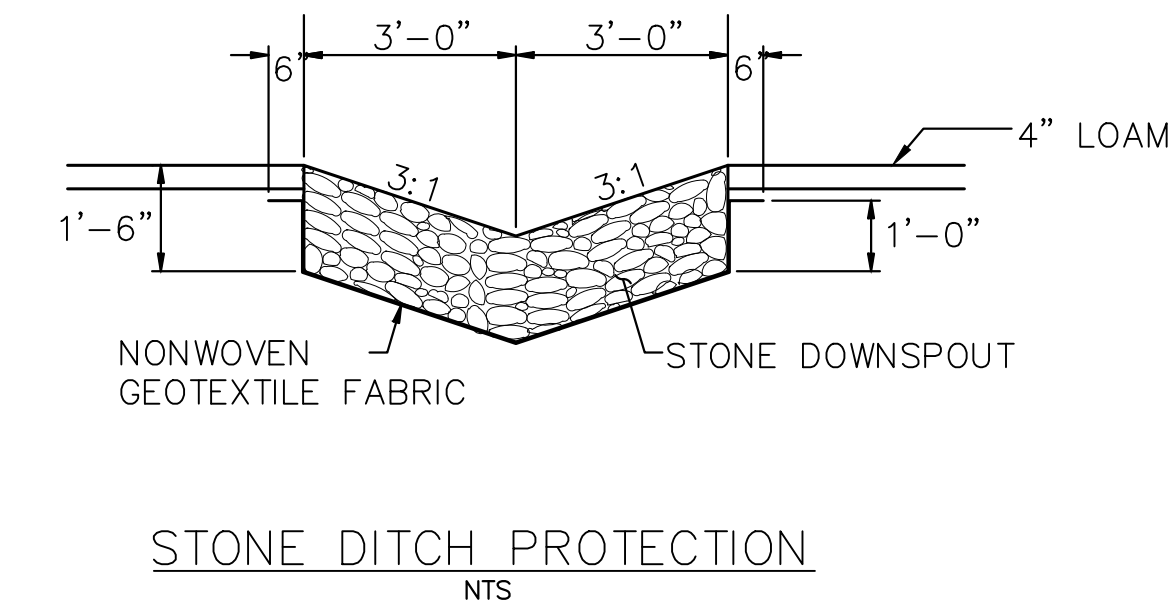
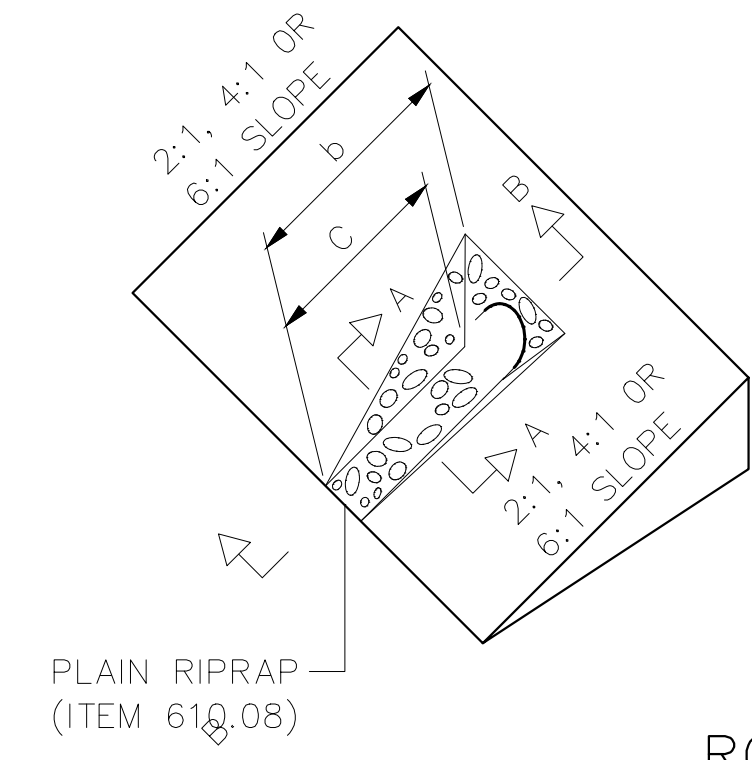
D	A (FT)	B (FT)	C (FT)	E (FT)	F (FT)	G (FT)	STONE DEPTH (FT)	STONE (CY)
12"	0.50	9.00	7.50	1.50	4.50	0.00	1.50	2.30
15"	0.50	10.50	8.75	1.75	5.50	0.00	1.50	2.93
18"	0.50	12.00	10.00	2.00	6.50	0.00	1.50	3.57
24"	0.50	15.00	12.50	2.50	8.00	0.00	1.50	5.44
30"	0.50	18.00	15.00	3.00	9.50	0.00	1.50	7.71

DIMENSIONS FOR SLOPE OF 4:1

D	A (FT)	B (FT)	C (FT)	E (FT)	F (FT)	G (FT)	STONE DEPTH (FT)	STONE (CY)
12"	1.00	8.00	6.00	2.00	6.00	0.00	1.50	2.20
15"	1.00	9.00	6.75	2.25	6.75	0.00	1.50	2.80
18"	1.00	10.00	7.50	2.50	7.50	0.00	1.50	3.40
24"	1.00	12.00	9.00	3.00	9.00	0.00	1.50	4.86
30"	1.00	14.00	10.50	3.50	10.50	0.00	1.50	6.58



PIPE DIA. (INCHES)	WIDTH OF CONC. (INCHES)
12	24
15	24
18	24
24	24
30	30
36	36
42	48
48	48



1 LF OF DOWNSPOUT = 0.33 CY OF STONE DITCH PROTECTION
 1 LF OF DOWNSPOUT = 1.04 SY OF EROSION CONTROL GEOTEXTILE FABRIC

NOTES:

- THE DIMENSIONS SHOWN ARE APPROXIMATE AND MAY BE MODIFIED BY THE RESIDENT.
- STONE QUANTITIES ARE FOR ONE END OF THE PIPE.

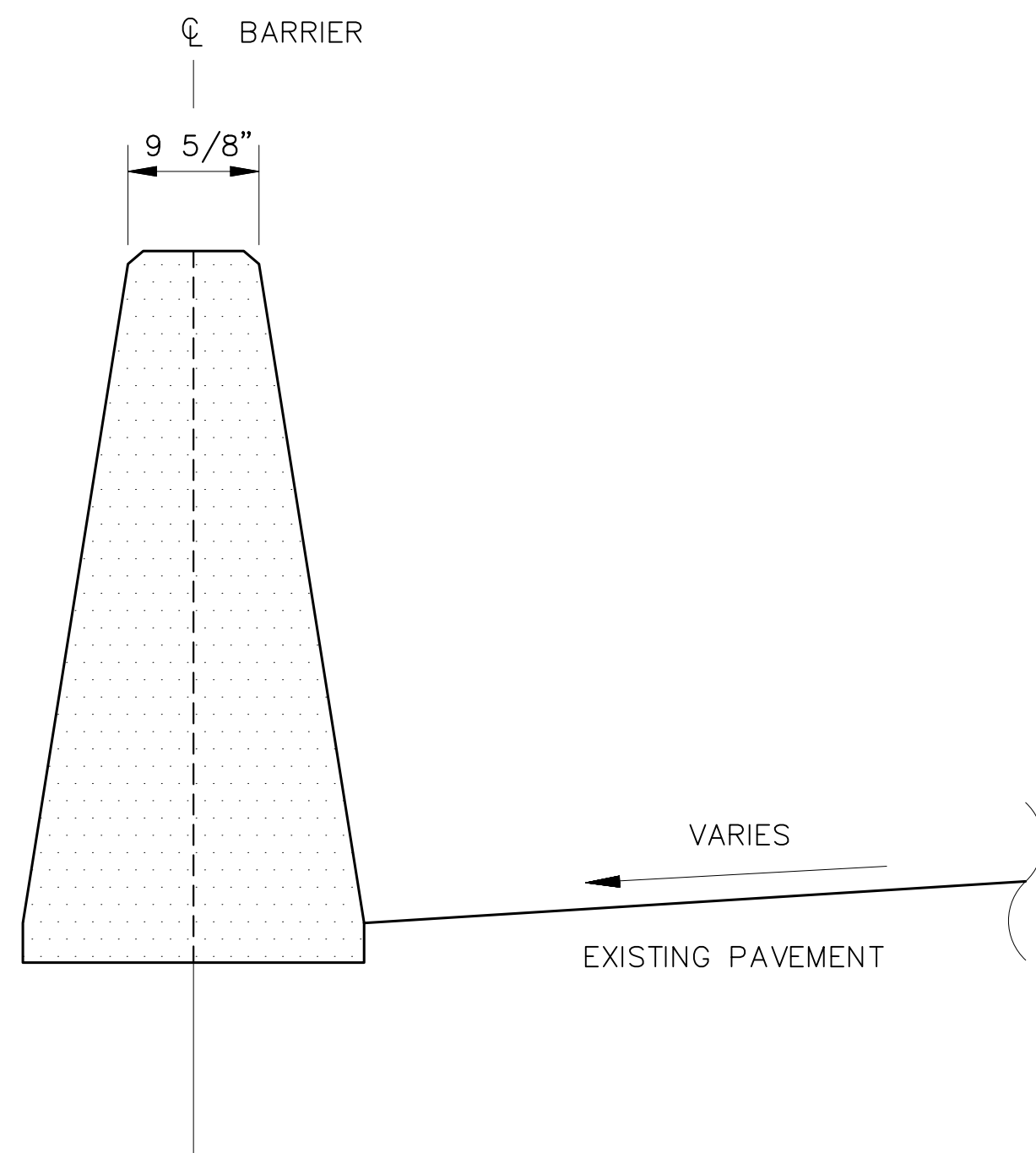
NOTES:

- STONE FOR STONE DOWNSPOUTS SHALL MEET THE REQUIREMENTS OF MAINEDOT SPECIFICATION 703.29 AND WILL BE PAID FOR UNDER ITEM 610.18 STONE DITCH PROTECTION.
- REMOVAL OF 12" SLOPE DRAIN PIPE IS INCIDENTAL TO CONSTRUCTION OF DOWNSPOUT
- SEE APPENDIX FOR LOCATIONS AT MM 73.0

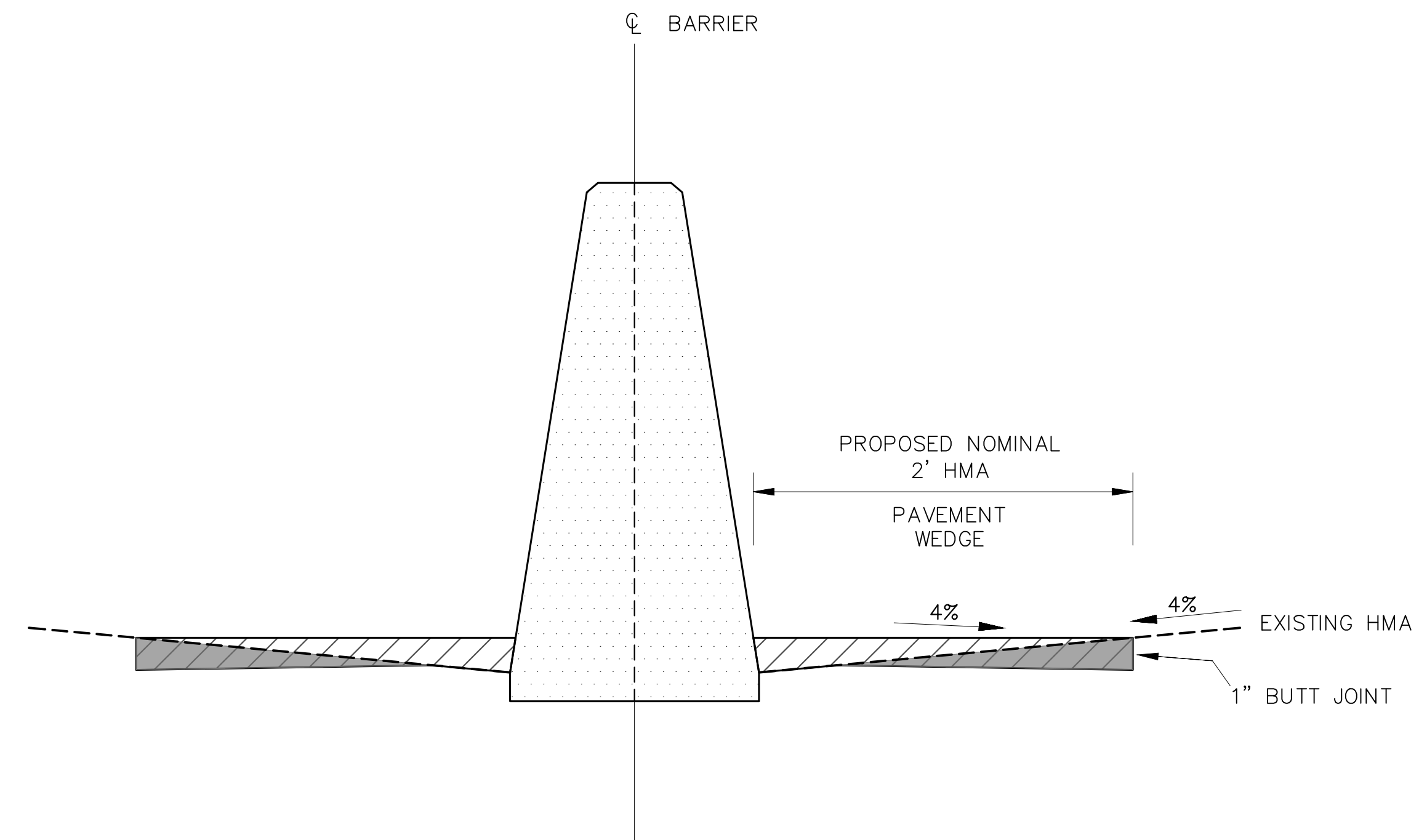
NOTES:

- CONNECTION AND PIPE TO BE BACKFILLED PER ASTM D2321 (CRUSHED STONE).
- CONCRETE SHALL BE CLASS A FIBER REINFORCED.
- SEE CONCRETE COLLAR WIDTH SCHEDULE FOR DIMENSIONS.
- CONCRETE COLLAR SHALL BE FORMED IN PLACE.

Scale: NOT TO SCALE	Designed by: 	MAINE TURNPIKE AUTHORITY 2360 Congress Street Portland, ME 04102 TEL (207) 871-7771 FAX (207) 879-5567	THE GOLD STAR MEMORIAL HIGHWAY	PAVEMENT REHABILITATION DRAINAGE DETAILS II																				
<table border="1" style="width:100%; border-collapse: collapse;"> <thead> <tr> <th>No.</th> <th>Revision</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr> <td>1</td> <td>Addendum #2</td> <td>JRL</td> <td>2/2/23</td> </tr> </tbody> </table>	No.	Revision	By	Date	1	Addendum #2	JRL	2/2/23	<table border="1" style="width:100%; border-collapse: collapse;"> <thead> <tr> <th>By</th> <th>Date</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr> <td>Designed JRL</td> <td>01/13/23</td> <td>Checked JED</td> <td>01/13/23</td> </tr> <tr> <td>Drawn BMB</td> <td>01/13/23</td> <td>In Charge of SRT</td> <td>01/13/23</td> </tr> </tbody> </table>	By	Date	By	Date	Designed JRL	01/13/23	Checked JED	01/13/23	Drawn BMB	01/13/23	In Charge of SRT	01/13/23	MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.	SHEET NUMBER: P-4 CONTRACT: 2023.01	7 of 18
No.	Revision	By	Date																					
1	Addendum #2	JRL	2/2/23																					
By	Date	By	Date																					
Designed JRL	01/13/23	Checked JED	01/13/23																					
Drawn BMB	01/13/23	In Charge of SRT	01/13/23																					



EXISTING CONCRETE BARRIER
NTS



PROPOSED PAVEMENT WEDGE (TYPICAL)
NTS

NOTES:

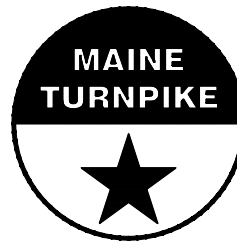
1. MAINE TURNPIKE AUTHORITY WILL PROVIDE MAINTENANCE OF TRAFFIC FROM 7pm TO 6am MONDAY THROUGH THURSDAY.
2. THE VERTICAL JOINTS SHALL BE HOT RUBBERED AND THIS INCIDENTAL TO THE PAVING.
3. RS-1 TACK WILL BE REQUIRED AND THIS IS INCIDENTAL TO THE PAVING.
4. THE WEDGE SHALL BE MACHINE PLACED.

103 ORT LOCATIONS

SOUTH BOUND LEFT	1,295'
SOUTH BOUND RIGHT	1,145'
NORTH BOUND LEFT	1,120'
NORTH BOUND RIGHT	1,320'

TOTAL 4,880 LF

Scale: NOT TO SCALE			
No.	Revision	By	Date
1	Addendum #2	JRL	2/2/23

Designed by:					
					
	By	Date		By	Date
	Designed	JRL 01/31/23	Checked	JED 01/31/23	
	Drawn	BMB 01/31/23	In Charge of	SRT 01/31/23	

MAINE TURNPIKE AUTHORITY
2360 Congress Street
Portland, ME 04102
TEL (207) 871-7771
FAX (207) 879-5567



THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

CONCRETE BARRIER & WEDGE MISCELLANEOUS DETAILS	
CONTRACT: 2023.01	SHEET NUMBER: ADD-1
1 of 1	

K:\Contracts\2023 Construction Project Files\2023.01 Paving_MW 888-9800 Project Development (GQA/CAD) Print Date: 2/2/2023

MAINE TURNPIKE AUTHORITY

Pre-Bid Conference

CONTRACT 2023.01

Pavement Rehabilitation

and Drainage Improvements MM 88.6 to MM 98.0

January 31, 2023 11:00 AM

1) Location:

- a) Pavement Rehabilitation Mile 88.6 to Mile 98.0 in the Town of Sabattus and the Town of Litchfield on the Maine Turnpike.
- b) Drainage improvements in New Gloucester (~MM73.0) northbound and southbound – see Appendix A, MTA to provide maintenance of traffic.
- c) Drainage Improvements and bituminous waterways MM 87.5 to MM 88.4 southbound in the median.
- d) Drainage improvements from MM 88.6 to MM 98.2 shown on plan sheet 3.

2) General Description:

The pavement rehabilitation work consists of milling and paving full width(36 feet) from MM 88.6 to MM92.8 and milling and paving two 12 foot wide lanes and the left inside shoulder for both northbound and southbound from MM92.8 to MM98.0. Excess material in the median will be removed to restore proper drainage, catch basins will be rebuilt and adjusted as necessary, drainage outlets will be replaced as necessary. Additional drainage work to be completed is located at approximate mile marker 73.0. Guardrail work, pavement markings, maintenance of traffic and all other work incidental thereto will be completed in accordance with the Plans and Specifications.

3) Bid Opening:

- a) February 9, 2023 at 11:00 A.M. at MTA headquarters 2360 Congress Street, Portland.
- b) All bid and contractual questions shall be directed to Mr. Nathaniel Carll. Phone No. (207) 871-7771 Ext. 115.
- c) All questions on plans and specifications shall be in faxed or emailed to Nathaniel Carll, Purchasing Manager(ncarll@maineturnpike.com), of the Maine Turnpike Authority. Fax No. (207) 871-7739.

4) Notification:

- a) Contractor shall notify and obtain approval from the Authority prior to visiting the Project site for field inspection. The contact person is Mr. Steve Tartre at (207) 871-7771, Ext. 144. **Please send an email to Steve T. (startre@maineturnpike.com) in lieu of a phone call.**

- 5) Construction Schedule/Prosecution of Work:
 - a) After Contract Execution.
 - b) September 15, 2023 – Substantial Completion, (defined in Special Provision Section 107.1.1 Interim Substantial Completion and Substantial Completion(SP-8).
 - c) October 13, 2023 - Contract Completion date.
 - d) Lane 2 and 8’ shoulder to be pulled as one while paving from MM 88.6 to MM 92.8.
 - e) Lanes and adjacent shoulders shall be kept even during milling.
 - f) Milling and paving thru nine bridge underpasses as shown plan sheet 4.

- 6) Maine Department of Labor – Fair Hourly Wages (Special Provision 104.3.8):
 - a) Highway and Earthwork wages in three counties(SP-3 thru SP-5).

- 7) Cooperation with Other Contractors (MTA Supplemental Specification 104.4.7)(SP-6):
 - a) Eastbound wide load sign location.

- 8) General Requirements
 - a) U-Turns at toll plazas and median openings not allowed.
 - b) Class III vests.

- 9) Permit Requirements (Special Provision 105.8.2)
 - a) Permit in Appendix A for work at MM73.0.
 - b) Pipe at Station 3668+65 needs to be done “in the dry” from July 15 thru September 30, 2023.

- 10) Limitations of Operations (Special Provision 107.4.7 SP-6):
 - a) Continuous work.
 - b) Paint pavement two weeks after a bound is paved.
 - c) Provide two weeks’ notice to MTA prior to wide load restrictions beginning.

- 11) Traffic Control (Supplemental Specification and Special Provision Section 652):
 - a) Contractor is responsible for supplying all traffic control devices except for work at MM 73.0 **and at the 103 ORT.**
 - b) Contractor is responsible for placement, relocation, removal and maintenance of traffic control devices. Maintenance of traffic control devices is a 24-hour a day, seven day per week, responsibility. Contractor shall inspect devices as required.
 - c) Stoppages of traffic for moving heavy or slow equipment across turnpike (stoppages less than five minutes)
 - i) Install signs “Expect Stopped Traffic” and “ Be Prepared to Stop”
 - ii) Fee of \$500 per minute for each minute in excess of the five minute allowance.
 - iii) Requests for short term traffic stoppages (up to 5 minutes) shall be submitted two days in advance of scheduled stoppage. Request subject to approval by MTA.
 - d) All signs, which do not apply to current construction activity, shall be 100% covered or removed in accordance with the plans. This includes any speed limit signs when work zone speed is in operation.
 - e) Daytime lane closures can be 3 miles long, Night time lane closures can be 5 miles long.

12) New Items

- i) SP-90 Truck Mounted Attenuator will be used when there is a lane closure-and used for each individual work activity in the lane closure
- ii) MTA Supplemental Specifications-On Line.
- iii) SP-22 Fine aggregate Micro-Duval same as last year.
- iv) SP-54 Zycotherm to 0.125%.
- v) SP-1 Juneteenth Day June 19, 2023.
- vi) Paving 401 changes SP-21 thru SP-47.
- vii) Paver speed 45fpm for surface.
- viii) Trackless Tack SP-55.
- ix) Chainsaw rental item for drainage work at MM73.0.

13) Addendum No. 2 will be issued ~~late next~~ **this** week.

- a) Any pertaining to the Pre-bid Meeting.
- b) 401 HMA Gradation % Deduction Chart.
- c) Potential MM103 ORT pavement wedge, see attached draft sketch.

14) Other

Questions shall be submitted by end of business Friday February 3, 2023 in order to be answered.

15) Questions

Q1: Will a test strip be required?

A1: No.

Q2: Where is the pay chart associated with the gradations?

A2: Will be issued in Addendum #2.

Q3: Are there penalties associated with control chart data points?

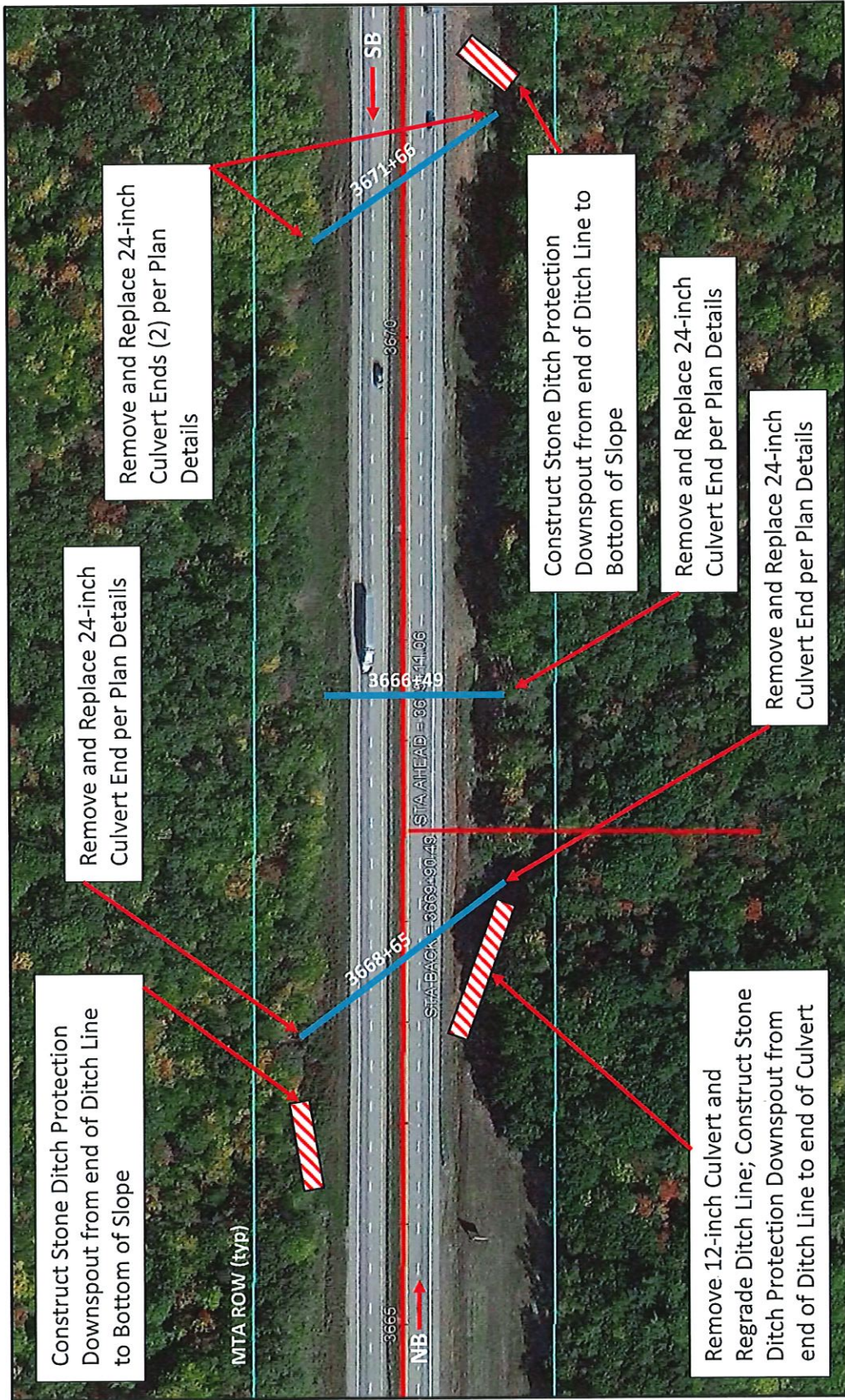
A3: No, disincentives are based on the master specifications limits.

Q4: Will the MTA consider changing the control chart limit for Gmm from 0.015 to 0.020?

A4: No.

Q5: Will a 12' mill be required?

A5: A 12' mill is required for the mainline travel lanes. The shoulders need to be milled by a machine capable of milling the entire shoulder in a single pass.



Construct Stone Ditch Protection Downspout from end of Ditch Line to Bottom of Slope

Remove and Replace 24-inch Culvert End per Plan Details

Remove and Replace 24-inch Culvert Ends (2) per Plan Details

MTA ROW (typ)

3665

3668+65

3668+74.9

3670

3671+66

NB

SB

STA BACK = 3668+90.49
STA AHEAD = 3671+96

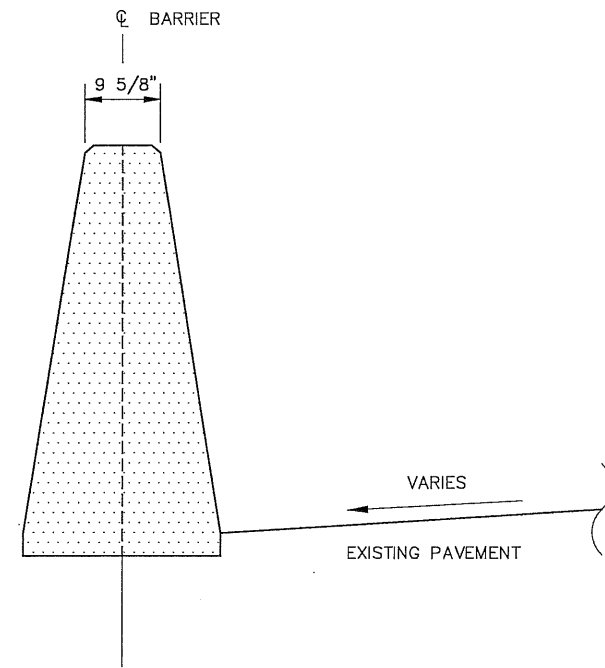
Remove 12-inch Culvert and Regrade Ditch Line; Construct Stone Ditch Protection Downspout from end of Ditch Line to end of Culvert

Construct Stone Ditch Protection Downspout from end of Ditch Line to Bottom of Slope

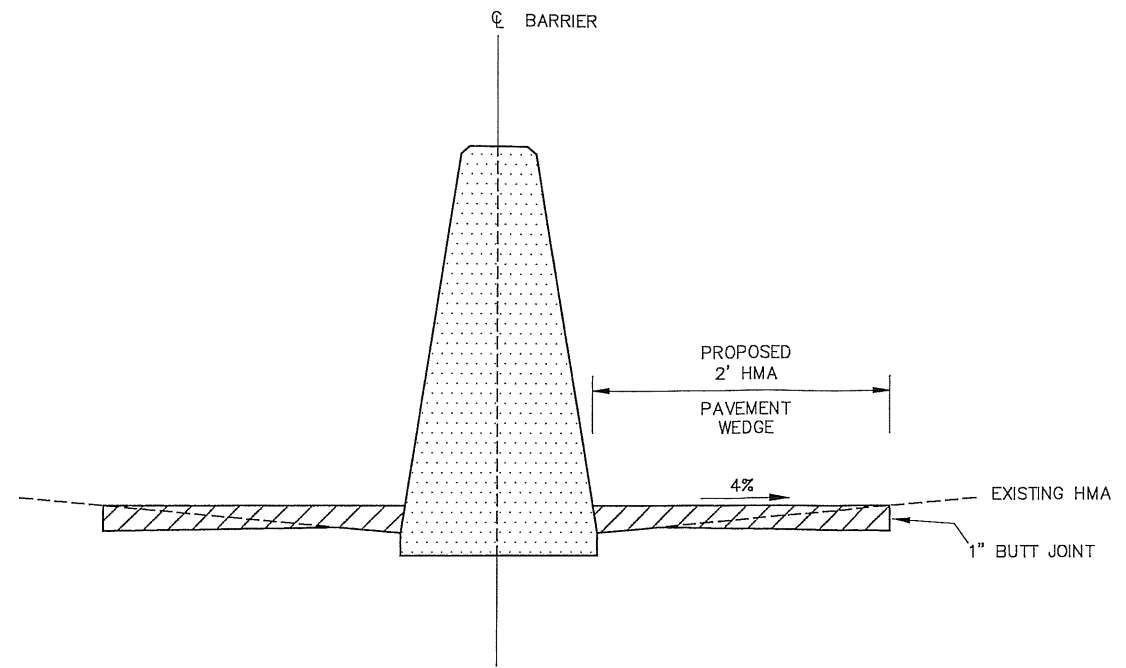
Remove and Replace 24-inch Culvert End per Plan Details

Remove and Replace 24-inch Culvert End per Plan Details

CONTRACT 2023.01
MIM 72.7 CULVERT END REPLACEMENTS



EXISTING CONCRETE BARRIER
NTS



PROPOSED PAVEMENT WEDGE (TYPICAL)
NTS

Scale:
NOT TO SCALE

Designed by:



MAINE TURNPIKE AUTHORITY
2360 Congress Street
Portland, ME 04102
TEL (207) 871-7771
FAX (207) 879-5567



THE GOLD STAR
MEMORIAL HIGHWAY

CONCRETE BARRIER & WEDGE
MISCELLANEOUS DETAILS

No.	Revision	By	Date

	By	Date	Checked	By	Date
Designed	JRL	01/31/23		JED	01/31/23
Drawn	BMB	01/31/23	In Charge of	SRT	01/31/23

MTA PROJECT MANAGER: Joseph R. Leavitt, P.E.

CONTRACT: 2023.01

SHEET NUMBER: ADD-1

1 of 1