

MAINE TURNPIKE AUTHORITY

ADDENDUM NO. 1

CONTRACT 2023.08

BRIDGE REPAIRS

SCARBOROUGH INTERCHANGE UNDERPASS (MM 42.50)

Questions:

The following are questions asked at the pre-bid meeting held on July 11, 2023 or submitted to the Maine Turnpike Authority in writing. Answers to the questions are noted. Bidders shall utilize this information in preparing their bid.

Question 1: How will epoxy injection crack repair be paid?
Response: *There is no anticipated crack repair as part of this project.*

Question 2: How much advance notice is required before closing the ramps for a weekend?
Response: *A minimum of 2-weeks' notice is required. When providing notice, MTA recommends including a secondary weekend in case poor weather requires the closure weekend to be delayed.*

Question 3: Is a lane closure required to place the temporary concrete barrier for the on-ramp shoulder closure?
Response: *As required by Special Provision 652, "Loading/unloading trucks shall not be closer than six feet from an open travel lane." If this and all other applicable requirements of Special Provision 652 can be met while placing the temporary concrete barrier, a lane closure will not be required. Otherwise, a lane closure will be required.*

Question 4: How soon after the placement of elastomeric concrete can traffic loads be placed on the joint?
Response: *Traffic loads may be placed on the joint once the elastomeric concrete has met the manufacturer's requirements and the elastomeric concrete has been accepted by the Resident.*

Make the following changes to the bid documents:

In the Contract Documents, **REMOVE** Special Provision Section 518, Structural Concrete Repair (Elastomeric Concrete Header Repair) and **REPLACE** with the attached revised Special Provision Section 518, Structural Concrete Repair (Elastomeric Concrete Header Repair).

Note: The above items shall be considered as part of the bid submittal.

Attachments

- Special Provision Section 518, Structural Concrete Repair (Elastomeric Concrete Header Repair) (2 pages)
- Pre-Bid Agenda (4 pages)
- Pre-Bid Presentation (18 pages)
- Pre-Bid Sign-In Sheet (1 page)

The total number of pages included with this addendum is twenty seven (27).

All bidders are requested to acknowledge the receipt of the Addendum No. 1 by signing below and faxing this sheet to Nate Carll, Purchasing Department, (207) 871-7739. Bidders are also required to acknowledge receipt of this Addendum No. 1 on Page P-5 of the bid package.

Business Name

Print Name and Title

Signature

Date

July 13, 2023

Very truly yours,

MAINE TURNPIKE AUTHORITY

Purchasing Manager
Maine Turnpike Authority

SPECIAL PROVISIONSECTION 518STRUCTURAL CONCRETE REPAIR

(Elastomeric Concrete Header Repair)

518.01 Description

The following paragraph is added:

The work includes removal of the existing and placement of new elastomeric concrete at the deck expansion joints, as shown on the Plans and as directed by the Resident. The work also includes removal of any areas of deteriorated concrete at the top of the backwall and preparation of the concrete surface in accordance with Supplemental Specification 518 and as directed by the Resident.

If the Contractor opts to complete the work utilizing closures for three (3) consecutive nights, the work shall also include placing and securing steel plates over the joint after the existing pavement and elastomeric concrete have been removed and before the proposed elastomeric concrete and pavement have been placed. Prior to the start of construction, the Contractor shall prepare a steel plating submittal and submit it to the Resident for approval. The steel plating submittal shall include design computations for the size, thickness, and span of the proposed steel plate(s) as well as details noting how the steel plates will be placed and secured. The steel plating submittal shall be stamped by a Professional Engineer licensed in the State of Maine.

518.02 Repair Materials.

The following paragraphs are added:

The materials shall be from one of the manufactures on the Maine Department of Transportation Qualified Products List of Elastomeric Concrete.

Products shall be delivered to the site in Manufacturer's original, intact, labeled containers. Products shall be handled and protected as necessary to prevent damage or deterioration during shipment, handling and storage. Products shall be stored in accordance with Manufacturer's instructions.

518.07 Placing Repair Materials

The following paragraph is added:

The installation shall be conducted in strict accordance with the selected manufacturer's recommendations.

518.10 Method of Measurement

The following paragraph is added:

The quantity of Elastomeric Concrete Header Repair will be measured by the cubic foot.

518.11 Basis of Payment

The following paragraphs are added:

Elastomeric Concrete Header Repair will be paid for at the contract unit price per cubic foot, which shall be payment in full for furnishing all materials, labor and equipment, including removal of the existing elastomeric concrete, removing any deteriorated backwall concrete, preparation of the surfaces of the joint in accordance with the manufacturer’s recommendations, placement of the proposed elastomeric concrete, and all incidentals necessary to complete the work. Steel plates, if necessary, shall be incidental to Elastomeric Concrete Header Repair.

Payment will be made under:

<u>Pay Item</u>	<u>Pay Unit</u>
518.864 Elastomeric Concrete Header Repair	Cubic Foot

MAINE TURNPIKE AUTHORITY

Pre-Bid Conference

CONTRACT 2023.08

BRIDGE REPAIRS
SCARBOROUGH INTERCHANGE UNDERPASS
MILE 42.50

July 11, 2023 10:00 AM

1) Location:

The general limits of work are as shown in the Contract Plans. The Scarborough Interchange Underpass is located at Mile 42.50 of the Maine Turnpike.

2) General Description:

The work consists of bridge repairs at the Scarborough Interchange Underpass in the Town of Scarborough, Maine. The work generally includes deck expansion joint header repairs, approach paving, substructure concrete patch repairs, maintenance of traffic and all other work incidental thereto in accordance with the Plans and Specifications.

3) Bid:

- a) Bid opening is July 18, 2023 at 11:00 A.M. at MTA headquarters 2360 Congress Street, Portland.
- b) All bid and contractual questions shall be directed to Purchasing Department, Phone No. (207) 482-8115.
- c) All questions on plans and specifications shall be in writing and shall be directed to Nate Carll, Purchasing Manager, at (207) 871-7739 (fax) or email ncarll@maineturnpike.com.
- d) All questions must be submitted by 5:00pm on Thursday July 13, 2023 to be considered.

4) Notification:

- a) Contractor shall notify and obtain approval from the Authority prior to visiting the Project site for field inspection. The contact person is Mr. Steve Tartre at (207) 482-8144 or startre@maineturnpike.com.

5) Construction Schedule/Prosecution of Work:

- a) MTA Board is scheduled to consider the Contract Award on July 27, 2023.
- b) Lane and shoulder closures on the Scarborough Interchange Underpass shall not occur until on or after September 5, 2023.
- c) The Contractor shall be allowed to close each barrel of the Scarborough Interchange Underpass for a maximum of one weekend, selected from the dates below. The on-ramp and off-ramp shall be closed simultaneously. The Scarborough Interchange Underpass shall not be closed until 8:00 p.m. on Friday and shall be reopened by 6:00 a.m. Monday. Supplemental liquidated damages of \$500 per hour per ramp shall be assessed for each hour that either ramp is closed past 6:00 a.m. Monday. The weekend closure shall occur during one of the following weekends:

- October 13 through 16, 2023
 - October 20 through 23, 2023
 - October 27 through 30, 2023
 - November 3 through 6, 2023
 - November 10 through 13, 2023
- d) At the Contractor’s option, the Scarborough Interchange Underpass may be closed for three (3) consecutive nights instead of one of the weekends noted above. The on-ramp and off-ramp shall be closed simultaneously. The Scarborough Interchange Underpass shall not be closed until 9:00 p.m. each night and shall be reopened by 5:00 a.m. each morning. Supplemental liquidated damages of \$500 per hour per ramp shall be assessed for each hour that the either ramp is closed past 5:00 a.m. each morning.
- e) All bridge repair work shall be substantially completed on or before November 9, 2023. Substantially complete shall be defined as:
- All bridge repair work required by the Contract that requires shoulder and/or lane closures.
 - All roads fully opened to two-way traffic including shoulders, and surface pavement.
- f) All work in this Contract shall be completed on or before November 22, 2023.
- g) Supplemental Liquidated damages on a calendar day basis in accordance with Subsection 107.8 shall be assessed for each calendar day that substantial completion is not achieved.
- 6) Maine Department of Labor – Fair Hourly Wages (Special Provision 104.3.8)
- a) Heavy and Bridge wages for Cumberland County are included in the Contract Book.
- 7) Utility Coordination (Special Provision 104.4.6)
- a) Existing utilities were not located for this project.
- b) The Contractor shall contact Dig Safe and any non-member utility operators through OK-TO-DIG prior to any work.
- c) Utility adjustments are not anticipated as part of this project. If any unexpected utility relocations become necessary, they will be scheduled in compliance with Section 104 of the Standard Specifications and will be done by the utilities in conjunction with the work by the Contractor.
- 8) Cooperation With Other Contractors (Special Provision 104.4.7)
- a) MTA Contract 2021.07 – Exit 45 Interchange Reconfiguration, MM 44.9
- b) MTA Contract 2022.07 – Interchange Improvements, Saco
- 9) Lead Paint (Special Provision 105.2.4.2)
- a) The Contractor shall note that the existing bridge structures may contain lead based paint. The Contractor shall institute every precaution when working with materials coated with lead based paints.
- 10) Permit Requirements (Special Provisions 105.8.2 and 105.8.3)
- a) The Project is subject to the Stormwater Memorandum of Agreement for Stormwater Management Between the Maine Department of Transportation, Maine Turnpike Authority, and Maine Department of Environmental Protection (Stormwater MOA).
- b) This Project is also subject to the requirements of the Maine Pollutant Discharge and Elimination System (MPDES) General Permit for the Discharge of Stormwater from MTA’s Municipal Separate Storm Sewer Systems (MS4).

- c) Compliance with the erosion and sedimentation control requirements outlined in this Contract is required by the Contractor.

11) General Requirements

- a) U-Turns at toll plazas and median openings not allowed. (Supplemental Specification 105.5.1)
- b) Contractor access to and from the mainline shall not negatively impact mainline traffic flow. The Contractor may be required to establish lane closures to provide for safe access. Refer to Special Provision 652, Specific Project Maintenance of Traffic Requirements, for lane closure requirements and restrictions.
- c) All vehicles used on the Project, including concrete delivery trucks, shall be equipped with amber flashing beacons in accordance with Supplemental Specification 652.3.4.
- d) Class III safety vests must be worn at all times.

12) Specific Contract Items:

- a) Section 518 – Structural Concrete Repair, Elastomeric Concrete Header Repair
 - i) The work includes removal of the existing and placement of new elastomeric concrete at the deck expansion joints.
 - ii) If the Contractor opts to complete the work utilizing closures for three (3) consecutive nights, the work shall also include placing and securing steel plates over the joint after the existing pavement and elastomeric concrete have been removed and before the proposed elastomeric concrete and pavement have been placed.
 - iii) Steel plates, if necessary, shall be incidental to Elastomeric Concrete Header Repair.

13) Traffic Control (Special Provision Section 652):

- a) Special Provision Section 652 replaces the MaineDOT Standard Specification 2014 Edition and MTA 2016 Supplemental Specification Section 652.
- b) Substantive revisions have been bolded in the 652 SP.
- c) Contractor is responsible for supplying all traffic control devices.
- d) Contractor is responsible for placement, relocation, removal and maintenance of traffic control devices. Maintenance of traffic control devices is a 24-hour a day, seven days per week responsibility. Contractor shall inspect devices as required.
- e) Temporary detours, lane closures and construction phasing shall be established and maintained at all times in accordance with the details shown on the Plans and the requirements of the Special Provisions.
 - i) A truck mounted attenuator shall be utilized for all lane closures on the Turnpike mainline, shall be utilized for all temporary shoulder closures (i.e. closures that do not include temporary concrete barrier) on the Turnpike mainline, and other construction operations where workers are exposed to traffic and not protected by positive means.
 - ii) A truck mounted attenuator shall be utilized for all work completed behind guardrail if that work is being completed within the deflection zone of the guardrail (within four feet behind the guardrail post).
- f) Turnpike Lane closures
 - i) One lane of traffic flowing in each direction at all times. Minimum traveling width of 14' required unless otherwise shown in the Plans.
 - ii) All lane closures require the approval of the Resident. The Resident is required to submit a request to MTA for lane closures by noon on Thursday for lane closures needed for the

following week. The Contractor shall plan the work and requests for lane closures accordingly. Requests are subject to approval by MTA.

- iii) Supplemental liquidated damages shall be assessed at \$1,000 per minute for every minute that a temporary lane closure is in place outside of the allowable times.
- g) All signs, which do not apply to current construction activity, shall be 100% covered or removed in accordance with the plans. This includes any speed limit signs when work zone speed is in operation.
- h) Traffic control devices shall be NCHRP 350 compliant. All traffic control devices shall meet MASH guidelines if the date of manufacture was after December 31, 2019.

14) Questions:



**Maine
Turnpike
Authority**

MTA Contract 2023.08



Bridge Repairs Scarborough Interchange Underpass

Pre-Bid Conference

July 11, 2023



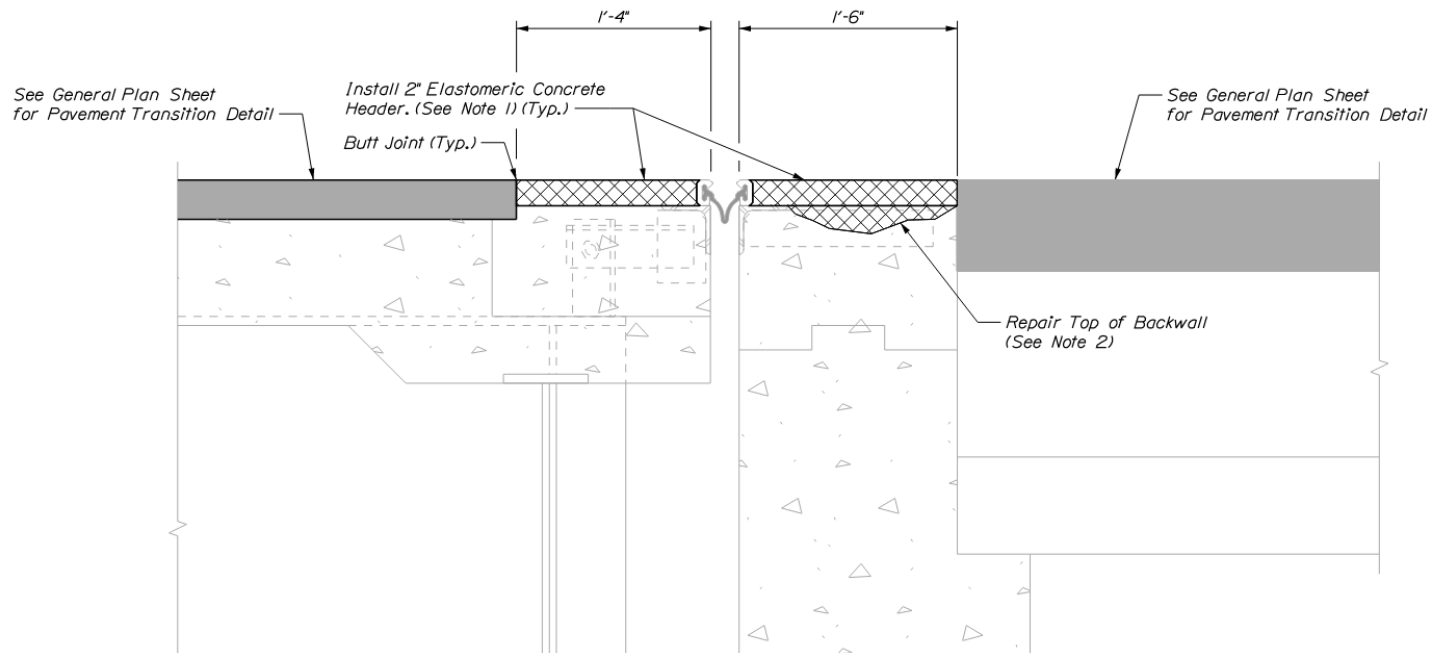
1) Location – Scarborough Interchange Underpass (42.50)





2) General Description

- At the Scarborough Interchange Underpass, the work includes:
 - Deck Expansion Joint Repairs
 - Approach Paving
 - Substructure Concrete Patch Repairs





3) Bid

- **Bid opening:** July 18, 2023, at 11:00 A.M.
- All **bid & contractual questions** direct to Purchasing Dept.
 - Ph#: (207) 482-8115
- All **plan & spec questions** submit in writing directed to Nate Carll, Purchasing Manager
 - Fax: (207) 871-7739
 - Email: ncarll@maineturnpike.com
- All questions **must be submitted** by 5:00 pm on Thursday July 13, 2023



4) Notification

- Contractor shall **notify and obtain approval** from the Authority (Steve Tartre) **prior to visiting the Project site** for field inspection.
 - Ph#: (207) 482-8144
 - email: startre@maineturnpike.com



5) Construction Schedule/Prosecution of Work

- MTA Board is scheduled to consider the **Contract Award on July 27, 2023.**
- Substantially complete: **November 9, 2023**/All work complete: **November 22, 2025**
- Closures shall not occur until on or after **September 5, 2023**
- The Contractor shall be allowed to close each barrel of the Scarborough Interchange Underpass for a maximum of one weekend, selected from the dates in the Contract Book. Closure shall not start until 8:00 p.m. Friday and shall be reopened by 6:00 a.m. Monday.
- At the Contractor's option, the Scarborough Interchange Underpass may be closed for three (3) consecutive nights instead of one of the weekend. Closures shall not start until 9:00 p.m. each night and shall be reopened by 5:00 a.m. each morning.



6) Maine Department of Labor – Fair Hourly Wages

- Included Wage Rates:
 - Cumberland County

7) Utility Coordination

- Existing utilities were not located for this project.
- The Contractor shall **contact Dig Safe** and any non-member utility operators through **OK-TO-DIG prior to any work.**
- If any **unexpected utility relocations** become necessary, they will be scheduled **in compliance with Section 104** of the Standard Specifications and will be **done by the utilities** in conjunction with the work by the Contractor.



8) Cooperation With Other Contractors

- **MTA Contract 2021.07** – Exit 45 Interchange Reconfiguration, MM 44.9
- **MTA Contract 2022.07** – Interchange Improvements, Saco

9) Lead Paint

- The Contractor shall note that the **existing bridge structures may contain lead-based paint**. The Contractor shall **institute every precaution** when working with materials coated with lead-based paints.



10) Permit Requirements

- The Project is subject to the **Stormwater Memorandum of Agreement for Stormwater Management** Between the Maine Department of Transportation, Maine Turnpike Authority, and Maine Department of Environmental Protection.
- This Project is also subject to the requirements of the **Maine Pollutant Discharge and Elimination System (MPDES)** General Permit for the Discharge of Stormwater from MTA's Municipal Separate Storm Sewer Systems (MS4).
- **Compliance with the erosion and sedimentation control** requirements outlined in this Contract is required by the Contractor.



11) General Requirements

- **U-Turns** at toll plazas and median openings **not allowed**.
- **Contractor access** to and from the mainline **shall not negatively impact** mainline traffic flow. The Contractor may be required to establish lane closures to provide for safe access. Refer to Special Provision 652.
- **All vehicles** used on the Project, including concrete delivery trucks, shall be **equipped with amber flashing beacons** in accordance with Supplemental Specification 652.3.4.
- **Class III safety vests** must be worn at all times.



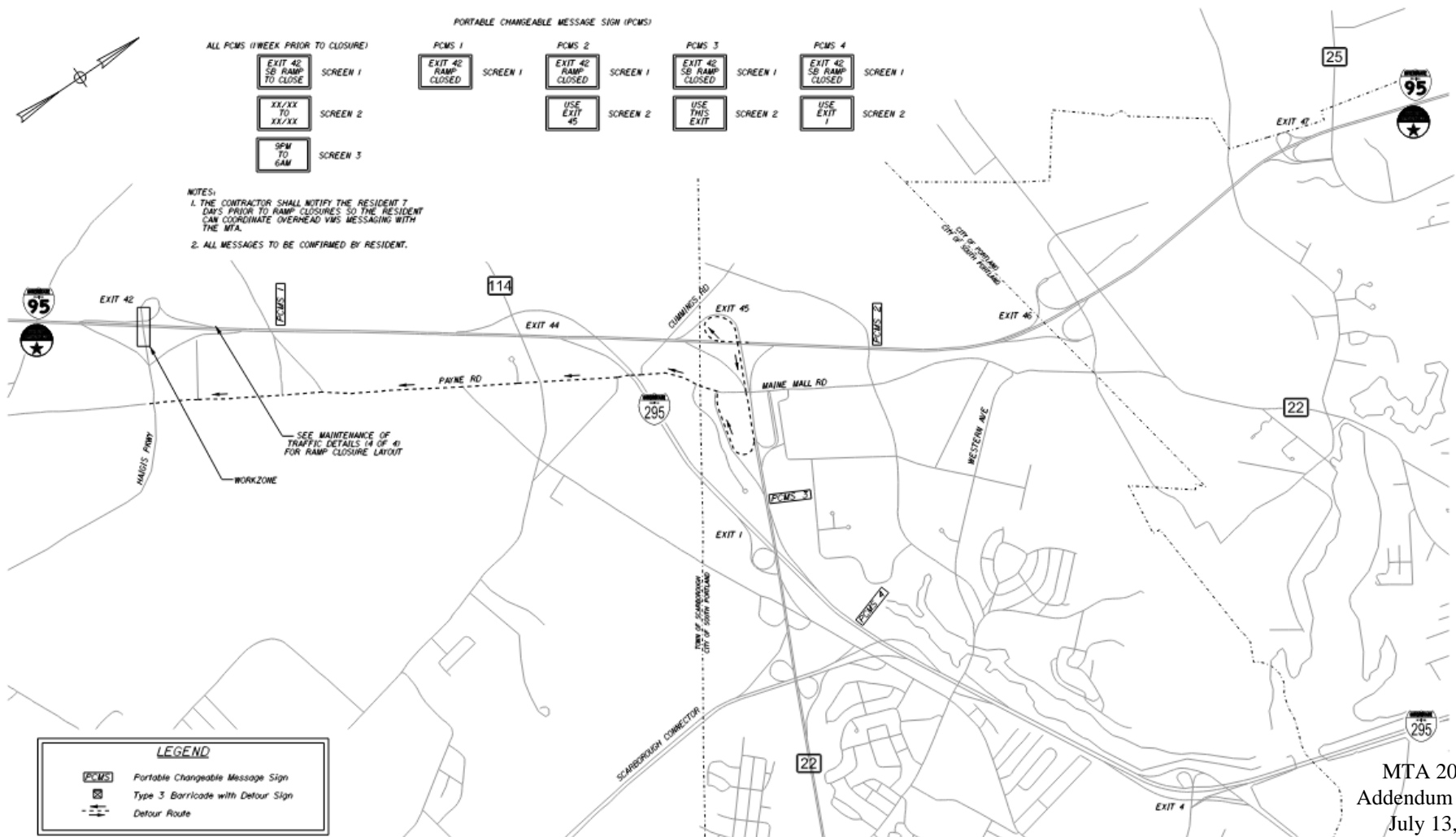
12) Specific Contract Items

- **Section 518** – Structural Concrete Repair, Elastomeric Concrete Header Repair
 - The work includes removal of the existing and placement of new elastomeric concrete at the deck expansion joints.
 - If the Contractor opts to complete the work utilizing closures for three (3) consecutive nights, the work shall also include placing and securing steel plates over the joint after the existing pavement and elastomeric concrete have been removed and before the proposed elastomeric concrete and pavement have been placed.
 - Steel plates, if necessary, shall be incidental to Elastomeric Concrete Header Repair



13) Traffic Control (Special Provision Section 652)

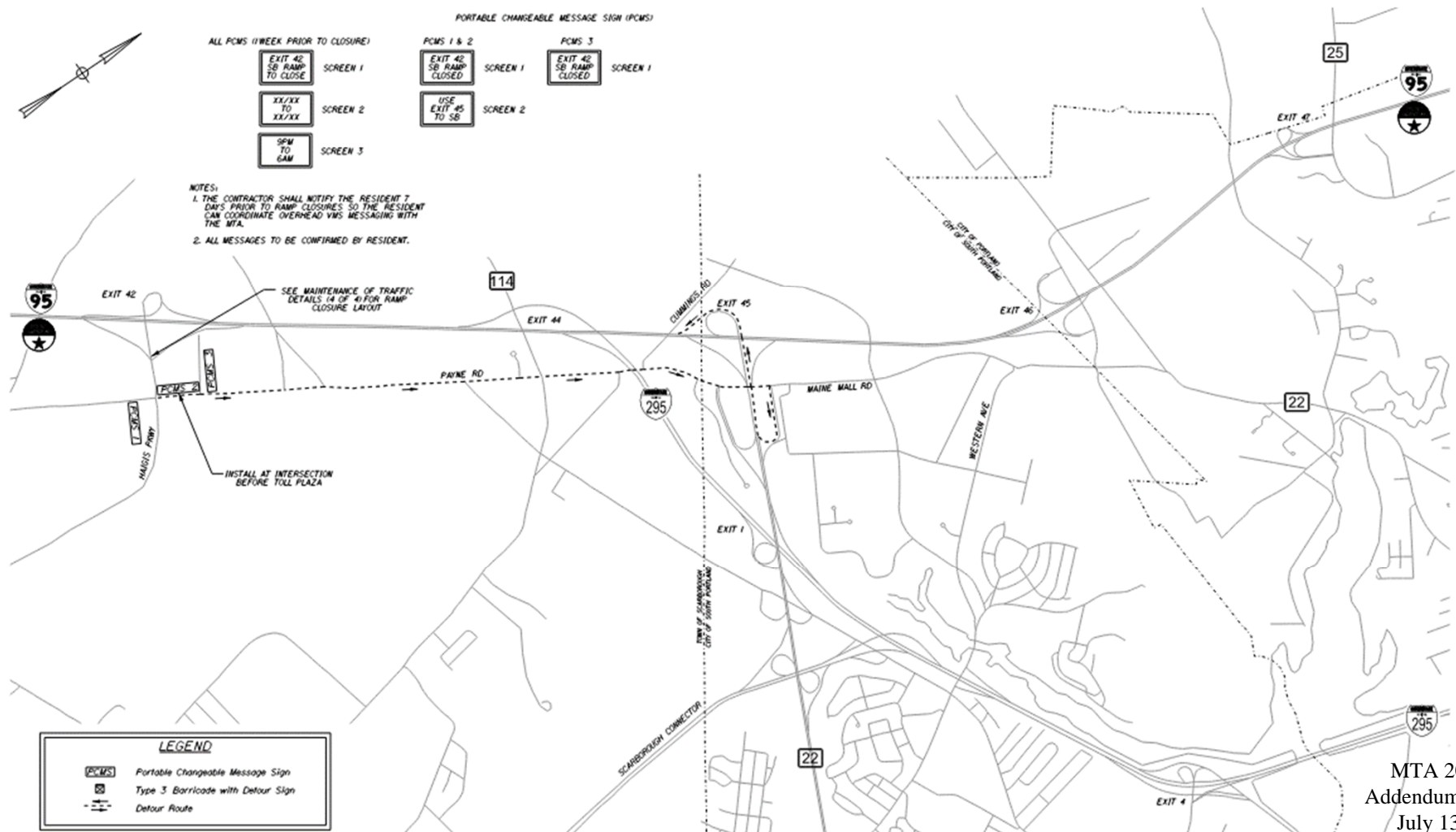
Off Ramp Closure Detour Plan





13) Traffic Control (Special Provision Section 652)

■ On Ramp Closure Detour Plan





13) Traffic Control (Special Provision Section 652)

- SP 652 **replaces** the MaineDOT Standard Specs 2014 Edition and MTA 2016 Supplemental Specs.
- Substantive **revisions have been bolded** in the 652 SP.
- Contractor is responsible for supplying all traffic control devices.
- Contractor is responsible for placement, relocation, removal and maintenance of traffic control devices. Maintenance of traffic control devices is a **24-hour a day, seven days per week** responsibility. Contractor shall inspect devices as required.



13) Traffic Control (Special Provision Section 652)

- Temporary detours, lane closures and construction phasing shall be established and **maintained at all times** in accordance with the details shown on the Plans and the requirements of the Special Provisions.
 - **A truck mounted attenuator** shall be utilized for **all lane closures** on the Turnpike mainline, shall be utilized for **all temporary shoulder closures** (i.e. closures that do not include temporary concrete barrier) on the Turnpike mainline, and other construction operations where workers are exposed to traffic and not protected by positive means.
 - **A truck mounted attenuator** shall be utilized for **all work completed behind guardrail** if that work is being completed **within the deflection zone** of the guardrail (within four feet behind the guardrail post).



13) Traffic Control (Special Provision Section 652)

- Turnpike Lane closures
 - **One lane of traffic flowing** in each direction at all times. **Minimum traveling width of 14'** required unless otherwise shown in the Plans.
 - All lane **closures require the approval of the Resident**. The Resident is required to submit a request to MTA for lane closures by **noon on Thursday** for lane closures needed for the following week. The Contractor shall plan the work and requests for lane closures accordingly. Requests are subject to approval by MTA.
 - **Supplemental liquidated damages** shall be assessed at **\$1,000 per minute** for every minute that a temporary lane closure is in place outside of the allowable times.



13) Traffic Control (Special Provision Section 652)

- All signs, which do not apply to current construction activity, **shall be 100% covered or removed** in accordance with the plans. This includes any speed limit signs when work zone speed is in operation.
- Traffic control devices **shall be NCHRP 350 compliant**. All traffic control devices **shall meet MASH guidelines** if the date of manufacture was **after December 31, 2019**.



**Maine
Turnpike
Authority**

MTA Contract 2023.08



Questions?

Bridge Repairs - Scarborough Interchange Underpass, MM 42.50

PREBID MEETING 10:00AM JULY 11, 2023 - ATTENDENCE SHEET

NAME	COMPANY	EMAIL	PHONE
ANDY KITTREDGE	CPM	ANDYK@CPMCONSTRUCTORS.COM	865-0000
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