

**MAINE TURNPIKE AUTHORITY**

**ADDENDUM NO. 1**

**CONTRACT 2019.07**

**ROUTE 109/EXIT 19 IMPROVEMENTS**  
**VOLUME II**

The following changes are made to the Notice to Contractors, Plans, and Specifications.

**NOTICE TO CONTRACTORS**

Notice to Contractors Sheet N-1 and N-2 are deleted and replaced with Notice to Contractors Sheet N-1 and N-2 (Revised 4/16/19). Changes were made to the Contractor prequalification requirements.

**PLANS**

Plan Sheet 11 of 12, “Route 109 Improvements Signal Plan” is deleted and replaced in its entirety with the attached revised sheet 11.

**SPECIFICATIONS**

Special Provision 107 Contract Time and Completion Date and Prosecution of Work: sheets SP-7 – SP-8 are deleted and replaced with revised sheets SP-7 – SP-8 (Revised 4/16/19).

Special Provision 643 Traffic Signals: sheets SP-33 – SP-34 are deleted and replaced with revised sheets SP-33 – SP-34 (Revised 4/16/19).

Special Provision 652 Maintenance of Traffic: sheet SP-38 is deleted and replaced with revised sheets SP-38 – SP-38A (Revised 4/16/19).

## **QUESTIONS**

**The following are questions asked and comments made at the pre-bid meeting held on April 11, 2019, or were submitted to the Maine Turnpike Authority in writing. The answers to the questions are noted. Bidders shall utilize this information in preparing their bid.**

- Question 1:** Due to current sign support fabrication durations, can the completion date be extended?  
**Answer:** SP 107 has been revised to extend the completion date into May 2020.
- Question 2:** Who will be onsite during construction representing the Authority for the signal work?  
**Answer:** The MTA will have a representatives/consultants on site one of which may be a Traffic Signal Inspector.
- Question 3:** Is video detection to be installed at all 4 poles as plans show only at 3?  
**Answer:** Yes, all 4 poles will have video detection which should be mounted on the existing wood/utility poles utilizing 6-foot luminaire arms.
- Question 4:** Will the signal communicate back to the Authority and if so, how?  
**Answer:** The intent is for the signal to be connected with fiber optic cable to the toll plaza. The contractor will be required to pull a new 62.5 micrometer, 6 fiber multimode cable from the new traffic control cabinet to the existing toll house. Payment is incidental to Pay Item 643.71.
- Question 5:** How will the signal power be connected?  
**Answer:** There is one unused splice location available at an existing the pull box in the northwesterly corner of the intersection.
- Question 6:** Is there a meter?  
**Answer:** No, power will be obtained from the junction box discussed in Question 5.
- Question 7:** What are the MOT details on Sheet 4 intended for?  
**Answer:** These MOT details provide the access to the existing mast arm such that the existing sign can be removed and the new sign installed.
- Question 8:** Are traffic officers needed?  
**Answer:** Signal installation is anticipated to be at night and flaggers should be adequate.
- Question 9:** What signing contractors has the MTA worked with recently?  
**Answer:** Some of the recent contractors that have recently been awarded signing work are Liddell, Northeast, Road Safe, and CPM.
- Question 10:** Are the existing span wires for the signals to remain?  
**Answer:** Yes

Question 11: Where should the existing signal equipment be disposed?

Answer: Crosby Maintenance Yard at MM 45.8 southbound

Question 12: Is temporary concrete barrier required for all shoulder closures?

Answer: Temporary concrete barrier will only be required for the shoulder closure associated with the cantilever overhead guide sign foundation.

**ATTACHMENTS**

- Notice to Contractors (2 pages)
- Plan Sheets (1 page)
- Specifications (6 pages)
- Pre-Bid Agenda (3 pages)
- Pre-Bid Sign-In Sheet (1 page)

**Notes:** The above items shall be considered as part of the bid submittal.

The total number of pages included with this addendum is seventeen (17).

All bidders are requested to acknowledge the receipt of the Addendum No. 1 by signing below and faxing this sheet to Nathaniel Carll, Purchasing Department, Maine Turnpike Authority at 207-871-7739. Bidders are also required to acknowledge receipt of this Addendum No. 1 on Page P-5 of the bid package.

\_\_\_\_\_  
Business Name

\_\_\_\_\_  
Print Name and Title

\_\_\_\_\_  
Signature

\_\_\_\_\_  
Date

April 17, 2019

Very truly yours,

MAINE TURNPIKE AUTHORITY

\_\_\_\_\_  
Nathaniel Carll  
Purchasing Department  
Maine Turnpike Authority

MAINE TURNPIKE AUTHORITY

NOTICE TO CONTRACTORS

Sealed Proposals will be received by the Maine Turnpike Authority for:

CONTRACT 2019.07

ROUTE 109/EXIT 19 IMPROVEMENTS  
VOLUME II

at the office of the Maine Turnpike Authority, 2360 Congress Street, Portland, ME, until 11:00 a.m., prevailing time as determined by the Authority on April 23, 2019 at which time and place the Proposals will be publicly opened and read.

Bids will be accepted from Contractors prequalified by the Maine Department of Transportation for Bridge Construction Projects or for Traffic Signals and Lighting Projects. All other bids may be rejected. In addition, contractors submitting bids must be themselves or utilize a subcontractor pre-qualified by the Maine Department of Transportation for Traffic Signals and Lighting Projects for the completion of the traffic signal work. In addition, contractors submitting bids must be themselves or utilize a subcontractor pre-qualified by the Maine Department of Transportation for Bridge Projects for the completion of the cantilevered guide sign or submit a list with their bid outlining similar work they have completed for the Authority or other State Highway Agencies.

Contractors or subcontractors not currently prequalified by MaineDOT can seek prequalification for this project prior to the award by submitting the prequalification application directly to the Authority at the above address.

This Project includes a wage determination developed by the State of Maine Department of Labor.

The work consists of intersection improvements to the Route 109 and Exit 19 intersection in the Town of Wells, Maine. The work includes adding signing and modifying signals, maintenance of traffic and all other work incidental thereto in accordance with the Plans and Specifications.

Plans and Contract Documents may be examined by prospective Bidders weekdays between 8:00 a.m. and 4:30 p.m. at the office of the Maine Turnpike Authority, 2360 Congress Street, Portland, Maine. **The half size Plans** and Contract Documents may be obtained from the Authority upon payment of Fifty (\$50.00) Dollars for each set, which payment will not be returned. Checks shall be made payable to: Maine Turnpike Authority. The Plans and Contract Documents may also be downloaded from a link on our website at <http://www.maineturnpike.com/project-and-planning/Construction-Contracts.aspx>.

For general information regarding Bidding and Contracting procedures, contact Nate Carll, Purchasing Manager, at (207)482-8115. For information regarding Schedule of Items, plan holders

list and bid results, visit our website at <http://www.maineturnpike.com/project-and-planning/Construction-Contracts.aspx> . For Project specific information, fax all questions to Nate Carll, Purchasing Manager, at (207) 871-7739 or email [ncarll@maineturnpike.com](mailto:ncarll@maineturnpike.com). Responses will not be prepared for questions received by telephone. Bidders shall not contact any other Authority staff or Consultants for clarification of Contract provisions, and the Authority will not be responsible for any interpretations so obtained.

All work shall be governed by the Specifications entitled "State of Maine, Department of Transportation, Standard Specifications, Revision of November 2014", "Standard Details, Revision of November 2014" and "Best Management Practices for Erosion and Sediment Control", latest issue. Copies and recent updates to these publications can be downloaded at: <http://www.maine.gov/mdot/contractors/publications/> .

Proposals must be accompanied by an original bid bond, certified or cashier's check payable to the Maine Turnpike Authority in an amount not less than Five (5%) Percent of the Total Amount in the Proposal, but not less than \$500.00. The Bidder to whom a Contract is awarded will be required to furnish a Surety Corporation Bond, satisfactory to the Authority, on the standard Contract Bond form of the Authority, for a sum not less than the Total Amount of the Proposal.

Proposals must be made upon the Proposal Forms furnished by the Authority separately with the Contract Documents, and must be enclosed in the sealed special addressed envelope provided therefore bearing the name and address of the Bidder, the name of the Contract, and the date and time of Proposal opening on the outside.

A pre-bid conference will be held on April 11, 2019 at 10:00 a.m. at the Maine Turnpike Authority, 2360 Congress Street, Portland, Maine.

The Authority reserves the unqualified right to reject any or all Proposals and to accept that Proposal which in its sole judgment will under all circumstances serve its best interest.

MAINE TURNPIKE AUTHORITY

Nate Carll  
Purchasing Manager  
Maine Turnpike Authority

Portland, Maine

WATER:

Kennebunk, Kennebunkport, & Wells Water District  
92 Main St.  
Kennebunk, ME 04043  
Tel: 207-985-3385  
Keith Archibald

SANITARY:

Wells Sanitary District  
197 Eldridge Road  
Wells, ME 04090  
Tel: 207-645-5906  
Nick Rico

GAS:

Unitil  
376 Riverside Ind Pkwy  
Portland, ME 04103  
Tel: 207-541-2572  
Cell: 207-210-3310  
Kelly Brown

104.4.7 Cooperation With Other Contractors

This Subsection is amended by the addition of the following:

Adjacent contracts currently scheduled for the 2019 construction season include:

MaineDOT Contract 023927.00 – North Berwick – Wells  
Rte 9 Ultra-Thin Bonded Wearing Course

107.1 Contract Time and Contract Completion Date

This Subsection is amended by the addition of the following:

The Contractor shall not begin work at the project site until September 3, 2019.

The contractor shall have the project substantially complete on or before November 15, 2019. The project shall be considered substantially complete when the following items are completed:

- 1) All foundation work for proposed guide signs.
- 2) The proposed overhead sign on the gantry at the toll plaza.
- 3) The span wire assembly and alternate merge sign for eastbound Route 109 traffic.

All remaining work shall be completed on or before May 21, 2020.

#### 107.4.6 Prosecution of Work

The Contractor shall submit to the Authority a construction schedule which shall document that the Contractor has the necessary labor and equipment to work immediately and continuously at the project site starting in September 2019. The intent of this specification is to start work after completion of the adjacent contract outlined in Section 104.4.7 Cooperation With Other Contractors above, while providing the Contractor sufficient time to complete the work in a diligent manner.

The contractor shall cover any conflicting proposed guide signs until the traffic signal work and new striping pattern is completed. The contractor's schedule of work shall show the following items being completed simultaneously prior to utilizing the new lane configuration:

- 1) Traffic signal revisions needed for the double left turning movement for Route 109 eastbound traffic.
- 2) 4" dotted white lane line striping for the double left turning movement for Route 109 eastbound traffic.
- 3) Uncovering all proposed roadside and cantilevered overhead guide signs.
- 4) The existing sign at Sta. 5+25 RT shall not be removed until new guide signs are uncovered.

Note: The white wire shall be used for all neutral connections and shall be connected to the service ground.

No street lighting splices will be permitted in the mast-arm shaft. Splices for street lighting and lightning arrestors shall be located inside the nearest street light pull box.

Ground connections. All installations and equipment shall be bonded and grounded to the service ground rod in accordance with the requirements of the electric power company.

Each signal cable run shall be installed with one green plastic covered copper ground wire to which all equipment shall be bonded in accordance with standard practice. Each base and post, cabinet, and any other component that would be considered a part of the signal system shall be bonded to the ground wire. This ground wire shall be connected to the ground rod at the controller cabinet.

643.133 Installation of signals and equipment. The signals and equipment shall be installed by competent workmen or the manufacturer's representative.

Prior to placing the signals in operation, the signal housing shall be hooded with approved non-transparent material or turned to clearly indicate that the signals are not in operation.

Signs mounted on the signals not applicable to construction conditions shall be covered as specified in Section 645 of the Standard Specifications.

All material including poles, foundations, fittings and cable shall be supplied and installed to make a complete operative installation.

Signs installed on signal arms shall be mounted with "Astro Sign Bracs" at a right angle to the roadway. Signs mounted on span wire shall be mounted with Pelco "Span Wire Sign Hangar Assemblies," or approved equal.

The existing controller cabinet, controller, cabinet equipment and spanwire mounting hardware shall be the property of the Authority. All existing signal heads and LED's shall become the property of the contractor. All equipment to become property of the Authority shall be delivered to the Authority at the Crosby Farm Maintenance Area at MM 45.8 southbound.

643.14 Operation. The Contractor shall commence the operation of the signal system only when permitted by the Engineer. Unless otherwise noted, signals shall be placed in flash a minimum of 1 week before the planned start of operation. New signals shall be made operational between the hours of 10:00 AM and 2:00 PM unless approved by the Engineer.

Operating sequences shall be as shown on the plans or as ordered.

Operating sequences shall be verified by testing.

In cooperation with the Fire Department, the Contractor shall make trial runs to ascertain proper timing of the fire pre-emption system. The minimum time shall be approved by the Chief of the Fire Department or the Chief's representative.

The Contractor shall provide a qualified technician to thoroughly review and confirm that the system is satisfactory and operational as designed. Prior to the final inspection, the Contractor shall have a review with the Authority’s Toll / ITS Manager and local officials (including Fire Department technician) to review and comment upon the system.

643.15 Warranty. Upon completion of the project, the Contractor shall forward to the Authority all warranties to the purchaser that the equipment which has been installed hereunder shall be free from defects in materials, workmanship and title, and shall be of the kind and quality designated or described in the Contract. The foregoing warranty supersedes all other warranties whether written, oral, or implied. If it appears within 24 months from the date of Acceptance of the work that the equipment installed hereunder does not meet the warranties specified above, the Contractor shall promptly correct any defect or nonconformance with the specifications. This warranty does not relieve the Contractor of the requirement of Section 106 of the Standard Specifications.

643.16 Method of Measurement. The traffic signal modifications will be measured as a lump sum unit. The video detection system will be measured as a lump sum unit.

643.17 Basis of Payment. The accepted quantity of traffic signals will be paid for at the Contract lump sum price complete in place.

When an item of conduit appears in the Contract, conduit for traffic signals will be paid for under Section 626 of the Standard Specification. When no item for conduit appears in the Contract, any conduit required will be incidental.

All miscellaneous electrical equipment required shall be subsidiary.

Video Detection System (Item 643.83) will be paid for at the contract lump sum price, which payment will be full compensation for installation and furnishing all materials and all appurtenances and incidentals required for a complete functioning installation. The Contractor shall coordinate with the Manufactures Representative for initial configuration and onsite training.

Payment will be made under:

<u>Pay Item</u>	<u>Pay Unit</u>
643.71            Traffic Signal Modifications at Exit 19 and Sanford Road	Lump Sum
643.83            Video Detection System	Lump Sum

## SPECIAL PROVISION

### SECTION 652

#### MAINTENANCE OF TRAFFIC

(Specific Project Maintenance of Traffic Requirements)

This Specification describes the specific project maintenance of traffic requirements for this Project.

The following minimum traffic requirements shall be maintained. These requirements may be adjusted based on the traffic volume when authorized by the Authority.

#### Traffic Control Requirements

A minimum of one lane of traffic in each direction shall be maintained on Route 109/Sanford Road at all times, in accordance with the details shown on the Plans. A minimum of two lanes of traffic on the Exit 19 off-ramp shall be maintained at all times except as noted below in accordance with the details shown on the Plans with the exception of installing or removing traffic control devices or as noted below. Travel lanes shall be at least 11 feet wide.

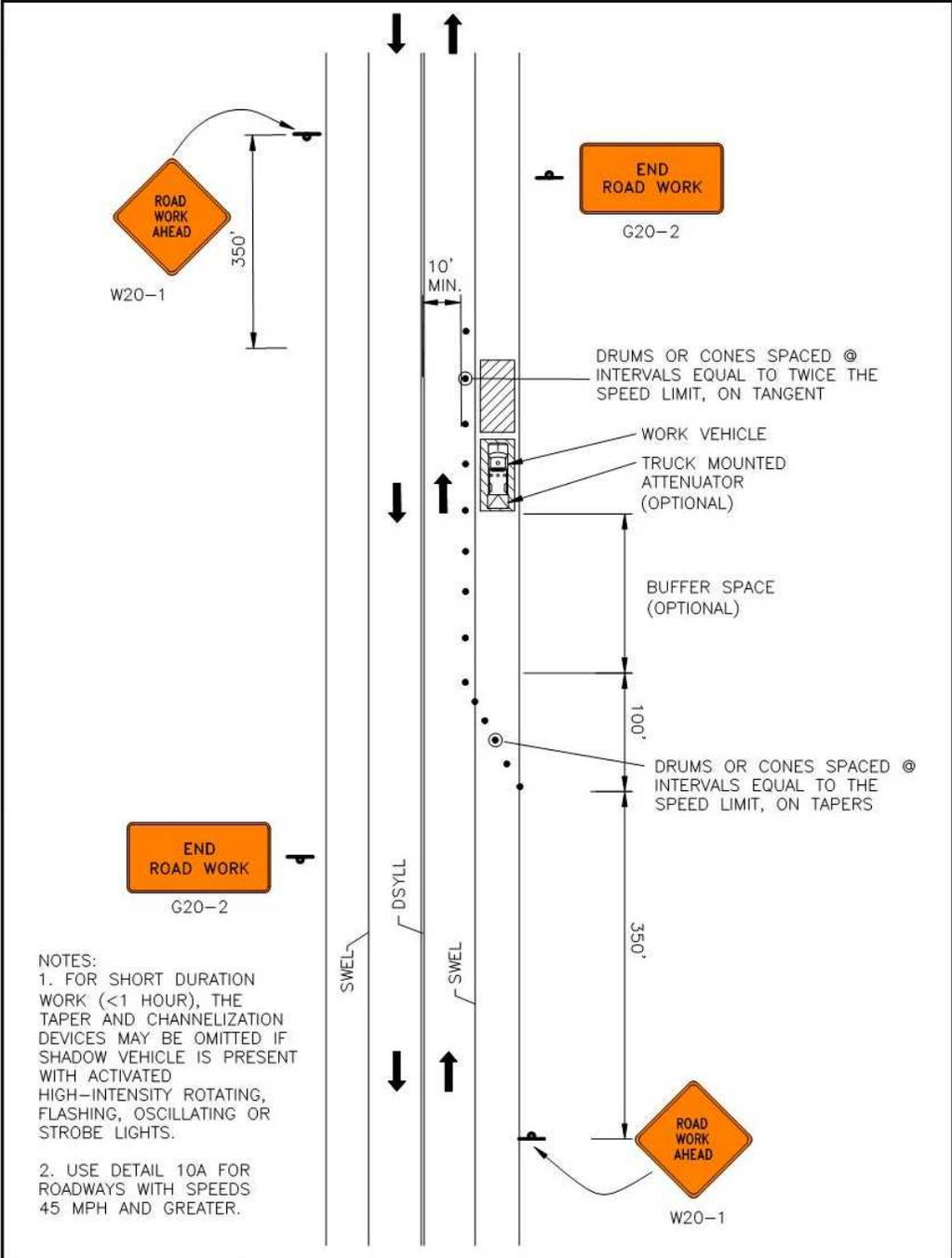
Work directly over traffic or within six feet of a travel lane as measured from the painted pavement marking line or traffic control device will require a lane closure. This work includes installation of sign structures and installation of signs.

No temporary lane closures will be allowed between 6AM and 9AM in the morning and between 3PM and 7PM in the evening. Travel lanes may not be impeded by traffic control devices until the time frames specified. Lane Closures shall be removed if construction is not ongoing. Unattended lane closures are not allowed.

All work associated with 643 pay items that are within travel lanes and associated temporary lane closures shall occur at night, between the hours of 9PM at night and 5AM the following morning.

A barrier-protected shoulder closure shall be used for installation of the cantilevered sign structure and foundations at Sta. 10+50 in accordance with the details shown on the Plans.

A local road shoulder closure utilizing drums may be used for short duration activities along Route 109 in accordance with the detail provided on page SP-38A.



DETAIL 6 NOT TO SCALE

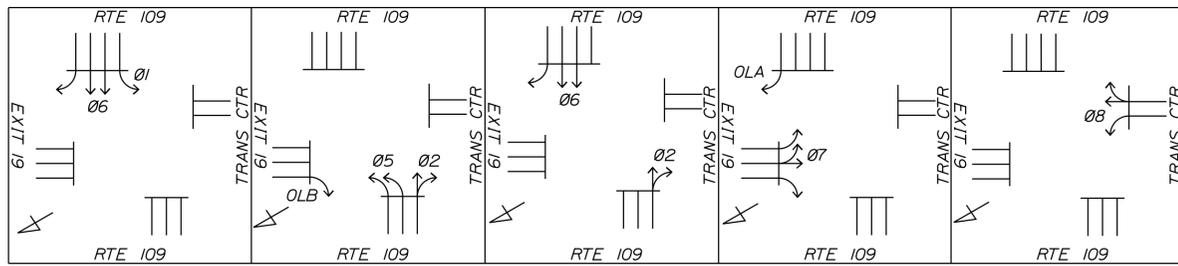


DATE: 12-22-2017



**MAINE TURNPIKE AUTHORITY**  
**TRAFFIC CONTROL DETAIL**  
 SHOULDER WORK WITH MINOR ENCROACHMENT  
 ON A MINOR ROAD WITH SPEEDS 40 MPH OR LESS  
 SHORT TERM (UP TO 12 DAYLIGHT HOURS)

Date: 4/17/2019



**PHASING DIAGRAM**

LOCATION	TIME PERIOD								
	RT. 109 LT.	RT. 109	ROAD NAME	ROAD NAME	RT. 109 LT.	RT 109	EXIT 19	TRANS. CTR.	ROAD NAME
PHASE	1	2	3	4	5	6	7	8	9
MINIMUM INITIAL	5	10	-	-	5	10	5	5	-
PASSAGE TIME	3	3	-	-	3	3	3	3	-
MAXIMUM 1*	10	25	-	-	30	25	20	15	-
MAXIMUM 2*	10	25	-	-	25	25	30	15	-
YELLOW	4	4	-	-	4	4	4	4	-
ALL RED	2	2	-	-	2	2	2	2	-
PED. WALK	-	-	-	-	-	-	-	-	-
PED. CLEARANCE (FDW)	-	-	-	-	-	-	-	-	-
PED. CLEARANCE (DW)	-	-	-	-	-	-	-	-	-
RECALL	-	SOFT	-	-	-	SOFT	-	-	-
DETECTOR OPERATION	PR	PR	-	-	PR	PR	PR	PR	-
PREEMPTION PRIORITY**	3	4	-	-	4	3	6	5	-
FLASH	Y	Y	-	-	Y	Y	R	R	-
DUAL ENTRY	-	-	-	-	-	-	-	-	-

NOTES:  
 S = SOFT RECALL      PR = PRESENCE      Y = YELLOW      FDW = FLASHING DON'T WALK  
 MAX = MAX RECALL      MAX1 = FREE OPERATION      R = RED      DW = DON'T WALK  
 C-MAX = COORDINATED MAX      MAX2 = UNDER COORDINATION      D = DARK  
 0 = RECALL OFF

\* MAX 1 IS FROM 6AM - 10AM, MAX 2 IS FOR ALL OTHER TIMES

\*\* PHASE 1 IS ACTIVATED WHEN PREEMPTION CALL IS RECEIVED.

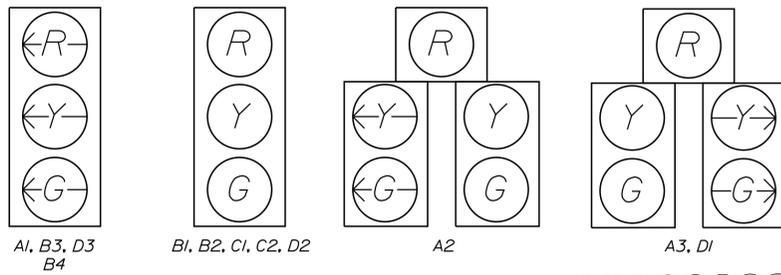
PHASES 1-5 SHALL NOT BE PERMITTED

**GENERAL NOTES:**

1. THE CONTRACTOR SHALL REMOVE ALL CONFLICTING SIGNS, PAYMENT WILL BE INCIDENTAL TO ITEM 643.80 - TRAFFIC SIGNAL MODIFICATIONS. ALL REMOVED EQUIPMENT IS THE PROPERTY OF THE MTA AND SHALL BE STACKED AT THE MTA SIGN SHOP AT MM 58.3.
2. THE CONTRACTOR SHALL VERIFY THE LOCATION, DEPTH, AND MATERIAL OF ALL SUBSURFACE UTILITIES.
3. ALL TRAFFIC SIGNAL WORK SHALL BE COMPLETED IN A MANNER THAT WILL CAUSE A MINIMUM DISRUPTION TO TRAFFIC.
4. EXISTING POWER SUPPLY TO CONTROLLER CABINET SHALL BE REMOVED.

**SIGNAL NOTES:**

1. TRAFFIC SIGNAL EQUIPMENT SHALL MEET THE REQUIREMENTS OF THE MTA SPECIAL PROVISIONS.
2. VEHICLE SIGNAL HEAD HOUSING SHALL BE A 12" ALUMINUM OR POLYCARBONATE HOUSING.
3. ALL SIGNAL HEADS SHALL BE 12" DIAMETER LED.
4. ALL SIGNAL HEADS SHALL HAVE 5" LOUVERED REFLECTORIZED BACKPLATES.
5. ALL SIGNAL HEADS SHALL BE EQUIPPED WITH TUNNEL VISORS.
6. ALL SIGNAL HEADS SHALL BE NEW.



7. EXISTING SPAN WIRES TO REMAIN.

MAJOR ITEMS REQUIRED	
QUANTITY	ITEM
12	NEW SIGNAL HEAD
1 EACH	VIDEO DETECTION SYSTEM
1 EACH	REMOVE AND DISPOSE OVERHEAD SIGN
1 EACH	NEW OVERHEAD SIGN
1 EACH	NEW CONTROLLER CABINET
1 EACH	REMOVE AND DISPOSE OF MISC. SIGNAL EQUIPMENT
1 EACH	NECESSARY DUCT, WIRE & CABLE, LABOR, TOOLS, MISC. MATERIAL & EQUIPMENT TO COMPLETE THE INSTALLATION

**ROUTE 109 EASTBOUND PROPOSED SPAN WIRE MOUNTED SIGNS**

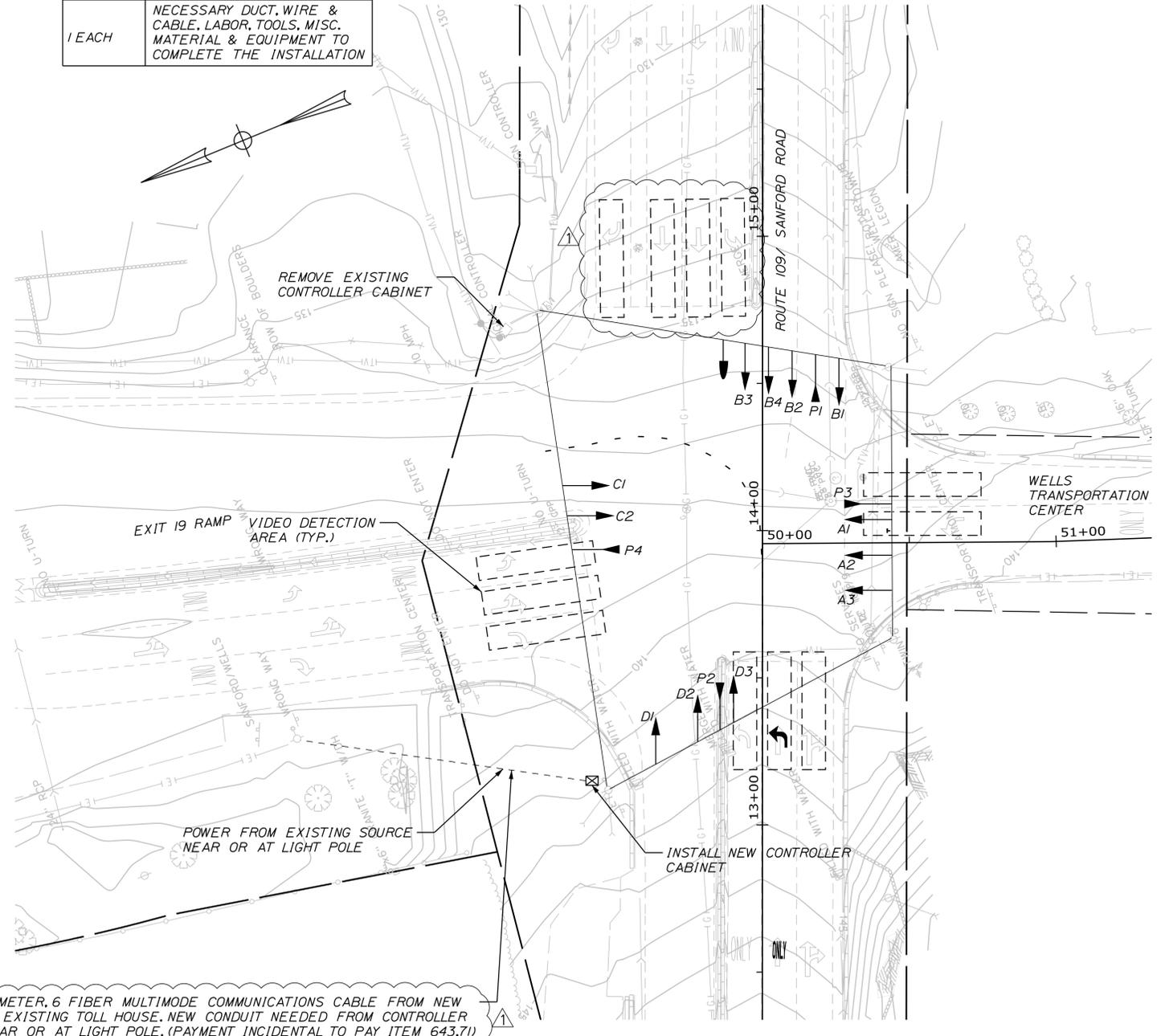


**ROUTE 109 EASTBOUND EXISTING SPAN WIRE MOUNTED SIGNS**



**SIGNAL FACE NTS**

INSTALL NEW 62.5 MICROMETER, 6 FIBER MULTIMODE COMMUNICATIONS CABLE FROM NEW CONTROLLER CABINET TO EXISTING TOLL HOUSE. NEW CONDUIT NEEDED FROM CONTROLLER TO EXISTING CONDUIT NEAR OR AT LIGHT POLE. (PAYMENT INCIDENTAL TO PAY ITEM 643.71)



Filename: ... \MSTA\XXX\_Signal\_Plan.DGN

Scale: 1" = 25'

No.	Revision	By	Date

Designed by:

**TYLIN INTERNATIONAL**

CONSULTANT PROJECT MANAGER: Kevin Ducharme

By	Date	By	Date
Designed TAE	3/2019	Checked KSD	3/2019
Drawn TWA	3/2019	In Charge of KSD	3/2019

T.Y. Lin International  
 12 Northbrook Drive  
 Building A, Suite One  
 Falmouth, Maine 04105  
 TEL: (207) 781-4721  
 FAX: (207) 781-4753

**MAINE TURNPIKE**

**THE GOLD STAR MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: Ralph C. Norwood, IV

**ROUTE 109 IMPROVEMENTS**

**SIGNAL PLAN**

SHEET NUMBER: 1 OF 1

CONTRACT: 2019.07

# MAINE TURNPIKE AUTHORITY

## Pre-Bid Conference

### CONTRACT 2019.07

#### ROUTE 109 / EXIT 19 IMPROVEMENTS VOLUME II

April 11, 2019

1. Locations:

The general limits of work are as shown in the contract plans. The project is located at the intersection of Exit 19 off Interstate 95 and Route 109, extending easterly approximately 500 feet and westerly 900 feet. The project is located near the Route 109 / Sanford Road Overpass located at Mile 18.8 of the Maine Turnpike in Wells.

2. General Description:

The work consists of intersection improvements to the Route 109 and Exit 19 intersection in the Town of Wells, Maine. The work includes adding signing and modifying signals, maintenance of traffic, and all other work incidental thereto in accordance with the plans and specifications.

3. Bid:

- a. Bid opening: April 23, 2019 at 11:00 AM, prevailing time as determined by the Authority at the MTA Administration Building, 2360 Congress Street, Portland.
- b. All bid and contractual questions shall be directed to Nate Carll, Purchasing Manager, at Phone No. (207) 482-8115.
- c. All questions on plans and specifications shall be in writing and shall be faxed or emailed to Nate Carll, Purchasing Manager, at Fax No. (207) 871-7739 or email [ncarll@maineturnpike.com](mailto:ncarll@maineturnpike.com).

4. Notification:

Contractor shall notify and obtain approval from the Authority prior to visiting the Project sites for field inspection. The contact person is Mr. Steve Tartre at (207) 482-8144 or [startre@maineturnpike.com](mailto:startre@maineturnpike.com).

5. Contract Specifications:

- a. The Specifications are divided into two parts: Part I, Supplemental Specifications and Part II, Special Provisions. Contractor is to review updated Supplemental Specifications.

- b. The Maine Turnpike Authority 2016 Supplemental Specifications are additions and alterations to the 2014 Maine Department of Transportation Standard Specifications. They are available online at <http://www.maineturnpike.com/Projects-Planning/Construction-Contracts.aspx>

6. Construction Schedule/Substantial Completion/Prosecution of Work:

- a. April 25, 2019 – MTA Board to consider Contract Award.
- b. The Contractor shall not begin work at the project site until September 3, 2019.
- c. All work shall be completed on or before November 15, 2019.

7. Maine Department of Labor – Fair Hourly Wages (Special Provision 104.3.8)

Contract will include Highway & Earthwork and Heavy & Bridge wage rates. See Special Provision 104.3.8 for The Wage Rate Determination.

8. Utility Coordination (Special Provision 104.4.6)

There are adjacent utilities on Route 109 and at the Exit 19 intersection.

The Contractor shall give Central Maine Power (CMP) 10 working days' notice prior to any construction near their facilities.

The Contractor shall give Consolidated Communications 10 working days' notice prior to any construction near their poles.

The Contractor shall give Spectrum 10 working days' notice prior to any construction near their facilities.

Temporary utility adjustments and or subsurface utility adjustment are not anticipated as part of this project.

9. Cooperation with Other Contractors (Special Provision 104.4.7)

Adjacent contracts currently scheduled for the 2019 construction season include:

- MaineDOT Contract 023927.00 – North Berwick – Wells  
(Route 9 Ultra-Thin Bonded Wearing Course)  
This project is expected to be completed by Labor Day Weekend 2019

10. Permit Requirements (Special Provision 105.8.2)

Section to be deleted from Table of Contents, not needed for project.

11. General Requirements:

- a. U-Turns at toll plazas and median openings are not allowed.
- b. All jobsite personnel shall wear a safety vest labeled as ANSI 107-2004 standard performance for Class 3 risk exposures at all times. This requirement also applies to truck drivers and equipment operators when out of an enclosed cab.

- c. All vehicles used on the Project shall be equipped with amber flashing beacons in accordance with the Special Provision 652.3.4.

## 12. Traffic Control (Supplemental Specification and Special Provision Section 652)

- a. The Contractor is responsible for supplying, inspecting and maintaining traffic control devices in accordance with the project specifications. Contractor is subject to Penalty Damages for violation(s) per Supplemental Specification 652 and Special Provision 652.
- b. All traffic control devices shall be NCHRP 350 or MASH (2009 edition) compliant.
- c. A minimum of one lane of traffic in each direction shall be maintained on Route 109/Sanford Road at all times, in accordance with the details shown on the Plans. A minimum of two lanes of traffic on the Exit 19 off-ramp shall be maintained at all times except noted below in accordance with the details shown on the Plans with the exception of installing or removing traffic control devices or as noted below. Travel lanes shall be at least 11 feet wide.
- d. Work directly over traffic or within six feet of a travel lane as measured from the painted pavement marking line or traffic control device will require a lane closure. This work includes installation of sign structures and installation of signs.
- e. No temporary lane closures will be allowed between 6AM and 9AM in the morning and between 3PM and 7PM in the evening. Travel lanes may not be impeded by traffic control devices until the time frames specified. Lane closures shall be removed if construction is not ongoing. Unattended lane closures are not allowed.
- f. All work associated with 643 pay items that are within travel lanes and associated temporary lane closures shall occur at night, between the hours of 9PM at night and 5AM the following morning.
- g. A barrier-protected shoulder closure shall be used for installation of sign structure poles / foundations in accordance with the details shown on the Plans.
- h. All signs that do not apply to current construction activity shall be 100% covered or removed in accordance with the plans.

## 13. Specific Contract Items:

- a. Safety Fence (Pay Item 607.44) shall be installed along the ROW line at the cantilever guide sign location to protect from the excavation hole.
- b. The shoulder closure detail should be used for constructing the cantilever guide sign foundation. The MOT Sheet 3 of 12 is for erecting the signs/sign supports.

## 14. Questions?

