

GENERAL NOTES

SPECIFICATIONS

DESIGN
AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES
1996 WITH 1998 INTERIM REVISIONS.

CONTRACT
STATE OF MAINE, DEPARTMENT OF TRANSPORTATION,
STANDARD SPECIFICATIONS, HIGHWAYS AND BRIDGES,
REVISION OF APRIL 1995.

DESIGN LOADING

LIVE LOAD
HS 25, 2,000,000 CYCLES

DESIGN METHOD
SERVICE LOAD DESIGN

MATERIALS

CONCRETE
SUPERSTRUCTURE SLAB, PARAPETS AND END POSTS (ABOVE CONST. JT.)
CONCRETE SHALL BE CLASS AAA, $f'_c = 4,500$ P.S.I.
ALL OTHER SUBSTRUCTURE
CONCRETE SHALL BE CLASS A, $f'_c = 4,000$ P.S.I.

REINFORCING STEEL

AASHTO M31 GRADE 60. (EPOXY-COATED AND UNCOATED
BARS).

STRUCTURAL STEEL

GIRDERS: COVER PLATES, BEARING STIFFENERS AND
DIAPHRAGMS SHALL BE AASHTO M270, GRADE 36.
HIGH STRENGTH BOLTS SHALL BE AASHTO M164.
BEARING PEDESTALS INCLUDING ANCHOR BOLTS SHALL
BE AASHTO M270, GRADE 50.

BASIC ALLOWABLE STRESSES

CONCRETE
 $f'_c = 1,800$ P.S.I. (SUPERSTRUCTURE)
 $f'_c = 1,600$ P.S.I. (ALL OTHER)

REINFORCING STEEL
 $f_s = 24,000$ P.S.I.

STRUCTURAL STEEL
AASHTO M270 GRADE 36 $f_s = 20,000$ P.S.I.
AASHTO M270 GRADE 50 $f_s = 27,000$ P.S.I. (BEARING PEDESTALS ONLY)

NOTES

- COPIES OF AS-BUILT PLANS ARE ON FILE AT THE MAINE TURNPIKE AUTHORITY. A PORTION OF THESE PLANS ARE INCLUDED IN THIS CONTRACT FOR THE CONTRACTOR'S CONVENIENCE. THE COMPLETENESS AND ACCURACY OF THESE PLANS IS NOT GUARANTEED.
- ALL PROPOSED ELEVATIONS REFERENCE THE NORTH AMERICAN VERTICAL DATUM (NAVD) 1988. THE ELEVATIONS REFERENCED ON THE AS-BUILT PLANS MAY DIFFER.
- FOR BORING LOCATIONS AND BORING LOGS REFER TO DRAWINGS FROM CONTRACT 93.7 (INCLUDED).
- FOR ADDITIONAL DETAILS REFERENCED IN THESE DRAWINGS, SEE THE STATE OF MAINE, DEPARTMENT OF TRANSPORTATION, STANDARD DETAILS, HIGHWAY AND BRIDGES, APRIL 1997.
- THE EXISTING PROFILES ARE TAKEN FROM THE CONTRACT 93.7 DRAWINGS AND HAVE BEEN ADJUSTED FOR THE NORTH AMERICAN VERTICAL DATUM. EXISTING ELEVATIONS ARE NOT GUARANTEED AND MAY VARY.

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Designed by:

HNTB
ARCHITECTS ENGINEERS PLANNERS

| By | Date | By | Date |
|--------------|-------|------------------|-------|
| Designed JFW | 10/99 | Checked AAD | 11/99 |
| Drawn LS | 10/99 | In Charge of RAL | 11/99 |

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**MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT**

**MAINLINE BRIDGE WIDENING
WELLS INTERCHANGE OVERPASS
GENERAL PLAN AND ELEVATION**

SHEET NUMBER: **WI-S1**

CONTRACT: 2000.03

118 OF 178

SUMMARY OF BRIDGE QUANTITIES

| ITEM | DESCRIPTION | UNIT | QUANTITY |
|----------|---|------|----------|
| 201.1224 | REMOVING EXISTING SUPERSTRUCTURE CONCRETE - WELLS INT (9 C.Y.) * | L.S. | 1 |
| 202.12 | REMOVING EXISTING STRUCTURAL CONCRETE | C.Y. | 115 |
| 206.082 | STRUCTURAL EARTH EXCAVATION - MAJOR STRUCTURES | C.Y. | 220 |
| 206.092 | STRUCTURAL ROCK EXCAVATION - MAJOR STRUCTURES | C.Y. | 65 |
| 304.10 | AGGREGATE SUBBASE COURSE - GRAVEL | C.Y. | 430 |
| 403.081 | HOT BITUMINOUS PAVEMENT, TURNPIKE GRADING C | TON | 18 |
| 502.21 | STRUCTURAL CONCRETE, ABUTMENTS AND RETAINING WALLS | C.Y. | 270 |
| 502.2634 | STRUCTURAL CONCRETE ROADWAY AND END POSTS ON STEEL BRIDGES - WELLS INT (25 C.Y.)* | L.S. | 1 |
| 502.2644 | STRUCTURAL CONCRETE PARAPETS - WELLS INT (7 C.Y.)* | L.S. | 1 |
| 502.314 | STRUCTURAL CONCRETE APPROACH SLAB - WELLS INT (11 C.Y.)* | L.S. | 1 |
| 503.12 | REINFORCING STEEL, FABRICATED AND DELIVERED | LB. | 22,100 |
| 503.13 | REINFORCING STEEL, PLACING | LB. | 22,100 |
| 503.14 | EPOXY-COATED REINFORCING STEEL, FABRICATED AND DELIVERED | LB. | 13,000 |
| 503.15 | EPOXY-COATED REINFORCING STEEL, PLACING | LB. | 13,000 |
| 504.7014 | STRUCTURAL STEEL FABRICATED AND DELIVERED, WELLS INT (22,000 LBS. GRADE 36, 700 LBS. GRADE 50)* | L.S. | 1 |
| 504.714 | STRUCTURAL STEEL ERECTION - WELLS INT (22,700 LB.)* | L.S. | 1 |
| 505.084 | STUD WELDED SHEAR CONNECTORS - WELLS INT (340 EA.)* | L.S. | 1 |
| 506.304 | SHOP COATING OF STRUCTURAL STEEL - WELLS INT (22,700 LB.)* | L.S. | 1 |
| 506.314 | FIELD REPAIR OF DAMAGED COATING - WELLS INT | L.S. | 1 |
| 507.0914 | ALUMINUM BRIDGE RAILING, 1 BAR - WELLS INT (228 LF.)* | L.S. | 1 |
| 508.134 | MEMBRANE WATERPROOFING - WELLS INT (100 S.Y.)* | L.S. | 1 |
| 511.094 | TEMPORARY EARTH SUPPORT SYSTEM - WELLS INT | L.S. | 1 |
| 512.0814 | FRENCH DRAINS - WELLS INT (75 L.F.)* | L.S. | 1 |
| 514.06 | CURING BOX FOR CONCRETE CYLINDERS - WELLS INT | EACH | 1 |
| 515.202 | CLEAR PROTECTIVE COATING FOR CONCRETE SURFACE | S.Y. | 245 |
| 524.40 | PROTECTIVE SHIELD | S.Y. | 115 |
| 609.15 | SLOPED CURB TYPE 1 | L.F. | 74 |
| 626.22 | 2 INCH NON-METALLIC CONDUIT | L.F. | 80 |

* QUANTITIES FOR ESTIMATING PURPOSES ONLY

INDEX OF DRAWINGS

| SHEET NO. | TITLE |
|-----------|-------------------------------------|
| WI-S1 | GENERAL PLAN AND ELEVATION |
| WI-S2 | GENERAL NOTES, INDEX AND QUANTITIES |
| WI-S3 | STAGE CONSTRUCTION |
| WI-S4 | SUBSTRUCTURE DEMOLITION |
| WI-S5 | FOUNDATION PLAN |
| WI-S6 | NORTH ABUTMENT |
| WI-S7 | NORTH ABUTMENT REINFORCING |
| WI-S8 | SOUTH ABUTMENT |
| WI-S9 | SOUTH ABUTMENT REINFORCING |
| WI-S10 | ABUTMENT DETAILS |
| WI-S11 | WINGWALL DETAILS |
| WI-S12 | MISCELLANEOUS DETAILS |
| WI-S13 | FRAMING PLAN AND DETAILS |
| WI-S14 | SUPERSTRUCTURE DETAILS |
| WI-S15 | BEARING DETAILS |
| WI-S16 | DECK PLAN AND DETAILS |
| WI-S17 | PARAPET DETAILS |
| WI-S18 | END POST DETAILS |
| WI-S19 | ALUMINUM BRIDGE RAIL DETAILS |
| WI-S20 | REINFORCING STEEL SCHEDULE I |
| WI-S21 | REINFORCING STEEL SCHEDULE II |

CONTRACT NO. 93.7 PLANS

| SHEET NO. | TITLE |
|-----------|-----------------------------|
| WI-S1 | GENERAL PLAN & ELEVATION |
| WI-S3 | BORING PLAN & LOGS |
| WI-S4 | SOUTH ABUTMENT FOOTING PLAN |
| WI-S5 | NORTH ABUTMENT FOOTING PLAN |
| WI-S6 | SOUTH ABUTMENT I |
| WI-S8 | SOUTH ABUTMENT REINFORCING |
| WI-S9 | NORTH ABUTMENT I |
| WI-S10 | NORTH ABUTMENT II |
| WI-S11 | NORTH ABUTMENT REINFORCING |
| WI-S15 | WINGWALL DETAILS |
| WI-S16 | FRAMING PLAN AND DETAILS |
| WI-S18 | DECK PLAN |
| WI-S19 | TYPICAL DECK SECTION |
| BD 101-89 | BEARING PEDESTALS |
| BD 112-89 | DIAPHRAGMS & CROSS-FRAMES |

SEE CONTRACT SHEETS 163 TO 178

ABBREVIATIONS

| | |
|------|------------|
| S.B. | SOUTHBOUND |
| N.B. | NORTHBOUND |
| N.F. | NEAR FACE |
| F.F. | FAR FACE |
| E.F. | EACH FACE |

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**MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT**



MAINLINE BRIDGE WIDENING
WELLS INTERCHANGE OVERPASS
GENERAL NOTES, INDEX AND QUANTITIES

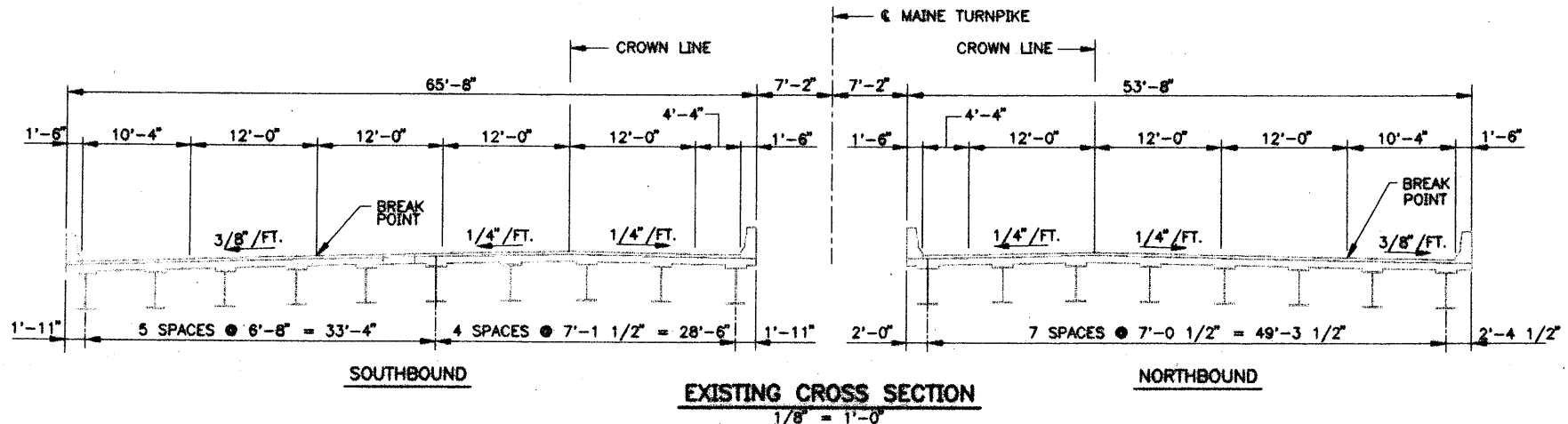
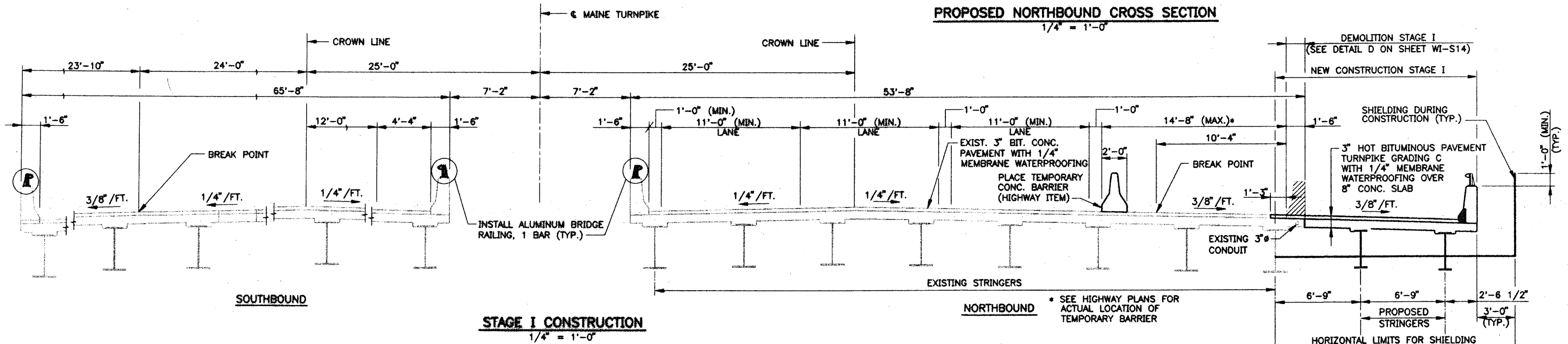
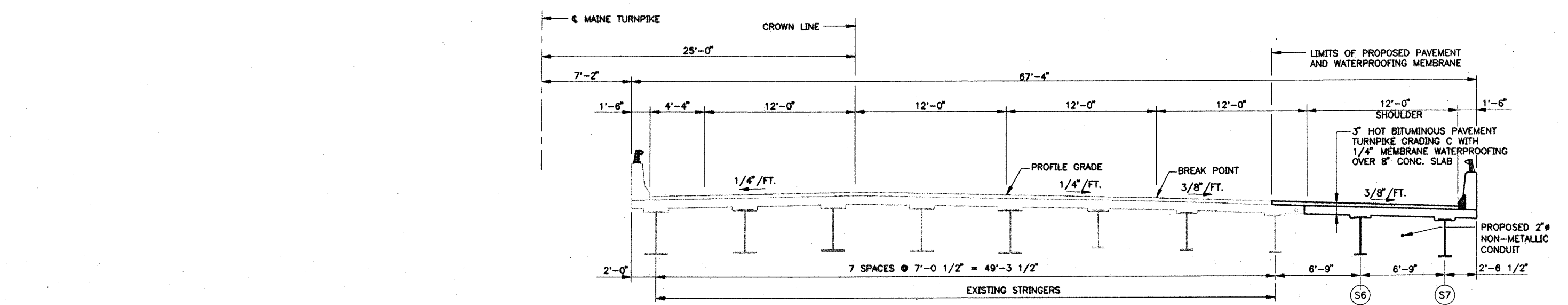
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| Designed | JFW | 11/99 | Checked | SJC | 11/99 |
| Drawn | LS | 11/99 | In Charge of | RAL | 11/99 |

SHEET NUMBER: WI-S2

CONTRACT: 2000.03

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NOTE:
 NORTHBOUND MEDIAN BRIDGE RAIL SHALL NOT BE INSTALLED WHILE TEMPORARY CONCRETE BARRIER AND TRAFFIC SHIFT IS IN PLACE

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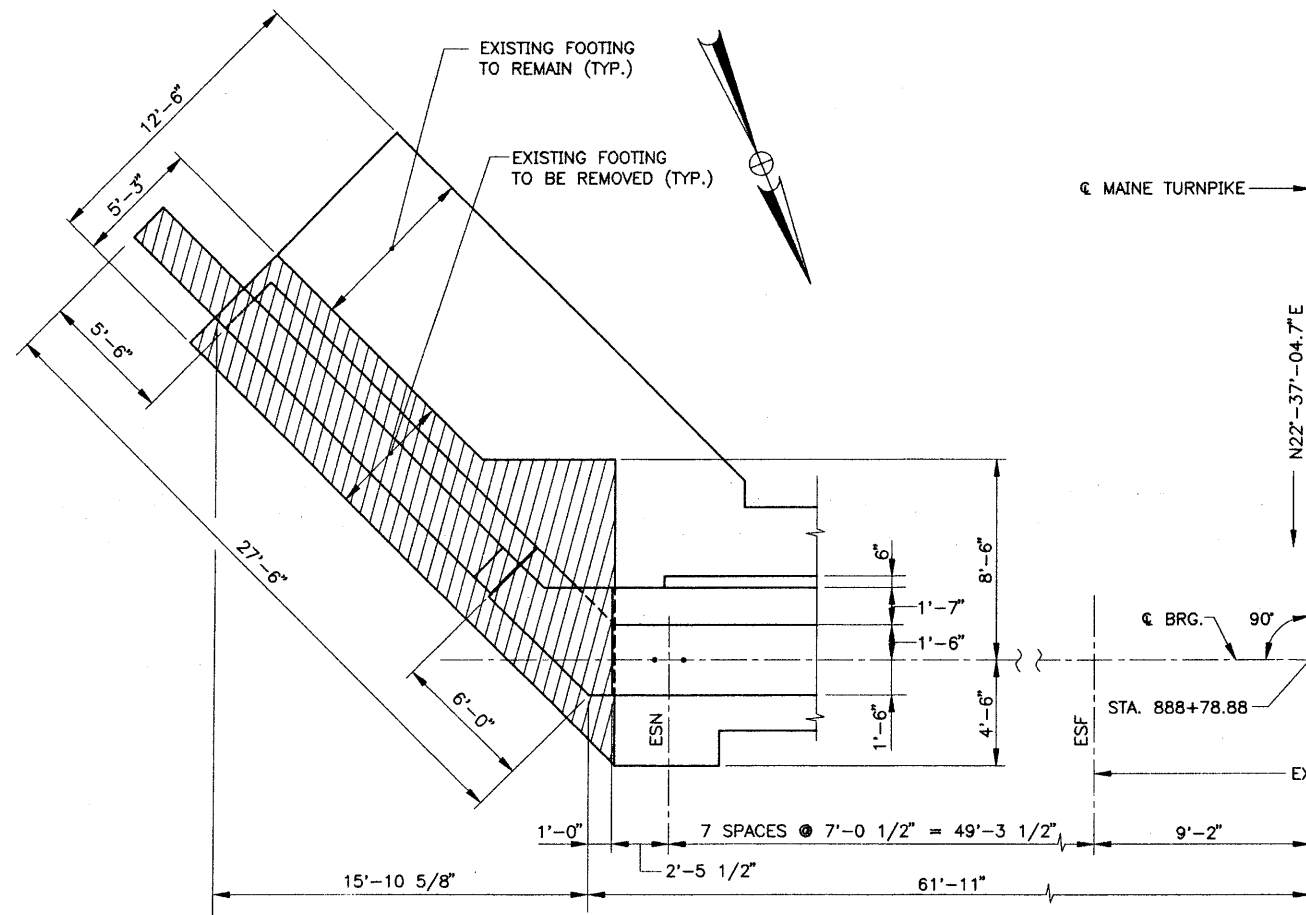
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| Designed | KAC | 9/99 | Checked | SJC | 11/99 |
| Drawn | LS | 9/99 | In Charge of | RAL | 11/99 |

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**MAINE TURNPIKE AUTHORITY
 MODERNIZATION AND WIDENING PROJECT**

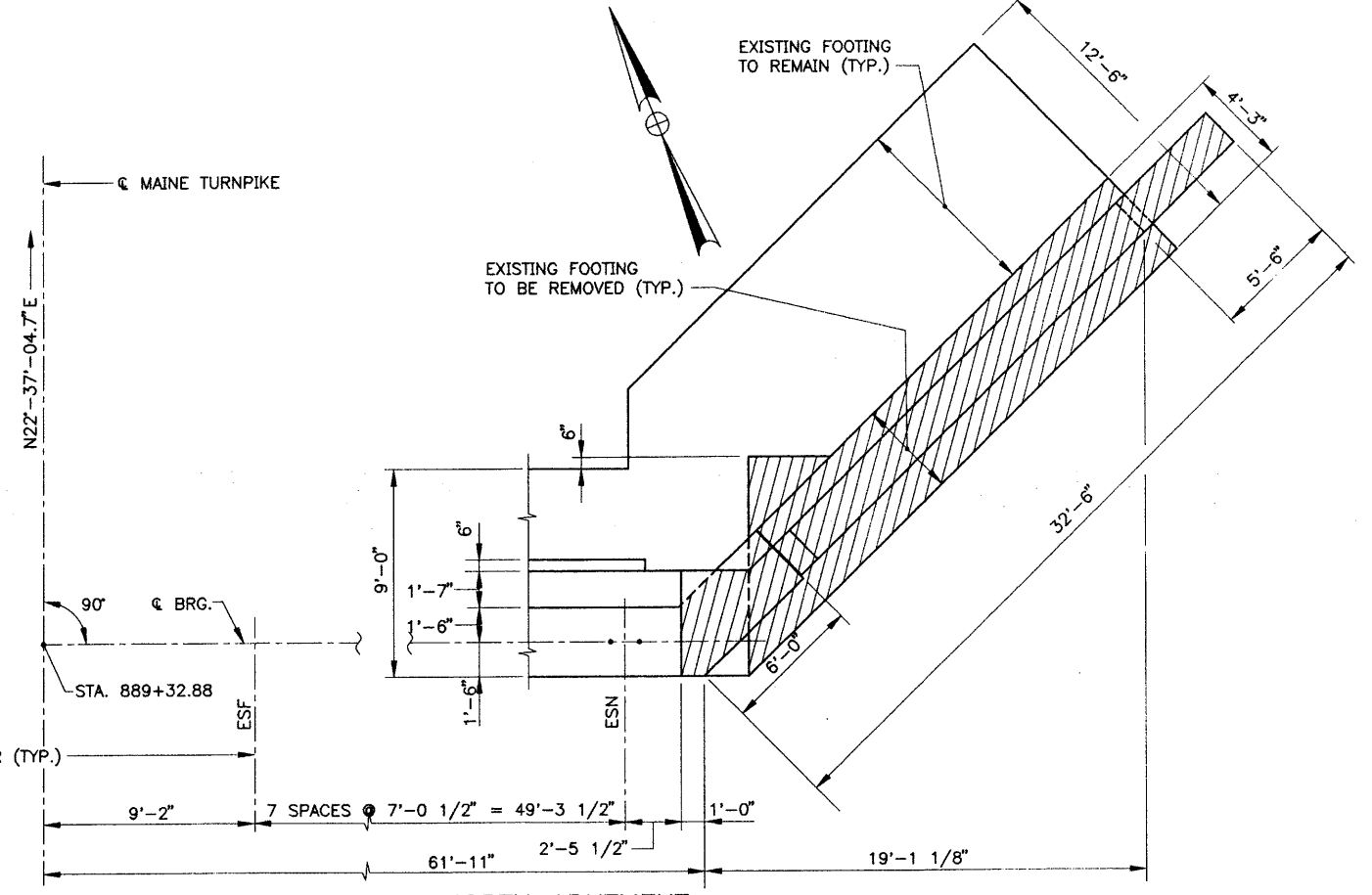
**MAINLINE BRIDGE WIDENING
 WELLS INTERCHANGE OVERPASS
 STAGE CONSTRUCTION**

SHEET NUMBER: WI-S3
 CONTRACT: 2000.03
 120 OF 178



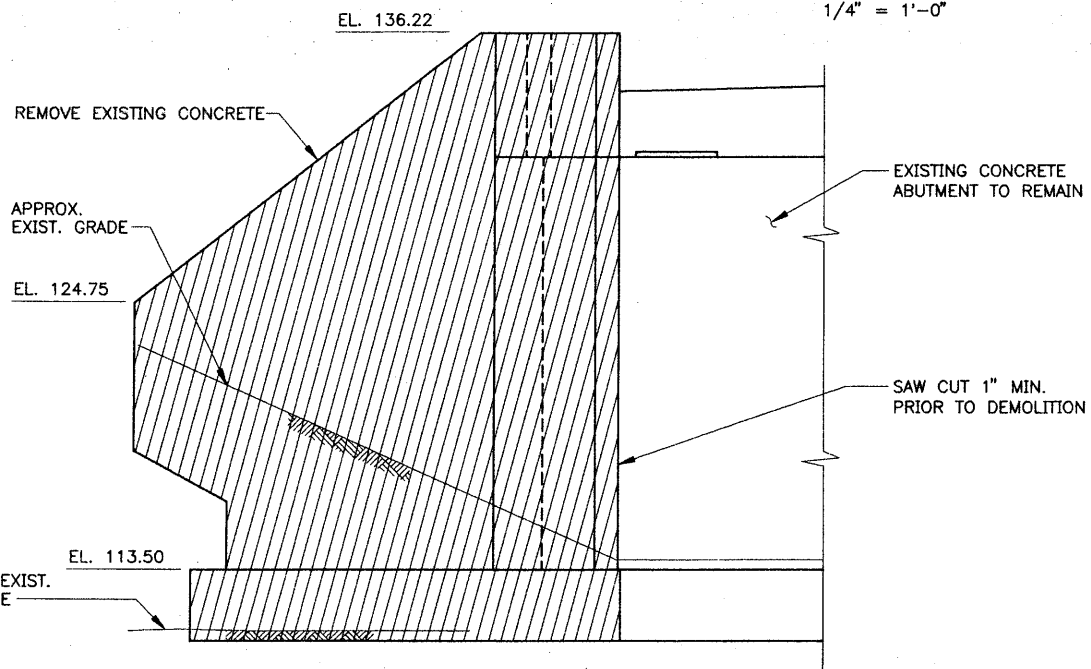
SOUTH ABUTMENT

1/4" = 1'-0"



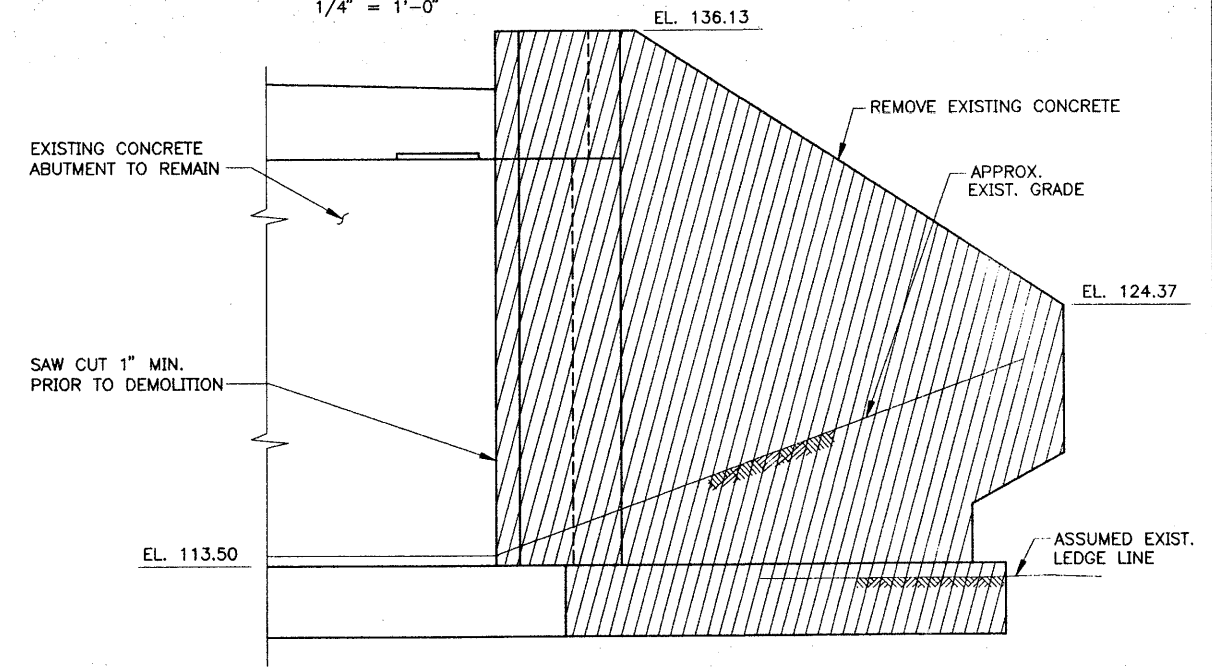
NORTH ABUTMENT

1/4" = 1'-0"



ELEVATION

1/4" = 1'-0"



ELEVATION

1/4" = 1'-0"

NOTE:

1. FOR ADDITIONAL DIMENSIONS AND DETAILS, SEE CONTRACT 93.7 DRAWINGS DATED JULY 1991.

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**MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT**



**MAINLINE BRIDGE WIDENING
WELLS INTERCHANGE OVERPASS
SUBSTRUCTURE DEMOLITION**

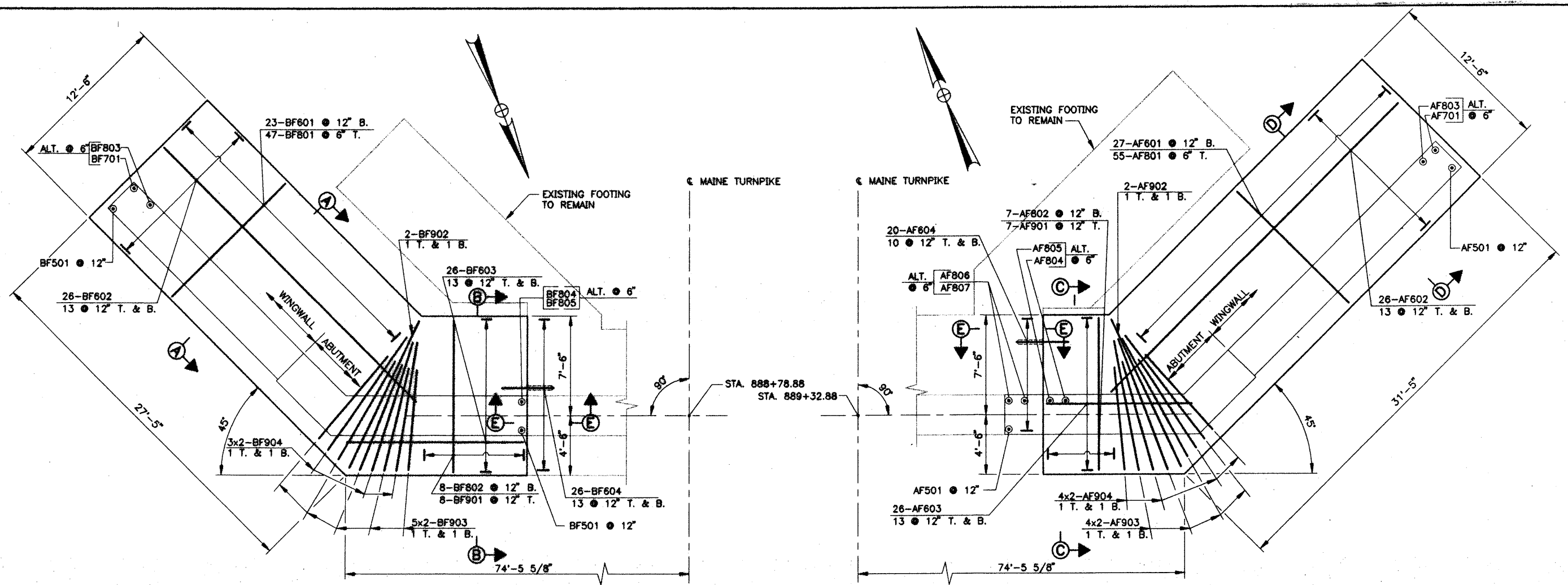
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| Designed | JFW | 10/99 | Checked | AAD | 11/99 |
| Drawn | LS | 10/99 | In Charge of | RAL | 11/99 |

SHEET NUMBER: WI-S4

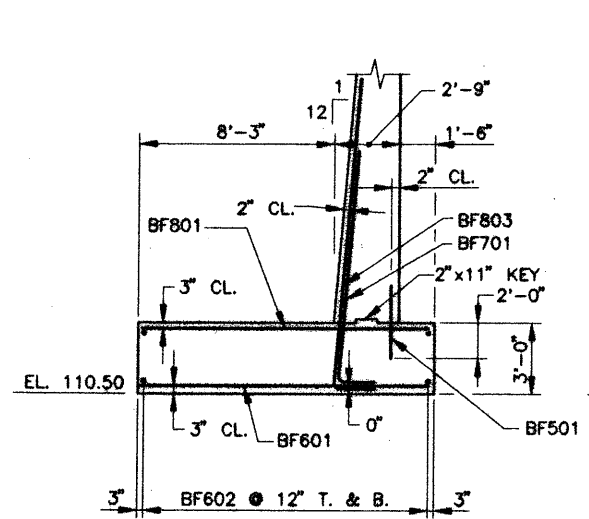
CONTRACT: 2000.03

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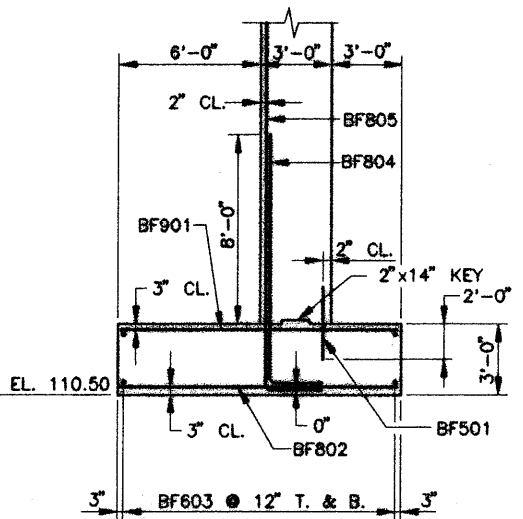


SOUTH ABUTMENT
1/4" = 1'-0"

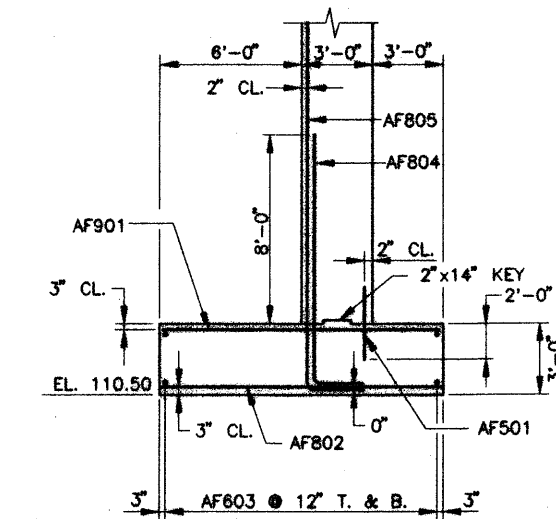
NORTH ABUTMENT
1/4" = 1'-0"



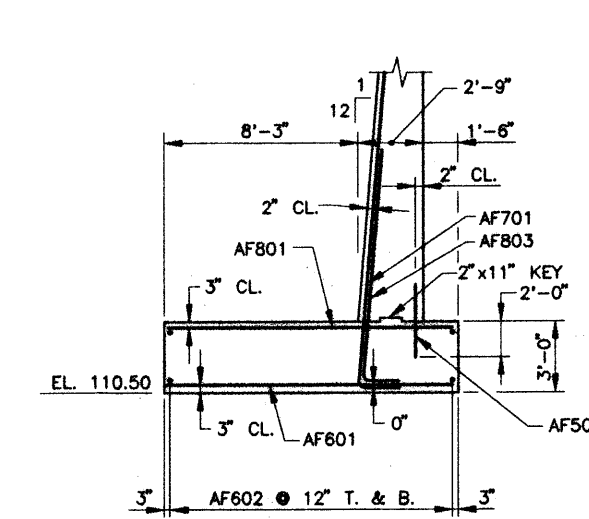
SECTION A-A
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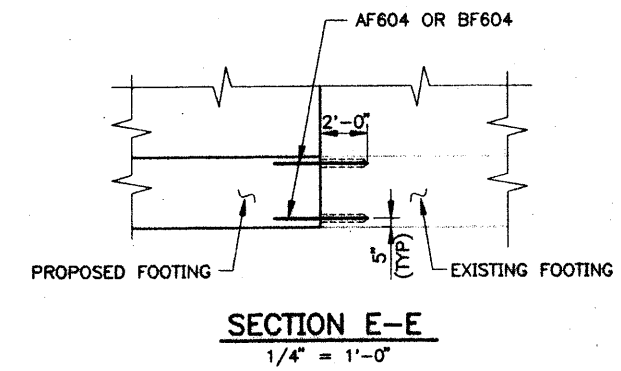
SECTION B-B
1/4" = 1'-0"



SECTION C-C
1/4" = 1'-0"



SECTION D-D
1/4" = 1'-0"



SECTION E-E
1/4" = 1'-0"

- NOTES:**
1. FOR SPACING OF DOWELS, SEE SHEETS WI-S7, WS-S9 AND WS-S11.
 2. FOR ADDITIONAL NOTES, SEE SHEET WS-S10.

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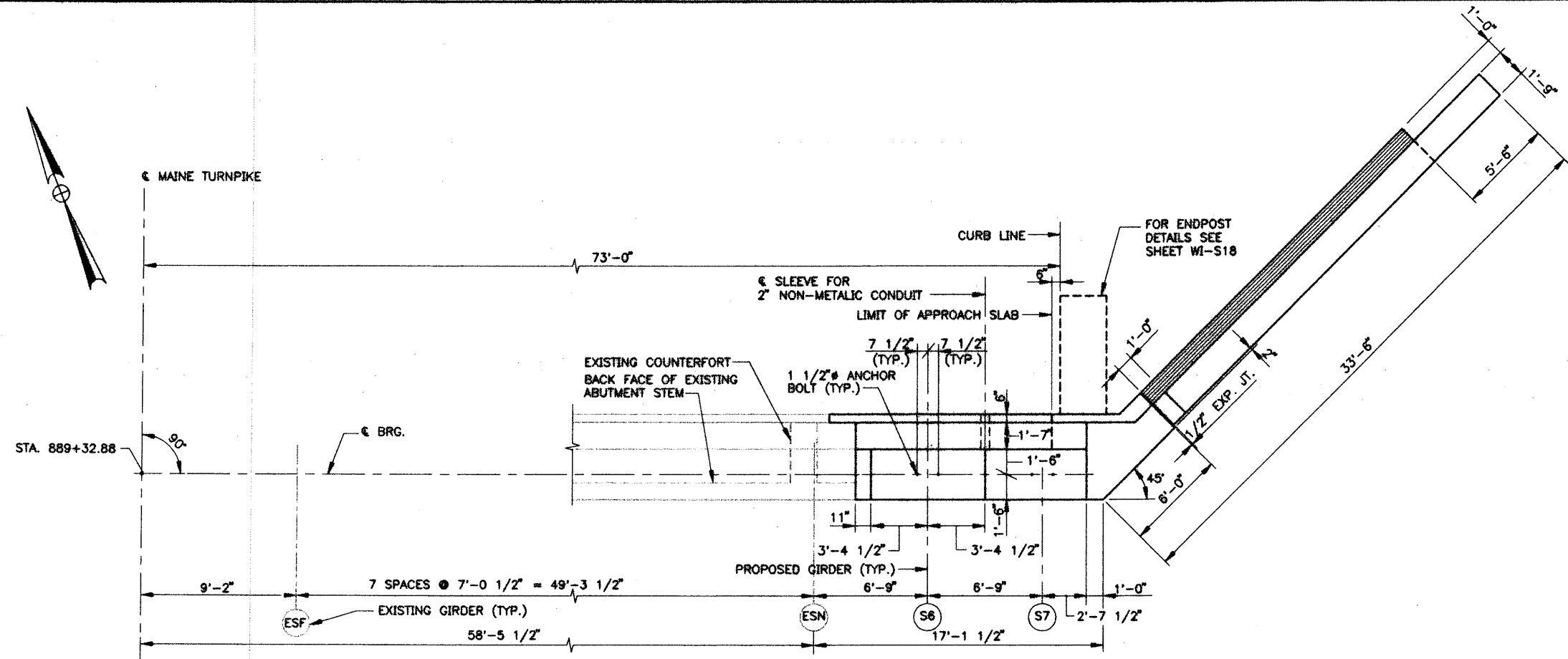
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| SJC | 11/99 | JFW | 11/99 |
| LS | 11/99 | RAL | 11/99 |

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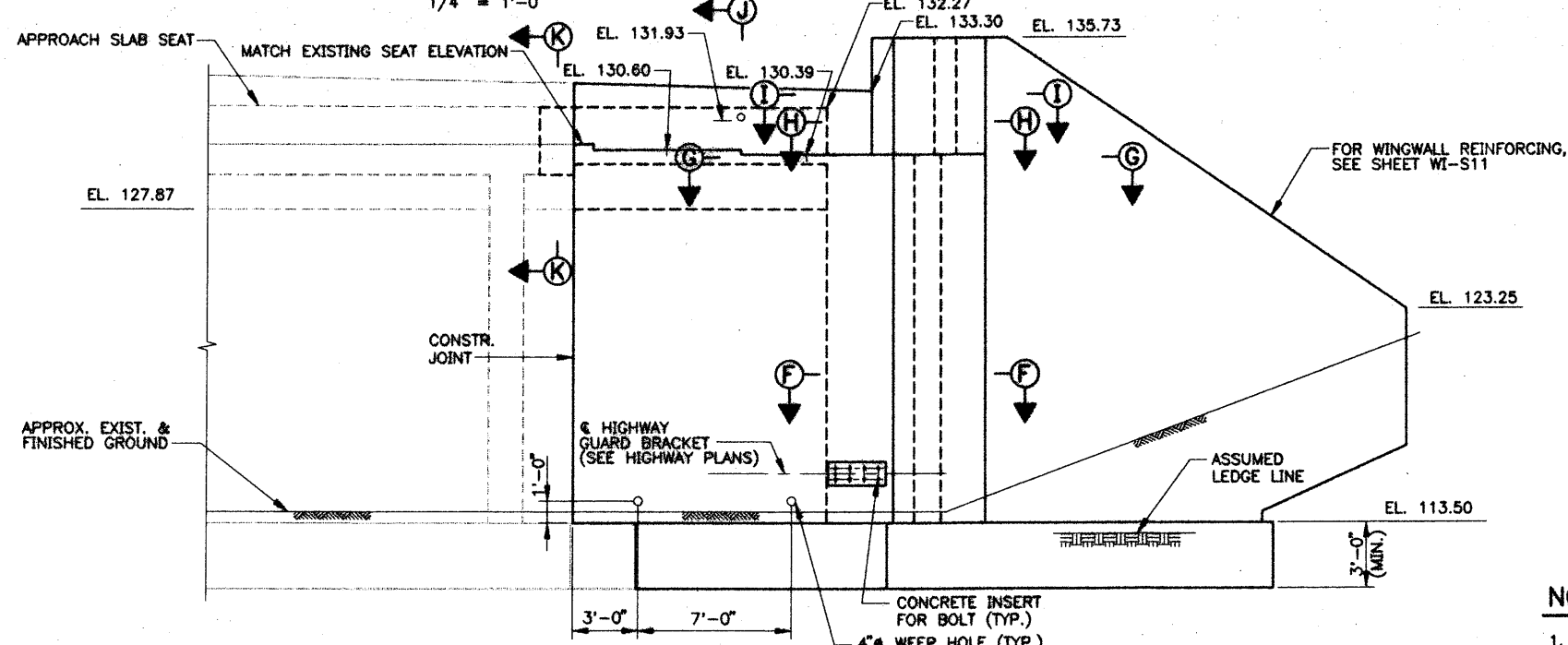
MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT

MAINLINE BRIDGE WIDENING
WELLS INTERCHANGE OVERPASS
FOUNDATION PLAN

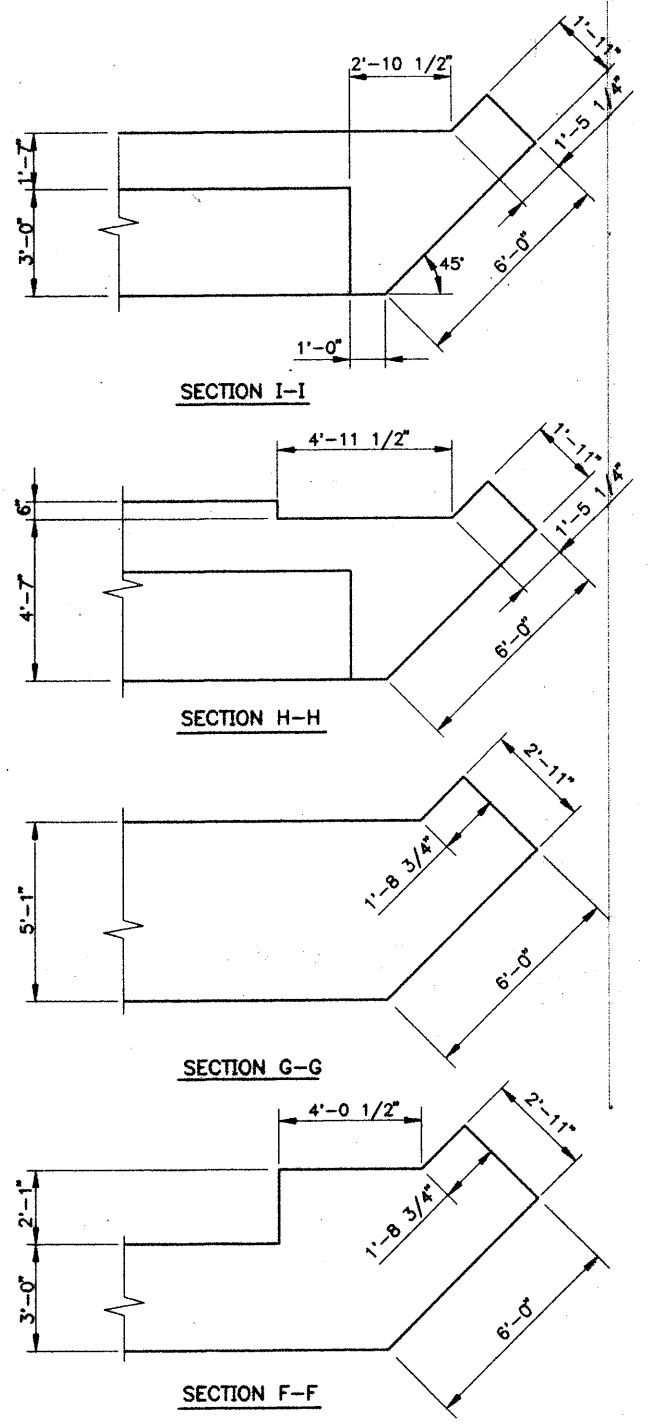
SHEET NUMBER: **WI-S5**
CONTRACT: **2000.03**
122 OF 178



NORTH ABUTMENT
1/4" = 1'-0"



ELEVATION
1/4" = 1'-0"



NORTHEAST CORNER
3/8" = 1'-0"

NOTES:
1. FOR ABUTMENT REINFORCING, SEE SHEETS WI-S7 AND WI-S10.

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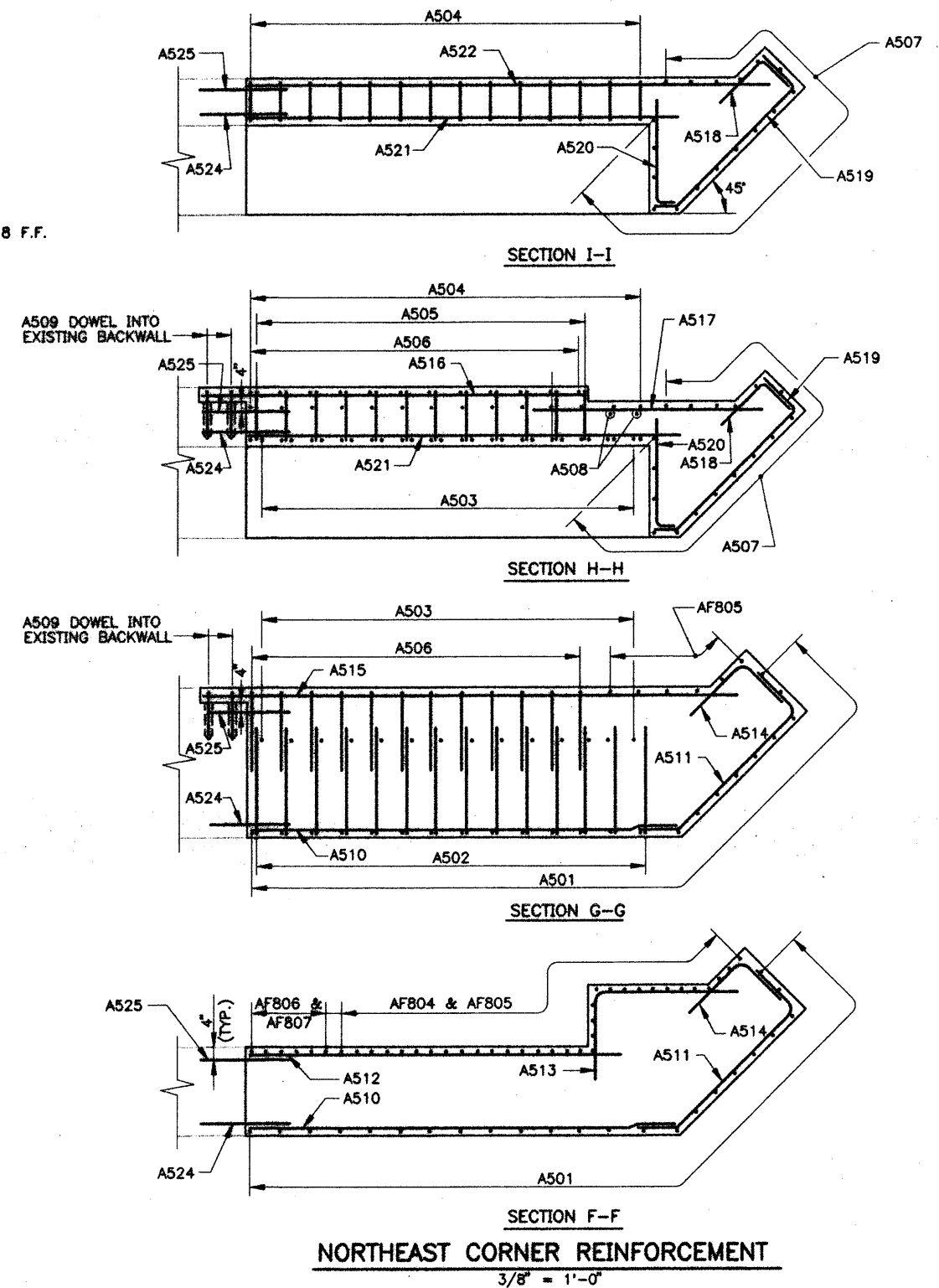
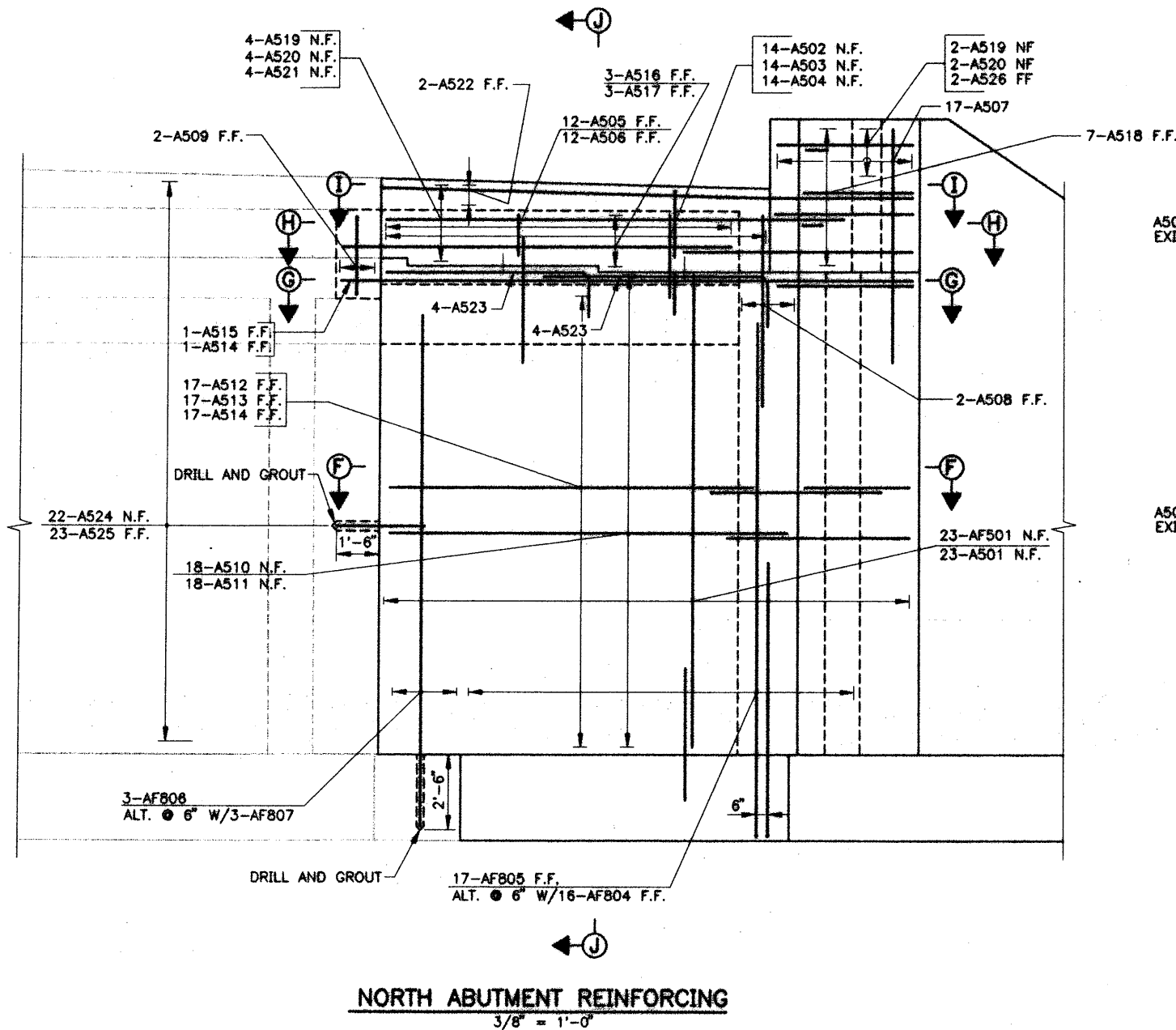
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| JFW | 10/99 | SJC | SJC | 11/99 |
| LS | 10/99 | RAL | RAL | 11/99 |

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**MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT**

**MAINLINE BRIDGE WIDENING
WELLS INTERCHANGE OVERPASS
NORTH ABUTMENT**

SHEET NUMBER: **WI-S6**
CONTRACT: 2000.03
123 OF 178



NOTES:

1. EPOXY COATED REINFORCING IS DESIGNATED IN THE REINFORCING SCHEDULES.
2. FOR ADDITIONAL DETAILS SEE SHEETS WI-S6 AND WI-S10.

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| Designed by: | | | | | |
| HNTB | | | | | |
| ARCHITECTS ENGINEERS PLANNERS | | | | | |
| By | Date | By | Date | By | Date |
| Designed | JFW 11/99 | Checked | SJC 11/99 | | |
| Drawn | LS 10199 | In Charge of | RAL 11/99 | | |

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**MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT**

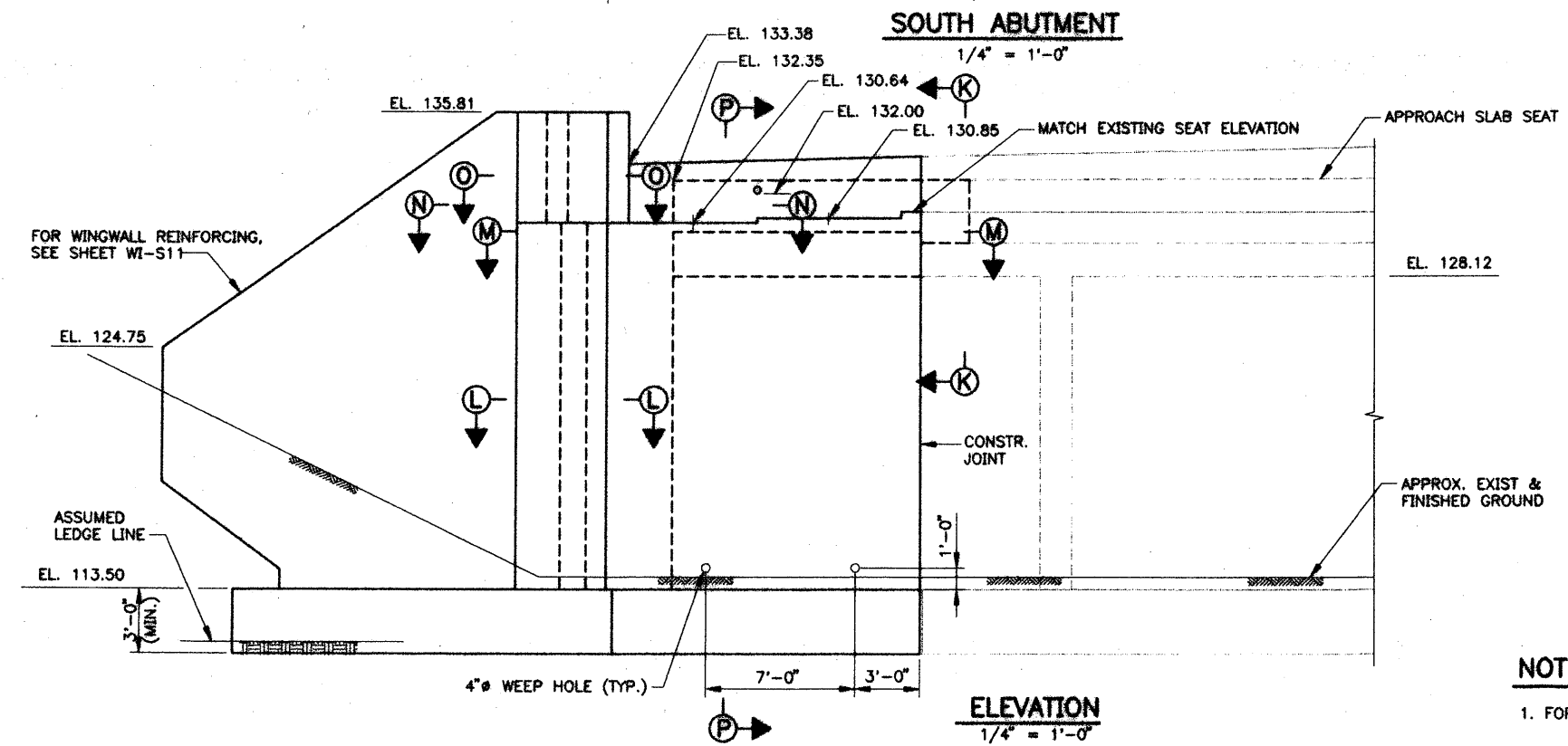
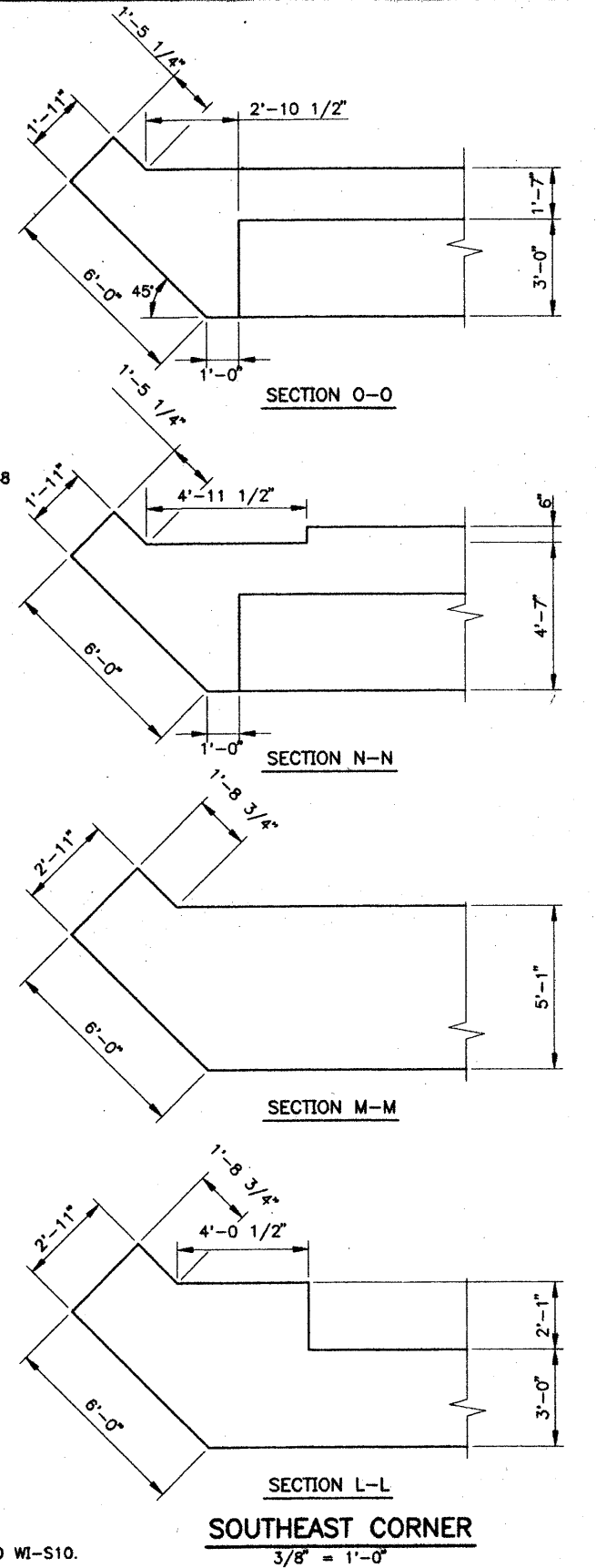
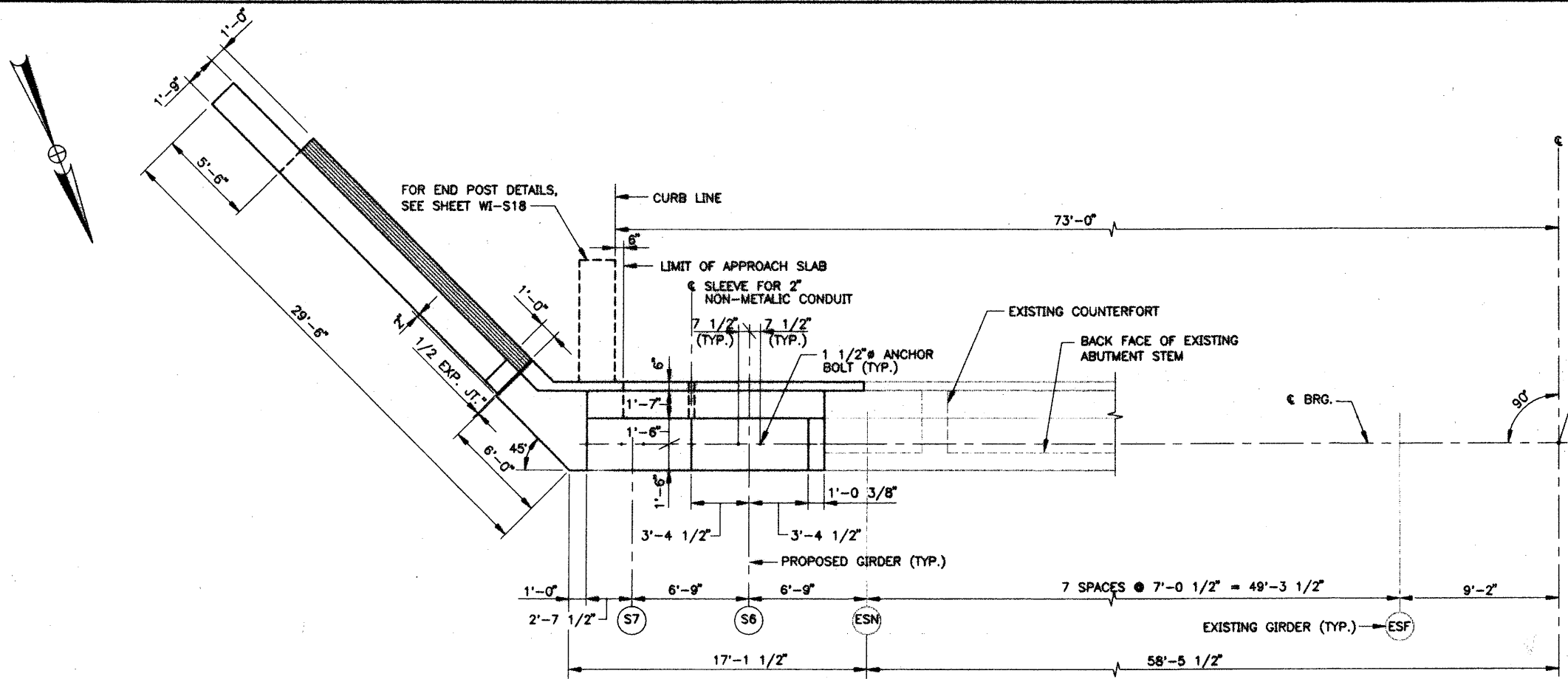


**MAINLINE BRIDGE WIDENING
WELLS INTERCHANGE OVERPASS
NORTH ABUTMENT REINFORCING**

CONTRACT: 2000.03

SHEET NUMBER: WI-S7

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NOTES:
 1. FOR ABUTMENT REINFORCING, SEE SHEETS WI-S9 AND WI-S10.

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| | By | Date | | By | Date |
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| Designed | SJC | 10/99 | Checked | JFW | 11/99 |
| Drawn | LS | 10/99 | In Charge of | RAL | 11/99 |

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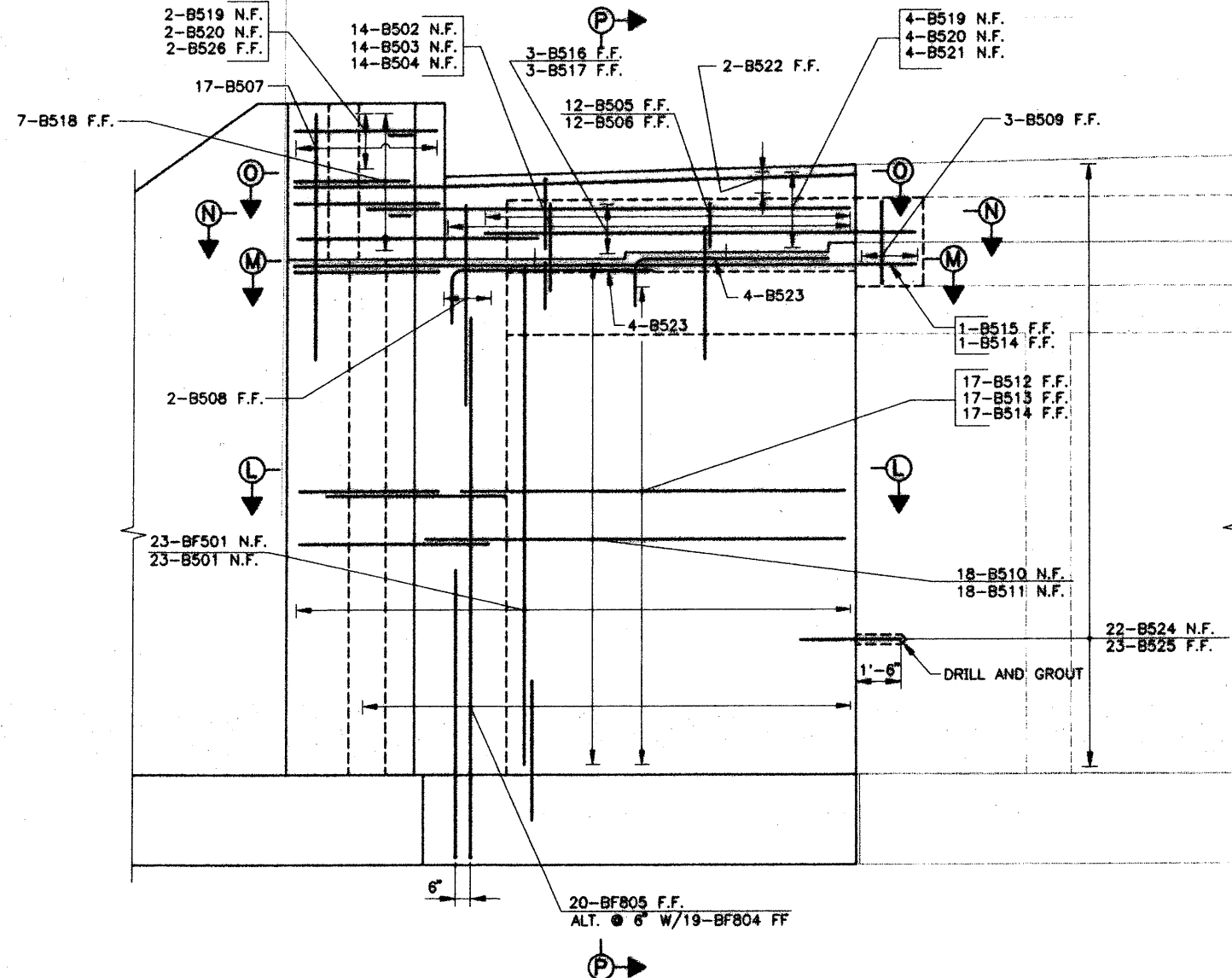
**MAINE TURNPIKE AUTHORITY
 MODERNIZATION AND WIDENING PROJECT**

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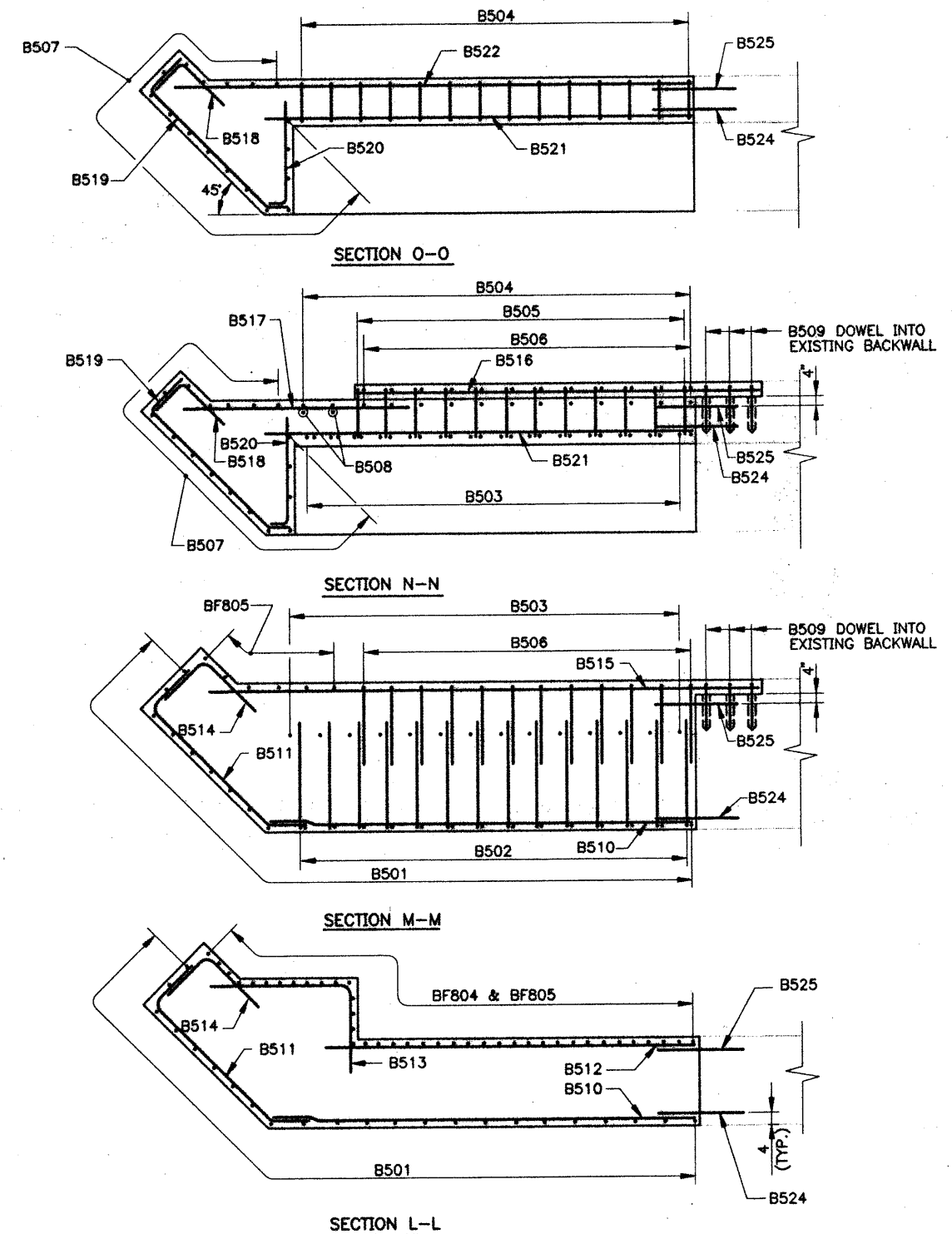
**MAINLINE BRIDGE WIDENING
 WELLS INTERCHANGE OVERPASS
 SOUTH ABUTMENT**

CONTRACT: 2000.03

SHEET NUMBER: WI-S8
 125 OF 178



SOUTH ABUTMENT REINFORCING
3/8" = 1'-0"



NORTHEAST CORNER REINFORCEMENT
3/8" = 1'-0"

- NOTES**
1. EPOXY COATED REINFORCING IS DESIGNATED IN THE REINFORCING SCHEDULES.
 2. FOR ADDITIONAL DETAILS SEE SHEETS WI-S8 AND WI-S10.

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| By | Date | Checked | By | Date |
|-----|-------|--------------|-----|-------|
| JFW | 11/99 | SJC | SJC | 11/99 |
| LS | 11/99 | In Charge of | RAL | 11/99 |

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**MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT**

**MAINLINE BRIDGE WIDENING
WELLS INTERCHANGE OVERPASS
SOUTH ABUTMENT REINFORCING**

SHEET NUMBER: **WI-S9**
CONTRACT: **2000.03**
126 OF 178

TROWEL TO SMOOTH FINISH & PLACE 2 LAYERS OF ROOFING FELT

3/8"x1" DEEP JOINT SAWED AND SEALED WITH BITUMINOUS CONCRETE JOINT SEALANT

8" APPROACH SLAB 15'-6" LONG. SEE STANDARD DETAIL SHEET 502 (4) AND CONTRACT SHEET WI-S12

FOR DECK REINFORCING, SEE SECTION B-B ON SHEET WI-S16

BITUMINOUS CONCRETE SURFACE MEMBRANE WATERPROOFING

BITUMINOUS CONCRETE SURFACE MEMBRANE WATERPROOFING

FOR DECK REINFORCING, SEE SECTION B-B ON SHEET WI-S16

TROWEL TO SMOOTH FINISH & PLACE 2 LAYERS OF ROOFING FELT

3/8"x1" DEEP JOINT SAWED AND SEALED WITH BITUMINOUS CONCRETE JOINT SEALANT

8" APPROACH SLAB 15'-6" LONG. SEE STANDARD DETAIL SHEET 502 (4) AND CONTRACT SHEET WI-S12

ABUTMENT NOTES

1. N.F. DENOTES NEAR FACE, F.F. DENOTES FAR FACE, E.F. DENOTES EACH FACE.
2. REINFORCING STEEL SHALL HAVE 2" MINIMUM COVER UNLESS OTHERWISE NOTED.
3. PLACE REINFORCING STEEL TO CLEAR ANCHOR BOLTS.
4. EXISTING REINFORCING STEEL THAT IS EXPOSED AFTER REMOVAL OF EXISTING CONCRETE SHALL NOT BE CUT, EXCEPT AS NOTED, BUT SHALL BE CLEANED AND EXTENDED INTO NEW CONCRETE AS FAR AS PRACTICAL. PAYMENT FOR CLEANING SHALL BE INCIDENTAL TO ITEM 202.12.
5. ANY EXISTING REINFORCING STEEL, INTENDED FOR RE-USE IN THE PROPOSED CONSTRUCTION, THAT IS DAMAGED OR BROKEN DURING THE REMOVAL OF EXISTING CONCRETE SHALL BE REPLACED IN DRILLED HOLES AND SECURED BY GROUTING. PAYMENT TO BE INCIDENTAL TO ITEM 202.12.
6. PAYMENT FOR DRILLING HOLES AND GROUTING DOWELS AND ANCHOR BOLTS TO BE INCIDENTAL TO ITEM 503.13 AND 503.15. ALL DOWELS TO BE GROUTED WITH NON-SHRINK GROUT.
7. CHAMFER ALL EXPOSED EDGES OF CONCRETE 3/4".
8. STEPS IN BRIDGE SEAT TO BE SPACED SO THEY OCCUR APPROXIMATELY MID-WAY BETWEEN STRINGERS.
9. DRESS BEARING PAD AREAS 1" LARGER ALL AROUND THE MASONRY PLATES TO EXACT ELEVATIONS SHOWN.
10. FOR REINFORCING SCHEDULES, SEE SHEETS WI-S20 AND WI-S21.
11. COVER HORIZONTAL & VERTICAL CONSTRUCTION JOINTS ON THE BACK WITH TWO LAYERS OF HEAVY ROOFING FELT. SEE MDOT STANDARD DETAIL SHEET 502 (1).
12. WATERSTOPS ARE NOT REQUIRED IN HORIZONTAL CONSTRUCTION JOINTS IN ABUTMENT BACKWALLS.
13. REMOVAL LIMITS OF EXISTING CONCRETE THAT DO NOT FALL AT AN EXISTING CONSTRUCTION JOINT AND THAT WILL BE EXPOSED IN THE COMPLETED STRUCTURE SHALL BE CUT ALONG THESE LIMITS WITH A MASONRY SAW TO A MINIMUM DEPTH OF 1" FROM THE SURFACE. THE CONCRETE SHALL THAN BE REMOVED IN A MANNER THAT WILL LEAVE THE SAW CUT EDGES UNDAMAGED.
14. CLEAR PROTECTIVE COATING FOR CONCRETE SURFACES SHALL BE APPLIED TO THE TOP OF ABUTMENT BACKWALLS AND ONE (1) FOOT BELOW TOP OF BACKWALL ON THE BACKFACE, AS WELL AS ALL EXPOSED NEW ABUTMENT CONCRETE.
15. FOR DETAILS OF CONSTRUCTION AND CONTRACTION JOINTS. SEE MDOT STANDARD DETAIL SHEET 502 (1).
16. FOR BEARING PEDESTALS AND ANCHOR BOLT DETAILS, SEE SHEET BD 101-89 (CONTRACT 93.7). BEARING PEDESTALS SHALL BE PLACED NORMAL TO STRINGERS.
17. THE EXISTING BACKWALL SURFACE, AT THE INTERFACE OF THE BACKWALL AND BRIDGE SEAT, SHALL BE CLEAN AND FREE OF BOND INHIBITING MATERIALS. THE SURFACE SHALL BE ROUGHENED AND THEN A BONDING AGENT, APPROVED BY THE ENGINEER, SHALL BE APPLIED PRIOR TO THE NEW CONCRETE ABUTMENT PLACEMENT. PAYMENT TO BE INCIDENTAL TO ITEM 502.21.
18. REMOVAL OF THE EXISTING ARMOR JOINT AT THE TOP OF THE SOUTH ABUTMENT BACKWALL SHALL BE INCIDENTAL TO ITEM 202.12.
19. DIMENSIONS AND ELEVATIONS SHOWN ARE TAKEN FROM PREVIOUS CONTRACT DRAWINGS AND ARE NOT GUARANTEED TO BE CORRECT.

EL. *

2" CL. (TYP.)

VARIES

1'-0"

2x8 KEY

1'-6" MIN.

1'-6" OR B511

EL. *

B512, B513 OR B514

* FOR ELEVATIONS, SEE SHEET WI-S8.

1'-6"

1'-7"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

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1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

DETAIL A
3/4" = 1'-0"

SEE DETAIL A FOR ADDITIONAL REINFORCING

B510, B511 N.F.
B512, B513, B514, B515 F.F.

A510, A511 N.F.
A512, A513, A514, A515 F.F.

SECTION P-P
(SOUTH ABUTMENT)
1/4" = 1'-0"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

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1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

1'-6"

SECTION J-J
(NORTH ABUTMENT)
1/4" = 1'-0"

SEE DETAIL B FOR ADDITIONAL REINFORCING

DETAIL B
3/4" = 1'-0"

EXISTING REINFORCING STEEL

PROPOSED 8" APPROACH SLAB

A509 OR B509

A516 OR B516

2" CL. (TYP.)

A515 OR B515

DRILL AND GROUT INTO EXISTING (TYP.)

EXISTING ABUTMENT

SECTION K-K
3/4" = 1'-0"

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MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT



MAINLINE BRIDGE WIDENING
WELLS INTERCHANGE OVERPASS
ABUTMENT DETAILS

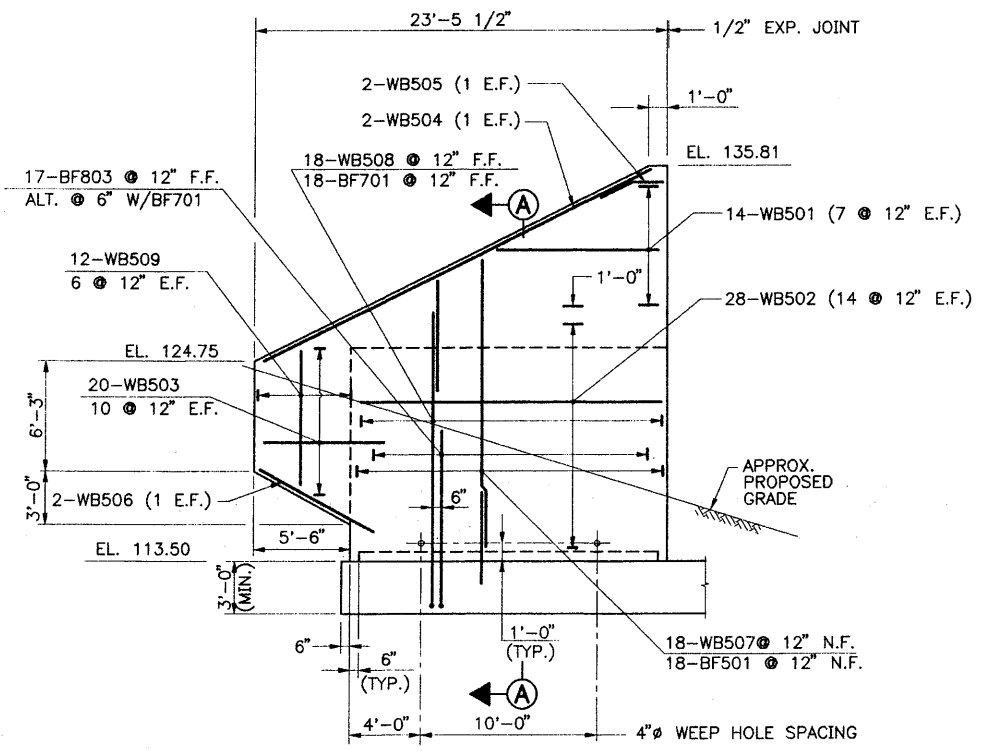
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|----------|-----|-------|--------------|-----|-------|
| | JFW | 11/99 | | SJC | 11/99 |
| Drawn | LS | 11/99 | In Charge of | RAL | 11/99 |

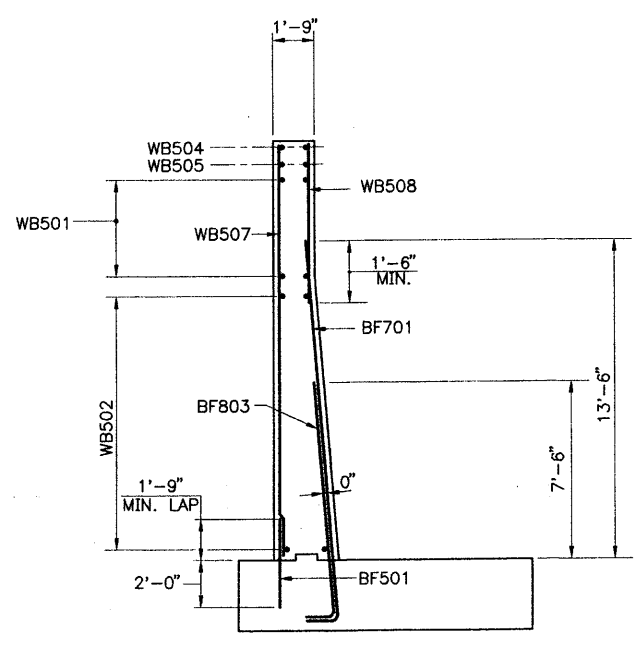
CONTRACT: 2000.03

SHEET NUMBER: WI-S10

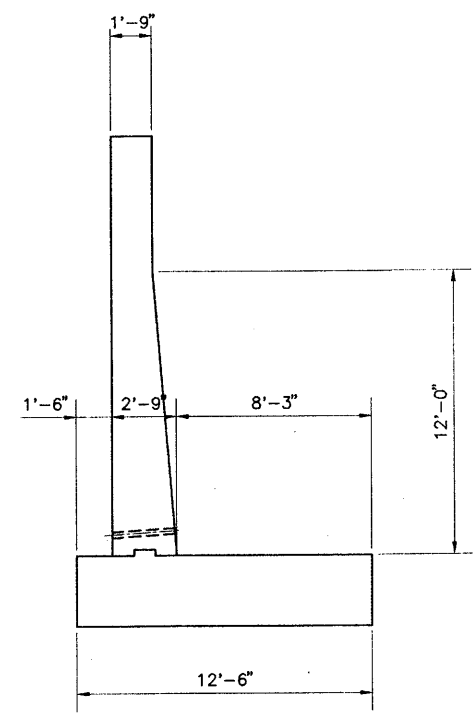
127 OF 178



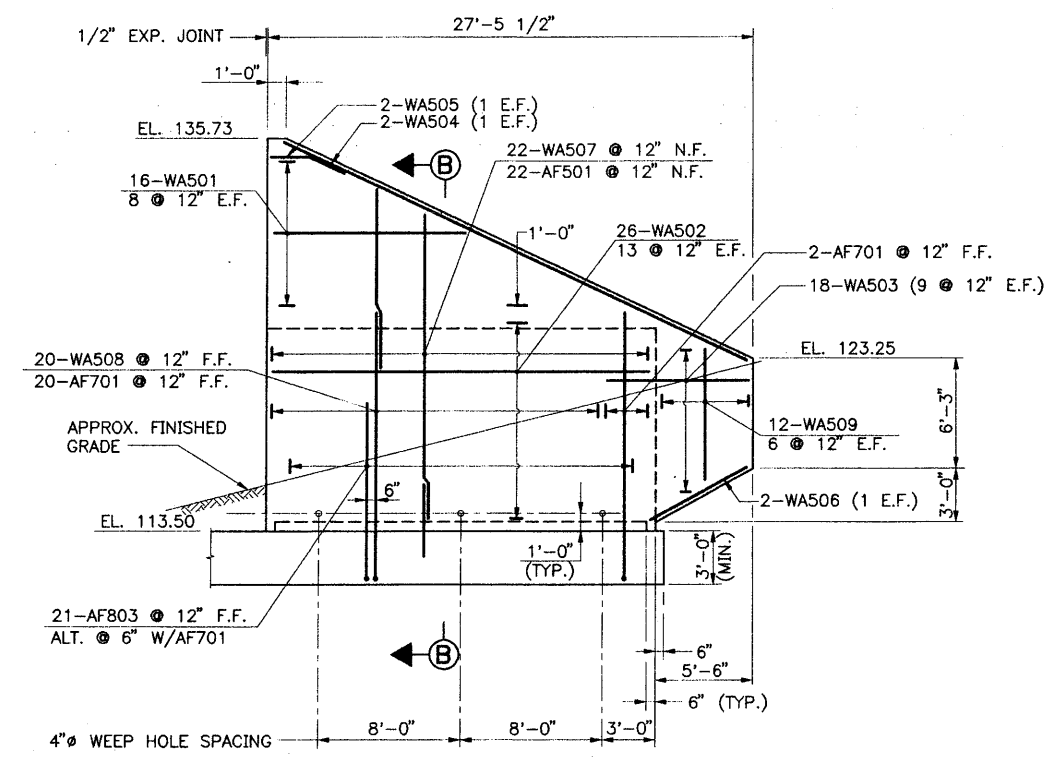
ELEVATION OF S.E. WINGWALL
3/16" = 1'-0"



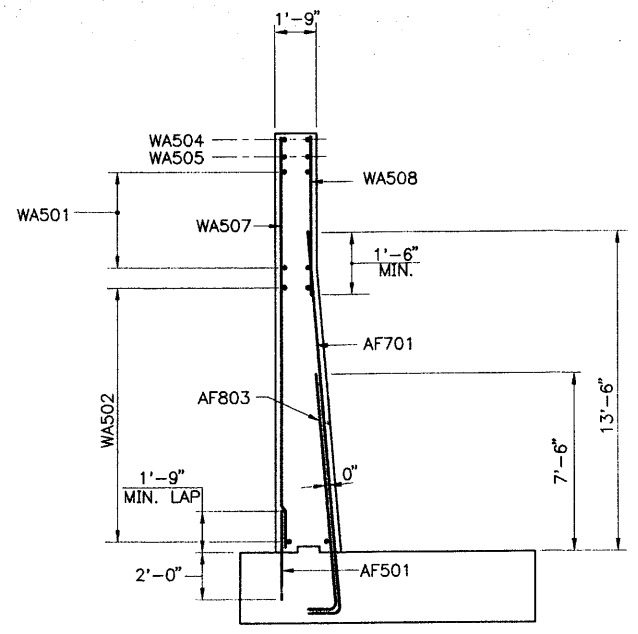
SECTION A-A
1/4" = 1'-0"



TYPICAL WINGWALL SECTION
1/4" = 1'-0"



ELEVATION OF N.E. WINGWALL
3/16" = 1'-0"



SECTION B-B
1/4" = 1'-0"

NOTES:

1. FOR ADDITIONAL DETAILS, SEE SHEET WS-S12.

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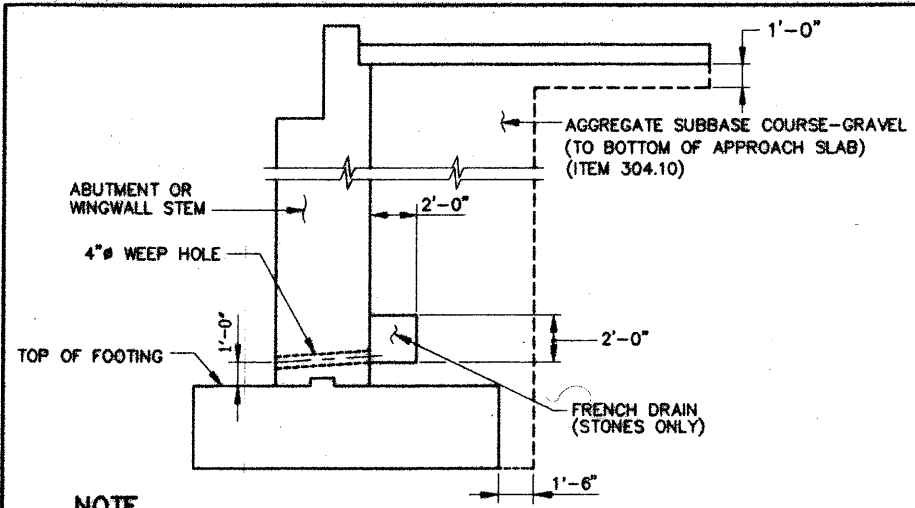
| By | Date | Checked | By | Date |
|-----|-------|---------|-----|-------|
| SJC | 11/99 | JFW | JFW | 11/99 |
| LS | 11/99 | RAL | RAL | 11/99 |

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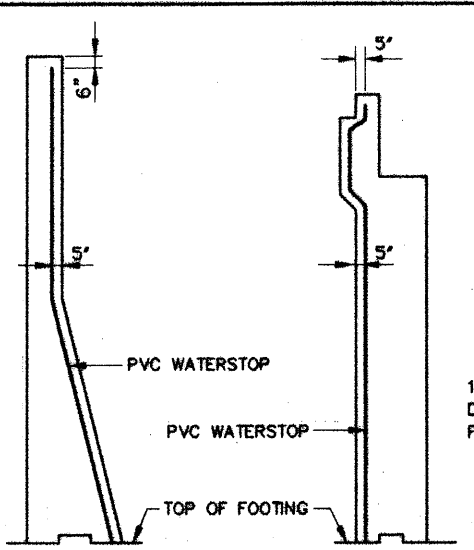
**MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT**

**MAINLINE BRIDGE WIDENING
WELLS INTERCHANGE OVERPASS
WINGWALL DETAILS**

SHEET NUMBER: **WI-S11**
CONTRACT: 2000.03
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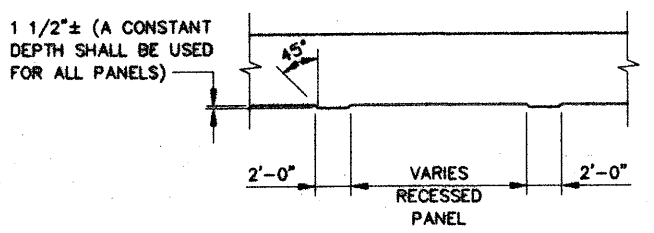


NOTE
THE FRENCH DRAIN SHALL EXTEND THE ENTIRE LENGTH OF THE PROPOSED STRUCTURE, NOT INCLUDING THE FLYING WINGWALL. A POROUS GEOTEXTILE WICK MATERIAL SHALL BE INSTALLED OVER THE WEEP HOLES ALONG THE BACKFACE OF THE ABUTMENT OR WINGWALL STEM PRIOR TO CONSTRUCTING THE FRENCH DRAIN. A NON-WOVEN GEOTEXTILE FABRIC SHALL BE PLACED AROUND THE FRENCH DRAIN STONES, SEPARATING THEM FROM THE AGGREGATE SUBBASE COURSE-GRAVEL.

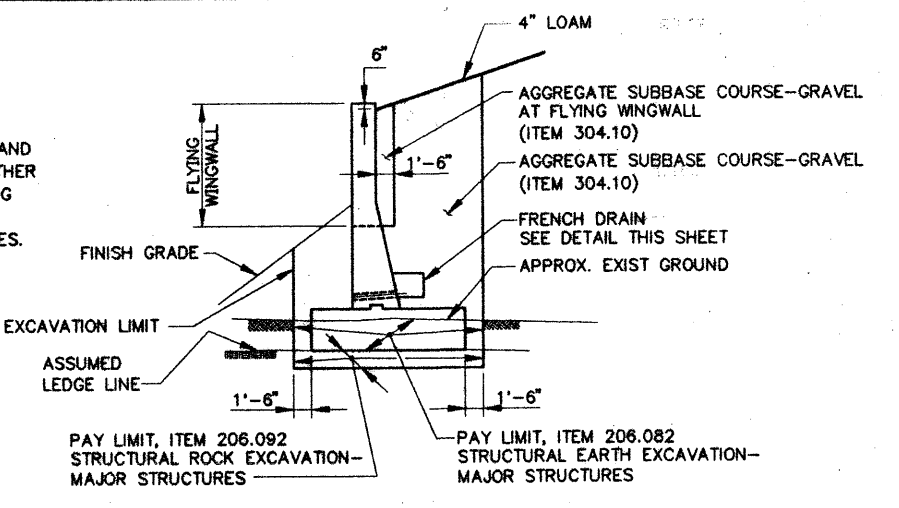


LIMITS OF PVC WATERSTOP
NO SCALE

NOTE FOR RECESSED PANEL
RECESSED PANEL FORMS SHALL BE SUFFICIENTLY RIGID, SECURELY BRACED, STRUTTED AND TIED TO PREVENT MOTION AND DISTORTION DUE TO THE PRESSURE OF THE CONCRETE AND OTHER LOADS INCIDENT TO THE CONSTRUCTION OPERATIONS, INCLUDING VIBRATION. AND SHALL BE SO CONSTRUCTED AS TO PRODUCE MORTAR-TIGHT JOINTS AND SMOOTH, EVEN CONCRETE SURFACES.

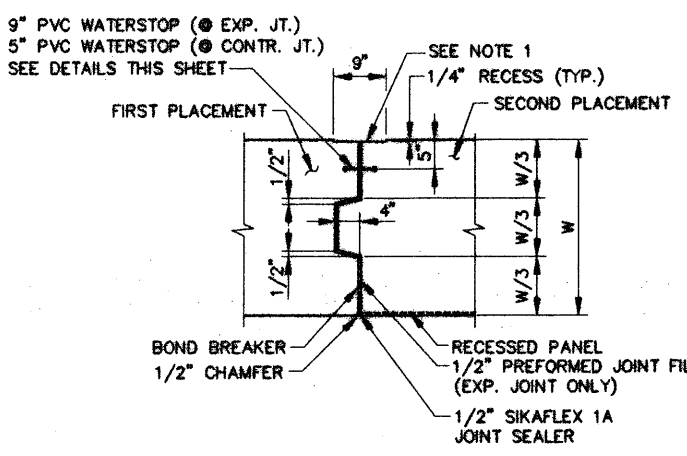
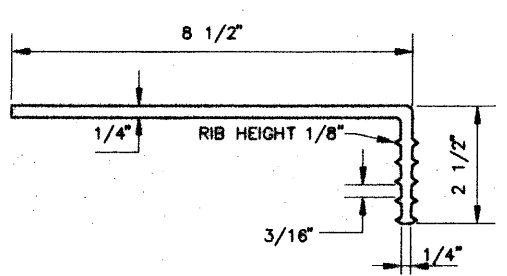


RECESSED PANEL DETAIL
3/16" = 1'-0"



PAY LIMITS - WINGWALL
NO SCALE

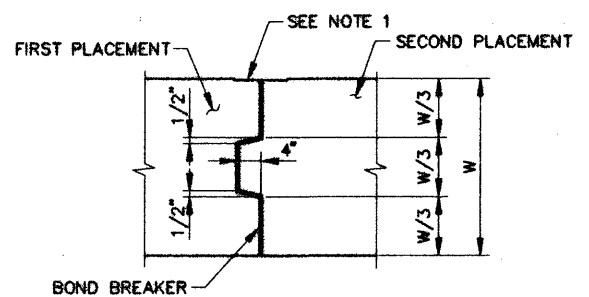
FRENCH DRAIN DETAIL
NO SCALE



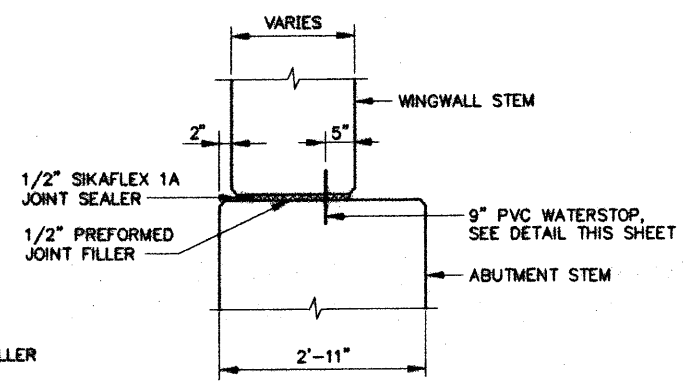
ABUTMENT CONTRACTION & EXPANSION JOINTS

NOTE: SIKAFLEX 1A AND PREFORMED JOINT FILLER ARE INCIDENTAL TO ITEM 502.21.

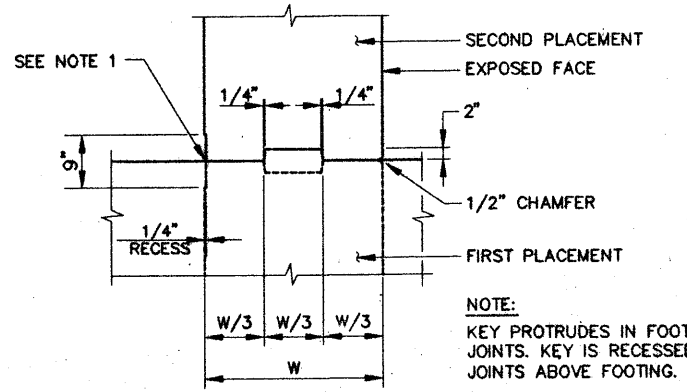
VERTICAL JOINT DETAILS - WALLS
NO SCALE



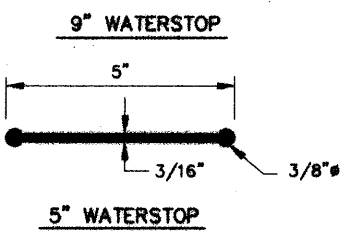
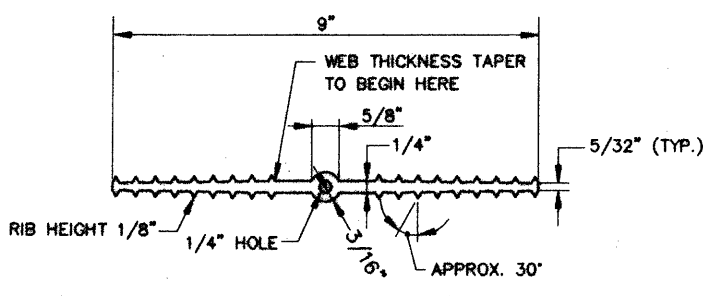
CONSTRUCTION JOINT DETAILS - FOOTINGS
NO SCALE



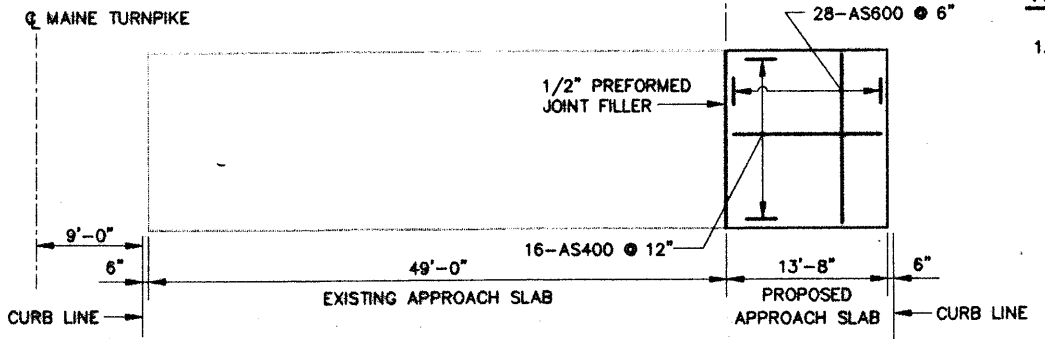
WINGWALL EXPANSION JOINT



HORIZONTAL CONSTRUCTION JOINT
NO SCALE



WATERSTOP DETAILS
NO SCALE



NORTHBOUND APPROACH SLABS
(2 REQUIRED)
1/8" = 1'-0"

NOTES:
1. APPLY TWO LAYERS OF HEAVY ROOFING FELT. COAT THE CONCRETE AND EACH LAYER WITH PLASTIC ROOFING CEMENT. (TYPICAL AT HORIZONTAL AND VERTICAL CONTRACTION, CONSTRUCTION AND EXPANSION JOINTS.)

NOTE:
FOR APPROACH SLAB DETAILS NOT SHOWN, SEE STANDARD DETAIL SHEET 502 (4).

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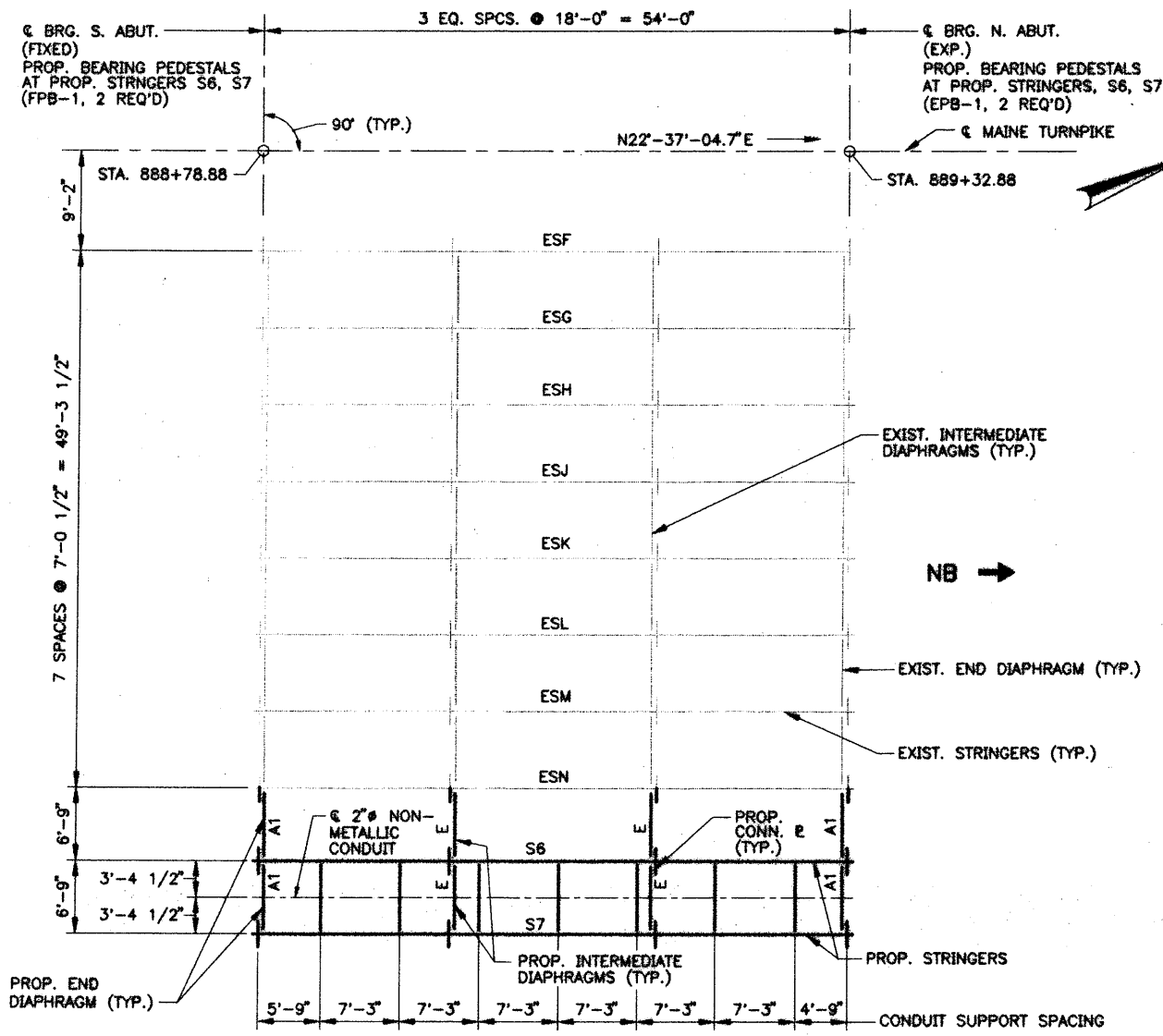
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| Designed | By | Date | Checked | By | Date |
| Drawn | AAD | 11/99 | JFW | JFW | 11/99 |
| | LS | 11/99 | | RAL | 11/99 |

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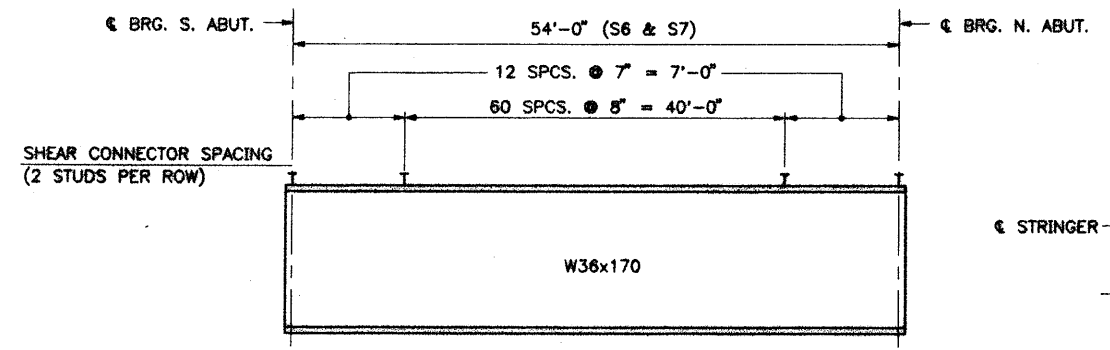
MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT

MAINLINE BRIDGE WIDENING
WELLS INTERCHANGE OVERPASS
MISCELLANEOUS DETAILS

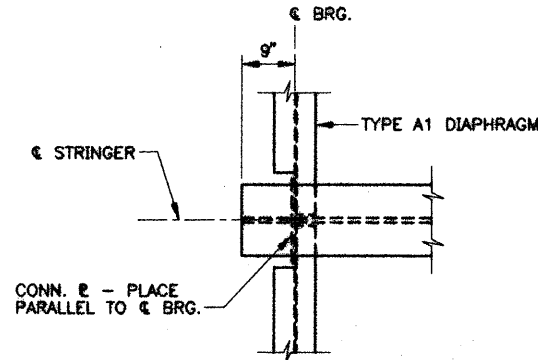
SHEET NUMBER: **WI-S12**
CONTRACT: **2000.03**
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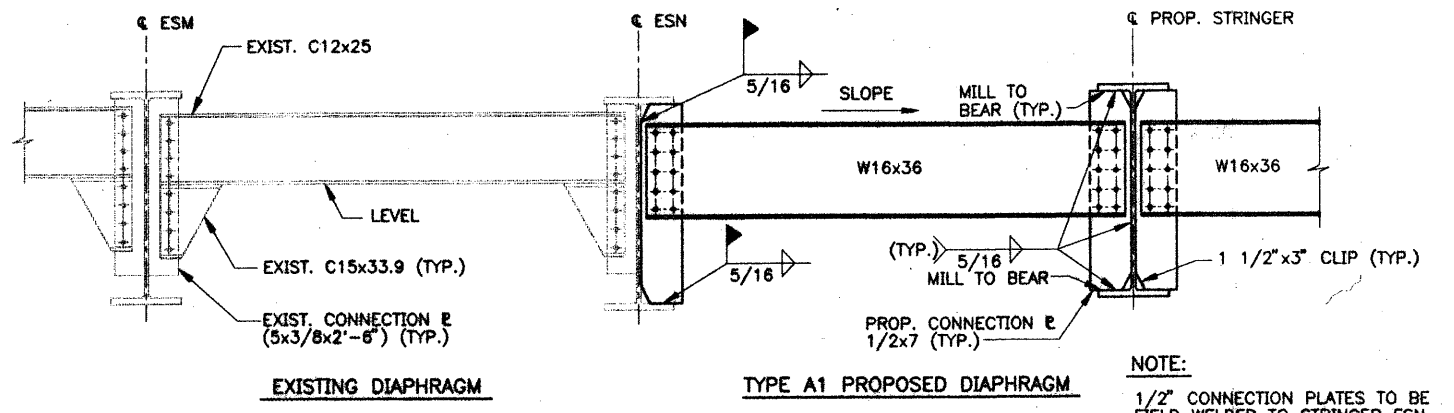
FRAMING PLAN
1/8" = 1'-0"



TYPICAL PROPOSED STRINGER ELEVATION
NO SCALE

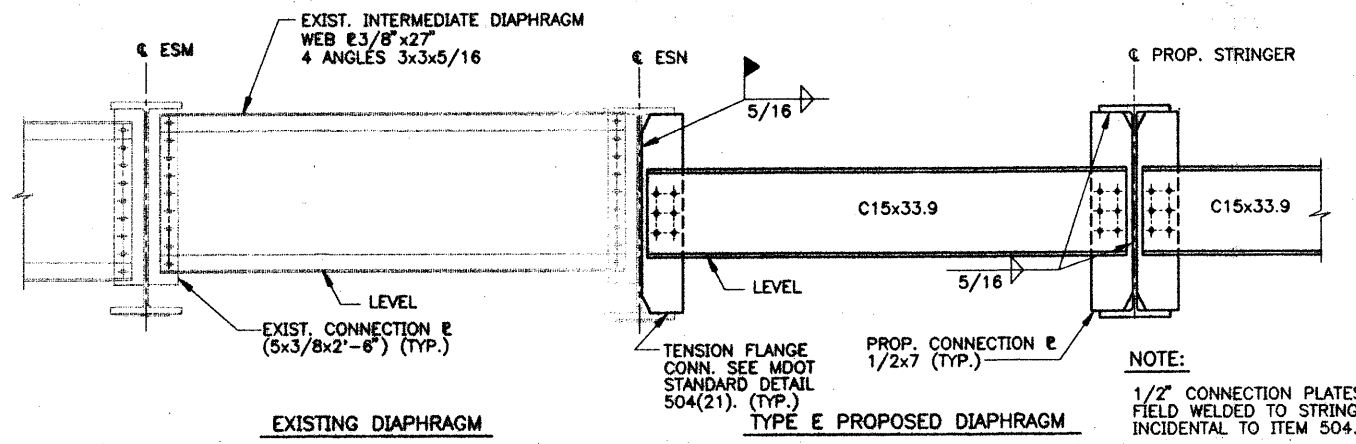


END OF PROPOSED BEAM DETAIL
3/4" = 1'-0"

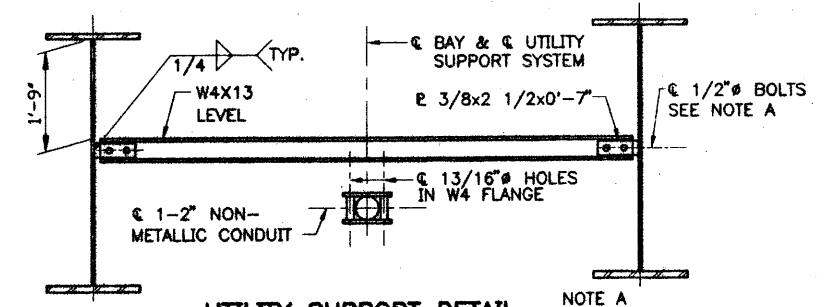


TYPICAL END DIAPHRAGM DETAILS
3/4" = 1'-0"

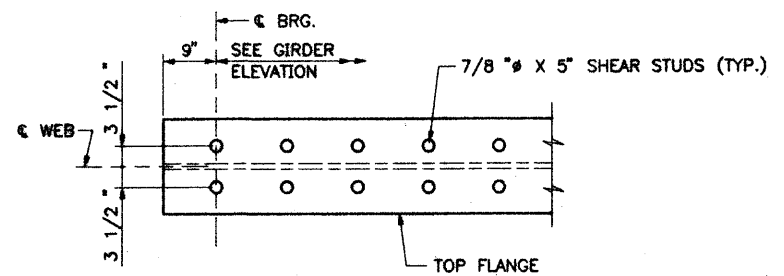
NOTE:
1/2" CONNECTION PLATES TO BE FIELD WELDED TO STRINGER ESN. INCIDENTAL TO ITEM 504.714.



NOTE:
1/2" CONNECTION PLATES TO BE FIELD WELDED TO STRINGER ESN. INCIDENTAL TO ITEM 504.714.



NOTE A
PROVIDE 9/16"x1" VERTICAL SLOTTED HOLES IN W4



SHEAR STUD CONNECTION
3/4" = 1'-0"

- NOTES:**
- FOR ADDITIONAL DIAPHRAGM DETAILS, SEE MDOT SHEETS 504(15-22).
 - FOR STRUCTURAL STEEL NOTES, SEE SHEET WI-S14.
 - FOR CONDUIT SUPPORT NOTES, SEE SHEET WI-S14.

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| By | Date | By | Date |
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| Designed AAD | 10/99 | Checked JFW | 11/99 |
| Drawn LS | 10/99 | In Charge of RAL | 11/99 |

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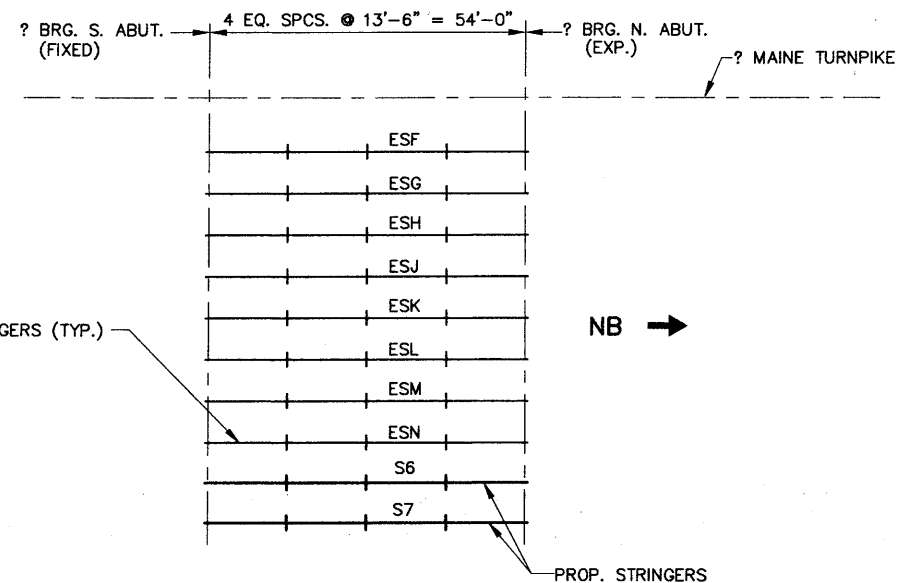
**MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT**

**MAINLINE BRIDGE WIDENING
WELLS INTERCHANGE OVERPASS
FRAMING PLAN AND DETAILS**

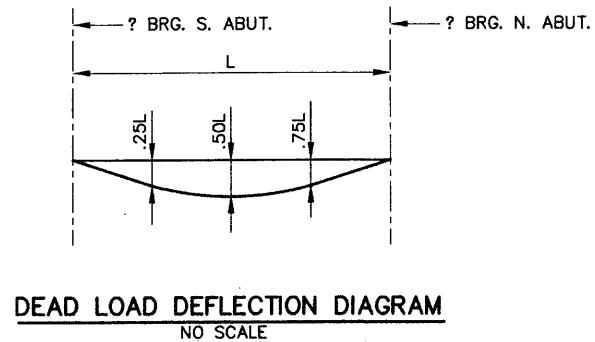
SHEET NUMBER: **WI-S13**

CONTRACT: 2000.03

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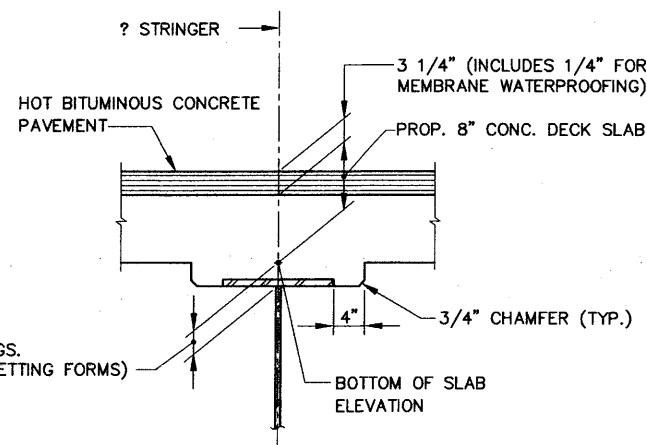


BLOCKING POINT LOCATION PLAN
1/16" = 1'-0"



DEAD LOAD DEFLECTION DIAGRAM
NO SCALE

| DEAD LOAD DEFLECTIONS (INCHES) | | | | | | |
|--------------------------------|-----------|------|-------|-------|-------|-------|
| STRINGER NO. | TYPE | .00L | .25 L | .50 L | .75 L | 1.00L |
| S6 & S7 | STEEL | 0 | 1/16 | 1/8 | 1/16 | 0 |
| | ALL OTHER | 0 | 7/16 | 9/16 | 7/16 | 0 |
| | TOTAL | 0 | 1/2 | 11/16 | 1/2 | 0 |

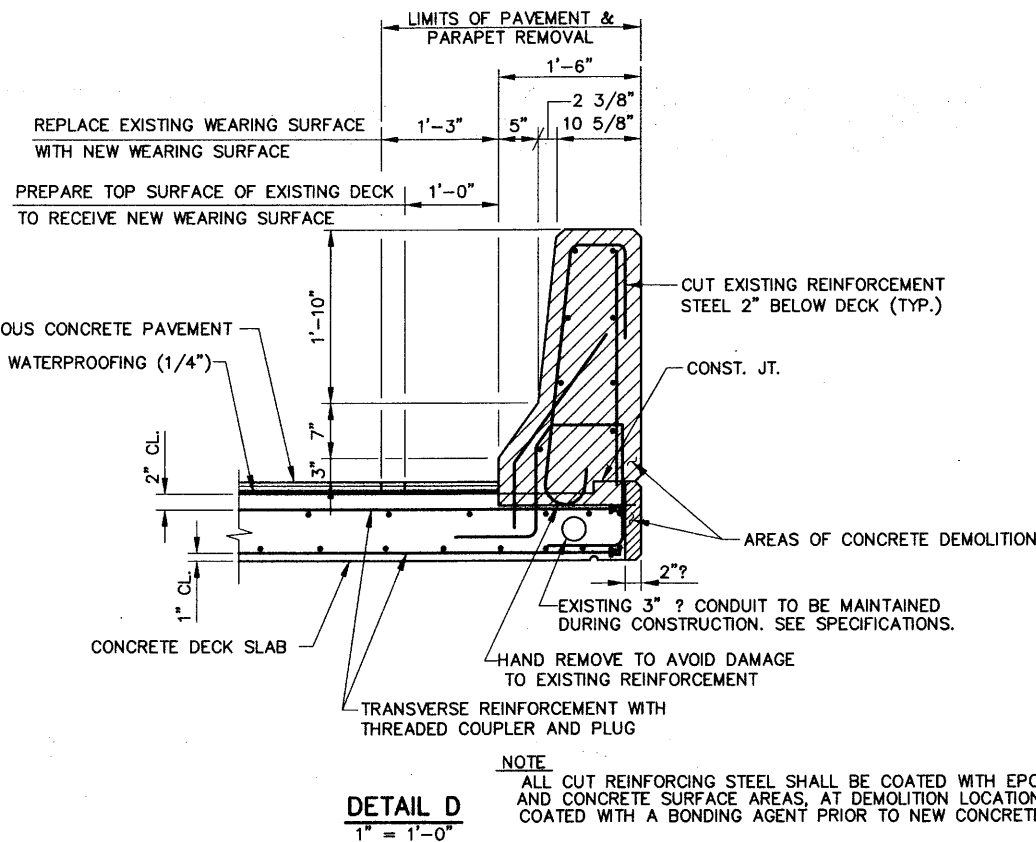


NOTE

TO COMPENSATE FOR DEAD LOAD DEFLECTIONS, AS WELL AS POSSIBLE IRREGULARITIES IN STRINGERS, SET THE BOTTOM OF SLAB ELEVATIONS AT THE POINTS INDICATED BEFORE ANY OF THE SLAB FORMWORK IS STARTED. SEE SUBSECTION 502.10(a) OF THE STANDARD SPECIFICATIONS. REVISION OF APRIL 1995.

BLOCKING POINT DETAIL
NOT TO SCALE

| BOTTOM OF SLAB ELEVATIONS AT BLOCKING POINTS | | | | | |
|--|-----------------|--------|--------|--------|-----------------|
| STRINGER NO. | ? BRG. S. ABUT. | .25 L | .50 L | .75 L | ? BRG. N. ABUT. |
| S6 | 134.33 | 134.28 | 134.26 | 134.25 | 134.25 |
| S7 | 134.11 | 134.08 | 134.05 | 134.04 | 134.04 |



DETAIL D
1" = 1'-0"

NOTE
ALL CUT REINFORCING STEEL SHALL BE COATED WITH EPOXY PAINT AND CONCRETE SURFACE AREAS, AT DEMOLITION LOCATIONS, SHALL BE COATED WITH A BONDING AGENT PRIOR TO NEW CONCRETE PLACEMENT.

SUPERSTRUCTURE NOTES

1. CHAMFER ALL EXPOSED EDGES OF CONCRETE 3/4", UNLESS OTHERWISE NOTED.
2. REINFORCING STEEL SHALL HAVE A MINIMUM COVER OF 2", UNLESS OTHERWISE NOTED.
3. MORTAR FOR BEDDING AND FOR JOINTS IN THE GRANITE CURB SHALL CONTAIN A NON-SHRINK ADDITIVE.
4. CURB JOINTS SHALL LINE UP WITH PARAFFIN AND DUMMY JOINTS.
5. THE CONCRETE DECK SHALL BE GIVEN A SMOOTH BULL FLOAT OR WOOD FLOAT FINISH.
6. THE SLAB SHALL BE PLACED IN ONE CONTINUOUS OPERATION WITH NO TRANSVERSE CONSTRUCTION JOINTS. THE ZIPPER STRIP WILL BE PLACED SEPARATELY IN ONE CONTINUOUS OPERATION AFTER STAGE 1 CONSTRUCTION.
7. CLEAR PROTECTIVE COATING FOR CONCRETE SURFACE SHALL BE APPLIED TO THE FOLLOWING AREAS: PARAPET SURFACES, FASCIA DOWN TO DRIP NOTCH AND ALL EXPOSED CONCRETE SURFACES ON THE END POSTS.
8. ALL BRIDGE PARAPET CONCRETE, INCLUDING INSIDE FACE, TOP AND OUTSIDE FACE, END POSTS AND DECK FASCIA SHALL HAVE A RUBBED FINISH PRIOR TO THE APPLICATION OF THE CLEAR PROTECTIVE COATING FOR CONCRETE SURFACE.
9. THE AUTHORITY'S PERSONNEL SHALL PROFILE THE TOP OF ALL GIRDERS BEFORE THE FORMWORK IS STARTED AND SHALL SUPPLY THE CONTRACTOR WITH FINAL BOTTOM OF SLAB ELEVATIONS. TEN (10) DAYS SHALL BE ALLOWED FOR THE BLOCKING POINT TURN AROUND TIME.
10. FOR 1-BAR ALUMINUM BRIDGE RAIL DETAILS, SEE SHEET WI-S19.
11. PAVEMENT PLACEMENT SHALL BE DONE WITH TWO (2) 1 1/2 INCH "LIFTS".
12. FOR SLAB DETAILS, SEE SHEET NO. WI-S16.
13. FOR RAIL POST SPACING, SEE SHEET NO. WI-S17.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAKING FIELD MEASUREMENTS TO ASSURE LOCATION AND ORIENTATION OF EXISTING ANCHOR BOLTS AND PROVIDE ON SHOP DRAWINGS PRIOR TO FABRICATION OF RAIL. COST OF STRAIGHTENING BENT BOLTS SHALL BE CONSIDERED INCIDENTAL TO ITEM 507.0914.
15. CONTRACTOR SHALL HAVE THE OPTION OF USING PRECAST CONCRETE DECK PANELS AS AN ALTERNATIVE FROM THE CONCRETE SLAB DETAILS SHOWN ON THE CONTRACT PLANS. THE REQUIREMENTS SHOWN ON PAGES 502(7)-502(12) OF THE MDOT STANDARD DETAILS SHALL APPLY. ALSO, THE DESIGN MUST MEET THE SATISFACTION OF THE ENGINEER AND THE RESPECTIVE SHOP DRAWINGS MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
16. FOR PVC DRAIN DETAILS, SEE MDOT STANDARD SHEET 502(3).

STRUCTURAL STEEL NOTES

1. ALL STRUCTURAL STEEL SHALL CONFORM TO AASHTO M270 GRADE 36 (ASTM A709), F_s=20,000 P.S.I. EXCEPT AS FOLLOWS:
BEARING PEDESTAL TO BE AASHTO M270, GRADE 50 (ASTM A572) F_s=27,000 P.S.I.
2. FOR BEARING PEDESTAL DETAILS, SEE SHEET WI-S15.
3. FOR SHEAR CONNECTOR DETAILS, SEE STANDARD DETAIL SHEET 505(1).
4. BEARING STIFFENERS SHALL BE PLUMB AFTER ERECTION AND DEAD LOADING OF THE STRUCTURE.
5. THE PROPOSED STRINGERS SHALL BE CAMBERED TO COMPENSATE FOR THE DEAD LOAD DEFLECTION GIVEN.

CONDUIT NOTES

1. SUPPORT SYSTEMS SHALL BE AS MANUFACTURED BY AMERCON INTERNATIONAL FOR 2" PVC (2.375" O.D.) OR APPROVED EQUAL.
2. A #10 PULLWIRE SHALL BE LEFT IN ALL CONDUITS.
3. THE CONDUITS SHALL BE RODDED AND CLEANED WITH A CLOSE FITTING MANDREL AND WIRE BRUSH BEFORE INSERTING PULLWIRES. CONDUITS SHALL BE FITTED WITH THREADED CAPS.
4. COST OF FURNISHING AND INSTALLING SUPPORTING SYSTEM, CONDUIT, PULLWIRES AND FITTINGS SHALL BE PAID FOR UNDER ITEM 626.22. (DOES NOT INCLUDE W4 OR #3/8)
5. PROVIDE EXPANSION JOINT AT EXPANSION ABUTMENTS.
6. SEE ABUTMENT DETAILS FOR OPENING ELEVATIONS & LOCATIONS.
7. SELECT CONDUIT LENGTHS SO THAT COUPLING LOCATIONS DO NOT COINCIDE WITH SUPPORT LOCATIONS.
8. SWEEP CONDUITS BEHIND GUARD RAIL. BEGIN SWEEP AT BACK FACE OF BACKWALL. (MINIMUM 20' RADIUS)
9. PVC CONDUITS NEED NOT BE SUPPORTED AT DIAPHRAGM LOCATIONS.

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Scale AS NOTED

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Designed by:

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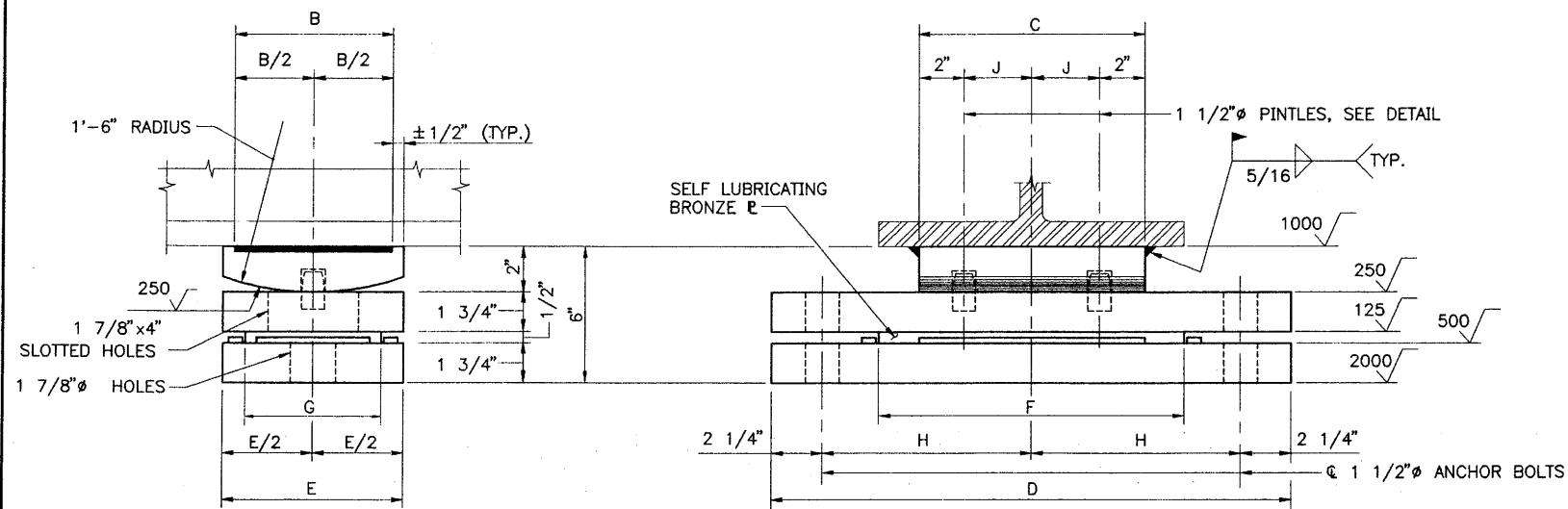
| By | Date | Checked | By | Date |
|--------------|-------|------------------|-----|-------|
| Designed AAD | 11/99 | Checked SJC | SJC | 11/99 |
| Drawn LS | 11/99 | In Charge of RAL | RAL | 11/99 |

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Westbrook, ME 04092

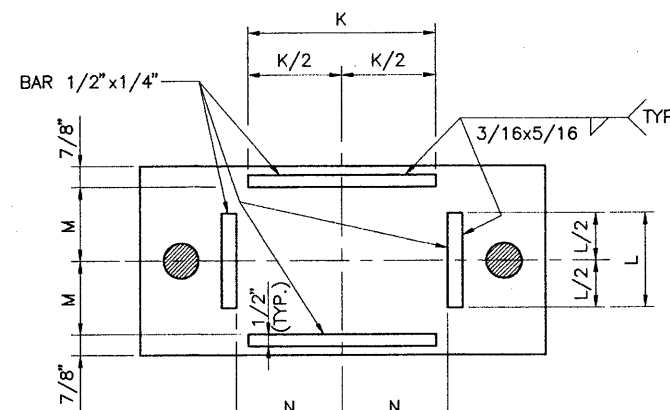
MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT

MAINLINE BRIDGE WIDENING
WELLS INTERCHANGE OVERPASS
SUPERSTRUCTURE DETAILS

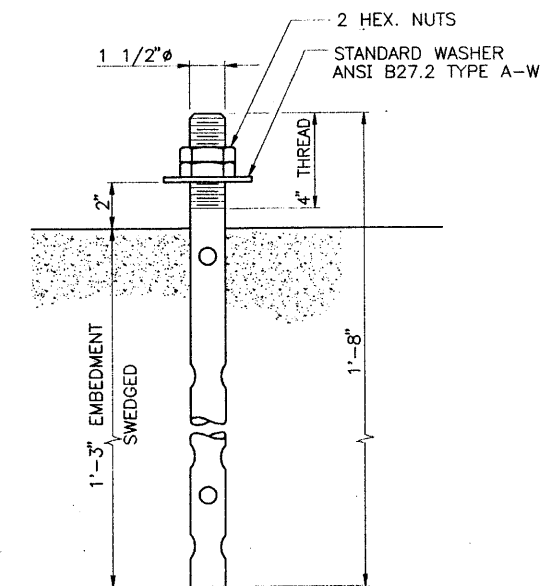
SHEET NUMBER: WI-S14
CONTRACT: 2000.03
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EXPANSION PEDESTAL - EPB
3"=1'-0"

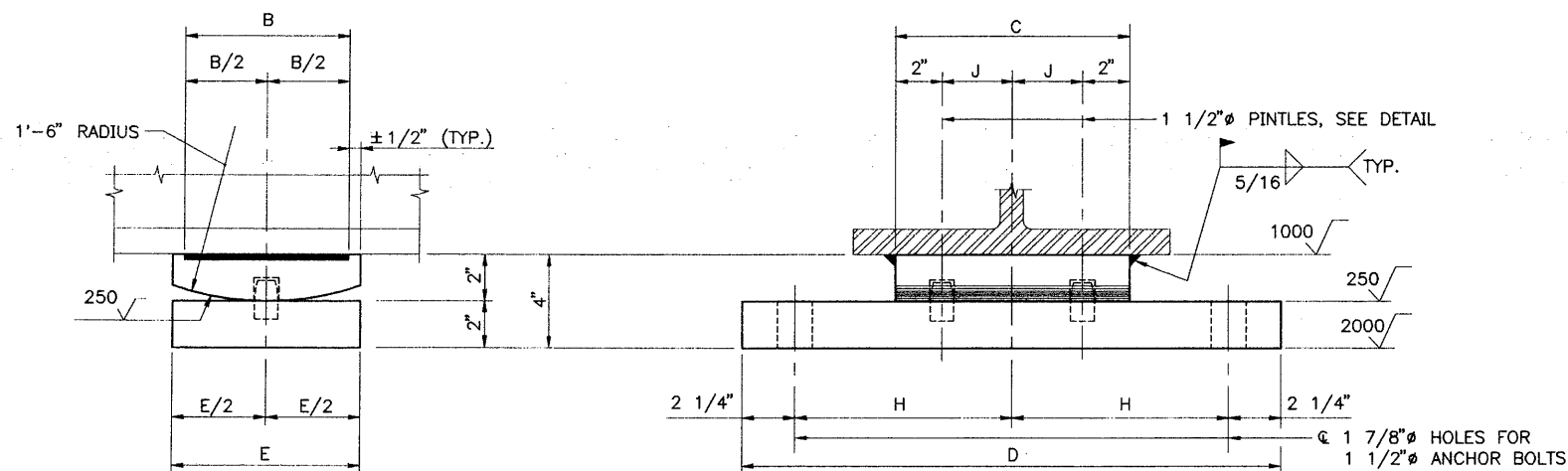


MASONRY PLATE
FOR EPB
3"=1'-0"

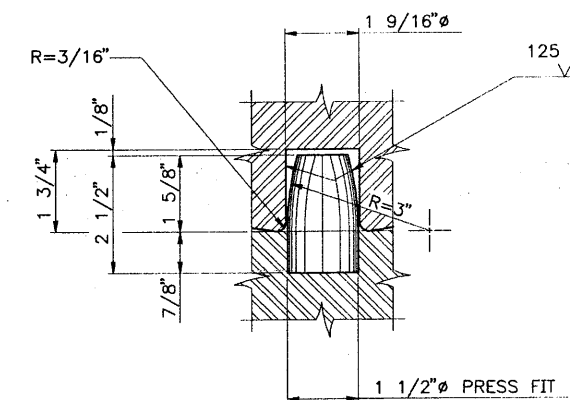


ANCHOR BOLT DETAIL
3"=1'-0"

NOTE
TWO REQUIRED FOR EACH EXPANSION PEDESTAL - EPC.
FOUR REQUIRED FOR EACH FIXED PEDESTAL - FPC.



FIXED PEDESTAL - FPB
3"=1'-0"



PINTLE DETAIL
HALF-SIZE

PEDESTALS - ALLOWABLE LOADS AND DIMENSIONS

| PEDESTAL | LOAD | A | B | C | D | E | F | G | H | J | K | L | M | N | LOCATION | QUANTITY |
|----------|------|---|----|----|-----------|----|-----|----|--------|----|----|----|--------|--------|----------------|----------|
| FPB-1 | 120K | - | 6" | 8" | 1'-7 1/2" | 8" | - | - | 7 1/2" | 2" | - | - | - | - | SOUTH ABUTMENT | 2 |
| EPB-1 | 120K | - | 6" | 8" | 1'-7 1/2" | 8" | 10" | 6" | 7 1/2" | 2" | 8" | 4" | 3 1/8" | 5 1/8" | NORTH ABUTMENT | 2 |

NOTES

1. ALL STEEL, INCLUDING THE ANCHOR BOLTS, SHALL BE AASHTO M270, GRADE 50.
2. NUTS SHALL CONFORM TO AASHTO M291, ASTM A563, HEAVY HEX, GRADE A.
3. UPSET THREADS ON ANCHOR BOLTS AFTER ASSEMBLY.
4. BEARING PLATES SHALL BE PLACED ON 1/8" PREFORMED FABRIC PAD.

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FAX (207) 772-7410

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MODERNIZATION AND WIDENING PROJECT**



**MAINLINE BRIDGE WIDENING
WELLS INTERCHANGE OVERPASS
BEARING DETAILS**

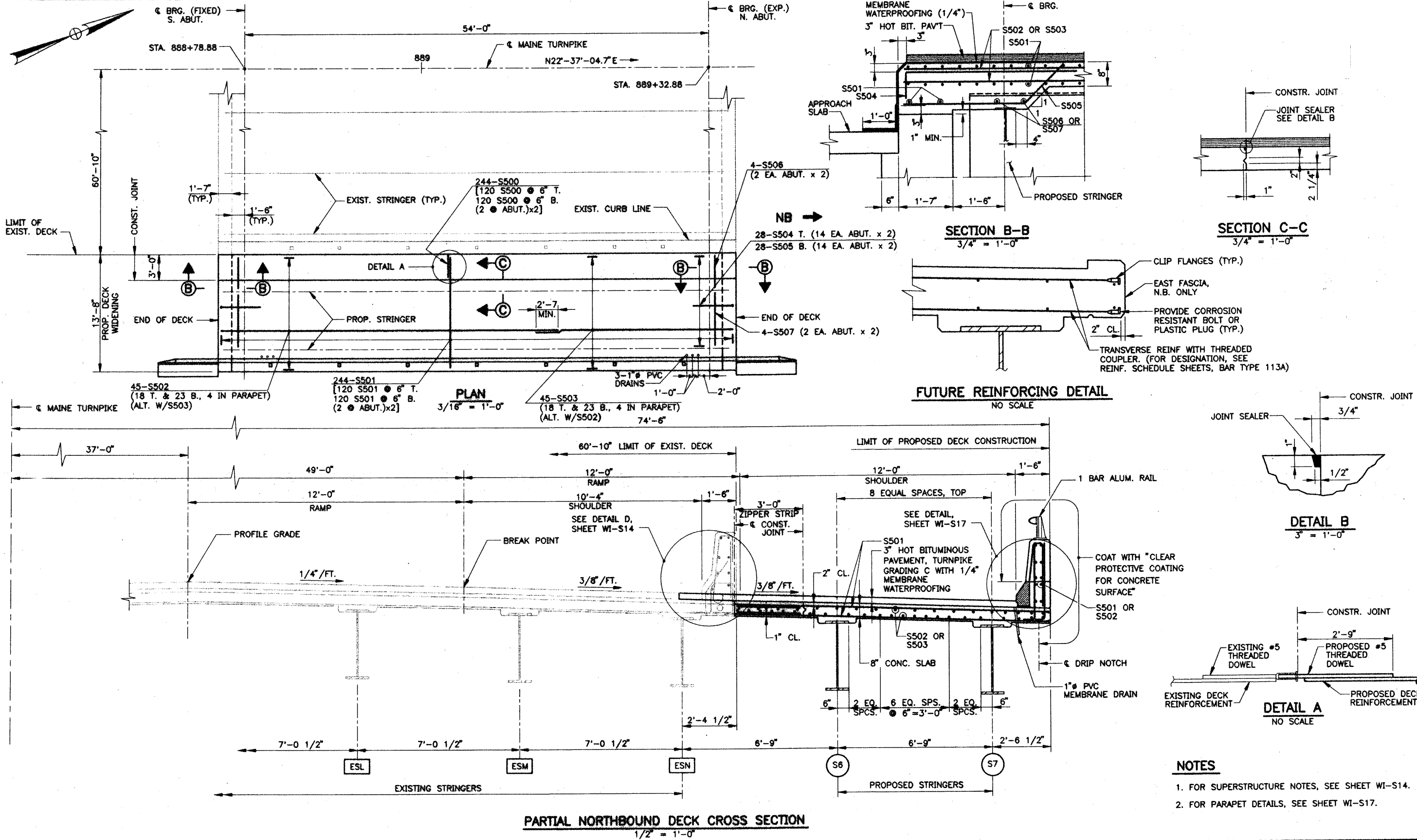
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| | By | Date | | By | Date |
|----------|-----|-------|--------------|-----|-------|
| Designed | AAD | 10/99 | Checked | JFW | 11/99 |
| Drawn | RJT | 10/99 | In Charge of | RAL | 11/99 |

CONTRACT: 2000.03

SHEET NUMBER: WI-S15

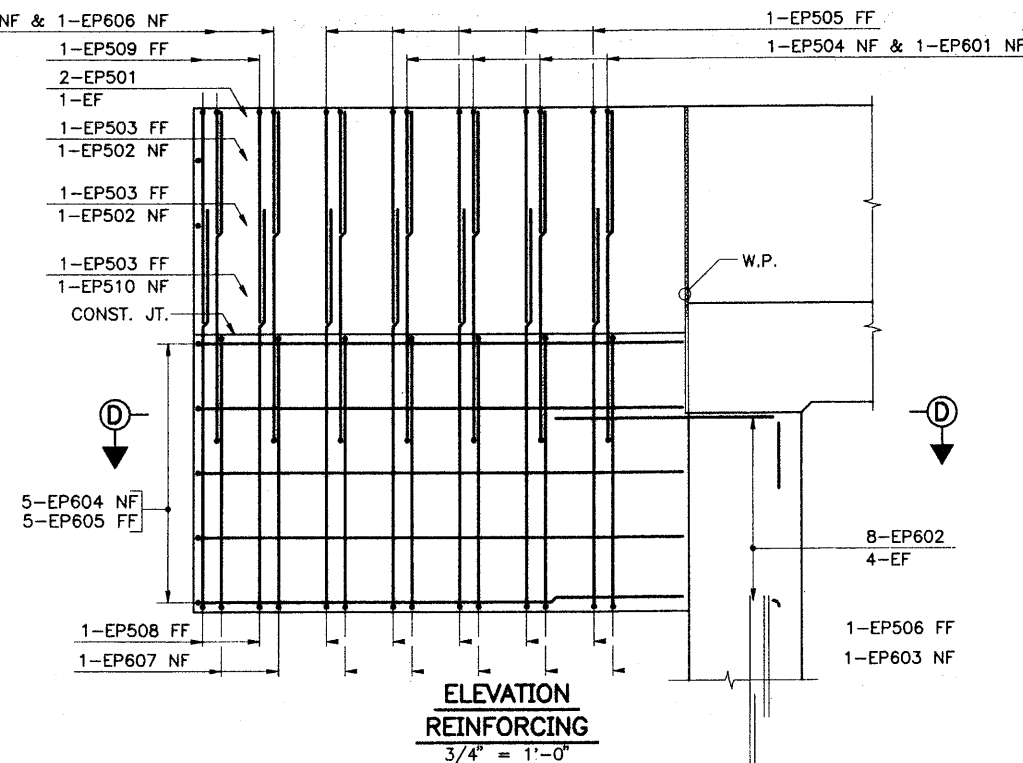
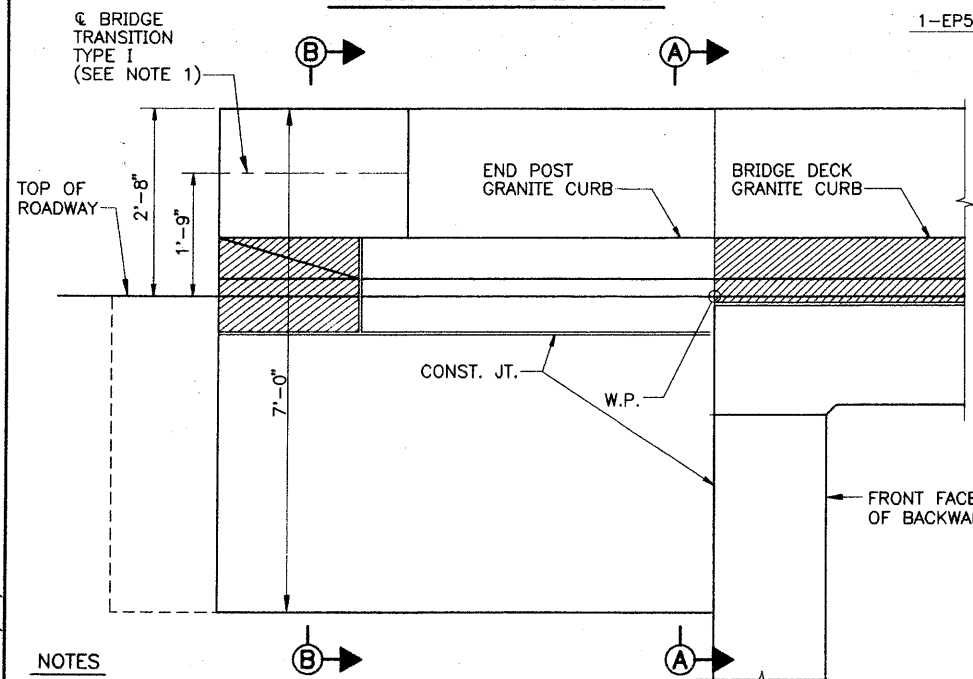
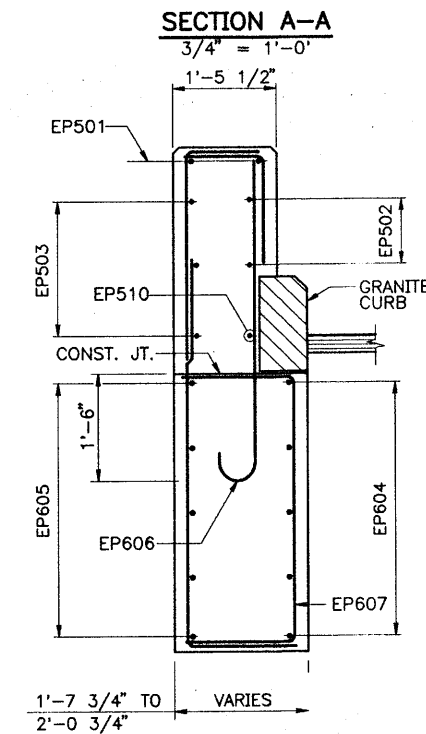
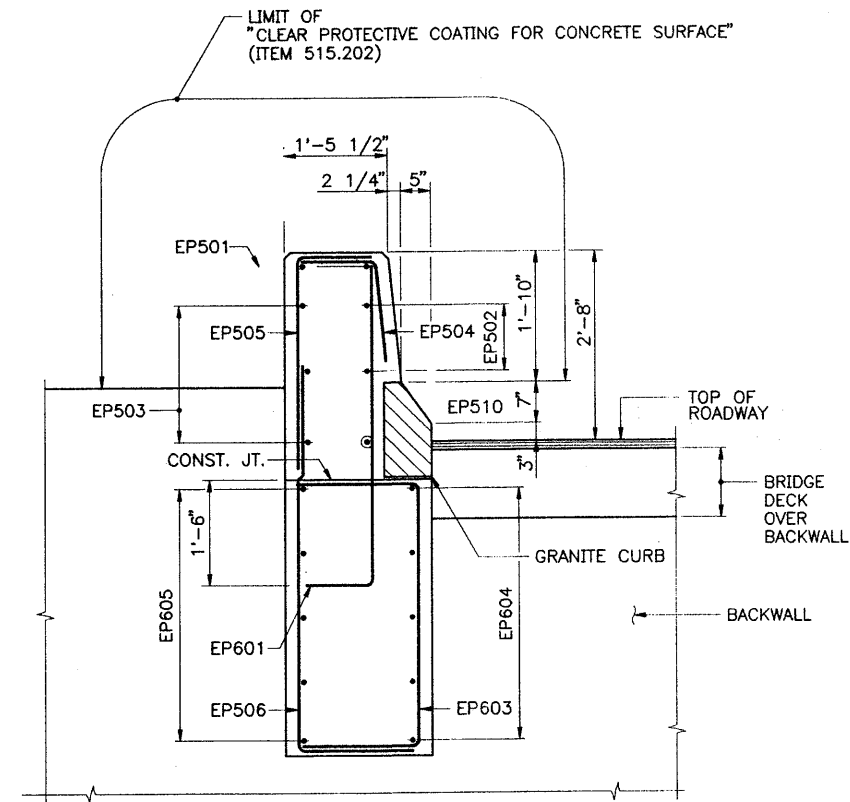
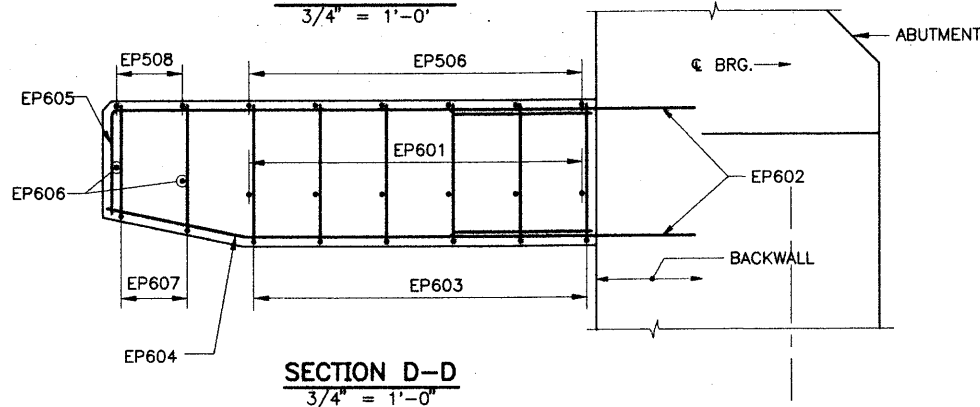
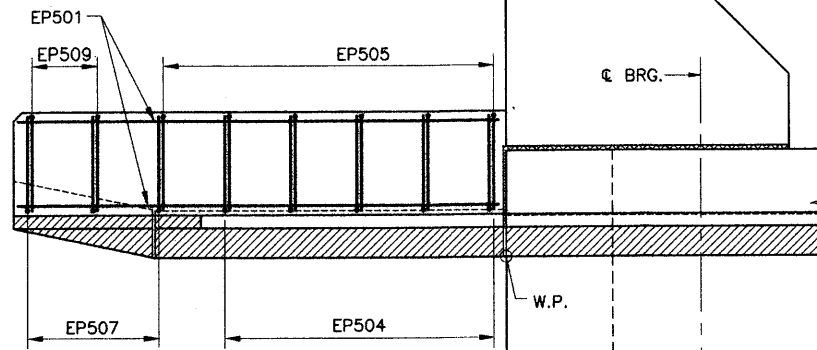
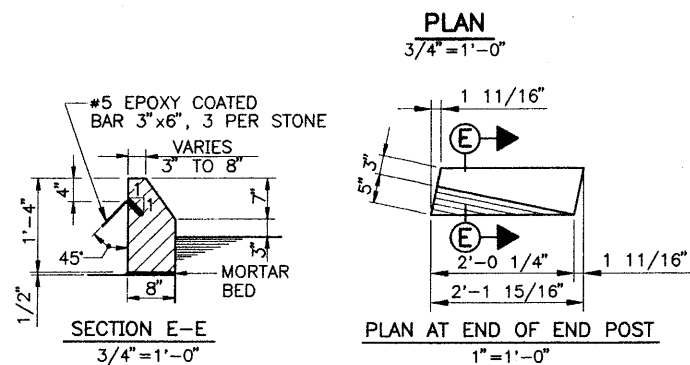
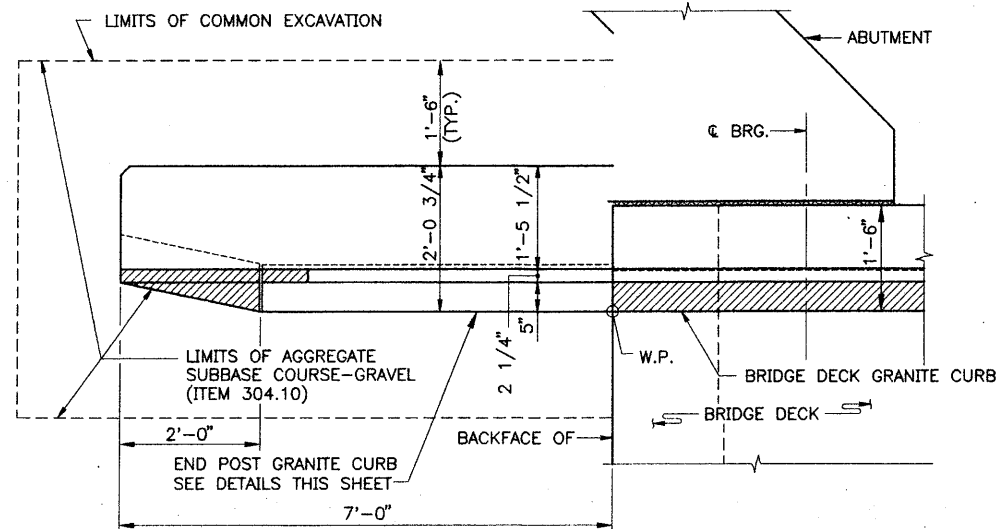
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- NOTES**
- FOR SUPERSTRUCTURE NOTES, SEE SHEET WI-S14.
 - FOR PARAPET DETAILS, SEE SHEET WI-S17.

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|---|----------|------------------|----------|--|--|---|--|--|------|--|--|--|--|--|--|--|--|--|--|--|--|--|--|----|------|---------|----|------|--------------|-------|-------------|-----|-------|----------|-------|------------------|
| <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>No.</th> <th>Revision</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table> | | No. | Revision | | | | | By | Date | | | | | | | | | | | | | <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>By</th> <th>Date</th> <th>Checked</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr> <td>Designed AAD</td> <td>10/99</td> <td>Checked JFW</td> <td>JFW</td> <td>11/99</td> </tr> <tr> <td>Drawn LS</td> <td>10/99</td> <td>In Charge of RAL</td> <td>RAL</td> <td>11/99</td> </tr> </tbody> </table> | | By | Date | Checked | By | Date | Designed AAD | 10/99 | Checked JFW | JFW | 11/99 | Drawn LS | 10/99 | In Charge of RAL |
| No. | Revision | By | Date | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| By | Date | Checked | By | Date | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Designed AAD | 10/99 | Checked JFW | JFW | 11/99 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Drawn LS | 10/99 | In Charge of RAL | RAL | 11/99 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |



NOTES

1. FOR BRIDGE TRANSITION TYPE I DETAIL, SEE CIVIL SHEET MD-C3.

ELEVATION
3/4" = 1'-0"

ELEVATION REINFORCING
3/4" = 1'-0"

SECTION B-B
3/4" = 1'-0"

Scale: AS NOTED

Designed by:

HNTB

ARCHITECTS ENGINEERS PLANNERS

HNTB CORPORATION
2 Thomas Drive
Westbrook, ME 04092
TEL (207) 774-5155
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MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT



MAINLINE BRIDGE WIDENING
WELLS INTERCHANGE
END POST DETAILS

| No. | Revision | By | Date |
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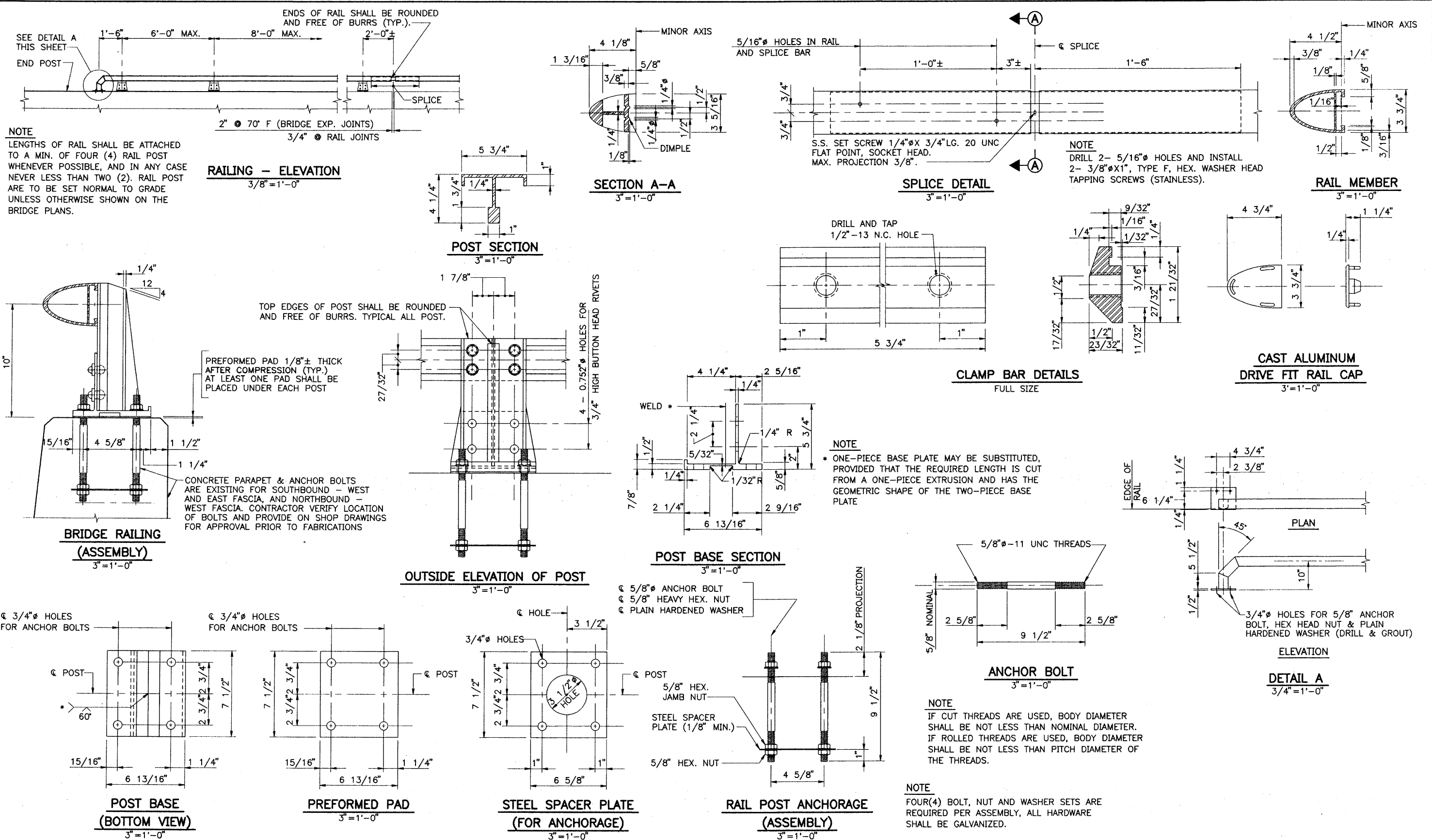
| | By | Date | | By | Date |
|----------|-----|-------|--------------|-----|-------|
| Designed | SHR | 10/99 | Checked | JFW | 11/99 |
| Drawn | SHR | 10/99 | In Charge of | RAL | 11/99 |

SHEET NUMBER: WI-S18

CONTRACT: 2000.03

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|---|-----------|--------------|-----------|--|------|---|--|--|--|---|--|--|--|--|--|--|--|---|--|----|------|----|------|----------|-----------|---------|-----------|-------|----------|--------------|-----------|--|--|--|--|--|--|---|--|
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| No. | Revision | By | Date | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| By | Date | By | Date | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Designed | JFW 10/99 | Checked | AAD 10/99 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Drawn | LS 10/99 | In Charge of | RAL 11/99 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

| MARK | SIZE | NO | LENGTH | | TYPE | A | | B | | C | | D | | INCR. | REMARKS |
|--|------|----|-------------------|----|------|----|----|----|----|----|--------|---|---|-------|-----------------------------|
| | | | FT | IN | | FT | IN | FT | IN | FT | IN | | | | |
| NORTH ABUTMENT AND WINGWALL FOOTING | | | | | | | | | | | | | | | |
| *AF501 | 5 | 45 | 4 | 0 | STR | | | | | | | | | | DOWELS - NEAR FACE |
| AF601 | 8 | 27 | 12 | 0 | STR | | | | | | | | | | TRANSVERSE - BOTTOM |
| AF602 | 8 | 28 | 31 | 2 | STR | | | | | | | | | | LONGITUDINAL - TOP & BOTTOM |
| AF603 | 8 | 26 | 10 | 4 | STR | | | | | | | | | | LONGITUDINAL - TOP & BOTTOM |
| AF604 | 6 | 20 | 4 | 0 | STR | | | | | | | | | | DOWEL - EXISTING FOOTING |
| AF701 | 7 | 22 | 17 | 5 | 111 | 16 | 4 | 1 | 2 | | 1 1/8 | | | | DOWEL - FAR FACE |
| AF801 | 8 | 55 | 12 | 0 | STR | | | | | | | | | | TRANSVERSE - TOP |
| AF802 | 8 | 6 | 11 | 6 | STR | | | | | | | | | | TRANSVERSE - BOTTOM |
| AF803 | 8 | 21 | 11 | 8 | 111 | 10 | 4 | 1 | 4 | | 1 3/8 | | | | DOWEL - FAR FACE |
| AF804 | 8 | 16 | 12 | 1 | 118 | 10 | 9 | 1 | 4 | | | | | | DOWEL - FAR FACE |
| AF805 | 8 | 17 | 20 | 7 | 118 | 19 | 3 | 1 | 4 | | | | | | DOWEL - FAR FACE |
| AF806 | 8 | 3 | 10 | 6 | STR | | | | | | | | | | DOWEL - FAR FACE |
| AF807 | 8 | 3 | 19 | 0 | STR | | | | | | | | | | DOWEL - FAR FACE |
| AF901 | 9 | 6 | 11 | 6 | STR | | | | | | | | | | TRANSVERSE - TOP |
| AF902 | 9 | 2 | 12 | 9 | STR | | | | | | | | | | CORNER - TOP & BOTTOM |
| AF903 | 9 | 8 | 9 | 0 | STR | | | | | | | | | | CORNER - TOP & BOTTOM |
| AF904 | 9 | 8 | 7 | 0 | STR | | | | | | | | | | CORNER - TOP & BOTTOM |
| N.E. WINGWALL | | | | | | | | | | | | | | | |
| WA501 | 5 | 16 | 4'-7" TO 19'-2" | | STR | | | | | | | | | 2'-1" | HORIZONTAL - EACH FACE |
| WA502 | 5 | 26 | 21 | 5 | STR | | | | | | | | | | HORIZONTAL - EACH FACE |
| WA503 | 5 | 18 | 7 | 6 | STR | | | | | | | | | | HORIZONTAL - EACH FACE |
| WA504 | 5 | 2 | 29 | 0 | STR | | | | | | | | | | DIAGONAL - EACH FACE |
| WA505 | 5 | 2 | 4 | 4 | 109 | 2 | 2 | | | 1 | 11 1/2 | 2 | 2 | E=11" | HORIZONTAL - EACH FACE |
| WA506 | 5 | 2 | 7 | 0 | STR | | | | | | | | | | DIAGONAL - EACH FACE |
| WA507 | 5 | 22 | 11'-2" TO 21'-8" | | STR | | | | | | | | | 6" | VERTICAL - NEAR FACE |
| WA508 | 5 | 20 | 2'-8" TO 12'-2" | | STR | | | | | | | | | 6" | VERTICAL - FAR FACE |
| WA509 | 5 | 12 | 5'-11" TO 10'-11" | | STR | | | | | | | | | 1'-0" | VERTICAL - EACH FACE |
| NORTH ABUTMENT END POST | | | | | | | | | | | | | | | |
| *EP501 | 5 | 2 | 6 | 8 | STR | | | | | | | | | | HORIZONTAL |
| *EP502 | 5 | 2 | 7 | 7 | 118 | 6 | 8 | 0 | 11 | | | | | | HORIZONTAL |
| *EP503 | 5 | 3 | 6 | 8 | STR | | | | | | | | | | HORIZONTAL |
| *EP504 | 5 | 5 | 2 | 6 | 119 | 1 | 1 | 1 | 5 | 0 | 2 | | | | TOP OF END POST |
| *EP505 | 5 | 6 | 3 | 11 | 118 | 2 | 10 | 1 | 1 | | | | | | VERTICAL |
| *EP506 | 5 | 6 | 7 | 5 | 118 | 5 | 9 | 1 | 8 | | | | | | VERTICAL |
| *EP507 | 5 | 3 | 2 | 7 | 118 | 1 | 6 | 1 | 1 | | | | | | TOP OF END POST |
| *EP508 | 5 | 2 | 7 | 0 | 118 | 5 | 9 | 1 | 3 | | | | | | VERTICAL |
| *EP509 | 5 | 2 | 3 | 11 | 118 | 2 | 10 | 1 | 1 | | | | | | VERTICAL |
| *EP510 | 5 | 1 | 6 | 7 | 104 | 4 | 8 | 1 | 11 | 0 | 5 | | | | HORIZONTAL |
| *EP601 | 6 | 6 | 5 | 6 | 118 | 4 | 6 | 1 | 0 | | | | | | VERTICAL |
| EP602 | 6 | 8 | 4 | 4 | 118 | 3 | 4 | 1 | 0 | | | | | | HORIZONTAL |
| EP603 | 6 | 6 | 6 | 11 | 102 | 3 | 7 | 1 | 8 | 1 | 8 | | | | VERTICAL |
| EP604 | 6 | 5 | 6 | 7 | 119 | 4 | 9 | 1 | 10 | 0 | 5 | | | | HORIZONTAL |
| EP605 | 6 | 5 | 7 | 9 | 118 | 6 | 8 | 1 | 1 | | | | | | HORIZONTAL |
| *EP606 | 6 | 2 | 5 | 6 | 108 | 4 | 6 | | | | | | | | VERTICAL (180° STD. HOOK) |

| MARK | SIZE | NO | LENGTH | | TYPE | A | | B | | C | | D | | INCR. | REMARKS |
|---|------|----|--------|----|------|----|----|----|----|----|----|---|---|-------|-------------------------------|
| | | | FT | IN | | FT | IN | FT | IN | FT | IN | | | | |
| NORTH ABUTMENT STEM AND BACKWALL | | | | | | | | | | | | | | | |
| *A501 | 5 | 23 | 16 | 7 | STR | | | | | | | | | | VERTICAL - NEAR FACE |
| *A502 | 5 | 14 | 5 | 6 | 118 | 3 | 8 | 2 | 0 | | | | | | ABUTMENT SEAT |
| *A503 | 5 | 14 | 4 | 6 | STR | | | | | | | | | | BACKWALL - NEAR FACE |
| *A504 | 5 | 14 | 5 | 9 | 101 | 1 | 3 | 2 | 3 | | | | | | BACKWALL - CAP |
| *A505 | 5 | 12 | 3 | 2 | 118 | 1 | 8 | 1 | 6 | | | | | | BACKWALL - FAR FACE |
| *A506 | 5 | 12 | 8 | 2 | 115 | | | | | 4 | 0 | 2 | 2 | | BACKWALL - FAR FACE |
| *A507 | 5 | 17 | 7 | 9 | STR | | | | | | | | | | CORNER VERTICAL |
| A508 | 5 | 2 | 6 | 0 | STR | | | | | | | | | | BACKWALL VERTICAL @ CORNER |
| *A509 | 5 | 2 | 5 | 0 | 101 | 3 | 0 | 1 | 0 | | | | | | APPROACH SLAB SEAT DOWEL |
| *A510 | 5 | 18 | 14 | 3 | STR | | | | | | | | | | HORIZONTAL - NEAR FACE |
| *A511 | 5 | 18 | 9 | 9 | 115 | | | 2 | 3 | 5 | 6 | 2 | 0 | | HORIZONTAL - NEAR FACE |
| A512 | 5 | 17 | 12 | 6 | STR | | | | | | | | | | HORIZONTAL - FAR FACE |
| A513 | 1 | 7 | 8 | 0 | 118 | 5 | 0 | 3 | 0 | | | | | | CORNER HORIZONTAL - FAR FACE |
| A514 | 5 | 18 | 4 | 0 | 118 | 2 | 0 | 2 | 0 | | | | | | CORNER HORIZONTAL - FAR FACE |
| A515 | 5 | 1 | 18 | 6 | STR | | | | | | | | | | HORIZONTAL - FAR FACE |
| A516 | 5 | 3 | 12 | 8 | STR | | | | | | | | | | HORIZONTAL - FAR FACE |
| A517 | 5 | 3 | 7 | 9 | STR | | | | | | | | | | CORNER HORIZONTAL - FAR FACE |
| A518 | 5 | 7 | 3 | 10 | 118 | 2 | 6 | 1 | 4 | | | | | | CORNER HORIZONTAL - FAR FACE |
| *A519 | 5 | 4 | 7 | 2 | 115 | | | 1 | 0 | 5 | 6 | 0 | 8 | | CORNER HORIZONTAL - NEAR FACE |
| *A520 | 5 | 4 | 4 | 2 | 118 | 3 | 6 | 0 | 8 | | | | | | CORNER HORIZONTAL - NEAR FACE |
| *A521 | 5 | 4 | 14 | 6 | STR | | | | | | | | | | BACKWALL HORIZ. - NEAR FACE |
| *A522 | 5 | 2 | 17 | 6 | STR | | | | | | | | | | BACKWALL HORIZ. - FAR FACE |
| *A523 | 5 | 8 | 8 | 9 | 118 | 7 | 3 | 1 | 6 | | | | | | ABUTMENT SEAT HORIZONTAL |
| *A524 | 5 | 22 | 3 | 0 | STR | | | | | | | | | | OLD/NEW ABUTMENT DOWEL |
| A525 | 5 | 23 | 3 | 0 | STR | | | | | | | | | | OLD/NEW ABUTMENT DOWEL |
| *A526 | 5 | 2 | 5 | 6 | 118 | 4 | 0 | 1 | 6 | | | | | | BACKWALL CORNER - TOP |

| NORTH APPROACH SLAB | | | | | | | | | | | | | | | |
|----------------------------|---|----|----|---|-----|--|--|--|--|--|--|--|--|--|---------------|
| AS400 | 4 | 16 | 13 | 4 | STR | | | | | | | | | | APPROACH SLAB |
| AS600 | 6 | 28 | 15 | 2 | STR | | | | | | | | | | APPROACH SLAB |

| DECK SLAB AND PARAPETS | | | | | | | | | | | | | | | |
|-------------------------------|---|-----|----|----|------|---|---|---|---|---|---|---|---|--|-------------------------|
| *S500 | 5 | 244 | 2 | 9 | 113B | | | | | | | | | | THREADED DOWEL TO EXIST |
| *S501 | 5 | 244 | 13 | 6 | 113A | | | | | | | | | | TRANSVERSE DOWEL |
| *S502 | 5 | 45 | 40 | 0 | STR | | | | | | | | | | LONGITUDINAL T & B |
| *S503 | 5 | 45 | 22 | 6 | STR | | | | | | | | | | LONGITUDINAL T & B |
| *S504 | 5 | 28 | 5 | 9 | 118 | 5 | 0 | 0 | 9 | | | | | | SLAB END LONGIT TOP |
| *S505 | 5 | 28 | 4 | 6 | 115 | 0 | 0 | 0 | 0 | 3 | 4 | 1 | 2 | | SLAB END LONGIT BOTTOM |
| *S506 | 5 | 4 | 4 | 0 | STR | | | | | | | | | | SLAB END TRANSVERSE |
| *S507 | 5 | 4 | 6 | 5 | STR | | | | | | | | | | SLAB END TRANSVERSE |
| *S508 | 5 | 18 | 7 | 8 | STR | | | | | | | | | | PARAPET |
| *S509 | 5 | 12 | 7 | 10 | STR | | | | | | | | | | PARAPET |
| *S510 | 5 | 12 | 6 | 5 | STR | | | | | | | | | | PARAPET |
| *S511 | 5 | 6 | 2 | 9 | STR | | | | | | | | | | PARAPET END PANEL |
| *S512 | 5 | 6 | 2 | 9 | STR | | | | | | | | | | PARAPET END PANEL |
| *S513 | 5 | 61 | 3 | 10 | 124A | 0 | 7 | 1 | 7 | 0 | 0 | 1 | 8 | | PARAPET TOP |
| *S601 | 6 | 61 | 4 | 2 | 118 | 3 | 2 | 1 | 0 | | | | | | SLAB DOWEL |
| *S602 | 6 | 61 | 4 | 2 | 118 | 3 | 2 | 1 | 0 | | | | | | SLAB DOWEL |


NOTE:
* INDICATES EPOXY COATED REINFORCEMENT

M:\09009\002\119\119\S6RESH1WI.DWG 12/28/99 14:28

Scale:

| No. | Revision | By | Date |
|-----|----------|----|------|
| | | | |
| | | | |
| | | | |


Designed by:



HNTB CORPORATION
2 Thomas Drive
Westbrook, ME 04092
TEL (207) 774-5155
FAX (207) 772-7410

| By | Date | Checked | By | Date |
|-------|-------|--------------|-----|-------|
| SJC | 11/99 | JFW | JFW | 11/99 |
| Drawn | | In Charge of | | |
| JFW | 11/99 | RAL | RAL | 11/99 |

**MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT**



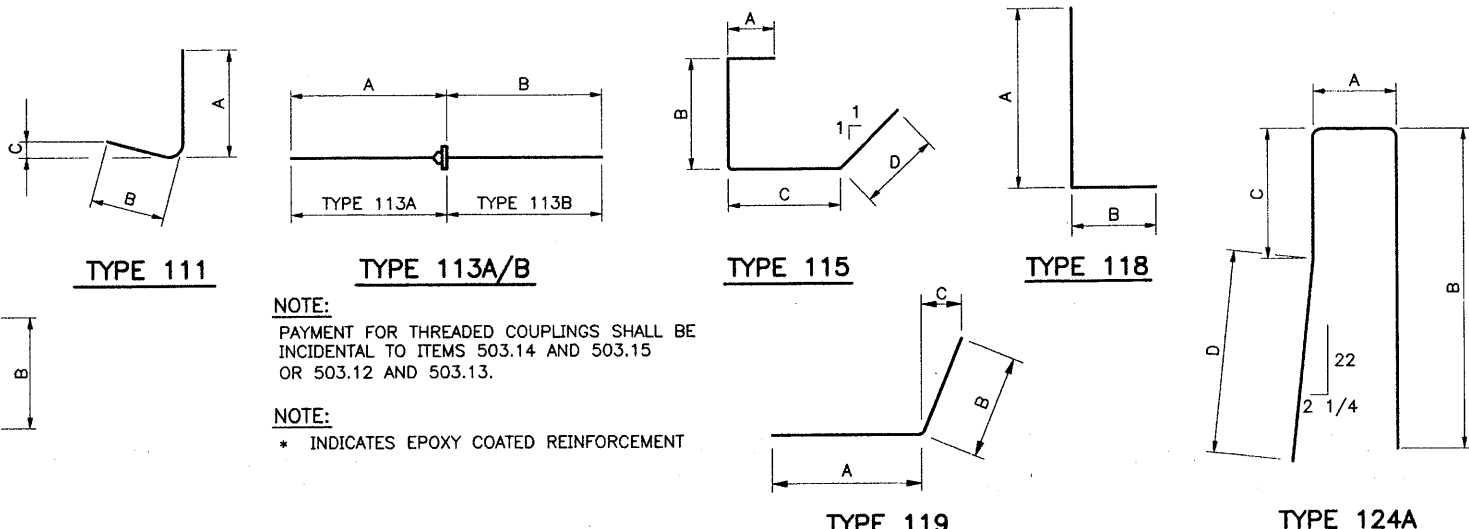
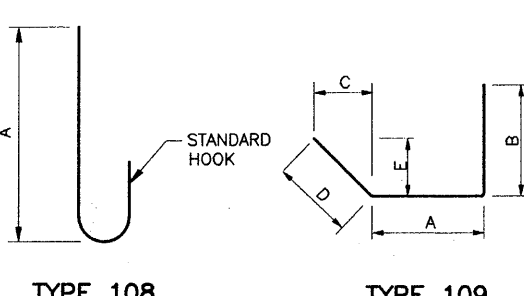
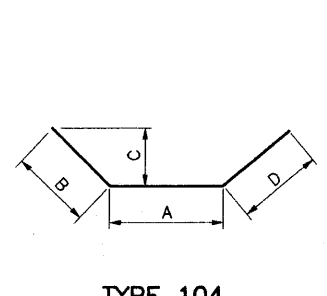
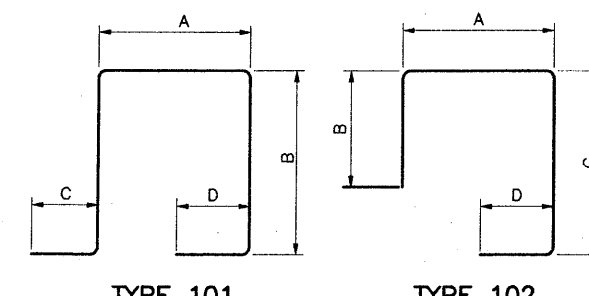
**BRIDGE REPLACEMENT
WELLS INTERCHANGE OVERPASS
REINFORCING STEEL SCHEDULE I**

SHEET NUMBER: **WI-S20**
CONTRACT: **2000.03**
137 OF 178

| MARK | SIZE | NO | LENGTH | | TYPE | A | | B | | C | | D | | INCR. | REMARKS |
|--|------|----|-------------------|----|------|----|----|----|----|----|-------|----|-------|----------|-----------------------------|
| | | | FT | IN | | FT | IN | FT | IN | FT | IN | FT | IN | | |
| SOUTH ABUTMENT AND WINGWALL FOOTING | | | | | | | | | | | | | | | |
| *BF501 | 5 | 41 | 4 | 0 | STR | | | | | | | | | | DOWEL - NEAR FACE |
| BF601 | 6 | 23 | 12 | 0 | STR | | | | | | | | | | TRANSVERSE - BOTTOM |
| BF602 | 6 | 26 | 27 | 0 | STR | | | | | | | | | | LONGITUDINAL - TOP & BOTTOM |
| BF603 | 6 | 26 | 13 | 6 | STR | | | | | | | | | | LONGITUDINAL - TOP & BOTTOM |
| BF604 | 6 | 26 | 4 | 0 | STR | | | | | | | | | | DOWEL - EXISTING FOOTING |
| BF701 | 7 | 18 | 17 | 6 | 111 | 16 | 4 | 1 | 2 | | 1 1/8 | | | | DOWEL - FAR FACE |
| BF801 | 8 | 47 | 12 | 0 | STR | | | | | | | | | | TRANSVERSE - TOP |
| BF802 | 8 | 8 | 11 | 6 | STR | | | | | | | | | | TRANSVERSE - BOTTOM |
| BF803 | 8 | 17 | 11 | 8 | 111 | 10 | 4 | 1 | 4 | | 1 3/8 | | | | DOWEL - FAR FACE |
| BF804 | 8 | 19 | 12 | 1 | 118 | 10 | 9 | 1 | 4 | | | | | | DOWEL - FAR FACE |
| BF805 | 8 | 20 | 20 | 7 | 118 | 19 | 3 | 1 | 4 | | | | | | DOWEL - FAR FACE |
| BF901 | 9 | 8 | 11 | 6 | STR | | | | | | | | | | TRANSVERSE - TOP |
| BF902 | 9 | 2 | 12 | 8 | STR | | | | | | | | | | CORNER - TOP & BOTTOM |
| BF903 | 9 | 10 | 7 | 0 | STR | | | | | | | | | | CORNER - TOP & BOTTOM |
| BF904 | 9 | 6 | 9 | 0 | STR | | | | | | | | | | CORNER - TOP & BOTTOM |
| S.E. WINGWALL | | | | | | | | | | | | | | | |
| WB501 | 5 | 14 | 3'-11" TO 15'-11" | | STR | | | | | | | | 2'-0" | | HORIZONTAL - EACH FACE |
| WB502 | 5 | 28 | 17 | 2 | STR | | | | | | | | | | HORIZONTAL - EACH FACE |
| WB503 | 5 | 20 | 7 | 6 | STR | | | | | | | | | | HORIZONTAL - EACH FACE |
| WB504 | 5 | 2 | 24 | 9 | STR | | | | | | | | | | DIAGONAL - EACH FACE |
| WB505 | 5 | 2 | 3 | 4 | 109 | 1 | 8 | | | 1 | 6 | 1 | 8 | E=8 3/4" | HORIZONTAL - EACH FACE |
| WB506 | 5 | 2 | 8 | 0 | STR | | | | | | | | | | DIAGONAL - EACH FACE |
| WB507 | 5 | 18 | 13'-5" TO 21'-11" | | STR | | | | | | | | 6" | | VERTICAL - NEAR FACE |
| WB508 | 5 | 18 | 2'-6" TO 11'-0" | | STR | | | | | | | | 6" | | VERTICAL - FAR FACE |
| WB509 | 5 | 12 | 5'-11" TO 11'-4" | | STR | | | | | | | | 1'-1" | | VERTICAL - EACH FACE |
| SOUTH ABUTMENT END POST | | | | | | | | | | | | | | | |
| *EP501 | 5 | 2 | 6 | 8 | STR | | | | | | | | | | HORIZONTAL |
| *EP502 | 5 | 2 | 7 | 7 | 118 | 6 | 8 | 0 | 11 | | | | | | HORIZONTAL |
| *EP503 | 5 | 3 | 6 | 8 | STR | | | | | | | | | | HORIZONTAL |
| *EP504 | 5 | 5 | 2 | 6 | 119 | 1 | 1 | 1 | 5 | | | | | | TOP OF END POST |
| *EP505 | 5 | 6 | 3 | 11 | 118 | 2 | 10 | 1 | 1 | | | | | | VERTICAL |
| *EP506 | 5 | 6 | 7 | 5 | 118 | 5 | 9 | 1 | 8 | | | | | | VERTICAL |
| *EP507 | 5 | 3 | 2 | 7 | 118 | 1 | 6 | 1 | 1 | | | | | | TOP OF END POST |
| *EP508 | 5 | 2 | 7 | 0 | 118 | 5 | 9 | 1 | 3 | | | | | | VERTICAL |
| *EP509 | 5 | 2 | 3 | 11 | 118 | 2 | 10 | 1 | 1 | | | | | | VERTICAL |
| *EP510 | 5 | 1 | 6 | 7 | 104 | 4 | 8 | 1 | 11 | 0 | 5 | | | | HORIZONTAL |
| *EP601 | 6 | 6 | 5 | 6 | 118 | 4 | 6 | 1 | 0 | | | | | | VERTICAL |
| EP602 | 6 | 8 | 4 | 4 | 118 | 3 | 4 | 1 | 0 | | | | | | HORIZONTAL |
| EP603 | 6 | 6 | 6 | 11 | 102 | 3 | 7 | 1 | 8 | 1 | 8 | | | | VERTICAL |
| EP604 | 6 | 5 | 6 | 7 | 119 | 4 | 9 | 1 | 10 | 0 | 5 | | | | HORIZONTAL |
| EP605 | 6 | 5 | 7 | 9 | 118 | 6 | 8 | 1 | 1 | | | | | | HORIZONTAL |
| *EP606 | 6 | 2 | 5 | 6 | 108 | 4 | 6 | | | | | | | | VERTICAL (180° STD. HOOK) |
| EP607 | 6 | 2 | 6 | 1 | 102 | 3 | 7 | 1 | 3 | 1 | 3 | | | | VERTICAL |

| MARK | SIZE | NO | LENGTH | | TYPE | A | | B | | C | | D | | INCR. | REMARKS |
|---|------|----|--------|----|------|----|----|----|----|----|----|---|---|-------|-------------------------------|
| | | | FT | IN | | FT | IN | FT | IN | FT | IN | | | | |
| SOUTH ABUTMENT STEM AND BACKWALL | | | | | | | | | | | | | | | |
| *B501 | 5 | 23 | 18 | 7 | STR | | | | | | | | | | VERTICAL - NEAR FACE |
| *B502 | 5 | 14 | 5 | 6 | 118 | 3 | 6 | 2 | 0 | | | | | | ABUTMENT SEAT |
| *B503 | 5 | 14 | 4 | 6 | STR | | | | | | | | | | BACKWALL - NEAR FACE |
| *B504 | 5 | 14 | 5 | 9 | 101 | 1 | 3 | 2 | 3 | | | | | | BACKWALL - CAP |
| *B505 | 5 | 12 | 3 | 2 | 118 | 1 | 8 | 1 | 6 | | | | | | BACKWALL - FAR FACE |
| *B506 | 5 | 12 | 6 | 2 | 115 | | | | | 4 | 0 | 2 | 2 | | BACKWALL - FAR FACE |
| *B507 | 5 | 17 | 7 | 9 | STR | | | | | | | | | | CORNER VERTICAL |
| B508 | 5 | 2 | 6 | 0 | STR | | | | | | | | | | BACKWALL VERTICAL @ CORNER |
| *B509 | 5 | 3 | 5 | 0 | 101 | 3 | 0 | 1 | 0 | | | | | | APPROACH SLAB SEAT DOWEL |
| *B510 | 5 | 18 | 14 | 3 | STR | | | | | | | | | | HORIZONTAL - NEAR FACE |
| *B511 | 5 | 18 | 9 | 9 | 115 | | | 2 | 3 | 5 | 6 | 2 | 0 | | HORIZONTAL - NEAR FACE |
| B512 | 5 | 17 | 12 | 6 | STR | | | | | | | | | | HORIZONTAL - FAR FACE |
| B5 3 | 5 | 17 | 8 | 0 | 118 | 5 | 0 | 3 | 0 | | | | | | CORNER HORIZONTAL - FAR FACE |
| B514 | 5 | 18 | 4 | 0 | 118 | 2 | 0 | 2 | 0 | | | | | | CORNER HORIZONTAL - FAR FACE |
| B515 | 5 | 1 | 18 | 6 | STR | | | | | | | | | | HORIZONTAL - FAR FACE |
| B516 | 5 | 3 | 13 | 4 | STR | | | | | | | | | | HORIZONTAL - FAR FACE |
| B517 | 5 | 3 | 7 | 9 | STR | | | | | | | | | | CORNER HORIZONTAL - FAR FACE |
| B518 | 5 | 7 | 3 | 10 | 118 | 2 | 6 | 1 | 4 | | | | | | CORNER HORIZONTAL - FAR FACE |
| *B519 | 5 | 4 | 7 | 2 | 115 | | | 1 | 0 | 5 | 6 | 0 | 8 | | CORNER HORIZONTAL - NEAR FACE |
| *B520 | 5 | 4 | 4 | 2 | 118 | 3 | 6 | 0 | 8 | | | | | | CORNER HORIZONTAL - NEAR FACE |
| *B521 | 5 | 4 | 14 | 6 | STR | | | | | | | | | | BACKWALL HORIZ. - NEAR FACE |
| *B522 | 5 | 2 | 17 | 6 | STR | | | | | | | | | | BACKWALL HORIZ. - FAR FACE |
| *B523 | 5 | 8 | 8 | 9 | 118 | 7 | 3 | 1 | 6 | | | | | | ABUTMENT SEAT HORIZONTAL |
| *B524 | 5 | 22 | 3 | 0 | STR | | | | | | | | | | OLD/NEW ABUTMENT DOWEL |
| B525 | 5 | 23 | 3 | 0 | STR | | | | | | | | | | OLD/NEW ABUTMENT DOWEL |
| *B526 | 5 | 2 | 5 | 6 | 118 | 4 | 0 | 1 | 8 | | | | | | BACKWALL CORNER - TOP |

| SOUTH APPROACH SLAB | | | | | | | | | | | | | | | |
|----------------------------|---|----|----|---|-----|--|--|--|--|--|--|--|--|--|---------------|
| AS400 | 4 | 16 | 13 | 4 | STR | | | | | | | | | | APPROACH SLAB |
| AS600 | 6 | 28 | 15 | 2 | STR | | | | | | | | | | APPROACH SLAB |



NOTE:
PAYMENT FOR THREADED COUPLINGS SHALL BE INCIDENTAL TO ITEMS 503.14 AND 503.15 OR 503.12 AND 503.13.

NOTE:
* INDICATES EPOXY COATED REINFORCEMENT

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|--------|----------|----|------|
| Scale: | | | |
| No. | Revision | By | Date |
| | | | |
| | | | |

Designed by:

HNTB
ARCHITECTS ENGINEERS PLANNERS

| | | | | | |
|----------|-----|-------|--------------|-----|-------|
| Designed | By | Date | Checked | By | Date |
| | SJC | 11/99 | | JFW | 11/99 |
| Drawn | JFW | 11/99 | In Charge of | RAL | 11/99 |

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**MAINE TURNPIKE AUTHORITY
MODERNIZATION AND WIDENING PROJECT**

**BRIDGE REPLACEMENT
WELLS INTERCHANGE OVERPASS
REINFORCING STEEL SCHEDULE II**

CONTRACT: 2000.03

SHEET NUMBER: WI-S21
138 OF 178