

MAINE TURNPIKE AUTHORITY

ADDENDUM NO. 2

CONTRACT 2024.01

PAVEMENT REHABILITATION

EXITS 1, 2, & 3

MM 1.3 TO MM 6.8

BRIDGE REPAIRS

SPRUCE CREEK

MM 2.2

BROADCAST SEALANT

YORK RIVER BRIDGE

MM 5.2

The bid opening date is Tuesday December 12, 2023 at 11:00 am.

The following changes are made to the Proposal, Specifications, and Plans.

GENERAL

All questions regarding Contract 2024.01 should be submitted by the 3:00 pm on Tuesday December 5, 2023, to be answered in the last addendum to be issued on Friday December 8, 2023, if necessary. Questions received after that time may not be answered.

PROPOSAL

- Proposal Sheet P-2 is deleted and replaced with Proposal Sheet P-2, included in this addendum. The revision to this proposal sheet increases the quantity of Item 202.2026 – Removing Pavement Surface – Drainage Paths.

SPECIAL PROVISIONS

- Page SP-10, Section 107.1 (Contract Time and Contract Completion Date): The first paragraph is deleted and replaced with the following:

All work shall be complete on or before July 18, 2025. The construction shall be substantially complete by June 27, 2025.

- Page SP-10, Section 107.4.6 (Prosecution of Work): The third paragraph is deleted and replaced with the following:

Milling of mainline shall not begin until the following activities have been completed for each work area:

- Page SP-11, Section 107.4.6 (Prosecution of Work): The following paragraph is added before the first paragraph:

Milling of ramp shall not begin until the following activities have been completed for each work area:

- All drainage work including riprap installation
- All guardrail work
- Berm correction

- Page SP-11, Section 107.4.6 (Prosecution of Work): The following paragraph is added:

Following the completion of the initial utility locate, the Contractor will GPS locate all utilities within the project limits and provide a copy of the Dig Safe records to the Authority. The Contractor, acting as the Authority's third-party locator, shall be responsible for remarking all Maine Turnpike Facilities when a Dig Safe utility is called for the project. This work shall be considered incidental to the Contract.

- Page SP-11, Section 107.4.7 (Limits of Operations): The first paragraph is deleted and replaced with the following:

Mainline work (including Spruce Creek bridge repairs) south of MM 3.1 (Cutts Road) shall not occur while ramp work at Exits 1, 2, & 3 is ongoing, unless previously approved by the Authority.

- Page SP-66, Section 424.02: Delete the first paragraph and replace with the following:

Elastoflex CA Type 4 shall be supplied by Maxwell Products or an approved equal designed especially for improving the strength and performance of the base asphalt cement with sealant.

- Page SP-69, Section 427.02: Delete the third paragraph and replace with the following:

Hot rubberized asphalt shall conform to ASTM D6690 Tyle IV.

- Page SP-151, Section 627.05: Delete the fourth paragraph and replace with the following:

When newly laid in the grove, tamp the tape thoroughly with a minimum of six passes (three passes back and forth), using an RTC-2 Tamper Cart with a minimum weight of 200 pounds. Alternate methods of tamping must be approved by the resident in advance. A "Peel Test" will be completed on the first day of application to verify proper adhesion.

- Page SP-155, Section 652 (Specific Project Maintenance of Traffic Requirements): Delete the third paragraph and replace with the following:

When installing construction signs along the turnpike northbound median south of the Route 236 underpass, the Contractor may use the MTA Traffic Control Detail 35L (Mobile Operation – Passing Lane Closure) with a minimum of three (3) shadow vehicles, each equipped with an arrow board and a TMA.

- Page SP-155, Section 652 (Specific Project Maintenance of Traffic Requirements): Delete the last paragraph and replace with the following:

Temporary lane closures that would restrict travel to one lane in any direction shall be conducted at night between the times presented in the tables below. Liquidated damages shall be assessed at \$1,000/ five minutes for every five minutes that a temporary lane closure is in place outside the times presented in the tables below.

PLANS

- Plan Sheet EQ-01 (2 of 40) change the quantity of item 202.2026 from 150 SF to 500 SF.
- Plan Sheet TS-01 (4 of 40) change the hot rubberized asphalt from SS-S-1401C to ASTM D6690 Type IV.
- Plan Sheet DT-04 (26 of 40) change the width of the drainage path from 18” on grade and 4 ft at sags to 4 ft on grade and 6 ft at sags. Milling depth changed from 1.5” to 1.75”.

QUESTIONS

1. Question: Does material removed from cleaning catch basins need to be stockpiled on site and tested prior to disposal?

Answer: Catch basin grit shall be disposed of offsite in accordance with Chapter 418 of the MaineDEPs Maine Solid Waste Management Rules. If hazardous materials are encountered they shall be tested and disposed of in accordance with section 105.8.4 of the Supplemental Specifications.

ATTACHMENTS

- Addendum No. 2 (4 pages)
- Pre-Bid Agenda (8 pages)
- Pre-Bid Sign-In Sheet (1 page)
- Proposal Sheets (1 pages)
- Plan Sheets (3 pages)

Notes: The above items shall be considered as part of the bid submittal.

The total number of pages included with this addendum is seventeen pages (17).

All bidders are requested to acknowledge the receipt of the Addendum No. 2 by signing below and faxing this sheet to Nathaniel Carll, Purchasing Department, Maine Turnpike Authority at 207-871-7739. Bidders are also required to acknowledge receipt of this Addendum No. 2 on Page P-13 of the bid package.

Business Name

Print Name and Title

Signature

Date
December 1, 2023

Very truly
yours,

MAINE TURNPIKE AUTHORITY

Nathaniel Carll
Purchasing Department
Maine Turnpike Authority

MAINE TURNPIKE AUTHORITY

Pre-Bid Conference

CONTRACT 2024.01

PAVEMENT REHABILITATION

EXITS 1, 2, & 3
MM 1.3 TO MM 6.8

BRIDGE REPAIRS

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MM 2.2

BROADCAST SEALANT

YORK RIVER BRIDGE
MM 5.2

NOVEMBER 28, 2023, 10:00 A.M.

1) Location:

The general limits of work are as shown on the Contract Plans and are:

- Pavement Rehabilitation - Exits 1, 2, & 3 Ramps & MM 1.3 to MM 6.8 (Northbound and Southbound) in the towns of Kittery and York
- Bridge Repairs- Spruce Creek- MM 2.2
- Broadcast Sealant – York River Bridge – MM 5.2

2) General Description:

The work consists of milling and paving the Exits 1, 2, and 3 interchange ramps and mainline from MM 1.3 to MM 6.8, drainage repairs, guardrail, median grading, repairs to the Spruce Creek bridge at MM 2.2, broadcast sealant of York River bridge at MM 5.2, maintenance of traffic, and all other work incidental thereto in accordance with the Plans and Specifications.

3) Bid:

- a) Opening: December 12, 2023 at 11:00 A.M. at MTA Headquarters 2360 Congress Street, Portland.
- b) All bid and contractual questions shall be directed to Nate Carll. Phone No.: (207) 482-8115. E-Mail: ncarll@maineturnpike.com.
- c) All questions on plans and specifications shall be in writing and shall be directed to Nate Carll, Purchasing Manager, of the Maine Turnpike Authority. Fax No. (207) 871-7739. Email ncarll@maineturnpike.com
- d) Bids will be accepted from prime Contractors **prequalified** by the Maine Department of Transportation for Highway Construction Projects with a bridge subcontractor **prequalified** by the Maine Department of Transportation for Bridge Construction Projects.

- 4) Notification:
 - a) Contractor shall notify and obtain approval from the Authority prior to visiting the Project sites for field inspection. The contact person is Mr. Steve Tartre at startre@maineturnpike.com
- 5) Contract Specifications
 - a) The Specifications are divided into three parts: Part I, Supplemental Specifications, Part II, Special Provisions, Part III, Appendices.
 - b) The Maine Turnpike Supplemental Specifications are additions and alterations to the 2014 Maine Department of Transportation Standard Specifications and are available on MTA's website.
- 6) Construction Schedule/Substantial Completion:
 - a) December 21, 2023 – Contract Award Date
 - b) November 15, 2024 – Mainline Paving Complete
 - c) May 30, 2025 – Substantial Completion
 - d) June 27, 2025 – Contract Completion Date.
- 7) Holidays (Special Provision 101.2)
 - a) Add the day of Juneteenth
- 8) Maine Department of Labor – Fair Hourly Wages (Special Provision 104.3.8)
 - a) Wage rates for Highway and Earthwork and for Heavy and Bridge for York County are included in the Special Provisions.
- 9) Cooperation With Other Contractors (Special Provision 104.4.7):
 - A. MTA Contract 2024.06 – Spur Road Underpass (MM 6.8)
 - B. MaineDOT WIN 026624.00 – Rehabilitation of Kittery Visitor Center Access Road and Parking Lot
 - C. MaineDOT WIN 026768.00 – Sanford Area Light Capital Paving
 - D. NHDOT Contract #16189B – Part Time Shoulder Use
- 10) Permit Requirements (Special Provision 105.8.2)
 - a) The Project is subject to the Stormwater Memorandum of Agreement for Stormwater Management Between the Maine Department of Transportation, Maine Turnpike Authority, and Maine Department of Environmental Protection (Stormwater MOA). Under the Stormwater MOA, all MTA construction, operation, and maintenance activities are subject to Maine Stormwater Law Basic Standards through implementation of MaineDOT's Best Management Practices for Erosion and Sedimentation Control (MaineDOT BMP Manual), which are the Contractor's responsibility to implement.
 - b) The LOD for this Contract has been estimated to be 4.3 acres (median restoration, berm correction, five pipe end replacements), the majority of which is considered "routine maintenance under the

Maine Construction General Permit. The Contractor shall prepare a LOD plan illustrating the Contractor's proposed limit of earthwork disturbance. This Plan shall be submitted for review and approval, to the Resident within 14 days of Contract award

- c) Maine Pollutant Discharge Elimination System (MPDES) General Permit for Stormwater Discharge from Construction Activity shall be followed.
- d) The project is within an MS4 Area and the Contractor will be required to follow and sign MTA Contractor Stormwater Awareness Affidavit provided in Appendix I of the Special Provisions. MS4 compliance requires all Contractors to be properly trained in Erosion and Sedimentation Control (ESC) measures (as per Special Provision Subsections 105.8.1 and 656.07) and implement measures to reduce pollutants in stormwater runoff from construction activities.

11) Wetland and Water Body Impacts (Special Provision 105.8.3)

- a) No temporary or permanent fill, mechanized excavation, or mechanized equipment is permitted within Spruce Creek.

12) As-Built Plans (Special Provision 105.11)

- a) The Contractor shall provide the Authority with as-built plans in PDF and MicroStation, or other approved CADD formats. The as-built plans shall note changes to the bid documents, including, but not limited to pavement, concrete, barrier, guardrail, culverts, drainage, foundations, wiring, signs, etc. The as-builts shall also provide GPS accurate locations of all underground work. Submittal of Draft, Final Draft, and 100% as-built plans to the resident shall be conditions of Mobilization payment, Retainage Reduction, and Final Payment as noted in Special Provision 108.

13) Prosecution of Work (107.4.6)

- a) Milling of ramps or mainline shall not begin until the following activities have been completed for each roadway work area:
 - i) All outside and median drainage work, including riprap installation, in the paving section
 - ii) All guardrail work except closing median openings
 - iii) Berm correction
 - iv) Spruce Creek Bridge repairs excluding asphaltic plug joints
 - v) York River Bridge broadcast sealant
 - vi) The Contractor shall verify existing cross slopes of each lane and shoulder at least every ½ mile and provide cross slope data to the resident at least 14 days prior to beginning milling of each roadway.
- b) The asphaltic plug joints for the Spruce Creek Bridge shall not be installed until the approach and bridge deck milling and paving is complete.
- c) Ramp D (Exit 2 Southbound Off Ramp) shall not be closed until on or after June 15, 2024.
- d) The existing median openings at MM 4.5 and MM 5.9 shall not be closed until after the Authority has installed new up-down gates with remote controls at the Cider Hill Road EVRs. The Authority

anticipates the up-down gates will be installed by July 2024. Closing of median openings may occur after milling and paving is complete in the area of each median opening.

14) Limits of Operations (Special Provision 107.4.7)

- a) Mainline work (including Spruce Creek bridge repairs) south of MM 3.1 (Cutts Road) shall not occur while ramp work at Exits 1, 2, & 3 is ongoing.
- b) The following ramps may not be closed at the same time:
 - i) Ramps B and E
 - ii) Ramp B and Ramps J and K
 - iii) Ramps C and G
 - iv) Ramp E and Ramps J and K
 - v) Ramp A and Ramps C and G
- c) Ramp closures will be allowed as noted below. Ramps shall be opened to traffic when work is not occurring. Nighttime closure times shall be in accordance with Section 652.
 - i) Ramp A – Daytime or Nighttime
 - ii) Ramp B – Daytime or Nighttime
 - iii) Ramp C – Nighttime or daytime on or before May 23 or on or after September 3
 - iv) Ramp D – Nighttime (see Section 107.4.6)
 - v) Ramp E – Nighttime
 - vi) Ramps G & H – Nighttime
 - vii) Ramps J & K - Nighttime or daytime on or before May 23 or on or after September 3
 - viii) Ramps M & L – Nighttime. A single lane closure on Ramp M will be allowed during the daytime shift prior to a nighttime ramp closure
 - ix) Southbound C-D Roadway – Lane 1 (thru lane) – Daytime or Nighttime
 - x) Southbound C-D Roadway – Lane 2 (merge lane) –Nighttime
- d) The construction shall proceed expeditiously. Once milling and/or paving operations commence, for every day/night not worked (milling or paving) when work is allowed by Contract and weather, the Contractor will be charged a fee in the amount of \$1,000 (excluding inclement weather days).
- e) Once the surface pavement of Spruce Creek Bridge is milled, it shall be paved for a minimum of 25 ft beyond each joint of the bridge within seven calendar days. For each calendar day beyond the seven days that the bridge and approaches paving is not complete, the Contractor will be charged a fee in the amount of \$1,000 (excluding inclement weather days).
- f) The Contractor will be allowed to work on both bounds and/or multiple ramps at the same time except as noted above. The Contractor shall complete their milling operation in one location

before beginning their milling operation in another location, unless otherwise approved by the resident. The paving operation shall begin within seven calendar days of milling being completed in that location. The Contractor shall complete their paving operation in one location prior to beginning their paving operation in another location. The Contractor will be allowed to work in two separate areas of each mainline bound. The work areas are not required to be in the same lane. The Contractor shall follow approved maintenance of traffic plans showing the work areas.

- g) The Contractor shall begin the mainline milling operation in Lane 3 (travel lane) followed by Lane 2, and then Lane 1 (passing lane). The Contractor shall begin the C-D Roadway milling operation in Lane 1 (thru lane), followed by Lane 2 (merge lane).
- h) The Contractor shall limit the milling operations such that the shoulders are milled up to the same station as the adjacent lane during each shift.
- i) The Contractor shall limit the milling operations such that temporary pavement markings or pavement markers are applied daily prior to the roadway being opened to traffic.
- j) Temporary bituminous ramps will be required at the end of each milled lane.
- k) Traffic will be allowed to traverse the longitudinal joint where pavement is lower in one lane than the adjacent lane.
- l) The Contractor shall begin the mainline paving operation in Lane 1 (passing lane) followed by Lane 2, and then Lane 3 (travel lane). The Contractor shall begin the C-D Roadway paving operation in Lane 2 (merge lane), followed by Lane 1 (thru lane).
- m) Permanent pavement markings shall be laid out for painting once location is completely paved so that the MTA can paint within two weeks.
- n) Lane closure(s) will not be allowed over a weekend or Holidays unless otherwise approved by the resident.
- o) The Contractor shall keep a 12 foot wide lane open for traffic during milling and paving operations unless otherwise approved by the resident.
- p) Temporary shoulder closures are prohibited northbound between Memorial Day and Labor Day on Thursdays from 3 PM to 6 PM and Fridays 12 PM to 8 PM.

15) Mobilization Payment (Special Provision 108.2.3)

- a) The second paragraph is deleted in its entirety and replaced with the following: Upon approval of all pre-construction submittals required for approval by this Contract, including those listed in Section 104.4.2 – Preconstruction Conference, the Contractor will receive payment of 50% of the Lump Sum price for Mobilization, not to exceed 5% of the Bid less the amount bid for Mobilization. After the Authority determines that the Work is 50% complete and the Contractor has submitted a Draft (50%) as-built submittal of all underground work to date (within the prior 30 day pay period) as defined in Special Provision 105., the Contractor will receive the other 50% of the Lump Sum price for Mobilization, not to exceed 5% (10% total for both payments) of the Bid less the amount bid for Mobilization. Any remaining Mobilization will be paid upon completion of physical work.

16) Retainage (Special Provision 108.3)

- a) The seventh paragraph is deleted in its entirety and replaced with the following: When requested by the Contractor an 80 percent reduction of retainage will be considered by the Authority when the Project is substantially complete and the Contractor has submitted a Final Draft (98%) as-built submittal of all underground work, in accordance with Special Provision 105. When requesting a reduction, the Contractor shall include an explanation of the outstanding Work, and estimate of the cost to complete the Work, and a schedule for completing the Work. Seasonal limitations as well as warranty and establishment periods (for vegetation) shall be addressed.

17) Price Adjustment for Diesel Fuel (Special Provision 108.4.2)

- a) A price adjustment for diesel fuel will be made based on the variance in costs using the weekly retail diesel price for the New England area as listed on the Energy Information Administration's webpage as noted in Special Provision 108.4.2

18) Final Payment (Special Provision 108.8)

- a) The first paragraph is deleted in its entirety and replaced with the following: Following conditional acceptance of the physical Work under subsection 107.9.3, and submission of 100% As-built plans to the Resident, in accordance with Special Provision 105, the Authority will prepare a final Invoice reflecting final quantities of the items of Work performed. The Authority may require the Contractor to provide information necessary to substantiate Pay Items, including Statements itemizing Force Account Work. The Authority will make final payment upon approval of the Authority's board, in the amount of the Work done, less all previous payments and all amounts to be retained or deducted under the provisions of the Contract. For a related provision, see Section 107.9.5 – Final Acceptance.

19) Specific Contract Items

a) Section 202 – Removing Structures and Obstructions

- i) A 12 ft rotary drum shall be used for removing pavement surface for all mainline travel lanes.
- ii) Milling machine for mainline shoulders shall be wide enough to remove entire width of shoulder pavement in a single pass.

b) Section 401 – Hot Mix Asphalt

- i) Hydrated lime may be provided as an option in all mixtures denoted in Special Provision 403 – Hot Mix Asphalt Pavement. No Separate payment will be made for the use of hydrated lime.

c) Section 403 – Hot Mix Asphalt Pavement

- i) The required PGAB for Item 403.2081 (12.5 m surface, polymer modified) shall be 70E-28.

d) Section 409 – Bituminous Tack Coat

- i) Tack coat on this project shall be trackless.
- ii) The equipment shall be maintained to provide a consistent application rate and uniform spray that is breaking correctly and is free from clotting throughout the project. If the product is not breaking correctly or if clotting is observed, the contractor shall cease production until necessary maintenance can be completed.

e) Section 627- Pavement Markings

- i) Grooving for pavement marking tape shall use gang stacked diamond cutting heads with free floating, independent heads for asphalt pavement surfaces.
- ii) When newly laid in the groove, tamp the tape thoroughly with a minimum of six passes (three passes back and forth), using an RTC-2 Tamper Cart. Alternate methods of tamping must be approved by the resident in advance.

f) Section 652 – Maintenance of Traffic – General

- i) When installing construction signs along the turnpike northbound median south of the Route 236 underpass, the Contractor shall use the MTA Traffic Control Detail 35L (Mobile Operation – Passing Lane Closure) with a minimum of three (3) shadow vehicles, each equipped with an arrow board and a TMA.
- ii) Where traffic control signs and/or channelizing devices are required on or south of the Piscataqua River Bridge (High Level Bridge), the Contractor shall coordinate the placement of signs and channelizing devices with the New Hampshire Department of Transportation (NHDOT) Transportation Management Center (TMC) at 603-271-6862 and the New Hampshire State Police at 603-223-3861.
- iii) Flaggers may be used only on local roads to allow for milling and paving of ramps adjacent to the roadway.
- iv) Temporary lane closures that would restrict travel to one lane in any direction shall be conducted at night between the times presented in the tables in Section 652. Liquidated damages shall be assessed at \$1,000/minute for every minute that a temporary lane closure is in place outside the times presented in the table below.
- v) Work zone speed limits shall only be in place when work is actively occurring in the work zone.
- vi) Equipment moves will only be allowed during the same times as temporary lane closures.
- vii) Portable light towers will be required to illuminate the night construction work area.
- viii) 652 SP replaces the MTA 2016 Supplemental Specification Section 652, substantive revisions have been “bolded”.
- ix) Cones may be used on all tangent portions of mainline lane and shoulder closures. Drums must be used on all tapers.
- x) The Contract will furnish, operate, and maintain at Automated Trailer Mounted Speed Limit Signs for project use. The automated speed sign shall be required when there is a Work Zone Speed Limit in place. Payment for Automated Trailer Mounted Speed Limit Signs shall be incidental to the Maintenance of Traffic Control Devices item.
- xi) Daytime lane closures shall be a maximum of three (3) miles.
- xii) The maximum time for which traffic may be stopped and held for an equipment move across mainline or ramp and any single time shall be five (5) minutes. The Contractor shall reimburse the Authority at a rate of \$500 per minute for each minute in excess of the five-minute allowance.

20) Addendum #1 Items

a) Section 403 – Hot Mix Asphalt Pavement

i) Page SP-56, Section 403.02 General: The first paragraph is deleted and replaced with the following:

(1) The Contractor shall compose the Hot Mix Asphalt Pavement with aggregate, Performance Graded Asphalt Binder (PGAB), and mineral filler if required. The Performance Graded Asphalt Binder (PGAB) shall be polymer modified as detailed in this special provision and shall conform to the requirements of AASHTO M 332. The PG70E-28 Binder shall be modified using Styrene-Butadiene-Styrene (SBS) polymer in a homogeneous blend. The stability of the modified binder shall be verified in accordance with ATSM D7173 using the Dynamic Shear Rheometer (DSR). The DSR $G^*/\sin(\delta)$ results from the top and bottom sections of the ATSM D7173 test shall not differ by more than 10%. The results of ASTM D7173 shall be included on the Certified Test Report.

b) Added P-Sheets

21) Addendum #2 Items

a) Prosecution of Work (107.4.6)

i) The following paragraph is added:

Following the completion of the initial utility locate, the Contractor will GPS locate all utilities within the project limits and provide a copy of the Dig Safe records to the Authority. The Contractor, acting as the Authority's third-party locator, shall be responsible for remarking all Maine Turnpike Facilities when a Dig Safe utility is called for the project. This work shall be considered incidental to the Contract.

22) Questions

a) Questions received prior to pre-bid conference will be answered with Addendum #1.

MAINE TURNPIKE AUTHORITY

Pre-Bid Conference

CONTRACT 2024.01

PAVEMENT REHABILITATION
EXITS 1, 2, & 3
MM 1.3 TO MM 6.8

BRIDGE REPAIRS
SPRUCE CREEK
MM 2.2

BROADCAST SEALANT
YORK RIVER BRIDGE
MM 5.2

November 28, 2023 10:00 AM

NAME (PRINT)	COMPANY	PHONE	EMAIL
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STEVE TAETRE	MTA		

SCHEDULE OF BID PRICES
CONTRACT NO. 2024.01
Pavement Rehabilitation Exits 1,2, & 3 and MM 1.3 - 6.8
Bridge Repairs Spruce Creek MM 2.2
Broadcast Sealant York River Bridge MM 5.2

Item No	Item Description	Units	Approx. Quantities	Unit Prices in Numbers		Bid Amount in Numbers	
				Dollars	Cents	Dollars	Cents
202.15	REMOVING EXISTING MANHOLE OR CATCH BASIN	Each	1				
202.202	REMOVING PAVEMENT SURFACE	Square Yard	78,900				
202.201	REMOVING PAVEMENT SURFACE - BRIDGE DECK	Square Yard	600				
202.205	REMOVING PAVEMENT SURFACE - MAINLINE	Square Yard	316,000				
202.206	REMOVING PAVEMENT SURFACE - DRAINAGE PATHS	Square Foot	500				
202.205	RUMBLE STRIPS	Each	96,000				
203.20	COMMON EXCAVATION	Cubic Yard	16				
211.50	MEDIAN RESTORATION	Linear Foot	10,400				
403.2081	HOT MIX ASPHALT, 12.5 mm (POLYMER/LATEX MODIFIED)	Ton	36,758				
403.20811	HOT MIX ASPHALT, 12.5 mm (POLYMER/LATEX MODIFIED) - RAMPS	Ton	1,450				
403.211	HOT MIX ASPHALT, 9.5 mm, SHIMMING	Ton	1,100				

CARRIED FORWARD:

Date: 11/30/2023

ESTIMATED QUANTITIES							
ITEM NO.	ITEM	UNIT	PAVING	BRIDGE REPAIR	GUARDRAIL	DRAINAGE	TOTAL
202.15	REMOVING EXISTING MANHOLE OR CATCH BASIN	EA				1	1
202.202	REMOVING PAVEMENT SURFACE	SY	78,900				78,900
202.2021	REMOVING PAVEMENT SURFACE - BRIDGE DECK	SY		600			600
202.2025	REMOVING PAVEMENT SURFACE - MAINLINE	SY	316,000				316,000
202.2026	REMOVING PAVEMENT SURFACE - DRAINAGE PATHS	SF	500				500
202.205	RUMBLE STRIPS	EA	96,000				96,000
203.20	COMMON EXCAVATION	CY	6			10	16
211.50	MEDIAN RESTORATION	LF	10,400				10,400
403.2081	HOT MIX ASPHALT, 12.5 mm (POLYMER/LATEX MODIFIED)	TON	36,700	58			36,758
403.20811	HOT MIX ASPHALT, 12.5 mm (POLYMER/LATEX MODIFIED) - RAMPS	TON	1450				1450
403.211	HOT MIX ASPHALT, 9.5 mm, SHIMMING	TON	1100				1100
403.213	HOT MIX ASPHALT, 12.5 mm (BASE AND INTERMEDIATE BASE COURSE)	TON	3				3
409.152	BITUMINOUS TACK COAT NTSS - IHM TRACKLESS - APPLIED	GAL	24,600	24			24,624
419.30	SAWING BITUMINOUS PAVEMENT	LF	300				300
424.323	ASPHALT RUBBER FIBER CRACK SEALER	LB	40,000				40,000
424.324	ASPHALT RUBBER MASTIC CRACK SEALER - APPLIED	LB	10,000				10,000
427.09	PAVEMENT CRACK REPAIR	LF	1950				1950
459.06	BITUMINOUS CONCRETE WATERWAY, TYPE I	EA				4	4
459.061	BITUMINOUS CONCRETE WATERWAY, TYPE II	EA				2	2
470.08	BERM DROPOFF CORRECTION - GRINDINGS	TON	900				900
470.081	BERM CORRECTION	LF	9000				9000
470.082	BERM CORRECTION - OUTSIDE	LF	8500				8500
502.702	REPLACE BRIDGE DRAIN (TYPE A)	LS		4			4
502.703	INSTALL OR REPLACE BRIDGE DRAIN (TYPE B)	LS		6			6
515.201	PIGMENTED PROTECTIVE COATING FOR CONCRETE SURFACES	SY		350			350
515.202	CLEAR PROTECTIVE COATING FOR CONCRETE SURFACES	SY		130			130
515.203	BROADCAST SEALANT FOR CONCRETE SURFACES	SY		9100			9100
518.10	ABUTMENT REPAIRS	SF		110			110
518.391	REPAIRING GRANITE CURB JOINT AND BEDDING MORTAR	LF		35			35
518.4	EPOXY INJECTION CRACK REPAIR	LF		20			20
518.70	REPAIR OF OVERHEAD SURFACES <8 INCHES	SF		40			40
518.865	ELASTOMERIC CONCRETE	CF		10			10
520.2301	ASPHALTIC PLUG JOINT (SPRUCE CREEK NB)	LF		125			125
520.2302	ASPHALTIC PLUG JOINT (SPRUCE CREEK SB)	LF		125			125
520.234	EXPANSION DEVICE - SILICONE COATED AND PRE-COMPRESSED SEAL	LF		50			50
523.521	BEARING REMOVAL AND INSTALLATION	EA		4			4
523.5301	STEEL BEARINGS, FIXED, SLIDING PLATE	EA		2			2
523.5302	STEEL BEARINGS, EXPANSION, SLIDING PLATE	EA		2			2
524.7212	JACKING EXISTING SUPERSTRUCTURE (SPRUCE CREEK)	LS		1			1
526.306	TEMPORARY CONCRETE BARRIER, TYPE I - SUPPLIED BY THE AUTHORITY	LS		1			1
527.341	WORK ZONE CRASH CUSHIONS - TL-3	UN		2			2
603.155	12 INCH REINFORCED CONCRETE PIPE - CLASS III	LF				16	16
603.159	12 INCH CULVERT PIPE OPTION III	LF				8	8
603.169	15 INCH CULVERT PIPE OPTION III	LF				16	16
603.175	18 INCH REINFORCED CONCRETE PIPE - CLASS III	LF				16	16
603.28	CONCRETE COLLAR	EA				3	3
604.182	CLEANING EXISTING CATCH BASIN AND MANHOLE	EA				60	60
604.184	REBUILD CATCH BASIN TO GRADE - TYPE II	EA				21	21
604.246	CATCH BASIN TYPE F5	EA				1	1
606.13	3"W-BEAM GUARDRAIL - MID-WAY SPLICE (7' STEEL POSTS, 8' OFFSET BLOCKS, SINGLE FACED)	LF			850		850
606.1307	3"W-BEAM GUARDRAIL - MID-WAY SPLICE FLARED TERMINAL	EA			4		4
606.132	3"W-BEAM GUARDRAIL - MID-WAY SPLICE (7' STEEL POSTS, 8' OFFSET BLOCKS, DOUBLE FACED)	LF			990		990
606.1351	3"W-BEAM GUARDRAIL - MID-WAY SPLICE TERMINAL END - ANCHORED END	EA			32		32
606.1723	BRIDGE TRANSITION TYPE III	EA		1			1
606.178	GUARDRAIL BEAM	LF			100		100
606.352	REFLECTORIZED BEAM GUARDRAIL DELINEATOR	EA			640		640

ESTIMATED QUANTITIES							
ITEM NO.	ITEM	UNIT	PAVING	BRIDGE REPAIR	GUARDRAIL	DRAINAGE	TOTAL
606.356	UNDERDRAIN DELINEATOR POST	EA			40	7	47
606.3621	GUARDRAIL ADJUST, SINGLE RAIL	LF			15,700		15,700
606.3622	GUARDRAIL ADJUST, DOUBLE RAIL	LF			11,400		11,400
606.471	SINGLE OFFSET BLOCK - W BEAM	EA			40		40
606.48	SINGLE GALVANIZED STEEL POST	EA			10		10
606.701	ASYMMETRIC THRIE BEAM TRANSITION	EA			5		5
609.40	RESET CURB TYPE 5	LF	70				70
610.08	PLAIN RIPRAP	CY				25	25
613.319	EROSION CONTROL BLANKET	SY	12,100				12,100
618.14	SEEDING METHOD NUMBER 2	UNIT	210				210
619.1201	MULCH - PLAN QUANTITY	UNIT	210				210
619.1202	TEMPORARY MULCH	LS	1				1
620.58	EROSION CONTROL GEOTEXTILE	SY				71	71
627.73	TEMPORARY 6 INCH PAVEMENT MARKING TAPE	LF	31,100				31,100
627.731	TEMPORARY 6 INCH BLACK PAVEMENT MARKING TAPE	LF	1150				1150
627.78	TEMPORARY PAVEMENT MARKING LINE, WHITE OR YELLOW	LF	275,000				275,000
627.812	TEMPORARY RAISED PAVEMENT MARKERS	EA	13,900				13,900
627.94	PAVEMENT MARKING TAPE	LF	3700				3700
627.941	PAVEMENT MARKING TAPE - DOTTED WHITE LANE LINE, 6-INCH WIDTH	LF	1600				1600
629.05	HAND LABOR, STRAIGHT TIME	HR	40				40
631.10	AIR COMPRESSOR (INCLUDING OPERATOR)	HR		40			40
631.11	AIR TOOL (INCLUDING OPERATOR)	HR		40			40
631.12	ALL PURPOSE EXCAVATOR (INCLUDING OPERATOR)	HR	40				40
631.133	SKID STEER (INCLUDING OPERATOR)	HR	20				20
631.171	TRUCK - SMALL (INCLUDING OPERATOR)	HR	40				40
631.172	TRUCK - LARGE (INCLUDING OPERATOR)	HR	20				20
631.22	FRONT END LOADER (INCLUDING OPERATOR)	HR	20				20
631.32	CULVERT CLEANER (INCLUDING OPERATOR)	HR	40				40
631.36	FOREMAN	HR	40				40
645.1099	REMOVE AND DISPOSE SIGN	EA	12				12
652.30	FLASHING ARROW	EA	7				7
652.312	TYPE III BARRICADES	EA	16				16
652.33	DRUM	EA	480				480
652.34	CONE	EA	425				425
652.35	CONSTRUCTION SIGNS	SF	5400				5400
652.361	MAINTENANCE OF TRAFFIC CONTROL DEVICES	LS	0.84	0.10	0.05	0.01	1
652.38	FLAGGERS	HR	80				80
652.41	PORTABLE-CHANGEABLE MESSAGE SIGN	EA	9				9
652.4502	TRUCK MOUNTED ATTENUATOR	EA	3				3
652.4503	TRUCK MOUNTED ATTENUATOR	WEEK	26				26
652.47	SEQUENTIAL FLASHING WARNING LIGHTS	EA	270				270
656.50	BALED HAY, IN-PLACE	EA	50				50
656.60	TEMPORARY BERMS	LF	300				300
656.62	TEMPORARY SLOPE DRAINS	LF	30				30
656.632	30' TEMPORARY SILT FENCE	LF	100		100		200
656.751	TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL - SPRUCE CREEK	LS		1			1
659.10	MOBILIZATION	LS	0.84	0.10	0.05	0.01	1


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Contract 2024.01
Addendum No. 2
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Scale: **NOT TO SCALE**

No.	Revision	By	Date
△	ADDENDUM #2 - REVISED QUANTITY	JRH	12/23

Designed by:



CONSULTANT PROJECT MANAGER: JOSEPH HOWE, P.E.

	By	Date	Checked	By	Date
Designed	JRH	10\23	Checked	JRH	10\23
Drawn	CHL	10\23	In Charge of	JRH	10\23

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RYAN BARNES, PE, CPESC

PAVEMENT REHABILITATION
EXITS 1, 2 & 3 AND MM 1.3 TO MM 6.8

ESTIMATED QUANTITIES

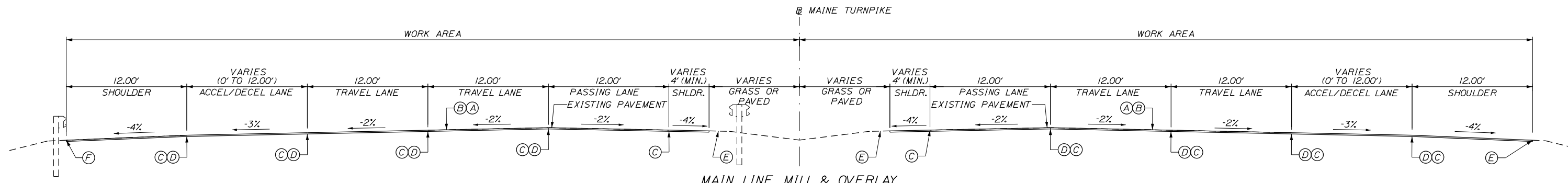
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CONTRACT: 2024.01

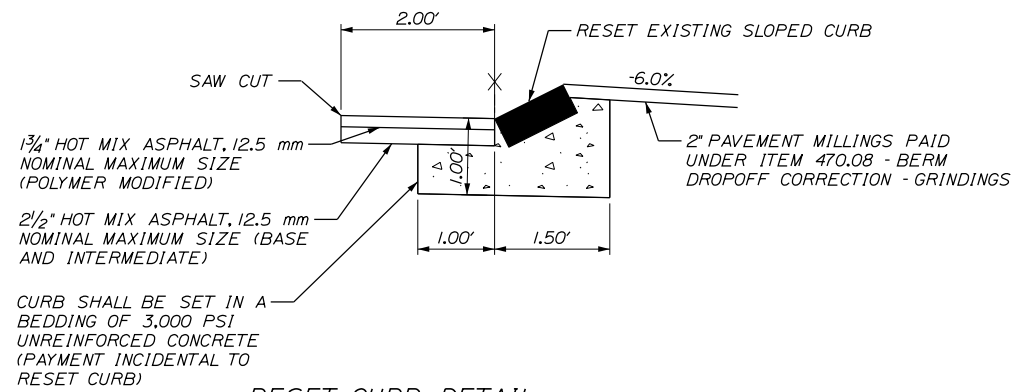
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Date: 11/30/2023

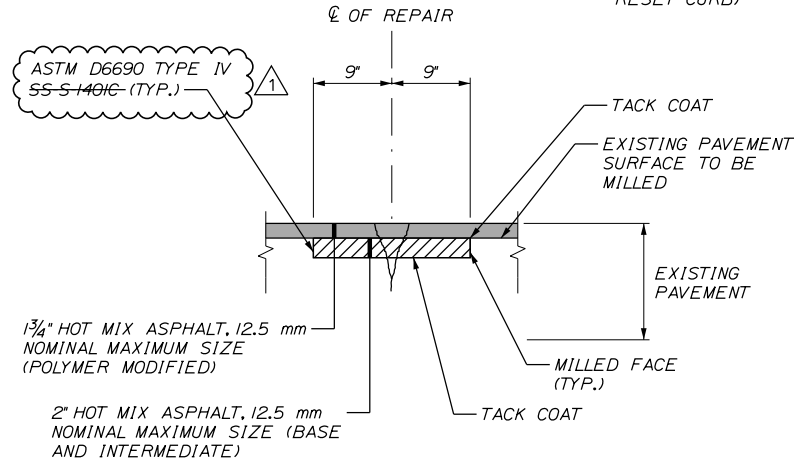
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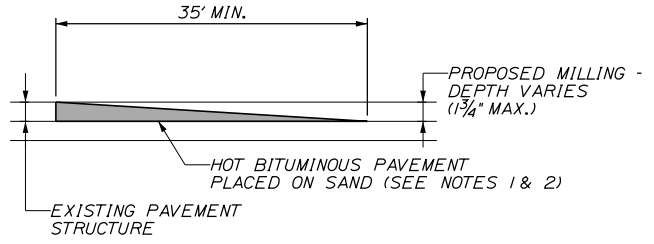
MAIN LINE MILL & OVERLAY



RESET CURB DETAIL



CRACK REPAIR DETAIL



TEMPORARY BITUMINOUS RAMP

NOTES:

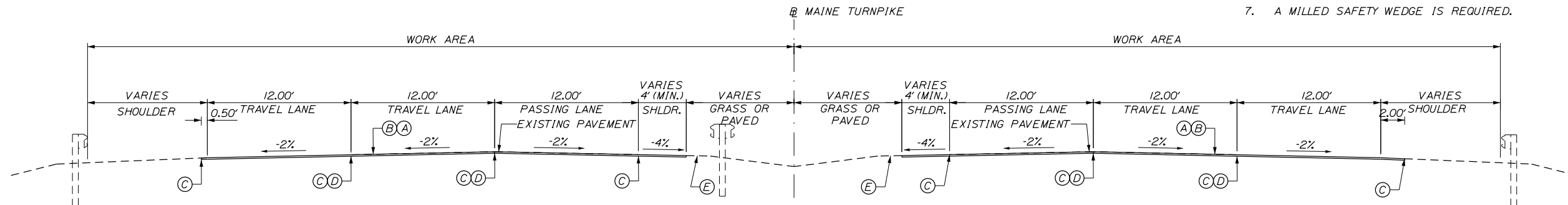
- HOT MIX ASPHALT FOR TEMPORARY RAMPS WILL NOT BE MEASURED FOR PAYMENT, BUT SHALL BE INCIDENTAL TO THE APPROPRIATE 403 ITEM.
- REMOVAL OF TEMP. BITUMINOUS RAMPS WILL NOT BE MEASURED FOR PAYMENT, BUT SHALL BE INCIDENTAL TO THE APPROPRIATE 403 ITEM.

PAVEMENT LEGEND:

- (A) = 1 3/4" HOT MIX ASPHALT 12.5 mm NOMINAL MAXIMUM SIZE (POLYMER MODIFIED)
- (B) = MILLING DEPTH VARIES TO ACHIEVE A 1 3/4" DEPTH AT THE EDGES OF LANE BEING MILLED.
- (C) = MILLING DEPTH VARIES AS CONTROLLED BY CROSS SLOPE AND RUT DEPTHS
- (D) = NOTCHED WEDGE JOINT
- (E) = BERM DROPOFF CORRECTION - GRINDINGS (SEE DETAIL A, SHEET DT-03).
- (F) = BERM DROPOFF CORRECTION - GRINDINGS SEE DETAIL B, SHEET DT-03).

NOTES:

- COATING OF HOT RUBBERIZED ASPHALT (SS-S1401G) SHALL BE APPLIED AT THE TOP SURFACE LIFT TO ALL TRANSVERSE BUTT JOINTS AND LONGITUDINAL JOINTS EXCEPT WHERE THE NOTCHED WEDGE IS USED.
- BITUMINOUS TACK COAT IS REQUIRED BETWEEN EXISTING PAVEMENT AND PROPOSED HMA AND BETWEEN ANY LIFTS OF PROPOSED PAVEMENT OR SHIM.
- THE CONTRACTOR SHALL STRING EACH BRIDGE AND TOLL SLAB APPROACH /DEPARTURE LANE IN ADVANCE OF MILLING AND PAVING OPERATIONS. THIS WORK SHALL BE COMPLETED BY THE CONTRACTOR'S PERSONNEL EXPERIENCED IN STRINGING BRIDGE APPROACH LANES. THE CONTRACTOR SHALL PROVIDE ADVANCE NOTICE TO THE AUTHORITY'S RESIDENT SO THAT THEY CAN BE PRESENT.
- CROSS SLOPE TRANSITIONS TO MATCH EXISTING CROSS SLOPES SHALL BE AS DIRECTED BY THE RESIDENT.
- 14' WIDE RAMP LANES SHALL BE PAVED IN A SINGLE PASS SUCH THAT LONGITUDINAL JOINTS ARE AT THE EDGES OF THE LANE.
- THE NOTCHED WEDGE SHALL BE DOUBLE TACKED.
- A MILLED SAFETY WEDGE IS REQUIRED.



YORK RIVER APPROACH MILL & OVERLAY

STA. 1143+58 TO STA. 1168+76, SOUTHBOUND
STA. 1143+56 TO STA. 1164+72, NORTHBOUND

Scale: NOT TO SCALE

No.	Revision	By	Date
Δ	ADDENDUM #2 - REVISED STANDARD	JRH	12/23

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: JOSEPH HOWE, P.E.

	By	Date	By	Date	
Designed	JRH	10\23	Checked	JRH	10\23
Drawn	CHL	10\23	In Charge of	JRH	10\23

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RYAN BARNES, PE, CPESC

PAVEMENT REHABILITATION
EXITS 1, 2 & 3 AND MM 1.3 TO MM 6.8
TYPICAL SECTIONS 1
MILL AND OVERLAY

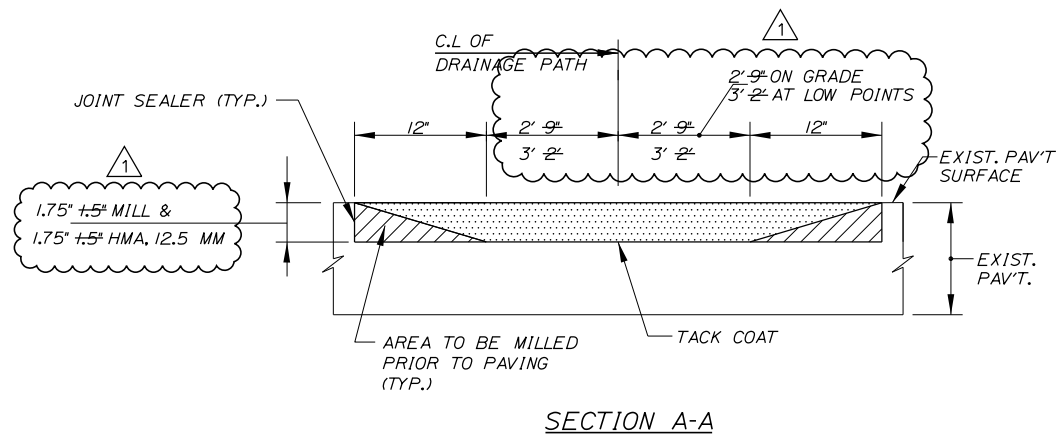
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CONTRACT: 2024.01

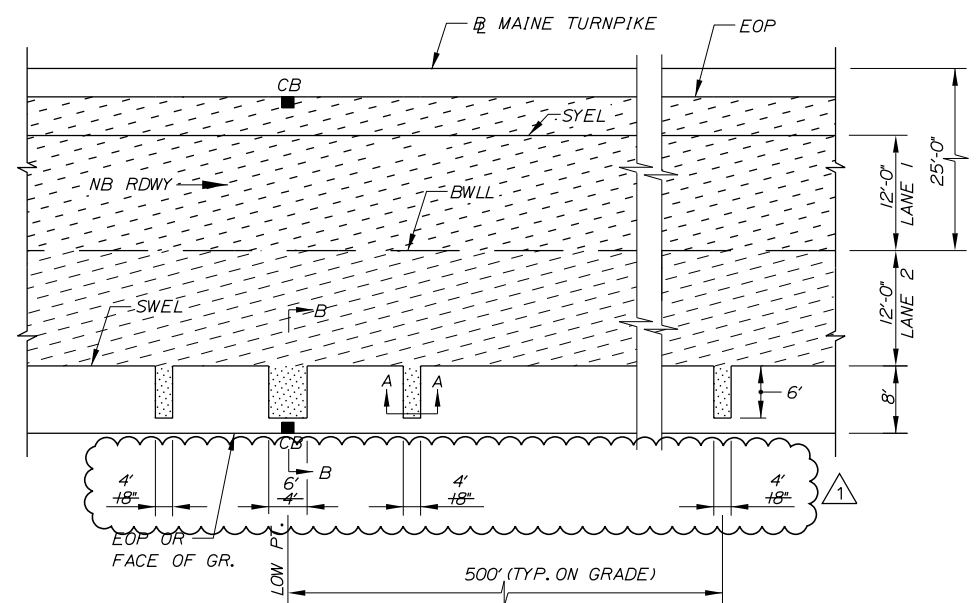
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Date: 11/30/2023

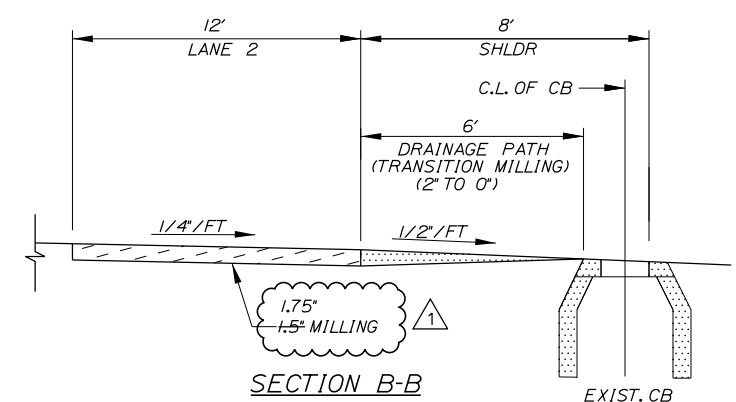
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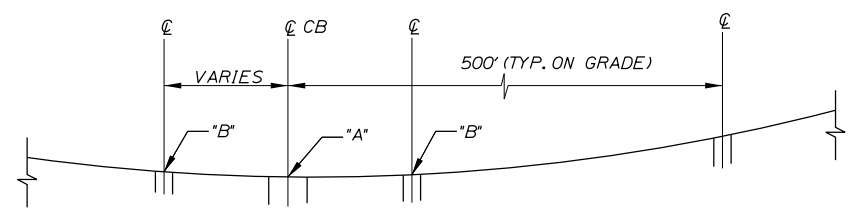
SECTION A-A



PLAN VIEW



SECTION B-B



PROFILE ALONG EDGE OF MILLING

DRAINAGE PATH DETAILS

LEGEND

- = DRAINAGE PATH MILLING
- = TRAVEL LANE MILLING

NOTES:

1. POINT "B" IS 1" HIGHER THAN POINT "A".
2. DRAINAGE PATH REQUIRED AT ALL LOW POINTS.

Scale: NOT TO SCALE

Designed by:

CONSULTANT PROJECT MANAGER: JOSEPH HOWE, P.E.			
By	Date	By	Date
Designed	JRH 10\23	Checked	LEM 10\23
Drawn	CHL 10\23	In Charge of	LEM 10\23

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THE GOLD STAR
MEMORIAL HIGHWAY

PAVEMENT REHABILITATION
EXITS 1, 2 & 3 AND MM 1.3 TO MM 6.8
DRAINAGE PATH FOR MILLING DETAILS

SHEET NUMBER: DT-04
CONTRACT: 2024.01
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