

MAINE TURNPIKE AUTHORITY

ADDENDUM NO. 2

CONTRACT 2023.03

BRIDGE REPAIRS

EAGLES NEST ROAD SOUTHBOUND OVERPASS (MM 60.81)

SUPERSTRUCTURE REPLACEMENT

ROUTE 122 UNDERPASS (MM 74.00)

Questions:

The following are questions asked at the pre-bid meeting held on March 2, 2023 or submitted to the Maine Turnpike Authority in writing. Answers to the questions are noted. Bidders shall utilize this information in preparing their bid.

Question 1: Will rolling lane closures be allowed?

Response: No, rolling lane closures will not be permitted.

Question 2: Will the as built plans be posted to the website?

Response: The as-built plans have been posted to the MTA website at www.maineturnpike.com. The completeness and accuracy of these plans is not guaranteed.

Question 3: Will any substitutes for Methyl Methacrylate Polymer Concrete Patch Material for the pier cap pedestals at piers 1 and 2 be allowed?

Response: No, the T-17 Methyl Methacrylate Polymer Concrete Patch Material, manufactured by Transpo Industries, Inc. shall be used for this work.

Make the following changes to the bid documents:

In the Contract Documents, **REMOVE** Special Provision Section 506, Steel Applied Protective Coating – Steel (Thermal Spray Coating – Shop Applied), pages SP-40 and SP-41 and **REPLACE** with the attached revised Special Provision Section 506, Steel Applied Protective Coating – Steel (Thermal Spray Coating – Shop Applied), pages SP-40 and SP-41.

Note: The above items shall be considered as part of the bid submittal.

Attachments

- Contract Book Pages SP-40 and SP-41 (2 pages)
- Pre-Bid Agenda (5 pages)
- Pre-Bid Presentation (24 pages)
- Pre-Bid Sign-In Sheet (1 page)

The total number of pages included with this addendum is thirty four (34).

All bidders are requested to acknowledge the receipt of the Addendum No. 2 by signing below and faxing this sheet to Nate Carll, Purchasing Department, (207) 871-7739. Bidders are also required to acknowledge receipt of this Addendum No. 1 on Page P-10 of the bid package.

Business Name

Print Name and Title

Signature

Date

March 8, 2023

Very truly yours,

MAINE TURNPIKE AUTHORITY

Purchasing Manager
Maine Turnpike Authority

SPECIAL PROVISION

SECTION 506

SHOP APPLIED PROTECTIVE COATING – STEEL

(Thermal Spray Coating – Shop Applied)

506.05 Inspection

This section is amended by the addition of the following:

The QAI shall be given ample notice in order to inspect the product prior to coating, recoating or removal of paint from the area. “Ample notice” shall be defined at the Pre-Job meeting depending on shop or site conditions.

Substrates that are coated without notification of the QAI may be rejected and no further coating shall be done on the piece until further notice from the Authority. Coating applied without notification of the QAI may be investigated by destructive and non-destructive testing as approved by the Authority and by a review of the JCR. The Resident may reject, conditionally accept, or accept the coating based on documentation and test results. Rejected coating shall be removed and re-applied. Conditionally accepted coatings shall be made acceptable as approved by the Authority. The cost of additional testing and repairs shall be borne by the Contractor.

506.11 Materials

This section is amended by the addition of the following:

Thermal Spray Coating shall utilize metallized 85-15 zinc-aluminum wire.

506.16 Touch-up and Repairs

This section is amended by the addition of the following:

The Contractor shall repair any damage that is done to the coating after the members have left the shop at no expense to the Authority. The Contractor shall document any damage and propose a repair that is in accordance with the manufacturer’s recommendations to the Resident for approval. No repairs shall be done prior to receiving approval of the proposed method of repair.

506.30 Description

This section is replaced in its entirety with the following:

This work shall consist of surface preparation and application of Thermal Spray Coatings (TSC) in accordance with the Plans and this Specification. Application of TSC to steel substrate shall be done in accordance with requirements, recommendations and appendices stated herein, and within referenced Specifications.

The applicator shall provide copies of application procedures, operator qualifications, QC Manuals and repair procedures.

506.32 Surface Preparation

Paragraph 2 in this section is replaced in its entirety with the following:

The anchor profile shall be 2.5-4.0 mils. Measure and record the anchor profile in accordance with ASTM D4417 Method B (depth micrometer) or C (Replica Tape) or both on each plane to be sprayed or at 120° intervals on pipe or tube. The applicator shall take measurements of blast profile every 200 sq. ft. for manual blast operations and every 2000 sq. ft. for automated blast operations. Readings shall be recorded, or replica tape shall be affixed to inspection records. If the anchor profile fails to meet the minimum required profile, re-blast the substrate until the required anchor profile is achieved.

506.35 Seal Coat and Top Coat Application (Paint)

This section is amended by the addition of the following:

The metallized girders shall be sealed with clear seal coat only of 2 to 3 mils thickness; additional coatings and pigmentation are not required. The clear seal coat shall be compatible with an epoxy intermediate coat and a polyurethane top coat from the MaineDOT NEPCOAT QPL. Provide certification of compatibility between the seal coat and epoxy intermediate coat and polyurethane top coat from the intermediate coat/top coat manufacturer.

The clear seal coat shall be applied within 8 hours after thermal spraying. If a sealer cannot be applied within 8 hours, it shall be verified that the TSC (a) has not been contaminated by visual inspection, and (b) is dust-free using the clear cellophane tape test per ISO 8502-3 before applying the sealer.

Top flanges of beams requiring shear connectors shall receive a 1.5 to 3 mil primer coat from the MaineDOT NEPCOAT QPL primer or be thermal sprayed. All faying surfaces shall be masked off during seal coat application.

All metallizing shall be done before assembly. The seal coat shall be adequately cured before handling, but under no circumstances shall the product be handled before the coating has achieved the manufacturer's published minimum cure time.

Material shall not be loaded for shipment until the seal coat has adequately cured and been inspected and accepted. The components will be stamped "APPROVED" only after the loading has been completed and approved, and no material shall be shipped without the prior approval of the Authority

506.61 Basis of Payment

This section is amended by the addition of the following:

All costs for clear seal coat shall be considered incidental to Thermal Spray Coating (Shop Applied).

MAINE TURNPIKE AUTHORITY

Pre-Bid Conference

CONTRACT 2023.03

BRIDGE REPAIRS
EAGLES NEST ROAD SOUTHBOUND OVERPASS
MILE 60.81

SUPERSTRUCTURE REPLACEMENT
ROUTE 122 UNDERPASS
MILE 74.00

March 2, 2023 10:00 AM

- 1) Location:
The general limits of work are as shown in the Contract Plans. The Eagles Nest Road Southbound Overpass is located at Mile 60.81 of the Maine Turnpike and the Route 122 Underpass is located at Mile 74.00 of the Maine Turnpike.
- 2) General Description:
The work consists of bridge repairs to the Eagles Nest Road Southbound Overpass bridge in the Town of Gray, Maine and replacing the Route 122 Underpass bridge over the Maine Turnpike in the City of Auburn, Maine. The work includes steel repairs, installation of a new diaphragm, and miscellaneous superstructure repairs at the Eagles Nest Road Southbound Overpass. The work also includes concrete deck and steel girder replacement, concrete substructure modifications and repairs, approach work and paving, and guardrail and bridge rails, at the Route 122 Underpass bridge as well as maintenance of traffic and all other work incidental thereto in accordance with the Plans and Specifications.
- 3) Bid:
 - a) Bid opening is March 14, 2023 at 11:00 A.M. at MTA headquarters 2360 Congress Street, Portland.
 - b) All bid and contractual questions shall be directed to Purchasing Department, Phone No. (207) 482-8115.
 - c) All questions on plans and specifications shall be in writing and shall be directed to Nate Carll, Purchasing Manager, at (207) 871-7739 (fax) or email ncarll@maineturnpike.com.
 - d) All questions must be submitted by 5:00pm on Wednesday March 8, 2023 to be considered.
- 4) Notification:
 - a) Contractor shall notify and obtain approval from the Authority prior to visiting the Project site for field inspection. The contact person is Mr. Steve Tartre at (207) 482-8144 or startre@maineturnpike.com.

5) Construction Schedule/Prosecution of Work:

a) MTA Board is scheduled to consider the Contract Award on March 23, 2023.

b) Eagles Nest Road Southbound Overpass

- All bridge repair work shall be substantially completed on or before September 1, 2023. Substantially complete shall be defined as:
 - All bridge repair work required by the Contract at the Eagles Nest Road Southbound Overpass.
 - Eagles Nest Road and Upper Marginal Way fully opened to all lanes of traffic.
- The Contractor shall be allowed to close the Eagles Nest Road/Upper Marginal Way for a maximum of five consecutive (5) calendar days.

c) Route 122 Underpass

- Construction shall be substantially completed on or before November 15, 2024. Substantially complete shall be defined as:
 - All substructure modifications and superstructure replacement including all bridge deck work, including curbing, steel bridge rail, snow fence, concrete sealing, surface pavement and guardrail installation including attachments complete and available for traffic.
 - Route 122 fully opened to two-way traffic including shoulders, guardrail, surface pavement, pavement markings and signage.
 - All disturbed slopes loamed, seeded and mulched, temporary erosion control mix and/or blanket installed where necessary.
- All tree clearing shall be completed prior to June 1, 2023.
- The Contractor shall not be allowed to close the Route 122 Underpass bridge prior to April 15, 2024.

d) All work in this Contract shall be completed on or before June 13, 2025.

e) Supplemental Liquidated damages on a calendar day basis in accordance with Subsection 107.8 shall be assessed for each calendar day that substantial completion is not achieved.

6) Maine Department of Labor – Fair Hourly Wages (Special Provision 104.3.8)

a) Heavy and Bridge wages for Androscoggin and Cumberland Counties are included in the Contract Book.

7) Utility Coordination (Special Provision 104.4.6)

a) The Contractor shall contact Dig Safe and any non-member utility operators through OK-TO-DIG prior to any work.

b) Following the completion of the initial utility locations, the Contractor shall GPS all utilities within the project limits and provide a copy of the Dig Safe record to the Authority. The Contractor, acting as the Authority's third party locator shall be responsible for remarking all Maine Turnpike facilities when a Dig Safe utility locate is called in for the Project. All costs for this work shall be incidental to the Contract.

c) The following utilities are anticipated to be located within the Project limits:

- i) Central Maine Power
 - d) Utility adjustments are anticipated as part of this project. The Contractor shall coordinate their work with the aerial utility's relocations, as necessary.
 - i) Central Maine Power will relocate three poles adjacent to Route 122 to accommodate the bridge and roadway work.
 - ii) Central Maine Power Company require 10 Working Days to complete this work and has committed to conduct this work between June 1, 2023 and September 30, 2023.
 - e) If any unexpected utility relocations become necessary, they will be scheduled in compliance with Section 104 of the Standard Specifications and will be done by the utilities in conjunction with the work by the Contractor.
- 8) Cooperation With Other Contractors (Special Provision 104.4.7)
- a) MTA Contract 2023.01 – Mainline Pavement Rehabilitation (MM 88.6 to MM 98)
 - b) MTA Contract 2023.XX – Bald Hill EVR Paving (MM 71.6)
 - c) MTA Contract 2024.XX – Bridge Painting (MM 64.3, MM 66.2, and MM 68.6)
 - d) MTA Contract 2024.XX – Bridge Repairs, Hunts Hill Road (MM 61.6) and Royal River Bridge (MM 71.1)
 - e) MTA Contract 2024.XX – Exit 75 Interchange Rehabilitation
 - f) MaineDOT WIN 25285 – Route 122 Highway Preservation
 - g) MaineDOT WIN 26698 – Route 122 Ultra-Thin Bonded Wearing Course
- 9) Lead Paint (Special Provision 105.2.4.2)
- a) The Contractor shall note that the existing bridge structures may contain lead based paint. The Contractor shall institute every precaution when working with materials coated with lead based paints.
- 10) Permit Requirements (Special Provisions 105.8.2 and 105.8.3)
- a) The Project is subject to the Stormwater Memorandum of Agreement for Stormwater Management Between the Maine Department of Transportation, Maine Turnpike Authority, and Maine Department of Environmental Protection (Stormwater MOA).
 - b) The Project is subject to the requirements of the Maine Construction General Permit (MCGP) as Project activities are expected to exceed 1 acre of disturbance but be less than 5 acres.
 - c) This Project is also subject to the requirements of the Maine Pollutant Discharge and Elimination System (MPDES) General Permit for the Discharge of Stormwater from MTA's Municipal Separate Storm Sewer Systems (MS4).
 - d) Compliance with the erosion and sedimentation control requirements outlined in this Contract is required by the Contractor.
- 11) Steel Cost Adjustment (108.10)
- a) Steel cost adjustment will be made to provide additional compensation to the Contractor, or a credit to the Authority, for fluctuations in steel prices.
- 12) General Requirements
- a) U-Turns at toll plazas and median openings not allowed. (Supplemental Specification 105.5.1)
 - b) Contractor access to and from the mainline shall not negatively impact mainline traffic flow. The Contractor may be required to establish lane closures to provide for safe access. Refer to Special

Provision 652, Specific Project Maintenance of Traffic Requirements, for lane closure requirements and restrictions.

- c) All vehicles used on the Project, including concrete delivery trucks, shall be equipped with amber flashing beacons in accordance with Supplemental Specification 652.3.4.
- d) Class III safety vests must be worn at all times.

13) Specific Contract Items:

- a) Section 502 – Pier Pedestal Concrete
 - i) Transpo Industries, Inc. T-17 Methyl Methacrylate Polymer Concrete Patch Material shall be used for the pier pedestal extensions at Piers 1 and 2.
- b) Section 504 – Eagles Nest Road Southbound Overpass Steel Repair
 - i) All work at the Eagles Nest Road Southbound Overpass, including Maintenance of Traffic, shall be included in the lump sum price for Item 504.80, Eagles Nest Road Southbound Overpass Steel Repair.
- c) Item 504.70 – Structural Steel, Fabricated and Delivered
 - i) At the Contractor’s option, W24x117 rolled beams may be used in place of the specified welded plate girder. If the Contractor elects to use W24x117 beams, they shall be responsible for ensuring proper detailing and fit up of the rolled beams. Rolled beams shall be paid as Item 504.70, Structural Steel, Fabricated and Delivered and no pay adjustment shall be made for this substitution.
- d) Section 526 – Temporary Concrete Barrier Type I - Supplied by Authority
 - i) Concrete barriers supplied by the Authority shall be available at the Former Service Plaza at Mile 98.

14) Traffic Control (Special Provision Section 652):

- a) Special Provision Section 652 replaces the MaineDOT Standard Specification 2014 Edition and MTA 2016 Supplemental Specification Section 652.
- b) Substantive revisions have been bolded in the 652 SP.
- c) Contractor is responsible for supplying all traffic control devices.
- d) Contractor is responsible for placement, relocation, removal and maintenance of traffic control devices. Maintenance of traffic control devices is a 24-hour a day, seven days per week responsibility. Contractor shall inspect devices as required.
- e) Temporary detours, lane closures and construction phasing shall be established and maintained at all times in accordance with the details shown on the Plans and the requirements of the Special Provisions.
 - i) A truck mounted attenuator shall be utilized for all lane closures on the Turnpike mainline, shall be utilized for all temporary shoulder closures (i.e. closures that do not include temporary concrete barrier) on the Turnpike mainline, and other construction operations where workers are exposed to traffic and not protected by positive means.
 - ii) A truck mounted attenuator shall be utilized for all work completed behind guardrail if that work is being completed within the deflection zone of the guardrail (within four feet behind the guardrail post).
- f) Turnpike Lane closures

- i) One lane of traffic flowing in each direction at all times. Minimum traveling width of 14' required unless otherwise shown in the Plans.
- ii) All lane closures require the approval of the Resident. The Resident is required to submit a request to MTA for lane closures by noon on Thursday for lane closures needed for the following week. The Contractor shall plan the work and requests for lane closures accordingly. Requests are subject to approval by MTA.
- iii) Supplemental liquidated damages shall be assessed at \$1,000 per minute for every minute that a temporary lane closure is in place outside of the allowable times.
- g) All signs, which do not apply to current construction activity, shall be 100% covered or removed in accordance with the plans. This includes any speed limit signs when work zone speed is in operation.
- h) Traffic control devices shall be NCHRP 350 compliant. All traffic control devices shall meet MASH guidelines if the date of manufacture was after December 31, 2019.

15) Questions:



**Maine
Turnpike
Authority**

MTA Contract 2023.03



Bridge Repairs Eagles Nest Road Southbound Overpass

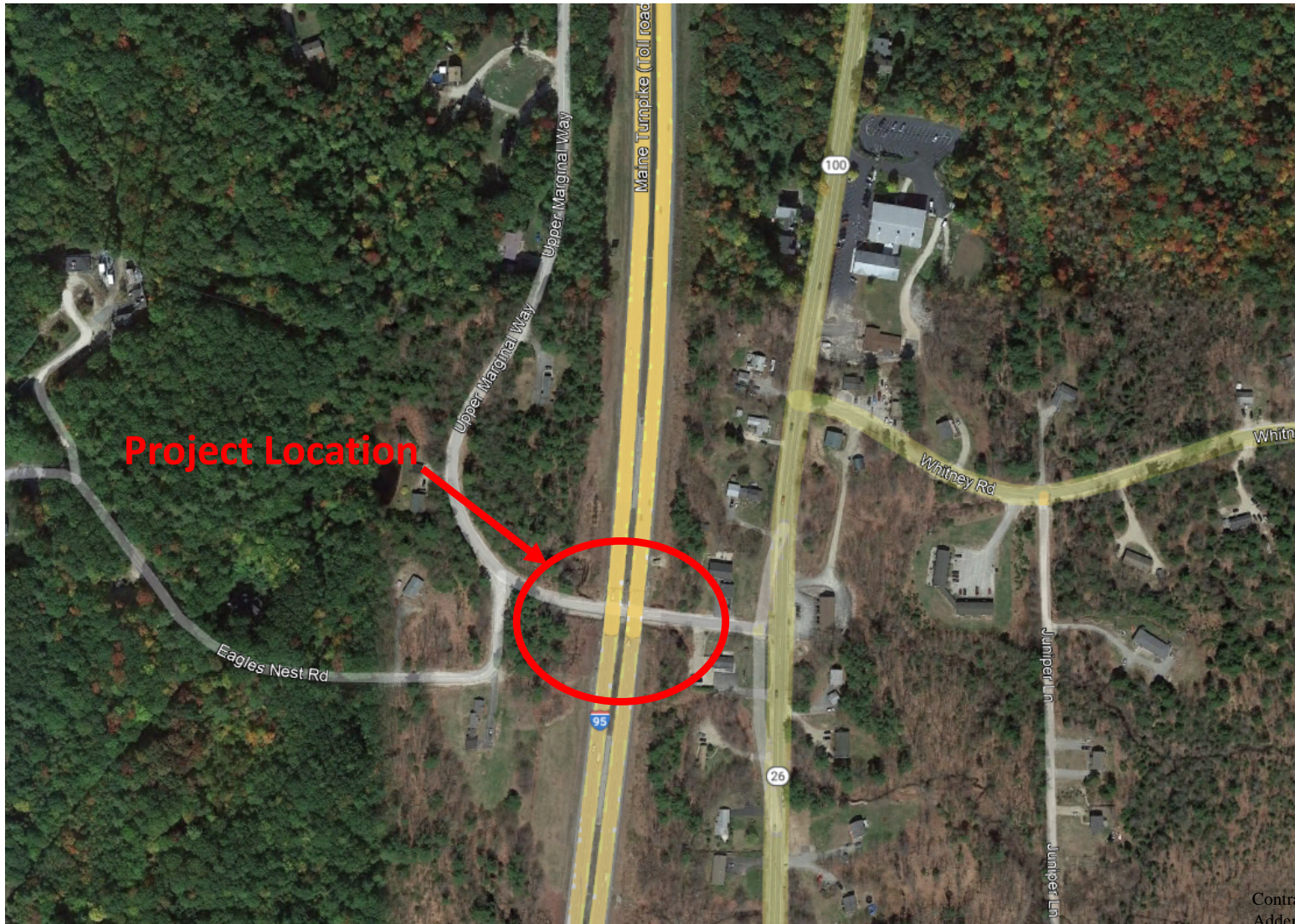
Superstructure Replacement Route 122 Underpass

Pre-Bid Conference

March 2, 2023

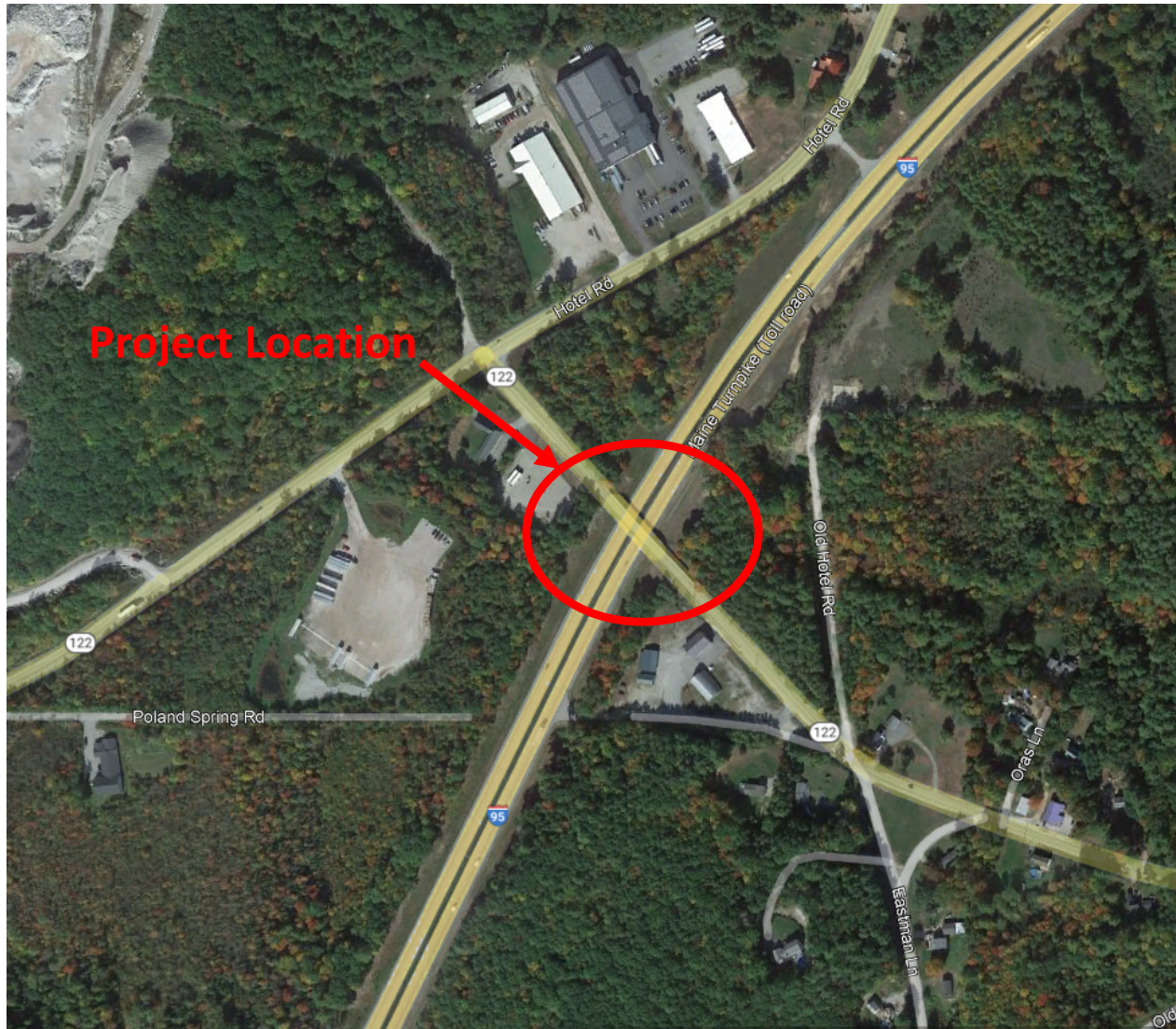


1) Location - Eagles Nest Road SB Overpass (MM 60.81)





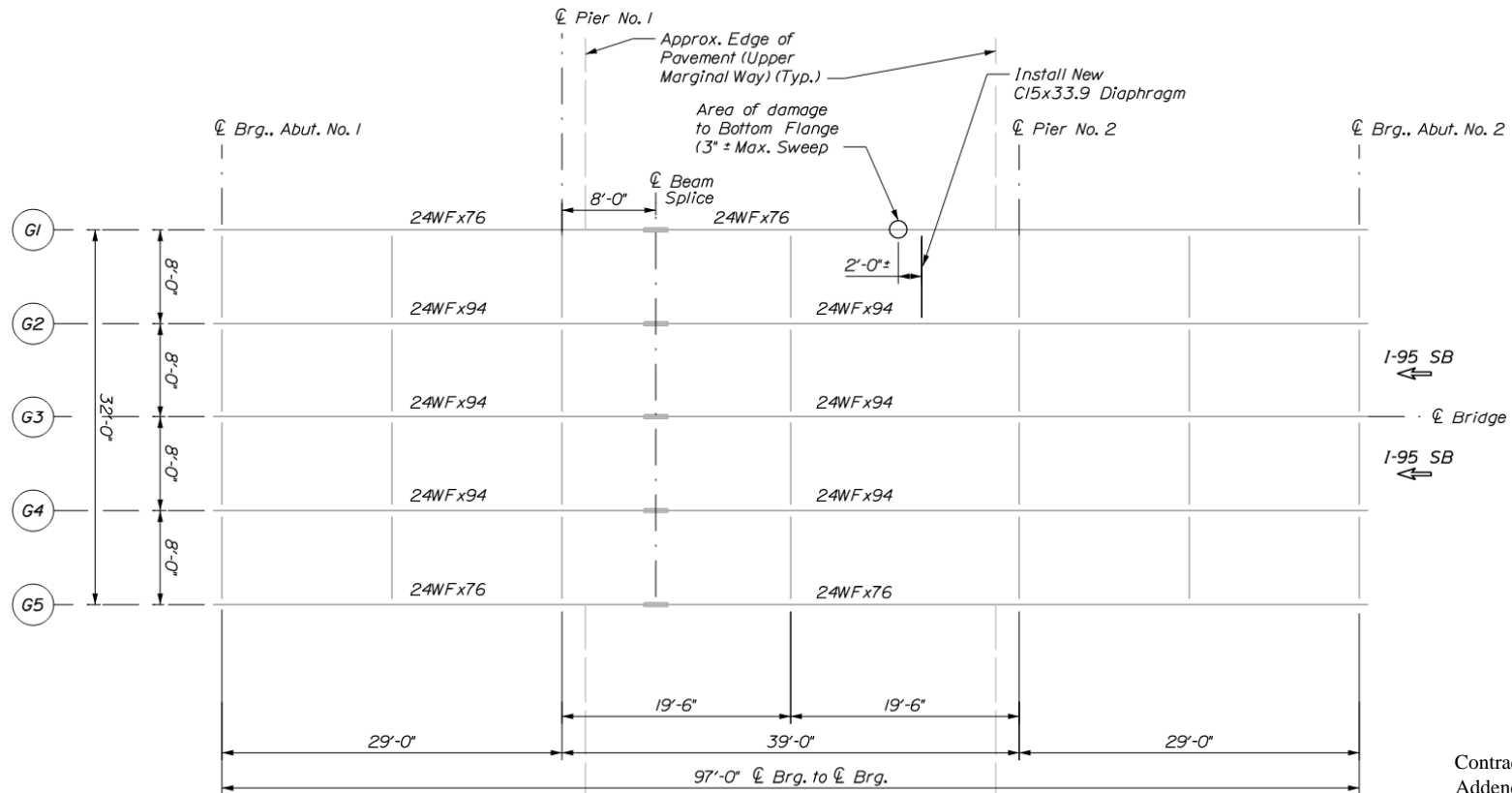
1) Location - Route 122 Underpass (MM 74.00)





2) General Description

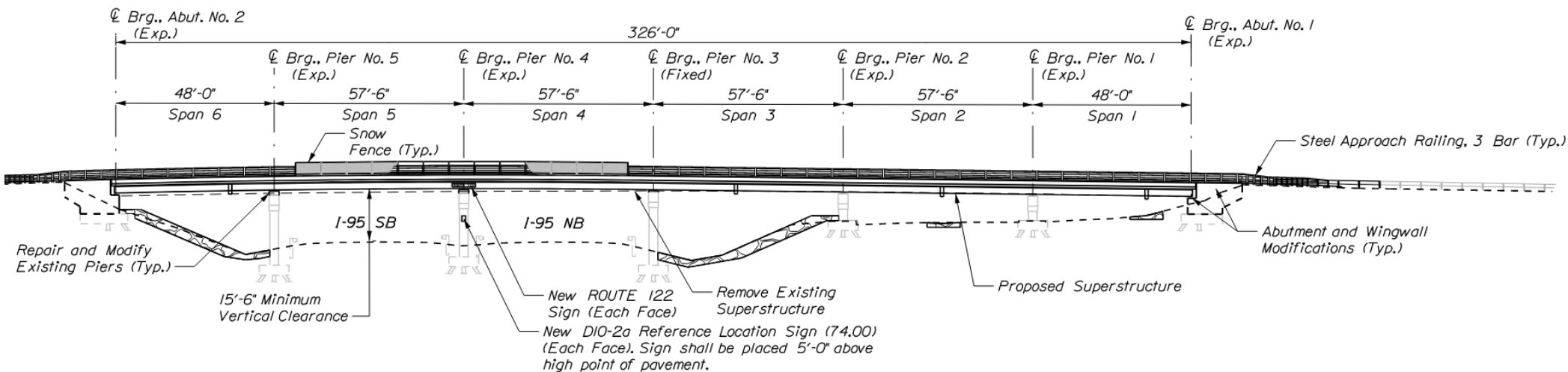
- At the Eagles Nest Road Southbound Overpass, the work includes:
 - Steel Repairs
 - Installation of New Diaphragm
 - Steel Coating Repairs





2) General Description (Cont'd)

- At the Route 122 Underpass, the work includes:
 - Concrete Deck and Steel Girder Replacement
 - Concrete Substructure Modifications and Repairs
 - Approach Work and Paving
 - Guardrail and Bridge Repairs





3) Bid

- **Bid opening:** March 14, 2023, at 11:00 A.M.
- All **bid & contractual questions** direct to Purchasing Dept.
 - Ph#: (207) 482-8115
- All **plan & spec questions** submit in writing directed to Nate Carll, Purchasing Manager
 - Fax: (207) 871-7739
 - Email: ncarll@maineturnpike.com
- All questions **must be submitted** by 5:00 pm on Wednesday March 8, 2023



4) Notification

- Contractor shall **notify and obtain approval** from the Authority (Steve Tartre) **prior to visiting the Project site** for field inspection.
 - Ph#: (207) 482-8144
 - email: startre@maineturnpike.com



5) Construction Schedule/Prosecution of Work

- MTA Board is scheduled to consider the **Contract Award on March 23, 2023.**

- Eagles Nest Road Southbound Overpass
 - Substantially complete: **September 1, 2023**
 - Eagles Nest Road/Upper Marginal Way shall be allowed to be closed for a maximum of five consecutive calendar days

- Route 122 Underpass
 - Substantially complete: **November 15, 2024**
 - All tree clearing complete: **June 1, 2023**
 - Route 122 may not be closed prior to **April 15, 2024**

- All work complete: **June 13, 2025**



6) Maine Department of Labor – Fair Hourly Wages

- Included Wage Rates:
 - Androscoggin County
 - Cumberland County



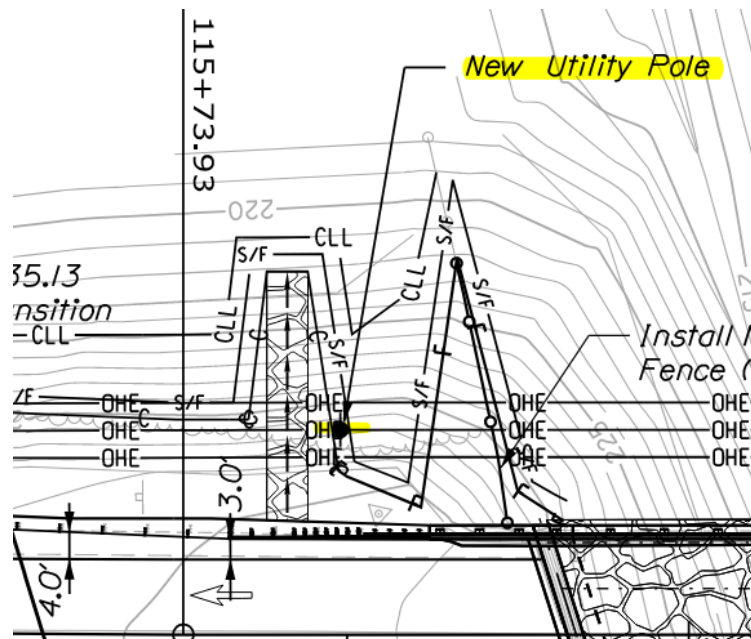
7) Utility Coordination

- The Contractor shall **contact Dig Safe** and any non-member utility operators through **OK-TO-DIG prior to any work.**
- Following the completion of the initial utility locations, the Contractor shall GPS all utilities within the project limits and provide a copy of the Dig Safe record to the Authority. The Contractor, acting as the Authority's third party locator shall be responsible for remarking all Maine Turnpike facilities when a Dig Safe utility locate is called in for the Project. All costs for this work shall be incidental to the Contract.
- The following utilities are located within the Project limits:
 - Central Maine Power
- If any **unexpected utility relocations** become necessary, they will be scheduled **in compliance with Section 104** of the Standard Specifications and will be **done by the utilities** in conjunction with the work by the Contractor.

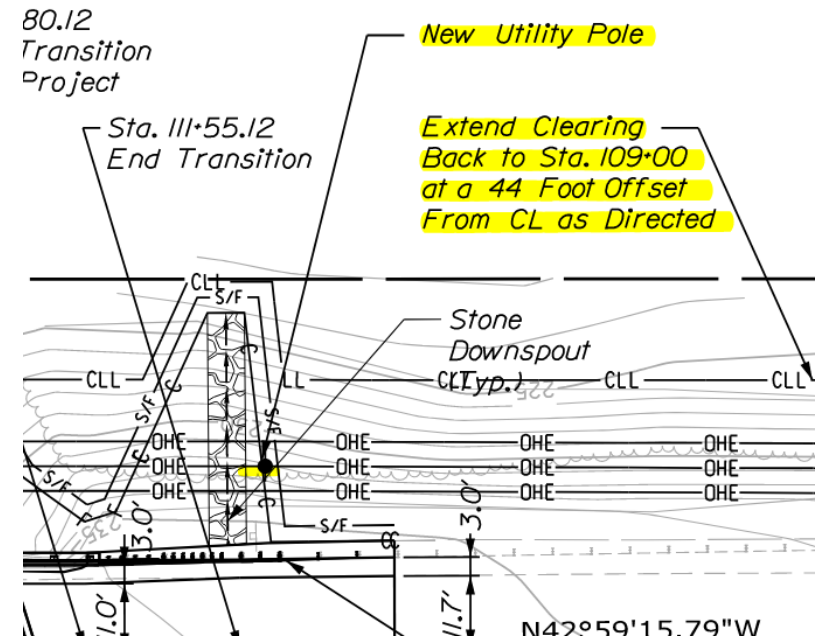


7) Utility Coordination (Cont.)

- Central Maine Power will relocate 3 utility poles along Route 122
- Central Maine Power will complete relocations between **June 1, 2023**, and **September 30, 2023**



West Approach



East Approach



8) Cooperation With Other Contractors

- **MTA Contract 2023.01** – Mainline Pavement Rehabilitation (MM 88.6 to MM 98)
- **MTA Contract 2023.XX** – Bald Hill EVR Paving (MM 71.6)
- **MTA Contract 2024.XX** – Bridge Painting (MM 64.3, MM 66.2, and MM 68.6)
- **MTA Contract 2024.XX** – Bridge Repairs, Hunts Hill Road (MM 61.6) and Royal River Bridge (MM 71.1)
- **MTA Contract 2024.XX** – Exit 75 Interchange Rehabilitation
- **MaineDOT WIN 25285** – Route 122 Highway Preservation
- **MaineDOT WIN 26698** – Route 122 Ultra-Thin Bonded Wearing Course



9) Lead Paint

- The Contractor shall note that the **existing bridge structures may contain lead-based paint**. The Contractor shall **institute every precaution** when working with materials coated with lead-based paints.



10) Permit Requirements

- The Project is subject to the **Stormwater Memorandum of Agreement for Stormwater Management** Between the Maine Department of Transportation, Maine Turnpike Authority, and Maine Department of Environmental Protection.
- The Project is subject to the requirements of the **Maine Construction General Permit (MCGP)** as Project activities are expected to exceed 1 acre of disturbance but be less than 5 acres.
- This Project is also subject to the requirements of the **Maine Pollutant Discharge and Elimination System (MPDES)** General Permit for the Discharge of Stormwater from MTA's Municipal Separate Storm Sewer Systems (MS4).
- **Compliance with the erosion and sedimentation control** requirements outlined in this Contract is required by the Contractor.



11) Steel Cost Adjustment

- Steel cost adjustment will be made to provide additional compensation to the Contractor, or a credit to the Authority, for fluctuations in steel prices.

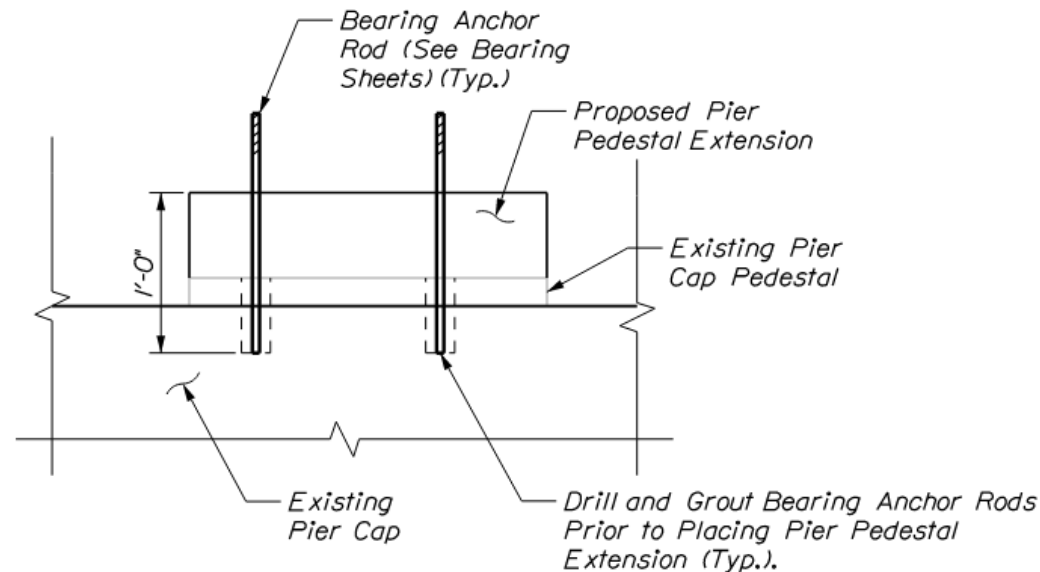


12) General Requirements

- **U-Turns** at toll plazas and median openings **not allowed**.
- **Contractor access** to and from the mainline **shall not negatively impact** mainline traffic flow. The Contractor may be required to establish lane closures to provide for safe access. Refer to Special Provision 652.
- **All vehicles** used on the Project, including concrete delivery trucks, shall be **equipped with amber flashing beacons** in accordance with Supplemental Specification 652.3.4.
- **Class III safety vests** must be worn at all times.

13) Specific Contract Items

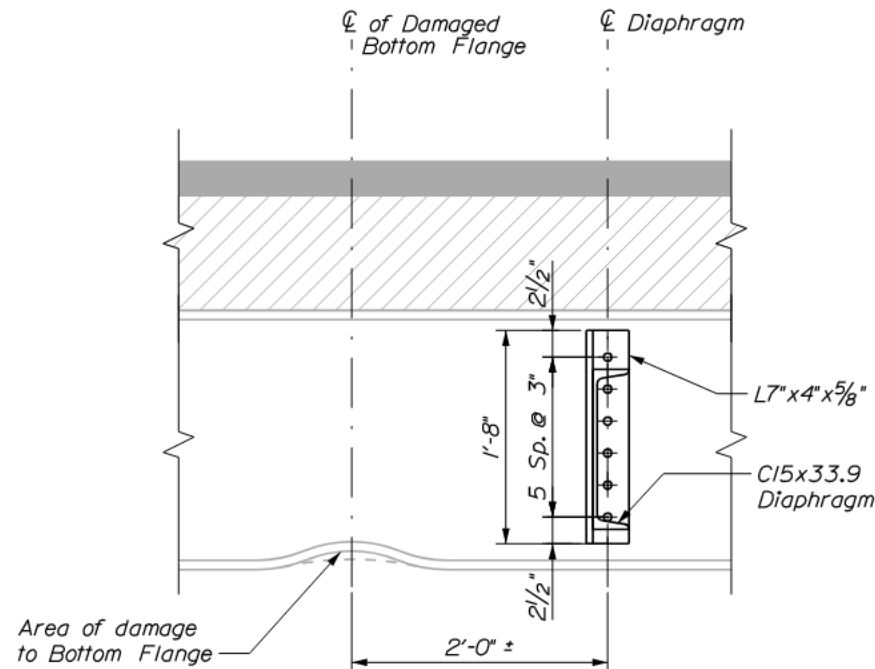
- Section 502 – Pier Pedestal Concrete
 - Transpo Industries, Inc. T-17 **Methyl Methacrylate Polymer Concrete Patch Material** shall be used for the pier pedestal extensions at Piers 1 and 2.





13) Specific Contract Items (Cont'd)

- Section 504 – Eagles Nest Road Southbound Overpass Steel Repair
 - All work at the Eagles Nest Road Southbound Overpass, **including Maintenance of Traffic**, shall be included in the lump sum price for Item 504.80, Eagles Nest Road Southbound Overpass Steel Repair.





13) Specific Contract Items (Cont'd)

- Item 504.70 – Structural Steel, Fabricated and Delivered
 - At the Contractor's option, **W24x117** rolled beams may be used in place of the specified welded plate girder.

- Section 526 – Temporary Concrete Barrier Type I - Supplied by Authority
 - **Concrete barriers supplied by Authority** will be available at the Former Service Plaza at Mile 98 (600 LF)



14) Traffic Control (Special Provision Section 652)

- SP 652 **replaces** the MaineDOT Standard Specs 2014 Edition and MTA 2016 Supplemental Specs.
- Substantive **revisions have been bolded** in the 652 SP.
- Contractor is responsible for supplying all traffic control devices.
- Contractor is responsible for placement, relocation, removal and maintenance of traffic control devices. Maintenance of traffic control devices is a **24-hour a day, seven days per week** responsibility. Contractor shall inspect devices as required.



14) Traffic Control (Special Provision Section 652)

- Temporary detours, lane closures and construction phasing shall be established and **maintained at all times** in accordance with the details shown on the Plans and the requirements of the Special Provisions.
 - **A truck mounted attenuator** shall be utilized for **all lane closures** on the Turnpike mainline, shall be utilized for **all temporary shoulder closures** (i.e. closures that do not include temporary concrete barrier) on the Turnpike mainline, and other construction operations where workers are exposed to traffic and not protected by positive means.
 - **A truck mounted attenuator** shall be utilized for **all work completed behind guardrail** if that work is being completed **within the deflection zone** of the guardrail (within four feet behind the guardrail post).



14) Traffic Control (Special Provision Section 652)

- Turnpike Lane closures
 - **One lane of traffic flowing** in each direction at all times. **Minimum traveling width of 14'** required unless otherwise shown in the Plans.
 - All lane **closures require the approval of the Resident**. The Resident is required to submit a request to MTA for lane closures by **noon on Thursday** for lane closures needed for the following week. The Contractor shall plan the work and requests for lane closures accordingly. Requests are subject to approval by MTA.
 - **Supplemental liquidated damages** shall be assessed at **\$1,000 per minute** for every minute that a temporary lane closure is in place outside of the allowable times.



14) Traffic Control (Special Provision Section 652)

- All signs, which do not apply to current construction activity, **shall be 100% covered or removed** in accordance with the plans. This includes any speed limit signs when work zone speed is in operation.
- Traffic control devices **shall be NCHRP 350 compliant**. All traffic control devices **shall meet MASH guidelines** if the date of manufacture was **after December 31, 2019**.



**Maine
Turnpike
Authority**

MTA Contract 2023.03



Questions?

Bridge Repairs - Eagles Nest Road Southbound Overpass
 Superstructure Replacement - Route 122 Underpass

PREBID MEETING 10:00AM MARCH 2, 2023 - ATTENDANCE SHEET

NAME	COMPANY	EMAIL	PHONE
RYAN BARNES	MTA	rbarnese@maineturnpike.com	207-671-8426
Matt Jones	McNamee Construction	matt@mcnamee-construction.net	207-550-1347
ANDY KITTREDGE	CPM CONSTRUCTORS	ANDYK@CPMCONSTRUCTORS.COM	207-865-0000
Mark Libby	Scott Construction	BScott207@gmail.com	207-632-0521
WESTON SCOTT	SCOTT CONSTRUCTION	WSCOTT@SCOTTCON.COM	207-899-5709
Jamie Mason	MTA	JMason@Maineturnpike.com	482-8172
Steve Tartre	MTA	sttartre@maineturnpike.com	871-7771 ext/44
KRISTI VAN DYK	MTA	KVAN DYK@MAINETURNPIKE.COM	482-8113
Nate Carll	MTA	ncarll@maineturnpike.com	482-8115
Soham Prabhudesai	New England Infrastructure	pindore@neinfrastructure.com	978-293-3535
GORDON EDINGTON	VHB	GEDINGTON@VHB.COM	207-889-3144
TIM BRYANT	VHB	TBRYANT@VHB.COM	207-889-3103