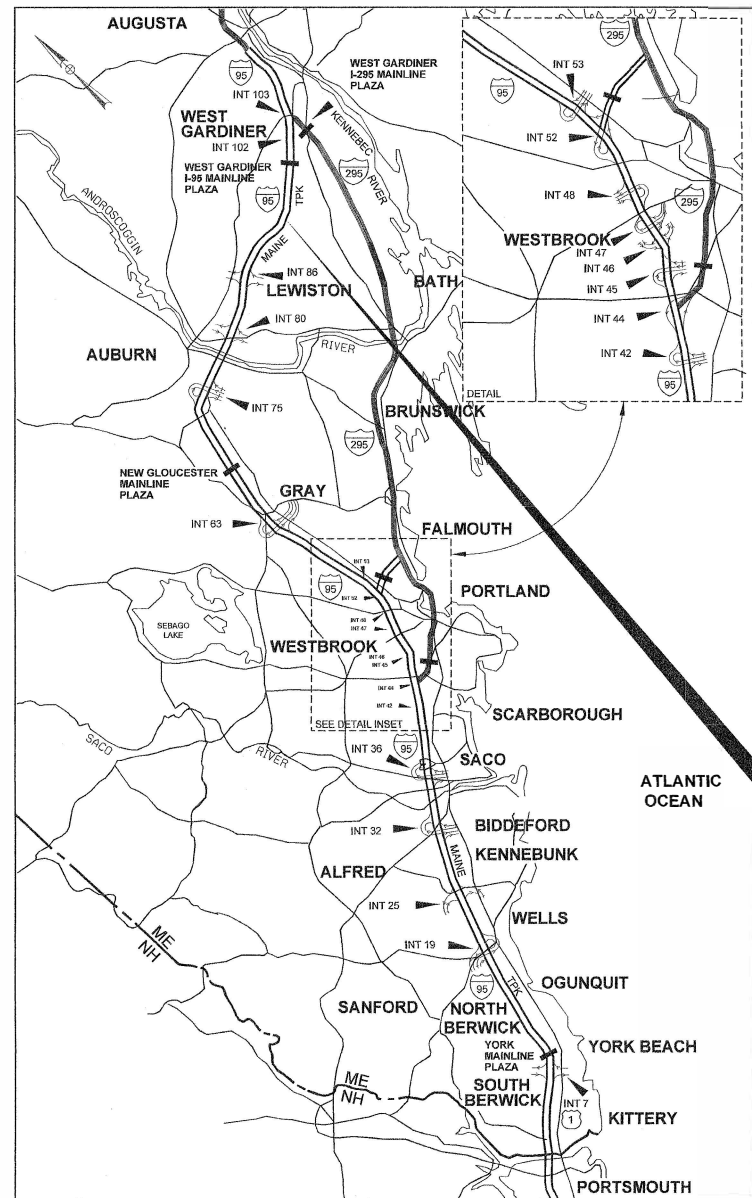


Date: 10/9/2024



LOCATION MAP



THE GOLD STAR MEMORIAL HIGHWAY

MAINE TURNPIKE AUTHORITY

MICHAEL J. CIANCHETTE, CHAIR
 JANE L. LINCOLN, MEMBER
 THOMAS J. ZUKE, MEMBER
 ANDREW MCLEAN, MEMBER
 NINA A. FISHER, MEMBER
 BRUCE VAN NOTE, MEMBER EX-OFFICIO

PETER S. MERFELD, INTERIM EXECUTIVE DIRECTOR

CONTRACT 2024.17
 STEVENSTOWN ROAD
 BRIDGE REPAIR
 MILE MARKER 96.5

CONTRACT 2024.17 STEVENSTOWN ROAD BRIDGE REPAIR MILE MARKER 96.5

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	GENERAL NOTES AND QUANTITIES
3	MAINTENANCE OF TRAFFIC - DETAILS AND GENERAL NOTES
4	MAINTENANCE OF TRAFFIC - SIGN SUMMARY
5	FRAMING PLAN AND REPAIR DETAILS
6	STRUCTURAL REPAIR DETAILS
7-12	AS-BUILTS

CONTRACT 2024.17

APPROVED: MAINE TURNPIKE AUTHORITY

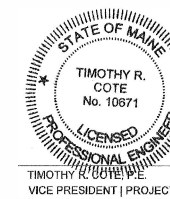
Peter S. Merfeld
 PETER S. MERFELD, P.E. - CHIEF OPERATIONS OFFICER & INTERIM EXECUTIVE DIRECTOR

10/10/24
 DATE

Stephen R. Martre
 STEPHEN R. MARTRE, P.E. - DIRECTOR OF ENGINEERING & CHIEF ENGINEER

10/10/24
 DATE

HNTB



Timothy R. Cote
 TIMOTHY R. COTE
 VICE PRESIDENT | PROJECT DIRECTOR
 DATE: *10/10/2024*

Filename: 001_Title Sheet.dgn

GENERAL NOTES

1. ALL DETAILS SHALL BE IN CONFORMANCE WITH MAINE DEPARTMENT OF TRANSPORTATION (MAINEDOT) STANDARD DETAILS HIGHWAY AND BRIDGES 2014 LATEST REVISIONS AND MAINEDOT BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL LATEST REVISION UNLESS OTHERWISE INCLUDED IN THESE PLANS.

2. DUE TO THE NATURE OF REPAIR PROJECTS, THE EXACT EXTENT OF REPAIR WORK CANNOT ALWAYS BE ACCURATELY DETERMINED PRIOR TO THE COMMENCEMENT OF WORK. THESE CONTRACT DOCUMENTS HAVE BEEN PREPARED BASED ON FIELD INSPECTION AND OTHER INFORMATION AVAILABLE AT THE TIME OF CONTRACT DEVELOPMENT. ACTUAL FIELD CONDITIONS MAY REQUIRE MODIFICATION TO THE CONSTRUCTION DETAILS, DIMENSIONS, AND WORK QUANTITIES. THE WORK SHALL BE PERFORMED IN ACCORDANCE WITH FIELD CONDITIONS AND AS DIRECTED BY THE RESIDENT.

3. ALL DIMENSIONS, ELEVATIONS, AND OTHER INFORMATION SHOWN ON THESE DRAWINGS THAT DEFINE THE STRUCTURE ARE BASED UPON THE ORIGINAL CONSTRUCTION DRAWINGS AND FIELD MEASUREMENTS AND ARE NOT GUARANTEED TO REPRESENT AS-BUILT DIMENSIONS. THE CONTRACTOR SHALL VERIFY ALL CONDITIONS AND DIMENSIONS IN THE FIELD AS NECESSARY AND AS REQUIRED FOR THE COMPLETION OF THE WORK UNDER THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ACCURACY AND FOR THE CORRECT FIT OF ALL CONSTRUCTION.

4. THE CONTRACTOR SHALL REPORT TO THE RESIDENT ANY DEVIATIONS OF THE ACTUAL CONDITIONS FROM THOSE DEPICTED ON THE DRAWINGS. SHOULD ANY DEVIATIONS BE FOUND, THE RESIDENT SHALL REVIEW AND PRESCRIBE CORRECTIVE ACTIONS TO BE TAKEN.

5. ALL WORK IS TO BE PERFORMED WITH CARE SO THAT MATERIALS WHICH ARE TO REMAIN IN PLACE, OR WHICH ARE TO REMAIN THE PROPERTY OF THE AUTHORITY, WILL NOT BE DAMAGED. IF ANY SUCH MATERIALS ARE DAMAGED, THE DAMAGED MATERIALS SHALL BE REPAIRED OR REPLACED IN A MANNER SATISFACTORY TO THE RESIDENT AT NO EXPENSE TO THE AUTHORITY.

6. THE CONTRACTOR IS ADVISED THAT THE EXISTING BRIDGE STEEL (BEAMS, SPLICE PLATES, DIAPHRAGMS, BEARINGS, NUTS, BOLTS, ETC.) ARE ASSUMED TO BE COATED WITH LEAD-BASED PAINT. SEE SPECIAL PROVISIONS SUBSECTION 105.21, LEAD PAINT, FOR ADDITIONAL INFORMATION.

7. CONTRACTOR SHALL PROVIDE RESIDENT ACCESS TO ALL AREAS UNDER CONSTRUCTION FOR INSPECTION PURPOSES. THIS SHALL BE INCIDENTAL TO THE RELATED CONTRACT ITEMS.

8. ALL EXISTING STEEL AT THE REPAIR AREAS, WITHIN THE LIMITS NOTED ON THE CONTRACT PLANS, SHALL BE CLEANED AND PAINTED IN ACCORDANCE WITH THE SPECIFICATIONS. TOUCH UP PAINT SHALL EXTEND TO ONE FOOT BEYOND THE PAINT REMOVAL AREA. THE TERMINATION POINT SHALL BE TAPED OFF TO PREVENT OVERRUN AND OVERSPRAY. THE FINISH COAT SHALL MATCH THE EXISTING PAINT SYSTEM.

9. COPIES OF THE AS-BUILT PLANS ARE ON FILE AT THE MAINE TURNPIKE AUTHORITY. A PORTION OF THESE PLANS ARE INCLUDED IN THIS CONTRACT FOR CONVENIENCE. THE COMPLETENESS AND ACCURACY OF THESE PLANS IS NOT GUARANTEED AND SHOULD NOT BE RELIED ON WITHOUT VERIFICATION.

LIST OF ABBREVIATIONS

ABUT. - ABUTMENT
 ADDL. - ADDITIONAL
 ALT. - ALTERNATE
 APPROX. - APPROXIMATELY
 BOT. - BOTTOM
 BRG. - BEARING
 CL. - CLEAR
 CL - CENTERLINE
 CONC. - CONCRETE
 CONSTR. - CONSTRUCTION
 DEMO. - DEMOLITION
 DIA. - DIAMETER
 EA. - EACH
 EB - EASTBOUND
 E.F. - EACH FACE
 EL. - ELEVATION
 EQ. - EQUAL
 EXIST. - EXISTING
 EXP. - EXPANSION
 F.F. - FAR FACE
 JT. - JOINT
 MAX. - MAXIMUM
 MAINEDOT - MAINE DEPARTMENT OF TRANSPORTATION
 MIN. - MINIMUM
 MTA - MAINE TURNPIKE AUTHORITY
 NB - NORTHBOUND
 N.F. - NEAR FACE
 N.T.S. - NOT TO SCALE
 PED. - PEDESTAL
 PGL - PROFILE GRADE LINE
 PL - PLATE
 PROP. - PROPOSED
 P.S.I. - POUNDS per SQUARE INCH
 RDWY. - ROADWAY
 SHLDR. - SHOULDER
 SB - SOUTHBOUND
 SP. - SPACES
 STA. - STATION
 T.&B. - TOP & BOTTOM
 TPKE. - TURNPIKE
 TYP. - TYPICAL
 U.O.N. - UNLESS OTHERWISE NOTED
 VERT. - VERTICAL
 WB - WESTBOUND
 W.P. - WORKING POINT

ESTIMATED QUANTITIES			
ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY
504.80	STRUCTURAL STEEL REPAIR (REPAIR AREA "A")	LS	1
504.81	STRUCTURAL STEEL REPAIR (REPAIR AREA "B")	LS	1
506.17	SURFACE PREPARATION OF EXISTING STEEL	LS	1
506.9103	ZINC RICH COATING SYSTEM (FIELD APPLIED)	LS	1
531.82	HEAT STRAIGHTENING	LS	1
629.05	HAND LABOR, STRAIGHT TIME	HR	20
631.10	AIR COMPRESSOR (INCLUDING OPERATOR)	HR	10
631.11	AIR TOOL (INCLUDING OPERATOR)	HR	10
631.17	TRUCK-SMALL (INCLUDING OPERATOR)	HR	5
631.36	FOREMAN	HR	10
652.30	FLASHING ARROW	EA	1
652.33	DRUM	EA	42
652.34	CONE	EA	42
652.35	CONSTRUCTION SIGNS	SF	440
652.361	MAINTENANCE OF TRAFFIC CONTROL DEVICES	LS	1
652.380	FLAGGERS	HR	600
652.41	PORTABLE-CHANGEABLE MESSAGE SIGN	EA	1
652.47	SEQUENTIAL FLASHING WARNING LIGHTS	EA	14
659.10	MOBILIZATION	LS	1

Date: 10/17/2024

Filename: 002_GeneralNotes and Quantities.dgn

Draft PS&E
October 11, 2024

Scale:				Designed by:																					
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				No.	Revision	By	Date																		
CONSULTANT PROJECT MANAGER: TIM COTE, P.E.				<table border="1" style="width: 100%;"> <thead> <tr> <th> </th> <th>By</th> <th>Date</th> <th> </th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr> <td>Designed</td> <td>NTB</td> <td>10\24</td> <td>Checked</td> <td>JDW</td> <td>10\24</td> </tr> <tr> <td>Drawn</td> <td>ERB</td> <td>10\24</td> <td>In Charge of</td> <td>TRC</td> <td>10\24</td> </tr> </tbody> </table>					By	Date		By	Date	Designed	NTB	10\24	Checked	JDW	10\24	Drawn	ERB	10\24	In Charge of	TRC	10\24
	By	Date		By	Date																				
Designed	NTB	10\24	Checked	JDW	10\24																				
Drawn	ERB	10\24	In Charge of	TRC	10\24																				

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**THE GOLD STAR
MEMORIAL HIGHWAY**

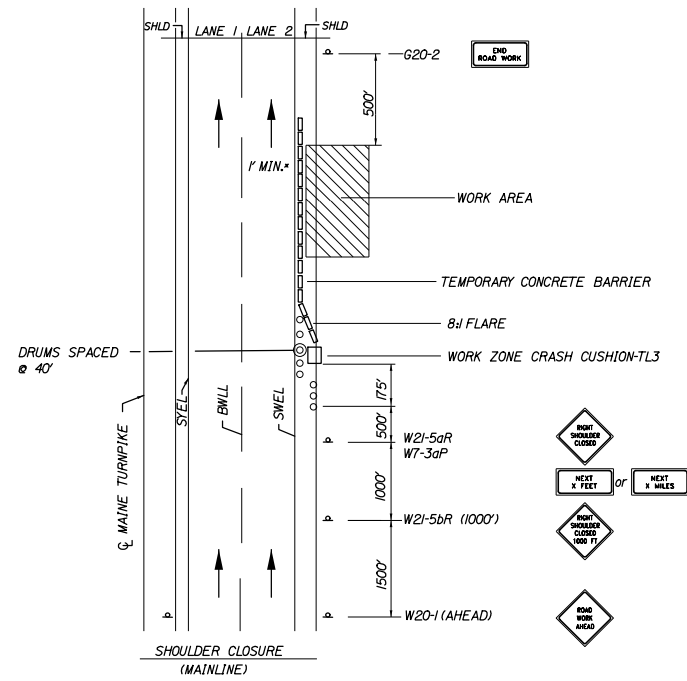
MTA PROJECT MANAGER: KRISTI VAN OUYEN, P.E.

STEVENSTOWN ROAD
BRIDGES REPAIRS

GENERAL NOTES AND QUANTITIES

SHEET NUMBER: GN-01
CONTRACT: 2024.17
2 OF 6

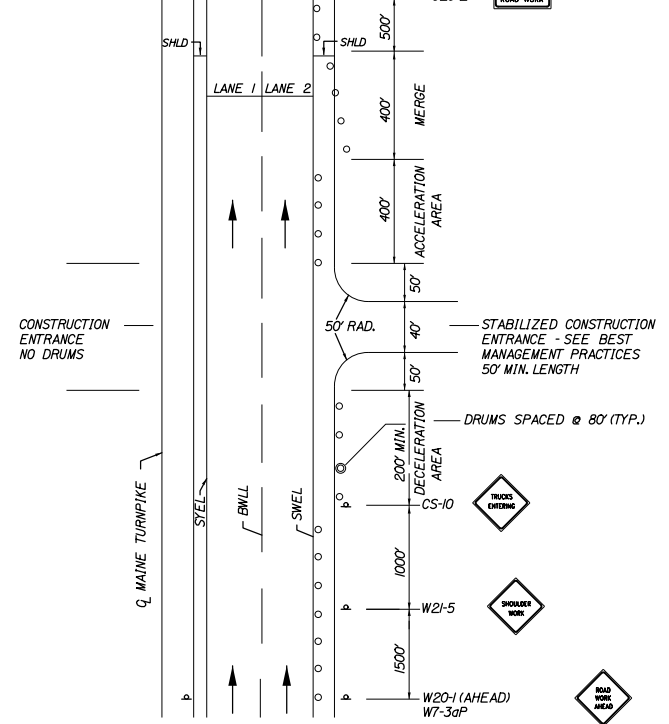
Date:



SHOULDER CLOSURE

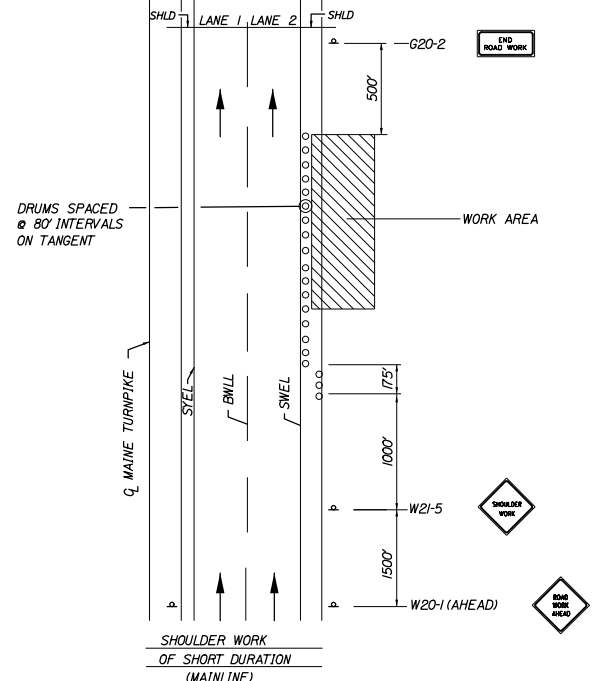
* FOR MAINLINE SHOULDER CLOSURES ONLY

1. THE W20-1 AND G20-2 SIGNS ARE NOT REQUIRED IF THE SHOULDER CLOSURE IS WITHIN A PREVIOUSLY ESTABLISHED WORK ZONE.
2. ONLY ONE SHOULDER CLOSURE PERMITTED PER BOUND AT ONE TIME.



STABILIZED CONSTRUCTION ENTRANCE

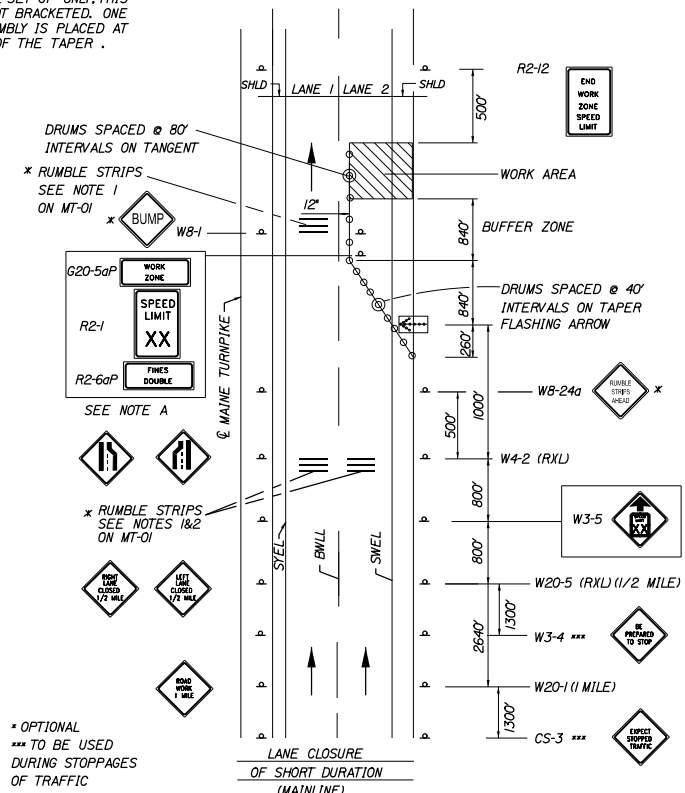
1. SIGN G20-2 IS NOT REQUIRED IF ENTRANCE IS WITHIN THE WORK ZONE.



SHOULDER WORK

1. THE W20-1 AND G20-2 SIGNS ARE NOT REQUIRED IF THE SHOULDER CLOSURE IS WITHIN A PREVIOUSLY ESTABLISHED WORK ZONE.

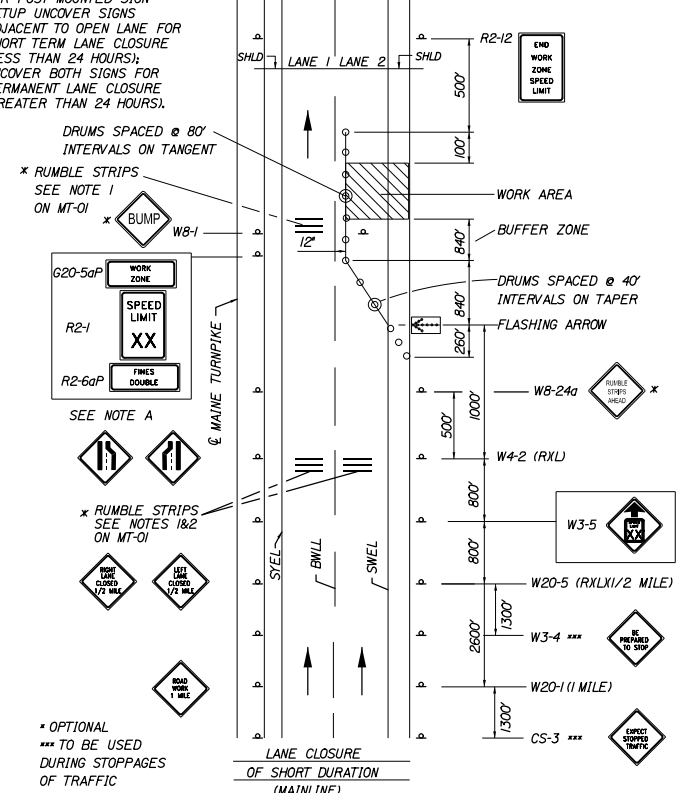
NOTE A: FOR EASEL SET UP ONLY, THIS SIGN IS NOT BRACKETED. ONE SIGN ASSEMBLY IS PLACED AT THE END OF THE TAPER.



SINGLE LANE CLOSURE - SIGNS ON EASELS

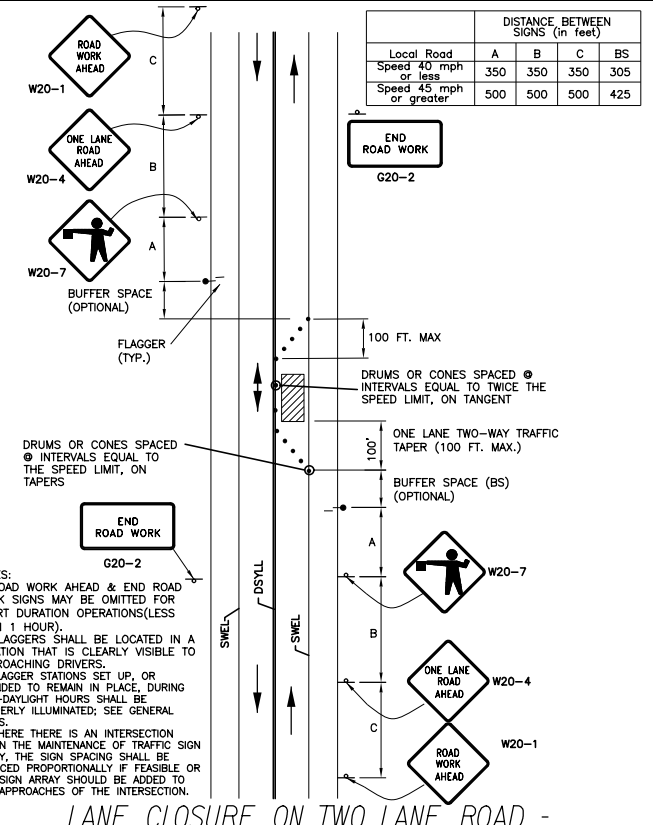
* OPTIONAL *** TO BE USED DURING STOPPAGES OF TRAFFIC

NOTE A: FOR POST MOUNTED SIGN SETUP UNCOVER SIGNS ADJACENT TO OPEN LANE FOR SHORT TERM LANE CLOSURE (LESS THAN 24 HOURS); UNCOVER BOTH SIGNS FOR PERMANENT LANE CLOSURE (GREATER THAN 24 HOURS).



SINGLE LANE CLOSURE - POST MOUNTED SIGN SETUP

* OPTIONAL *** TO BE USED DURING STOPPAGES OF TRAFFIC



LANE CLOSURE ON TWO LANE ROAD - WITH FLAGGERS

- NOTES:
1. ROAD WORK AHEAD & END ROAD WORK SIGNS MAY BE OMITTED FOR SHORT DURATION OPERATIONS (LESS THAN 1 HOUR).
 2. FLAGGERS SHALL BE LOCATED IN A LOCATION THAT IS CLEARLY VISIBLE TO APPROACHING DRIVERS.
 3. FLAGGER STATIONS SET UP, OR INTENDED TO REMAIN IN PLACE, DURING NON-DAYLIGHT HOURS SHALL BE PROPERLY ILLUMINATED; SEE GENERAL NOTES.
 4. WHERE THERE IS AN INTERSECTION WITHIN THE MAINTENANCE OF TRAFFIC SIGN ARRAY, THE SIGN SPACING SHALL BE REDUCED PROPORTIONALLY IF FEASIBLE OR THE SIGN ARRAY SHOULD BE ADDED TO THE APPROACHES OF THE INTERSECTION.

Local Road	DISTANCE BETWEEN SIGNS (in feet)			
	A	B	C	BS
Speed 40 mph or less	350	350	350	305
Speed 45 mph or greater	500	500	500	425

- #### GENERAL MAINTENANCE OF TRAFFIC NOTES:
1. ALL PAVEMENT STRIPING & SIGNING SHALL BE IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", U.S.D.O.T., F.H.W.A., LATEST EDITION.
 2. THESE PLANS SHOW THE GENERAL CONDITION FOR TURNPIKE MAINLINE TRAFFIC CONTROL DURING CONSTRUCTION. SLIGHT MODIFICATIONS IN CONSTRUCTION PROCEDURE MAY OCCUR AND MAY REQUIRE SOME MINOR ADJUSTMENTS TO BE MADE IN THE FIELD. ALL PROCEDURES MUST BE APPROVED BY THE RESIDENT.
 3. EXPOSED BARRIER ENDS SHALL BE PROTECTED BY A WORK ZONE CRASH CUSHION. PAYMENT WILL BE UNDER ITEM 527.341-WORK ZONE CRASH CUSHION - TL-3.
 4. SPEED LIMIT, STOP, YIELD AND EXIT SIGNS SHALL BE A MINIMUM OF 5' ABOVE THE PAVEMENT.

- #### ABBREVIATIONS FOR ALL M.O.T. PLANS
- BWLL = BROKEN WHITE LANE LINE
 - SWEL = SOLID WHITE EDGE LINE
 - SYEL = SOLID YELLOW EDGE LINE
 - TBLL = TEMPORARY BROKEN WHITE LANE LINE
 - TSWLL = TEMPORARY SOLID WHITE LANE LINE
 - TSYLL = TEMPORARY SOLID YELLOW LANE LINE

- #### TEMPORARY RUMBLE STRIP NOTES
1. IF RUMBLE STRIPS ARE USED THEY SHALL BE PLACED IN ONE OF THE FOLLOWING CONFIGURATIONS:
 - * ADJACENT TO THE WORK ZONE (1 UNIT)
 - * UPSTREAM FROM THE TAPER FOR THE WORKZONE (2 UNITS)
 - * BOTH ADJACENT TO THE WORKZONE AND PRIOR TO THE TAPER (3 UNITS)
 W8-1 SIGNS SHALL BE PLACED ADJACENT TO THE FIRST RUMBLE STRIP AT ANY LOCATION. ONLY ONE SET OF W8-24a SIGNS ARE REQUIRED FOR ANY OF THE ABOVE CONFIGURATIONS.
 2. RUMBLE STRIPS MAY BE PLACED UPSTREAM OF THE TAPER BETWEEN THE W3-5 SIGNS AND THE W4-2 SIGNS. IF RUMBLE STRIPS ARE INSTALLED PRIOR TO TAPER, W8-1 SIGNS SHALL BE PLACED ADJACENT TO THE FIRST STRIP AND THE W8-24a SIGNS SHALL BE MOVED TO 400' AFTER THE W20-5 SIGNS.

Draft PS&E
October 11, 2024

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: TIM COTE, P.E.

By	Date	By	Date
Designed	PDB 10/24	Checked	PDB 10/24
Drawn	CET 10/24	In Charge of	TRC 10/24

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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: KRISTI VAN OUYEN, P.E.

BRIDGE REPAIRS
STEVENSTOWN BRIDGE UNDERPASS
MAINTENANCE OF TRAFFIC
DETAILS AND GENERAL NOTES

SHEET NUMBER: MOT-01
CONTRACT: 2024.17
3 OF 6

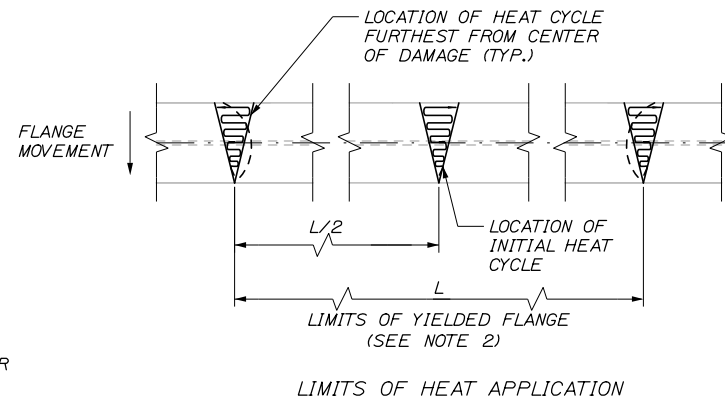
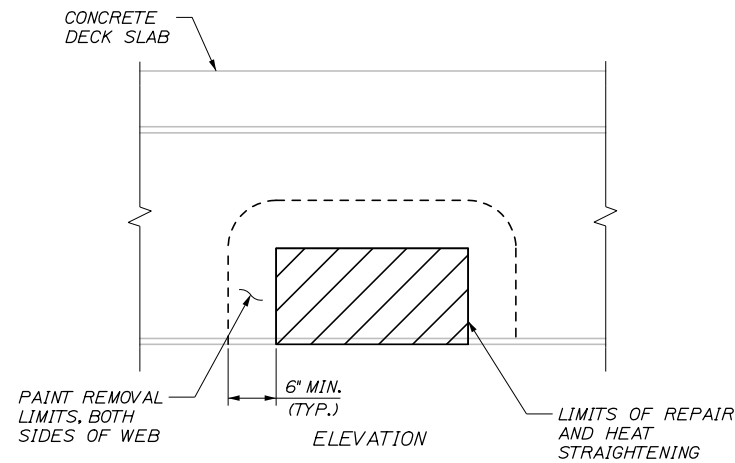
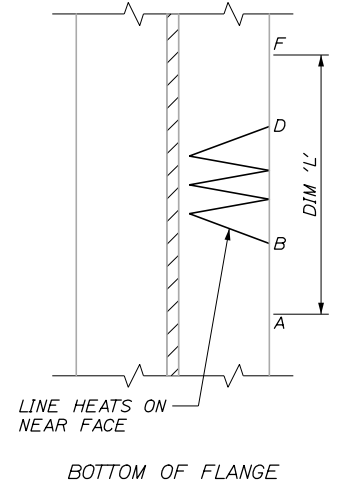
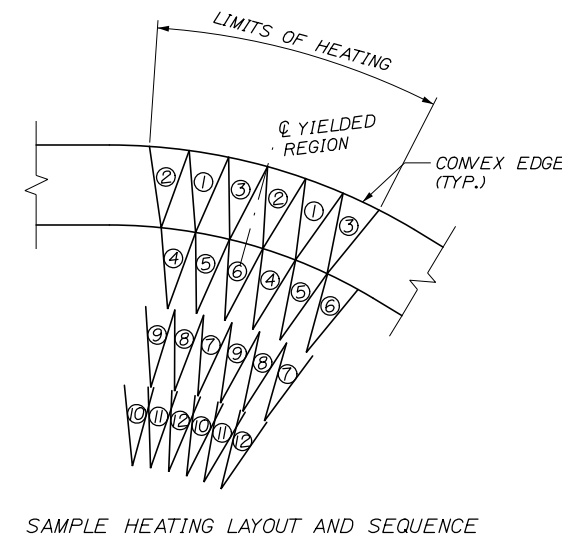
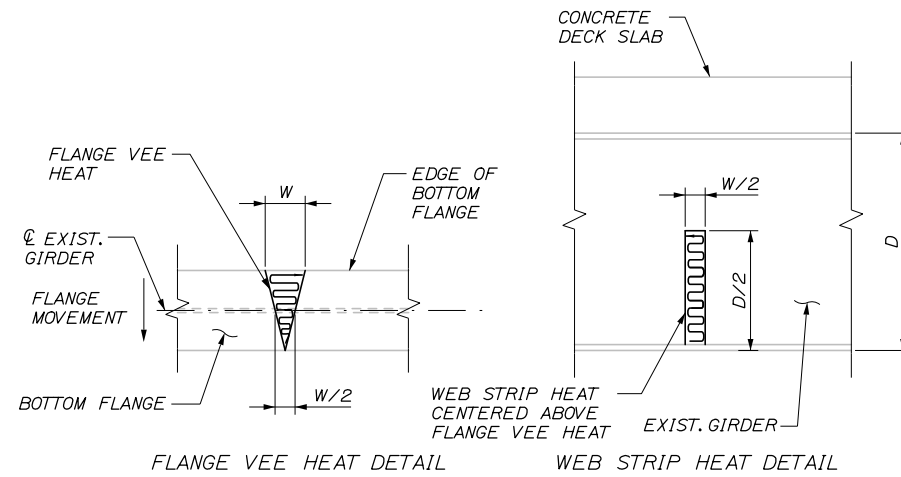
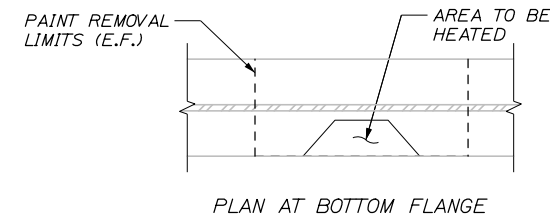
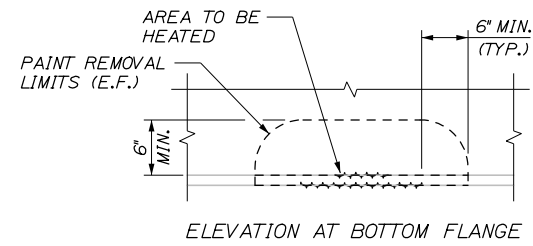
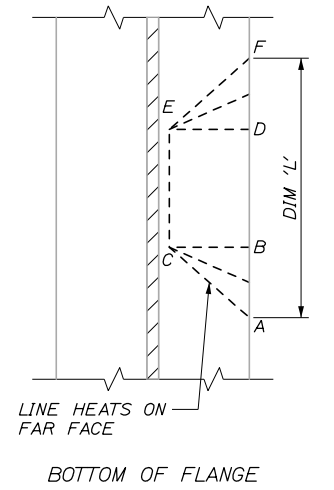
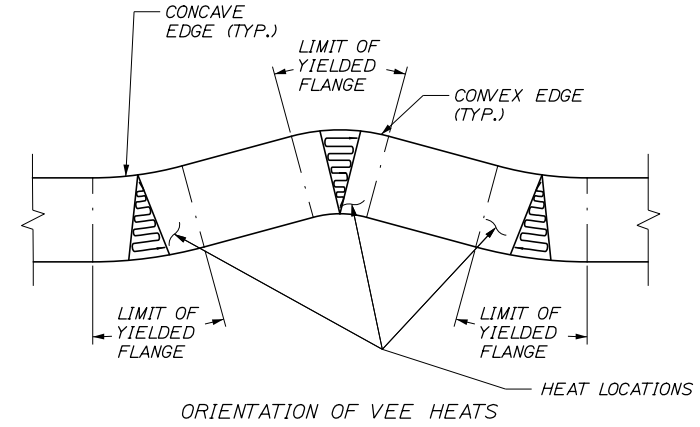
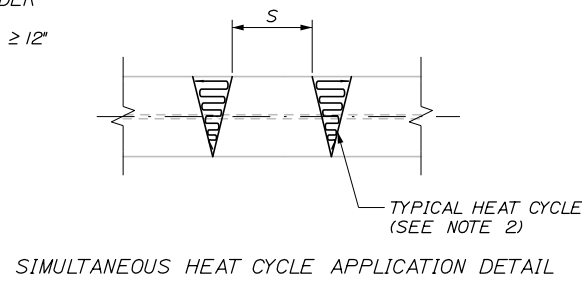
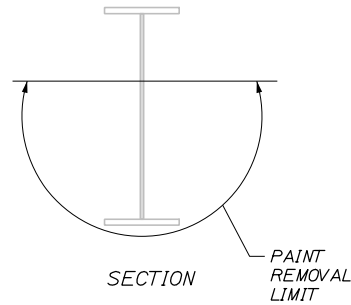
GENERAL NOTES:

1. THE HEAT STRAIGHTENING DETAILS AND PATTERNS SHOWN ON THIS SHEET ARE PROVIDED FOR CONTRACTOR REFERENCE AND CONVENIENCE.
2. ALL PROPOSED HEAT STRAIGHTENING PATTERNS, DETAILS AND PROCEDURES SHALL BE IN ACCORDANCE WITH THE PROJECT SPECIAL PROVISIONS AND SHALL BE SUBMITTED TO THE RESIDENT FOR APPROVAL.

Draft PS&E
October 11, 2024

LEGEND:

- W = FLANGE WIDTH/2 ≤ 10"
- D = DEPTH OF GIRDER
- S = FLANGE WIDTH ≥ 12"



HEATING PATTERN TO REPAIR BEAM SWEEP

HEATING PATTERN TO REPAIR FLANGE BULGE

NOTES:

1. IF TRAFFIC STOPPAGES ARE UTILIZED, ALL AREAS TO RECEIVE HEAT SHALL BE HEATED SIMULTANEOUSLY OR IN RAPID SUCCESSION. RAPID SUCCESSION SHALL BE DEFINED AS MOVING FROM ONE LOCATION TO THE NEXT WITHOUT DELAY.
2. VEE HEATS SHALL BE PLACED STARTING AT THE CENTER OF EACH HEAT LOCATION. SUCCESSIVE VEE HEATS SHALL BE PLACED WITH THE APEX CENTERED BETWEEN THE APEXES OF PREVIOUS HEATS. RETRACING PREVIOUS HEAT LOCATIONS SHALL ONLY BE ALLOWED ONCE THE SPACE BETWEEN PREVIOUS APEXES IS LESS THAN 1/2".
3. THE LIMIT OF YIELDED FLANGE SHALL BE CONSIDERED THE LENGTHS OF FLANGE THAT ARE CURVED IN PLAN.

NOTES:

1. HEATS SHOULD BE APPLIED ON THE FACES OF THE FLANGE THAT ARE CONVEXLY CURVED.
2. LENGTHS OF CONTINUOUS CURVATURE OFTEN OCCUR IN REGIONS SUCH AS A-B-C AND D-E-F. IN THESE AREAS LINE HEATS SHOULD BE SPACED APPROXIMATELY ONE-FOURTH THE FLANGE WIDTH.
3. APPLY LINE HEATS TO AREAS A-B-C AND D-E-F FIRST FOLLOWED BY AREA B-C-D-E.
4. LINE HEATS ALONG LINE ACEF SHOULD BE APPLIED ALONG EDGE OF DEFORMATION.

Date:10/17/2024

Filename: 004_Repair_Details.dgn

Scale: AS NOTED			
No.	Revision	By	Date

Designed by:					
HNTB					
CONSULTANT PROJECT MANAGER: TIM COTE, P.E.					
	By	Date		By	Date
Designed	NTB	10\24	Checked	JDW	10\24
Drawn	ERB	10\24	In Charge of	TRC	10\24

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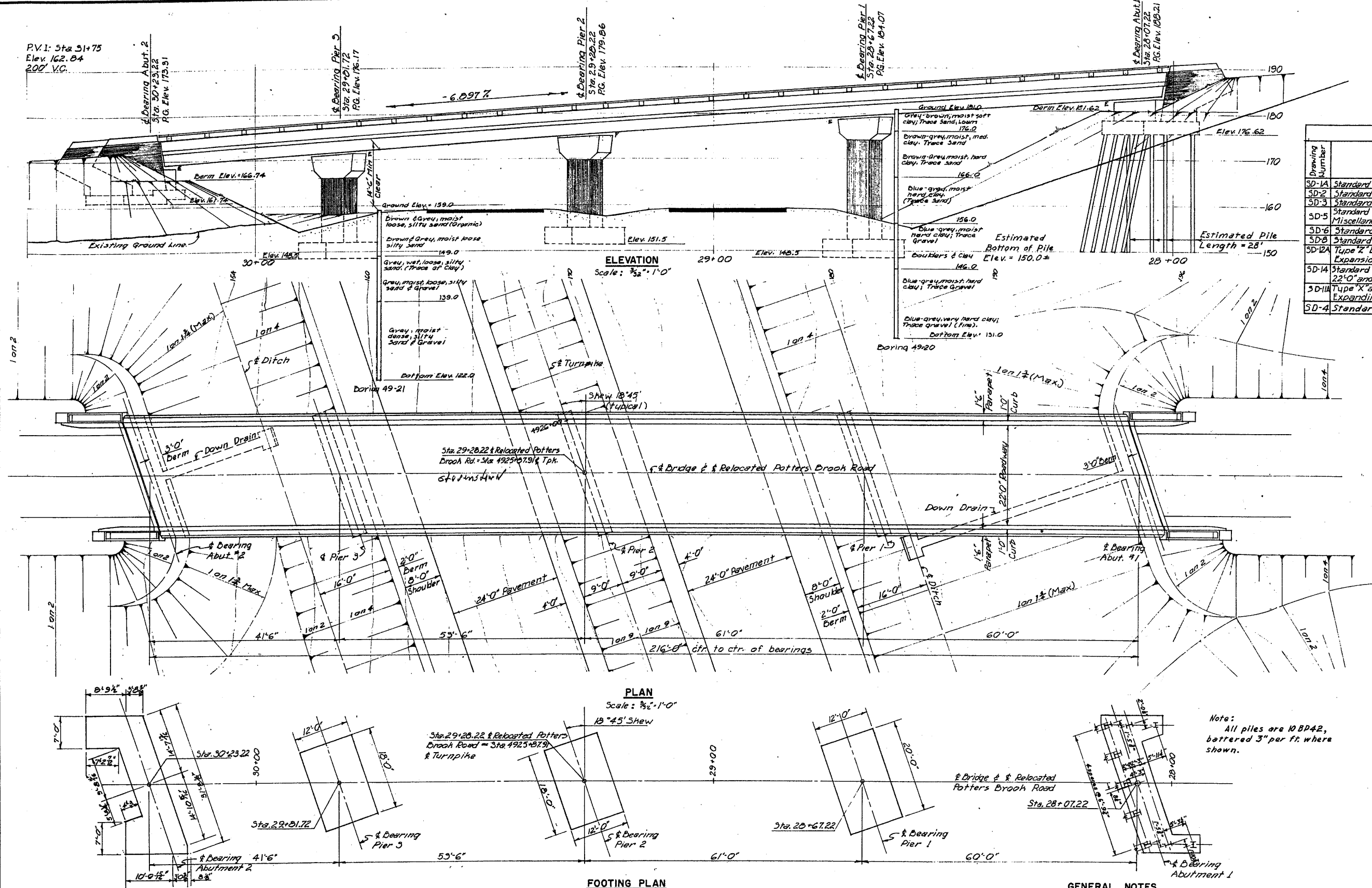
**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: KRISTI VAN OUYEN, P.E.

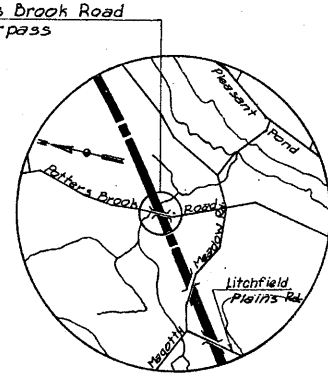
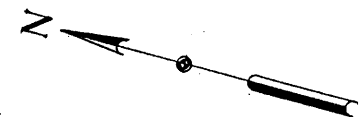
STEVENSTOWN ROAD
BRIDGES REPAIRS

STRUCTURAL REPAIR DETAILS

SHEET NUMBER: S-02
CONTRACT:2024.17
6 OF 6



REFERENCES				
Drawing Number	Title	Substructure	Superstructure	
		Steel	Abutment	Floor
		Reactor	Tractor	Contract
SD-1A	Standard Abutment Details	✓	✓	✓
SD-2	Standard Pier Details	✓	✓	✓
SD-3	Standard Abut. Drainage Details	✓	✓	✓
SD-5	Standard Handrail-Bearing Devices and Miscellaneous Details	✓	✓	✓
SD-6	Standard Diaphragm Details	✓	✓	✓
SD-8	Standard Type 'A' Splices for 30W Beam	✓	✓	✓
SD-12A	Type 'Z' Expansion Joint	✓	✓	✓
SD-14	Expansion Length over 100 Feet	✓	✓	✓
SD-14	Standard Bridge Floor Cross-Section 22'-0" and 20'-0" Roadway	✓	✓	✓
SD-11A	Type 'X' and 'Y' Expansion Joints Expanding Length to 100 Feet	✓	✓	✓
SD-4	Standard Pile Details	✓	✓	✓



Note:
All piles are 10 BP42,
battered 3" per ft. where
shown.

GENERAL NOTES

Design Specifications: AASHTO (1953) with minor modification.
Design live Load: H15-44
Max. Base Pressure:
Abutment 2 - 1.5 Tons/sq ft
Pier 1 -
Piers 2 & 3 -
Max. Pile Load:
Abutment 1 - 26.6 Tons/pile

DRAWING 75-01.03

MADE	BY	DATE	REVISION	BY	DATE
WGA	WGA	5-4-54	3 As-Built	WGA	12-5-54
			2 Revised Abut. 1	BSK	5-16-55
			1 Correction S.D. Number	KJL	5-11-54

Note:
For details of Abutment drainage, see
Std. Dwg. No. 3.

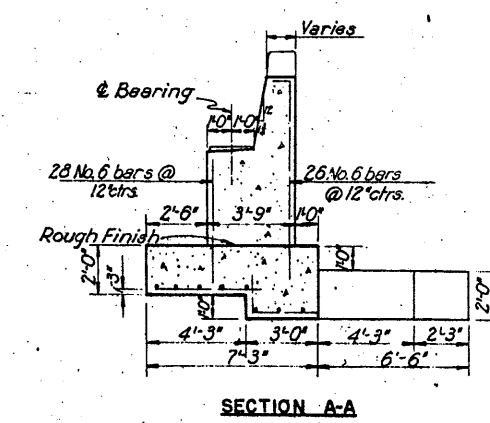
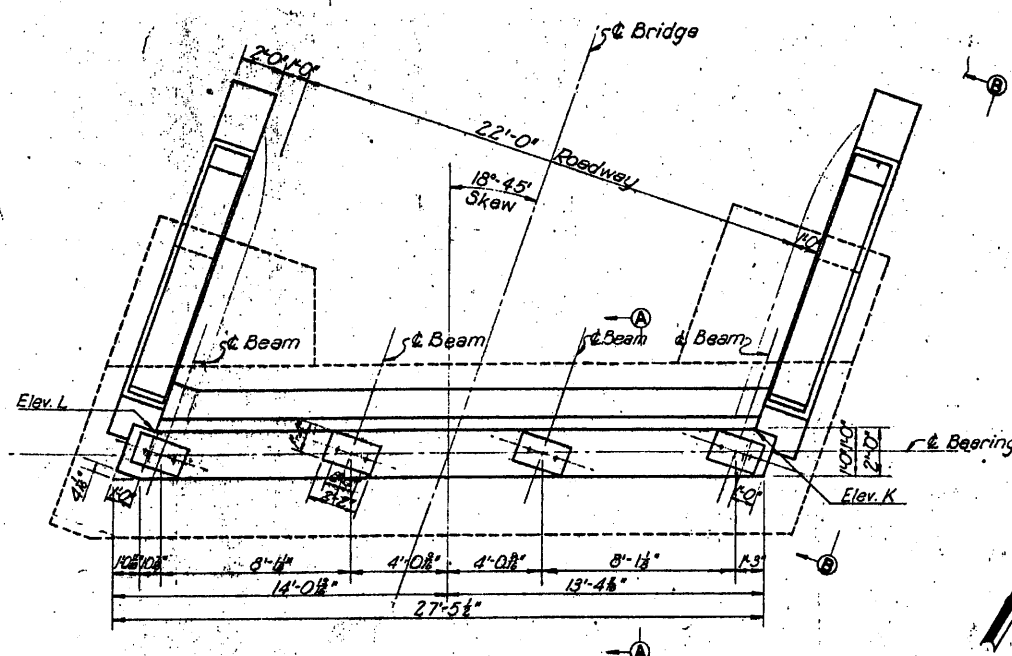
MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
SECTION 2 - PORTLAND TO AUGUSTA

STRUCTURE NO. 75 TURNPIKE UNDER
POTTERS BROOK ROAD
STA. 4925+87.91

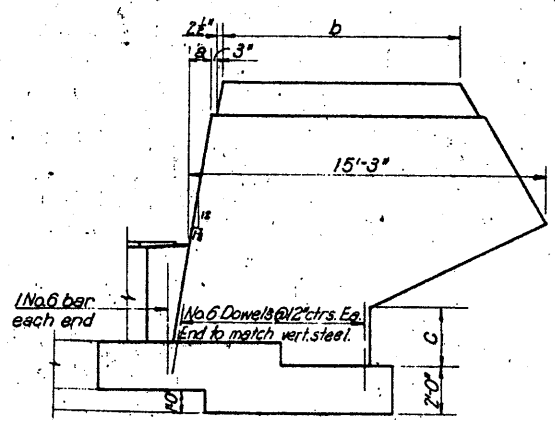
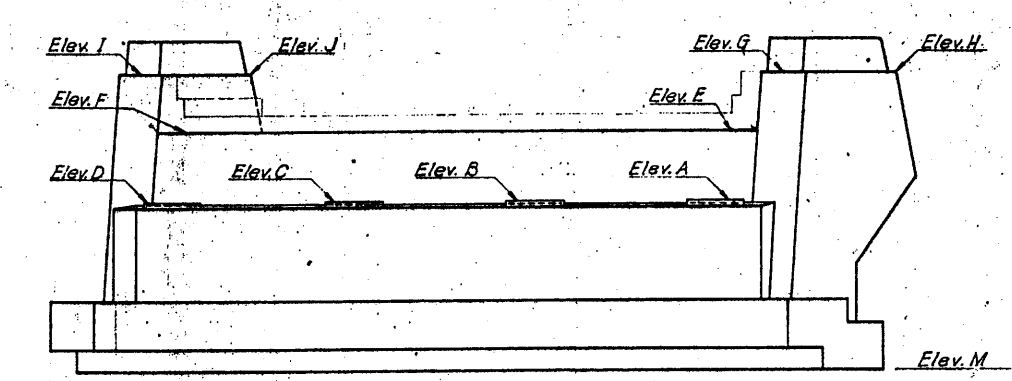
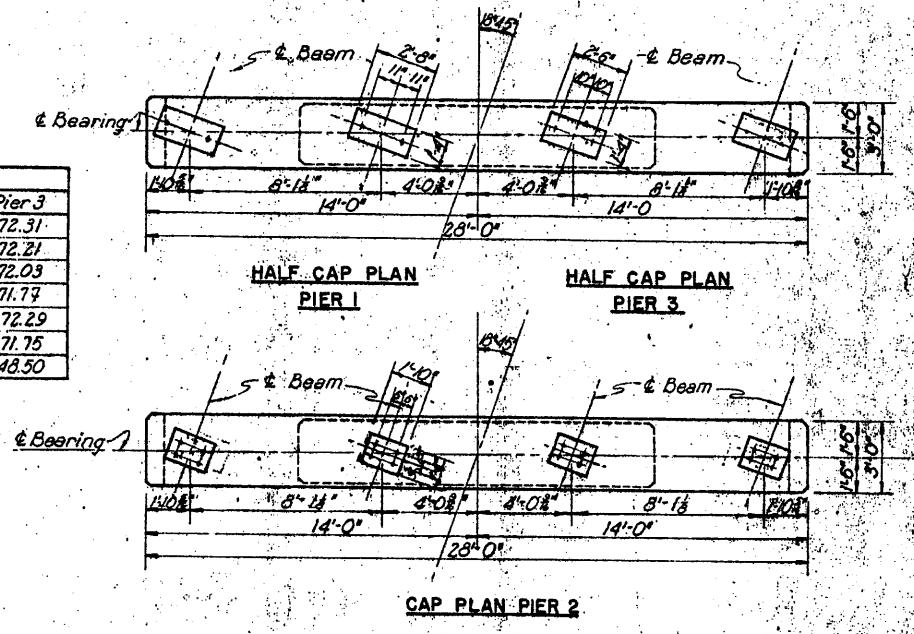
GENERAL PLAN AND ELEVATION

HOWARD, NEEDLES, TAMMEN & BERGENOFF
CONSULTING ENGINEERS
NEW YORK KANSAS CITY

SCALE: As Shown
CONTRACT NO.:
SHEET NO. 222 OF 222

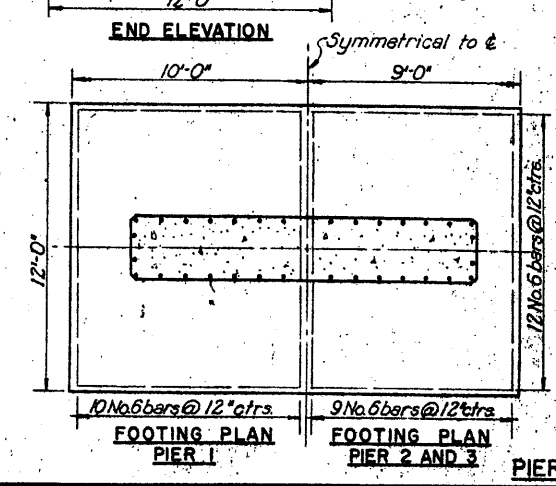
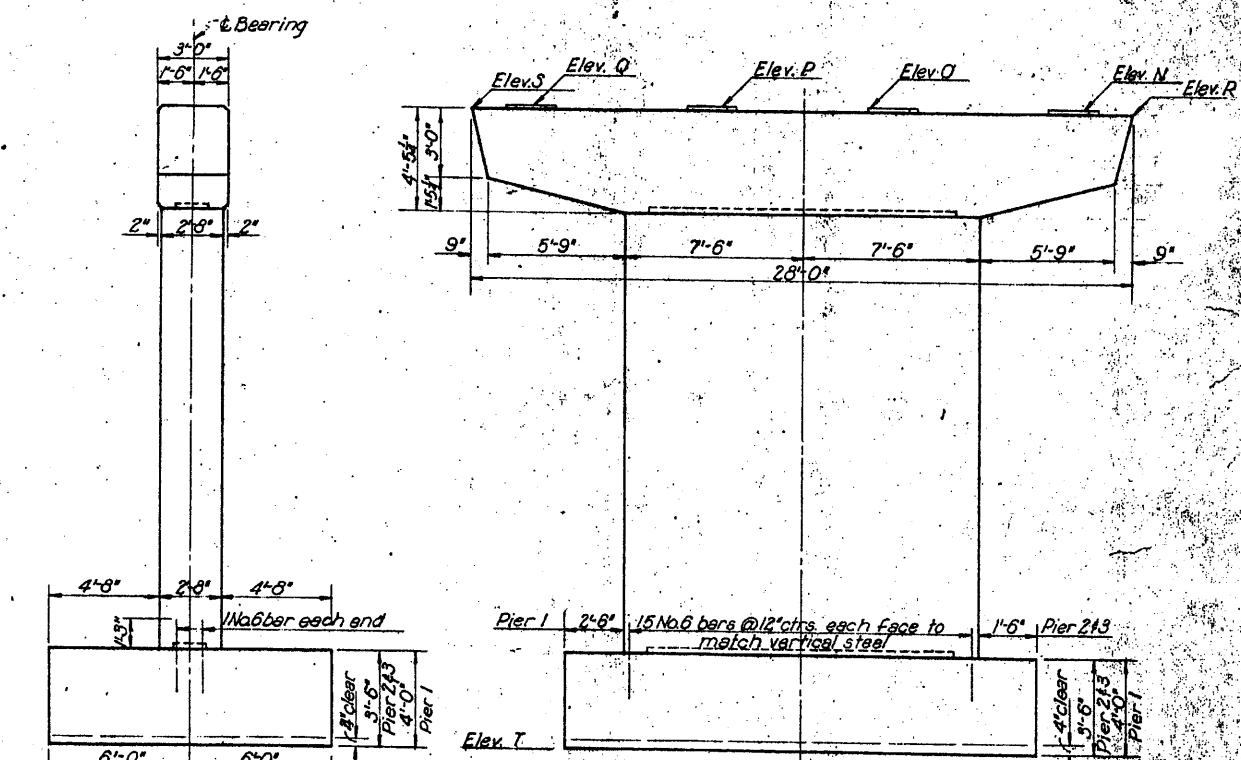
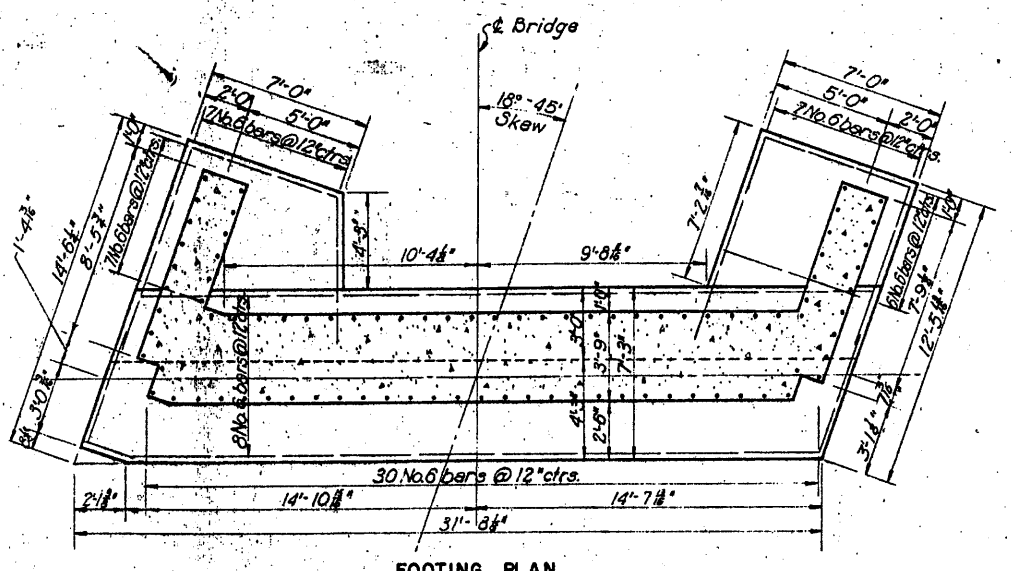


PIER ELEVATIONS			
Elev.	Pier 1	Pier 2	Pier 3
N	179.97	175.97	172.31
O	179.91	176.90	172.21
P	179.73	175.72	172.03
Q	179.44	175.43	171.73
R	179.95	175.95	172.29
S	179.42	175.41	171.75
T	148.50	151.50	148.50



WINGWALL DIMENSIONS		
Abutment 2		
	E. W.W.	W. W.W.
a	11'-8"	11'-8"
b	10'-2 1/2"	10'-2 1/2"
c	2'-1 1/2"	2'-7 1/2"

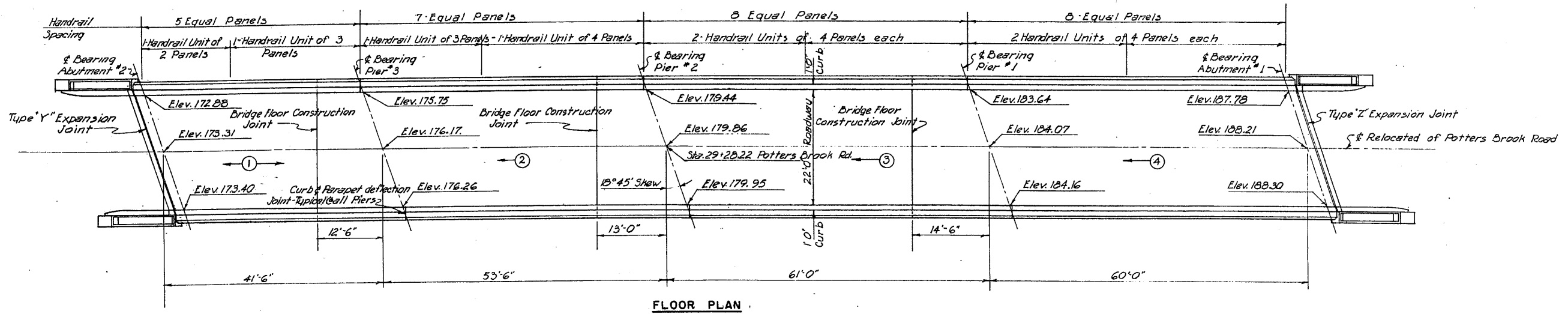
ABUTMENT ELEVATIONS	
Elev.	Abutment 2
A	168.91
B	169.20
C	169.38
D	169.44
E	172.00
F	172.57
G	172.89
H	174.03
I	175.45
J	174.64
K	168.82
L	169.36
M	161.74



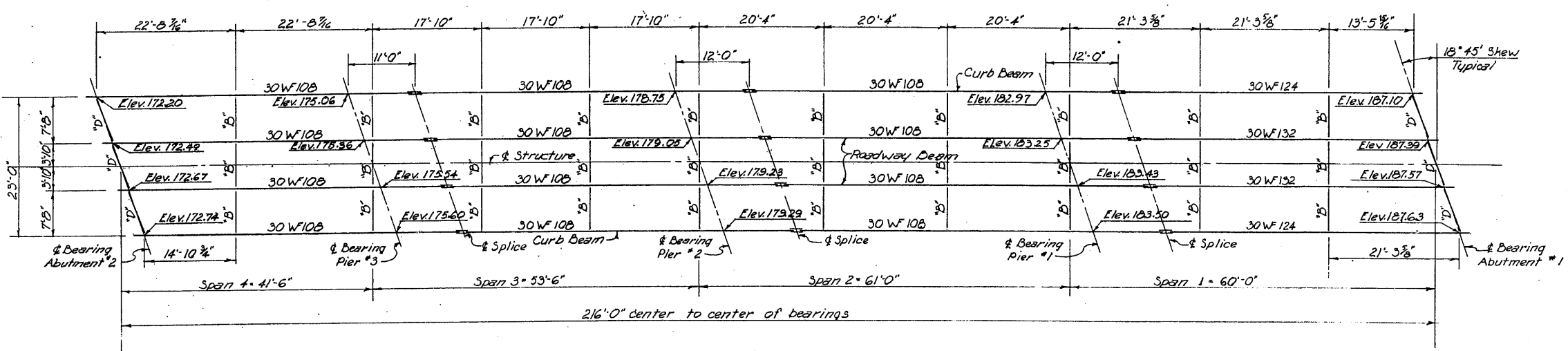
DRAWING 75.02A.03			
BY	DATE		
MADE	L.I.B. 4-23-64		
TRACED		2 As-Built	HBH/2556
CHECKED	W.E.G. 5-7-64	1 Removed Abutment SK 5-7-64	
IN CHARGE OF	I.D.S.K.	NO.	REVISION
			BY DATE

ABUTMENT 2

MAINE TURNPIKE AUTHORITY
SECTION 2 - PORTLAND TO AUGUSTA
 STRUCTURE NO. 78 TURNPIKE UNDER
 POTTERS BROOK ROAD
 STA. 4925+87.91
ABUTMENTS AND PIERS
 HOWARD, NEEDLES, TAMMEN & BERGENDOFF
 CONSULTING ENGINEERS
 NEW YORK KANSAS CITY
 CONTRACT NO. 1-110
 SHEET NO. 225 OF 228

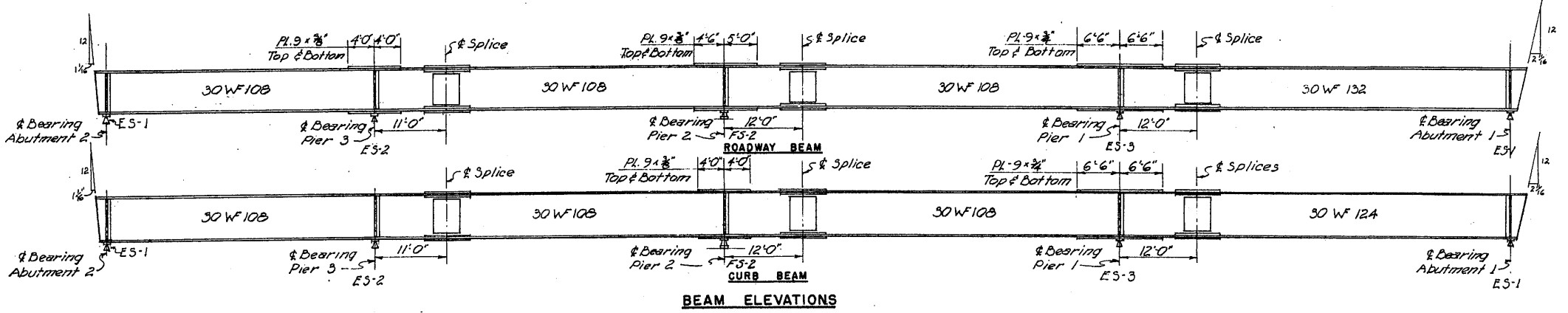


FLOOR PLAN

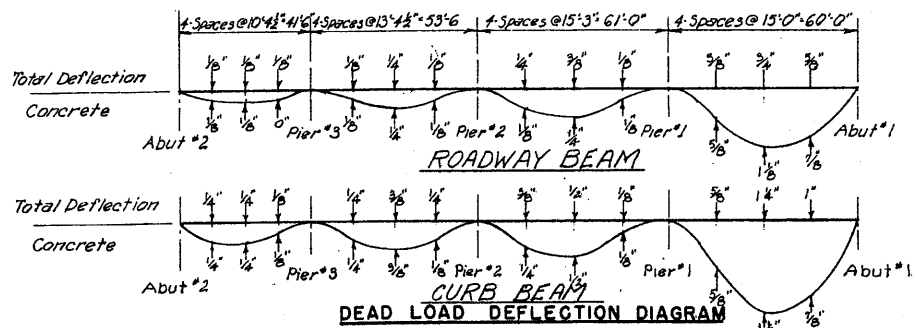


FRAMING PLAN

Notes:
 Elevations shown on the Floor Plan are finished elevations at the top of the wearing surface.
 Elevations shown on Framing Plan are to top of beam flanges under full dead load.
 Sequence & direction of placing concrete is noted thus: (1)
 Marks "B" and "D" on Framing Plan refer to Interior and End Diaphragms shown on Standard Drawing No. 6.
 Type "Z" Expansion Joint is shown on Standard Drawing No. 12-A.
 All bearing stiffeners are 7x4x1/2 Angles.
 Bevel at ends of beams is with respect to Axis of beams.



BEAM ELEVATIONS

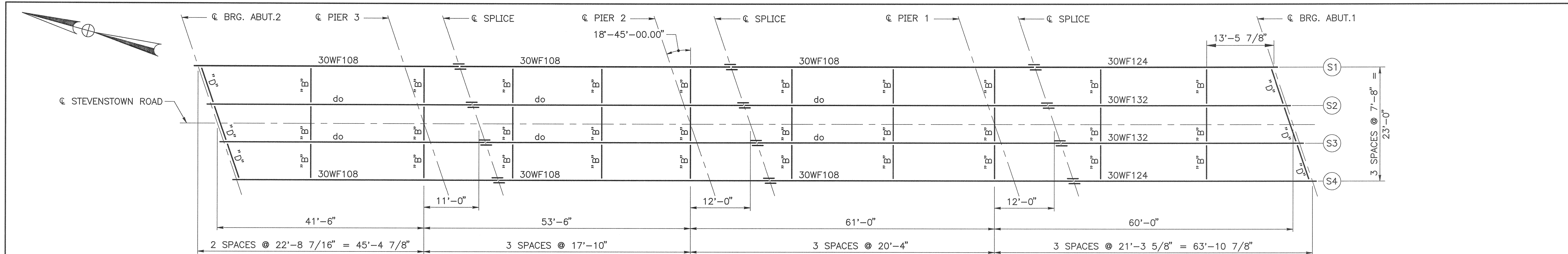


DEAD LOAD DEFLECTION DIAGRAM

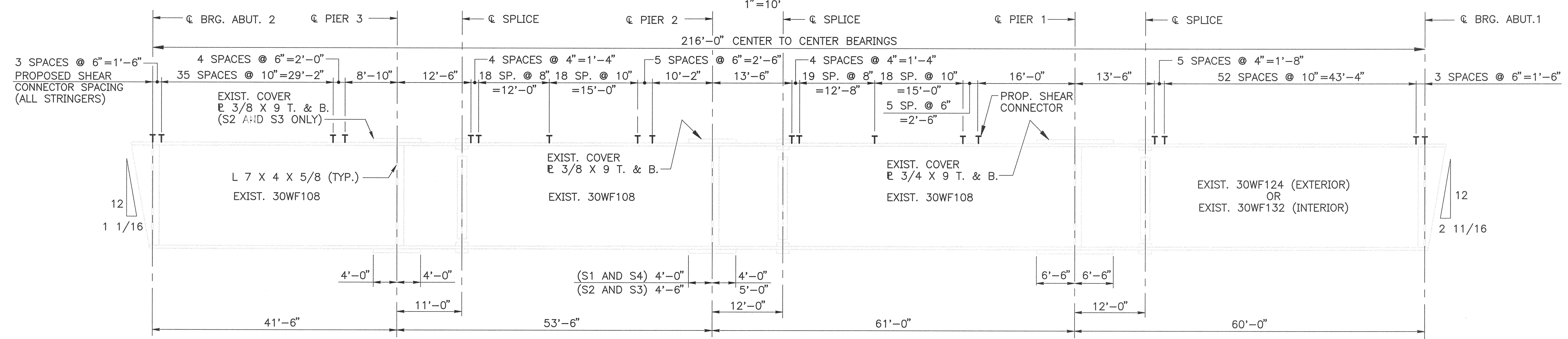
DRAWING 75.03.03

NO.	BY	DATE	REVISION	BY	DATE
MADE	W.E.G.	4-20-54			
TRACED			2 As-Built	HBN	12-25-54
CHECKED	L.I.B.	5-8-54	1 Added FS-2	DHL	5-11-54
IN CHARGE OF	I.D.S.H.				

MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
 SECTION 2 - PORTLAND TO AUGUSTA
 STRUCTURE NO. 75 TURNPIKE UNDER
 POTTERS BROOK ROAD
 STA. 4925 + 87.91
SUPERSTRUCTURE
 HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS SCALE: AS NOTED
 NEW YORK KANSAS CITY CONTRACT NO. SHEET NO. 335 OF 382

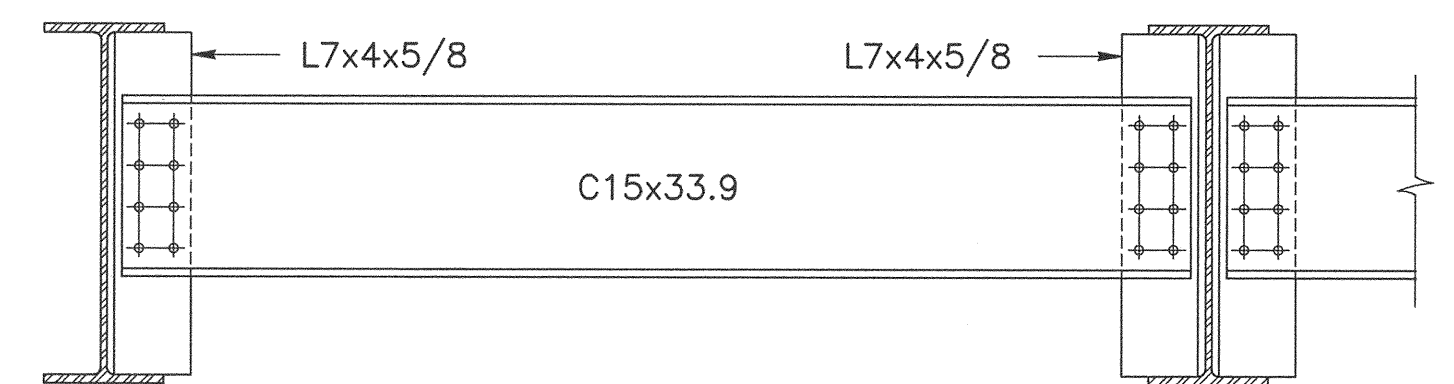


EXISTING FRAMING PLAN
1"=10'

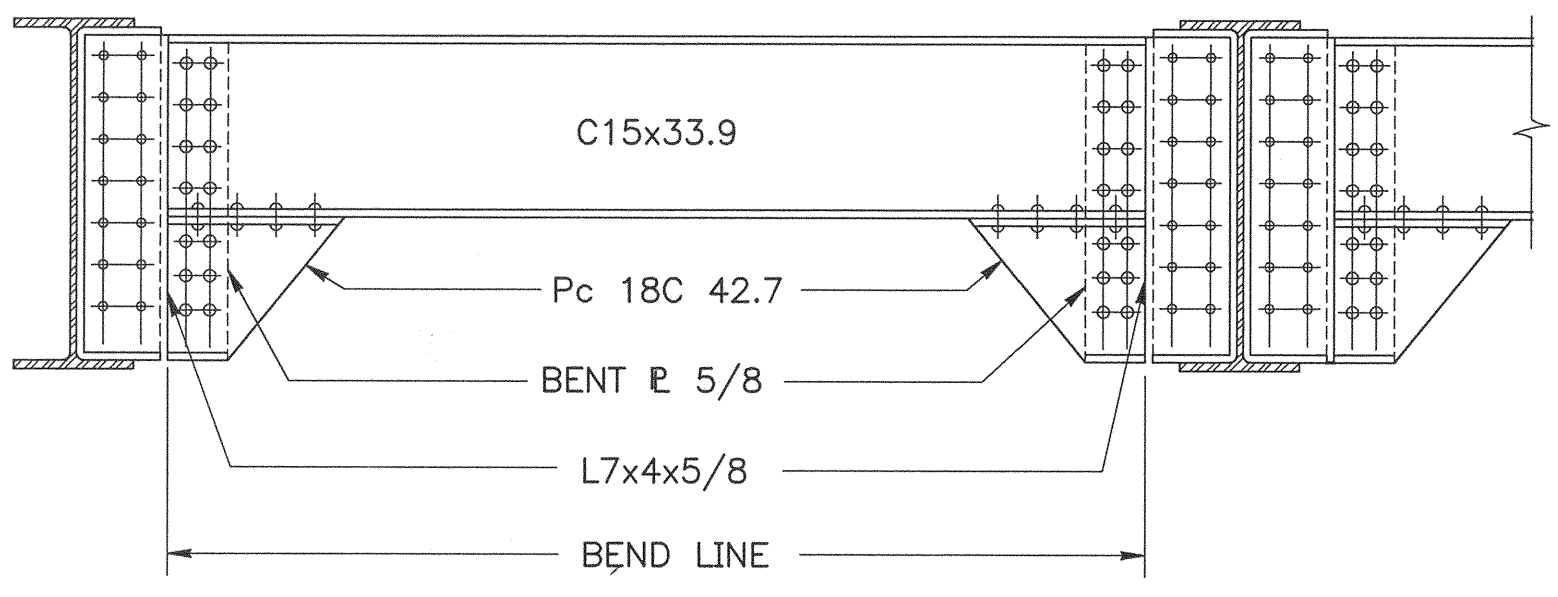


STRINGER ELEVATION
1"=10' (HORIZONTAL)

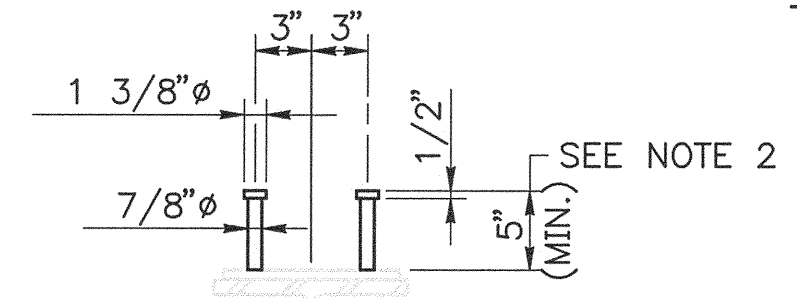
NOTE
SOME DIAPHRAGMS NOT SHOWN FOR CLARITY.



EXISTING DIAPHRAGM TYPE B
3/4"=1'-0"



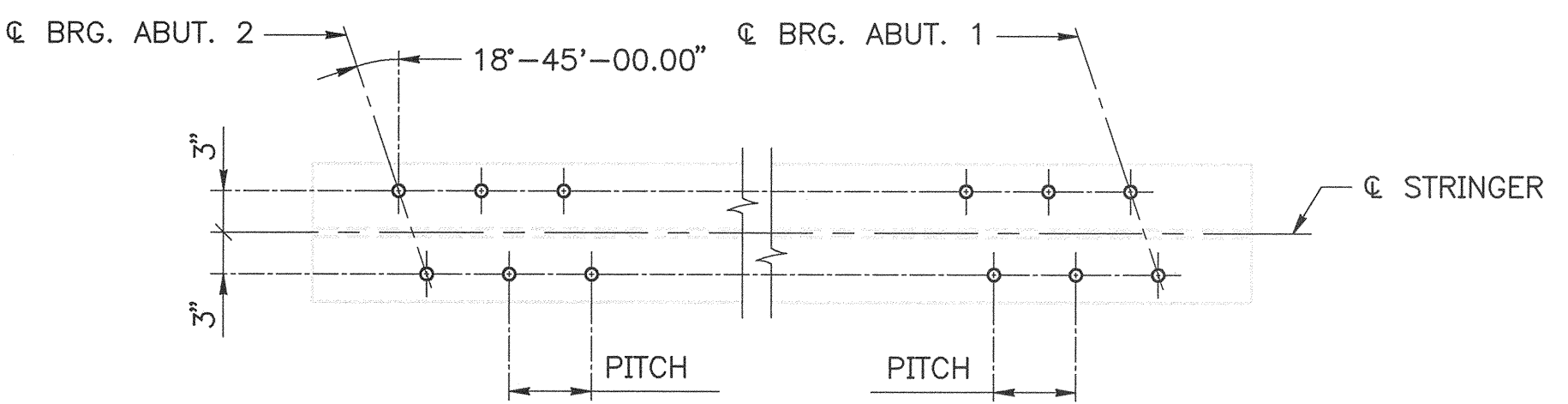
EXISTING DIAPHRAGM TYPE D
3/4"=1'-0"



SHEAR CONNECTOR NOTES

1. PROPOSED SHEAR CONNECTOR SPACING ARE SHOWN ON THE STRINGER ELEVATION.
2. SHEAR CONNECTOR HEIGHT SHALL BE 7" FOR EXTRA DEPTH HAUNCH AREAS, SEE SHEET NO. SR-11.

PROPOSED SHEAR CONNECTOR DETAIL
1"=1'-0"



SHEAR CONNECTOR LAYOUT
1"=1'-0"

M:\09009\958\049DECK\SI\FRMSR.DWG 04/18/00 08:27

Scale: AS NOTED

No.	Revision	By	Date

Designed by:

HNTB
ARCHITECTS ENGINEERS PLANNERS

By	Date	Checked	By	Date
CLC	4/00	HNL	HNL	4/00
Drawn	SHR	In Charge of	RAL	4/00

HNTB CORPORATION
2 Thomas Drive
Westbrook, ME 04092
TEL (207) 774-5155
FAX (207) 772-7410

MAINE TURNPIKE AUTHORITY

STEVENSTOWN ROAD UNDERPASS

FRAMING PLAN AND
STRINGER ELEVATION

SHEET NUMBER: SR-9
CONTRACT: 2000.11
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