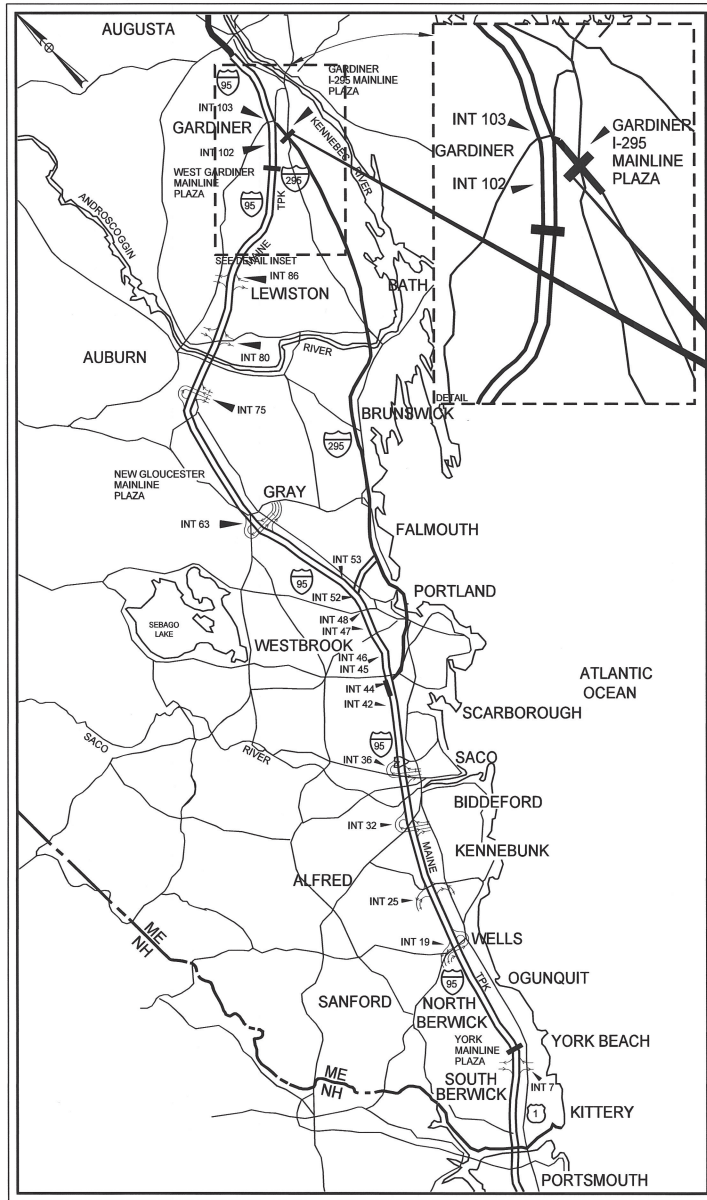


Date: 2/8/2019

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LOCATION MAP

INTERCHANGE 103
BARRIER TOLL PLAZA
OPEN ROAD TOLLING CONVERSION
MM 103.0



THE GOLD STAR
MEMORIAL HIGHWAY

MAINE TURNPIKE AUTHORITY

DANIEL E. WATHEN, CHAIR
ROBERT D. STONE, VICE CHAIR
MICHAEL J. CIANCHETTE, MEMBER
JOHN E. DORITY, MEMBER
ANN R. ROBINSON, MEMBER
THOMAS J. ZUKE, MEMBER
BRUCE A. VAN NOTE, MEMBER EX-OFFICIO

S. PETER MILLS, EXECUTIVE DIRECTOR

INTERCHANGE 103 BARRIER TOLL PLAZA OPEN ROAD TOLLING CONVERSION

(MM 103.0)

2019.04

VOLUME 1 OF 3



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BUILDING ARCHITECTURAL

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APPROVED:

MAINE TURNPIKE AUTHORITY

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LAUREN MEEK, P.E. - ASSOCIATE
STANTEC

3-20-2019
DATE

SHEET 413
SHEETS 415-416
SHEETS 473-492

ALAIN HADDAD, P.E. - SR. MECHANICAL ENGINEER
STANTEC

3-20-2019
DATE

William Yates III
WILLIAM YATES III - DIRECTOR OF INFORMATION SERVICES AND COMMUNICATIONS
MTA

DATE

SHEETS 230-231
SHEETS 359-410

GREGORY EDWARDS, P.E. - SENIOR PRINCIPAL
STANTEC

3-20-2019
DATE

SHEETS 463-472

THOMAS DENSFORD, P.E. - ASSOCIATE
STANTEC

3-20-2019
DATE

SHEETS 172-205
BRANDON HAVU, P.E. - PROJECT MANAGER
GORRILL PALMER

3-20-2019
DATE

Stephen R. Tartre
STEPHEN R. TARTRE, P.E. - DIRECTOR OF ENGINEERING AND BUILDING MAINTENANCE
MTA

DATE

SHEETS 206-220
SHEETS 414-415
SHEET 417
SHEETS 493-503

DAVID GLENNON, P.E. - SENIOR ELECTRICAL ENGINEER
STANTEC

3-20-2019
DATE

SHEETS 411-412
SHEETS 448-462

MICHAEL F. HAYS, R.A. - PRINCIPAL
GRANT HAYS

3-20-2019
DATE

SHEETS 419-447
ROLAND LAVALLÉE, P.E., PLS - VICE PRESIDENT
DIRECTOR OF OPERATION, HNTB

2/11/19
DATE

CONTRACT 2019.04

Date: 3/20/2019

Filename: ...00.HIGHWAY\MSTA...Estimate.dgn


ESTIMATED QUANTITIES			
ITEM NO.	DESCRIPTION	TOTAL QUANTITY	UNIT
201.11	CLEARING	4.0	AC
201.23	REMOVING SINGLE TREE TOP ONLY	4	EA
201.24	REMOVING STUMP	4	EA
202.071	REMOVING ASBESTOS CONTAINING MATERIALS, TOLL BOOTH LAB TOPS	1	LS
202.081	REMOVING EXISTING BUILDING	1	LS
202.15	REMOVING EXISTING MANHOLE OR CATCH BASIN	6	EA
202.17	REMOVING EXISTING STRUCTURAL CONCRETE	1	LS
202.202	REMOVING PAVEMENT SURFACE	27,850	SY
202.203	PAVEMENT BUTT JOINTS	1,910	SY
202.205	RUMBLE STRIPS	29,850	EA
202.206	REMOVING RUMBLE STRIPS	8,650	LF
203.20	COMMON EXCAVATION	88,000	CY
203.21	ROCK EXCAVATION	200	CY
203.2310	UNDERGROUND STORAGE TANK REMOVAL	1	LS
203.2312	DISPOSAL / TREATMENT OF SPECIAL EXCAVATION	50	TON
203.25	GRANULAR BORROW	28,325	CY
206.082	STRUCTURAL EARTH EXCAVATION - MAJOR STRUCTURES	2,350	CY
304.09	AGGREGATE BASE COURSE - CRUSHED	11,285	CY
304.10	AGGREGATE SUBBASE COURSE - GRAVEL	18,135	CY
403.207	HOT MIX ASPHALT, 19.0 MM NOMINAL MAXIMUM SIZE	16,150	TON
403.208	HOT MIX ASPHALT, 12.5 MM	900	TON
403.2081	HOT MIX ASPHALT, 12.5 MM (POLYMER MODIFIED) - RAP	9,110	TON
403.209	HOT MIX ASPHALT, 9.5 MM NOMINAL MAXIMUM SIZE (SIDEWALKS, DRIVES, ISLANDS & INCIDENTALS)	100	TON
403.211	HOT MIX ASPHALT, 9.5 MM NOMINAL MAXIMUM SIZE (SHIMMING)	1,658	TON
403.213	HOT MIX ASPHALT, 12.5 MM NOMINAL MAXIMUM SIZE (BASE AND INTERMEDIATE BASE COURSE)	7,570	TON
409.152	BITUMINOUS TACK COAT NTSS-IHM TRACKLESS - APPLIED	15,700	GAL
419.30	SAWING BITUMINOUS PAVEMENT	5,760	LF
470.08	BERM DROPOFF CORRECTION - GRINDINGS	248	TON
470.081	BERM CORRECTION	4,500	LF
502.231	STRUCTURAL CONCRETE, SPACE FRAME PEDESTALS & FOOTINGS	96	CY
502.232	STRUCTURAL CONCRETE, UTILITY PITS	145	CY
502.261	STRUCTURAL CONCRETE, ORT SLABS	210	CY
502.262	STRUCTURAL CONCRETE, CASH SLABS	280	CY
502.263	STRUCTURAL CONCRETE, PLAZA ISLANDS, BUMPERS, AND CURTAIN WALLS	200	CY
503.14	EPOXY-COATED REINFORCING STEEL, FABRICATED & DELIVERED	43,700	LB
503.15	EPOXY-COATED REINFORCING STEEL, PLACING	43,700	LB
503.181	GLASS FIBER REINFORCED POLYMER (GFRP) REINFORCING BARS (*5), FAB & DELIVERED	83,500	LF
503.182	GLASS FIBER REINFORCED POLYMER (GFRP) REINFORCING BARS (*12), FAB & DELIVERED	180	LF
503.191	GLASS FIBER REINFORCED POLYMER (GFRP) REINFORCING BARS (*5), PLACING	83,500	LF
503.192	GLASS FIBER REINFORCED POLYMER (GFRP) REINFORCING BARS (*12), PLACING	180	LF
503.90	SYNTHETIC FIBER REINFORCEMENT	2,450	LB
504.50	TOLL PLAZA CANOPIES	1	LS
504.80	SPACE FRAME CANOPIES, FABRICATED AND DELIVERED	1	LS
504.81	SPACE FRAME CANOPIES, ERECTION	1	LS
504.90	STEEL POST SUPPORT SYSTEMS	1	LS
504.91	MOUNTING BRACKET ASSEMBLIES	1	LS
504.96	METAL STAIRS	1	LS
508.14	HIGH PERFORMANCE WATERPROOFING MEMBRANE	1	LS
511.091	TEMPORARY EARTH SUPPORT SYSTEMS	1	LS
515.2011	PIGMENTED CONCRETE PROTECTIVE COATING - TUNNEL AND STAIRWAY WALLS & CEILING	750	SY
515.2012	PIGMENTED CONCRETE PROTECTIVE COATING - TUNNEL FLOOR	270	SY
515.202	CLEAR PROTECTIVE COATING FOR CONCRETE SURFACES	2,500	SY
515.23	EPOXY OVERLAY	225	SF
526.306	TEMPORARY CONCRETE BARRIER TYPE I (9,100 LF)	1	LS
526.351	MEDIAN BARRIER TYPE I	450	LF
526.3511	MEDIAN BARRIER TYPE IA - PRECAST	2,806	LF
526.3513	MEDIAN BARRIER TYPE IB - PRECAST	938	LF
526.3514	MEDIAN BARRIER TYPE IC - PRECAST	170	LF
526.3515	MEDIAN BARRIER TYPE ID - PRECAST	107	LF
526.3516	MEDIAN BARRIER TYPE IE - PRECAST	51	LF
526.352	MEDIAN BARRIER TYPE II	134	LF
526.3611	MEDIAN BARRIER TRANSITION TYPE IA - PRECAST	3	EA
526.3612	MEDIAN BARRIER TRANSITION TYPE IB - PRECAST	1	EA
526.362	MEDIAN BARRIER TRANSITION TYPE II - PRECAST	4	EA
526.371	MEDIAN BARRIER WITH MOUNTED LIGHT POLE TYPE I	17	EA
527.306	CENTER BARRIER CRASH ATTENUATOR	3	EA
527.342	WORK ZONE CRASH CUSHIONS - TL-2	5	UNIT
527.343	WORK ZONE CRASH CUSHIONS - TL-3	7	UNIT
535.70	PRECAST CONCRETE TUNNEL	1	LS
602.30	FLOWABLE CONCRETE FILL	8	CY
603.155	12 INCH REINFORCED CONCRETE PIPE - CLASS III	99	LF
603.165	15 INCH REINFORCED CONCRETE PIPE - CLASS III	95	LF
603.175	18 INCH REINFORCED CONCRETE PIPE - CLASS III	603	LF
603.195	24 INCH REINFORCED CONCRETE PIPE - CLASS III	73	LF

ESTIMATED QUANTITIES			
ITEM NO.	DESCRIPTION	TOTAL QUANTITY	UNIT
603.205	30 INCH REINFORCED CONCRETE PIPE - CLASS III	40	LF
603.280	CONCRETE COLLAR FOR REINFORCED CONCRETE PIPE	2	EA
604.072	CATCH BASIN TYPE A1-C	2,125	EA
604.09	CATCH BASIN TYPE B1	5,125	EA
604.097	60" CATCH BASIN TYPE B5-C	1,125	EA
604.15	MANHOLE	2,125	EA
604.18	ADJUSTING MANHOLE OR CATCH BASIN TO GRADE	5	EA
604.245	CATCH BASIN TYPE F4-C	1	EA
604.26	CATCH BASIN TYPE B5	40	EA
605.09	6 INCH UNDERDRAIN TYPE B	1,729	LF
605.11	12 INCH UNDERDRAIN TYPE C	236	LF
605.111	12 INCH UNDERDRAIN TYPE C - SDR	600	LF
605.12	15 INCH UNDERDRAIN TYPE C	236	LF
605.13	18 INCH UNDERDRAIN TYPE C	140	LF
606.13	3" W-BEAM GUARDRAIL-MIDWAY SPLICE (7' STEEL POSTS, 8" OFFSET BLOCKS, SINGLE FACED)	2,940,625	LF
606.1306	3" W-BEAM GUARDRAIL-MIDWAY SPLICE TANGENT TERMINAL	7	EA
606.131	3" W-BEAM GUARDRAIL-MIDWAY SPLICE (8" STEEL POSTS, 8" OFFSET BLOCKS, SINGLE FACED)	662.5	LF
606.132	3" W-BEAM GUARDRAIL-MIDWAY SPLICE (7' STEEL POSTS, 8" OFFSET BLOCKS, DOUBLE FACED)	1,612.5	LF
606.1351	3" W-BEAM GUARDRAIL - MIDWAY SPLICE TERMINAL END - ANCHORED END	12	EA
606.1724	BRIDGE TRANSITION - TYPE III, MODIFIED	2	EA
606.2651	TERMINAL END - REMOVE AND RESET	1	EA
606.2652	TERMINAL END - REMOVE AND STACK	9	EA
606.275	TERMINAL END - DOUBLE RAIL - GALVANIZED STEEL	2	EA
606.352	REFLECTORIZED BEAM GUARDRAIL DELINEATOR	130	EA
606.353	DELINEATOR POST	94	EA
606.3561	DELINEATOR POST - REMOVE AND RESET	102	EA
606.3605	GUARDRAIL - REMOVE, MODIFY, AND RESET SINGLE RAIL	950	LF
606.3621	GUARDRAIL ADJUST, SINGLE RAIL	3,400	LF
606.82	GUARDRAIL - REMOVE AND STACK EXISTING CRASH END	1	EA
607.2326	AUTOMATIC ENTRY GATE	1	EA
607.40	CHAIN LINK FENCE - 3' HIGH	510	LF
607.41	POST ASSEMBLY FOR SIGN OR CHAIN LINK FENCE	66	EA
607.4211	DUMPSTER ENCLOSURE	1	LS
608.08	REINFORCED CONCRETE SIDEWALK	110	SY
608.26	CURB RAMP DETECTABLE WARNING FIELD	36	SF
609.11	VERTICAL CURB TYPE 1	1,000	LF
609.12	VERTICAL CURB TYPE 1 - CIRCULAR	200	LF
609.234	TERMINAL CURB TYPE 1 - 4 FT	4	EA
609.238	TERMINAL CURB TYPE 1 - 8 FT	2	EA
610.08	PLAIN RIPRAP	34	CY
610.18	STONE DITCH PROTECTION	27	CY
610.181	TEMPORARY STONE CHECK DAM	325	CY
613.319	EROSION CONTROL BLANKET	13,750	SY
614.30	GEOCELL CONFINEMENT SYSTEM FOR SLOPE PROTECTION	16,000	SF
615.07	LOAM	7,690	CY
618.13	SEEDING METHOD NUMBER 1	45	UNIT
618.14	SEEDING METHOD NUMBER 2	584	UNIT
619.1201	MULCH - PLAN QUANTITY	629	UNIT
619.1202	TEMPORARY MULCH	1	LS
619.1401	EROSION CONTROL MIX	50	CY
620.58	EROSION CONTROL GEOTEXTILE	910	SY
621.043	EVERGREEN TREE (6' - 8') GROUP A	2	EA
621.396	DWARF EVERGREEN (18" - 24") GROUP B	8	EA
621.512	HYBRID RHODODENDRON (2' - 2.5')	3	EA
621.553	DECIDUOUS SHRUB (3' - 4') GROUP B	4	EA
625.106	WATER SERVICE SUPPLY LINE (<3 IN)	2,400	LF
626.121	QUAZITE JUNCTION BOX (36X24)	11	EA
626.122	QUAZITE JUNCTION BOX (18X11)	75	EA
626.13	4' X 6' SPLICE BOX WITH ACCESS DOOR	5	EA
626.21	METALLIC CONDUIT	310	LF
626.22	NON-METALLIC CONDUIT	35,500	LF
626.223	HORIZONTAL DIRECTIONAL DRILLED CONDUIT	530	LF
626.31	18 INCH DIAMETER FOUNDATION	33	EA
626.32	24 INCH DIAMETER FOUNDATION	94	EA
626.333	30 INCH DIAMETER FOUNDATION, 8 FEET OR LESS	8	EA
626.36	REMOVE OR MODIFY CONCRETE FOUNDATION	66	EA
627.18	12 INCH SOLID WHITE PAVEMENT MARKING LINE	9,100	LF
627.681	TEMPORARY 6 INCH PAINTED PAVEMENT MARKING LINE - YELLOW OR WHITE	97,400	LF
627.73	TEMPORARY 6 INCH PAVEMENT MARKING TAPE	7,050	LF
627.731	TEMPORARY 6 INCH BLACK PAVEMENT MARKING TAPE	7,050	LF
627.733	4" WHITE OR YELLOW PAINTED PAVEMENT MARKING LINE	1,520	LF
627.744	6" WHITE OR YELLOW PAINTED PAVEMENT MARKING LINE	51,900	LF
627.75	WHITE OR YELLOW PAVEMENT & CURB MARKING	1,220	SF
627.77	REMOVING EXISTING PAVEMENT MARKING	37,650	SF

Scale:

No.	Revision	By	Date

Designed by:



CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

By	Date	By	Date
Designed	PLP 3\20\19	Checked	LEM 3\20\19
Drawn	EJB 3\20\19	In Charge of	GAE 3\20\19

STANTEC CONSULTING SERVICES INC.
 482 PAYNE ROAD
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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 ESTIMATED QUANTITIES 1

SHEET NUMBER: QN-01
 CONTRACT: 2019.04
 2 OF 503

Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA...Estimate.dgn


ESTIMATED QUANTITIES			
ITEM NO.	DESCRIPTION	TOTAL QUANTITY	UNIT
627.812	TEMPORARY RAISED PAVEMENT MARKERS	2,000	EA
627.94	PERMANENT PAVEMENT MARKING TAPE	335	LF
627.941	PAVEMENT MARKING TAPE - DOTTED WHITE LANE LINE, 6-INCH WIDE	820	LF
627.944	PAVEMENT MARKING - RECESSED TAPE - WORDS, ARROWS AND STOP BARS	170	SF
629.05	HAND LABOR, STRAIGHT TIME	70	HR
631.10	AIR COMPRESSOR (INCLUDING OPERATOR)	70	HR
631.11	AIR TOOL (INCLUDING OPERATOR)	70	HR
631.12	ALL PURPOSE EXCAVATOR (INCLUDING OPERATOR)	70	HR
631.14	GRADER (INCLUDING OPERATOR)	70	HR
631.171	TRUCK - SMALL (INCLUDING OPERATOR)	70	HR
631.18	CHAIN SAW RENTAL (INCLUDING OPERATOR)	30	HR
631.21	ROAD BROOM (INCLUDING OPERATORS AND HAULER)	10	HR
631.22	FRONT END LOADER (INCLUDING OPERATOR)	70	HR
631.32	CULVERT CLEANER (INCLUDING OPERATOR)	20	HR
631.36	FOREMAN	70	HR
631.50	JACKHAMMER (AIR TOOL INCLUDING OPERATOR)	60	HR
631.51	BUCKET TRUCK	60	HR
631.52	SCISSOR LIFT	60	HR
631.53	ELECTRICIAN	100	HR
631.54	ELECTRICIAN'S APPRENTICE	100	HR
631.55	PLUMBER	60	HR
632.01	OVER HEIGHT VEHICLE DETECTION SYSTEM: EXIT 103 AREA	1	LS
633.01	PROPANE SERVICE TRENCH	66	LF
633.21	PROPANE TANK SUPPORTS	2	EA
633.31	PROPANE TANK PAD	27	SY
634.051	REMOVE AND STACK LIGHT STANDARD	9	EA
634.052	REMOVE HIGH MAST LIGHT STANDARD	2	EA
634.208	REMOVE AND RESET LIGHT STANDARD	1	EA
634.231	CONVENTIONAL LIGHT STANDARD WITH LED FIXTURE	81	EA
639.18	FIELD OFFICE, TYPE A	1	EA
643.711	LANE USE SIGNAL INSTALLATION	6	EA
643.712	FLASHING BEACON - SOLAR POWERED	2	EA
645.105	REMOVE AND STACK SIGN	111	EA
645.107	REMOVE AND STACK CANOPY MOUNTED SIGN	10	EA
645.109	REMOVE AND RESET SIGN	7	EA
645.1092	CANOPY MOUNTED DYNAMIC MESSAGE SIGN	4	EA
645.121	OVERHEAD GUIDE SIGN NB 2 (STA. 7450+00)	1	LS
645.122	OVERHEAD GUIDE SIGN NB 3 (STA. 7463+00)	1	LS
645.123	OVERHEAD GUIDE SIGN NB 5 (STA. 7477+25)	1	LS
645.124	OVERHEAD GUIDE SIGN SB 1 (STA. 449+00)	1	LS
645.125	OVERHEAD GUIDE SIGN SB 2 (STA. 4508+75)	1	LS
645.126	OVERHEAD GUIDE SIGN SB 3 (STA. 27+00)	1	LS
645.127	OVERHEAD GUIDE SIGN SB 4 (STA. 43+00)	1	LS
645.14	CANOPY MOUNTED SIGN	4	EA
645.151	CANTILEVER GUIDE SIGN NB 1 (STA. 7427+25)	1	LS
645.152	CANTILEVER GUIDE SIGN NB 4 (STA. 7471+75)	1	LS
645.155	VARIABLE SPEED LIMIT SIGN	2	EA
645.161	BREAKAWAY DEVICE SINGLE POLE	51	EA
645.162	BREAKAWAY DEVICE MULTI POLE	24	EA
645.251	ROADSIDE GUIDE SIGNS, TYPE 1	1,065	SF
645.271	REGULATORY, WARNING, CONFIRMATION AND ROUTE ASSEMBLY SIGN, TYPE 1	1,440	SF
645.289	STEEL H-BEAM POLES	24,020	LB
645.501	REMOVE AND RESET MAINLINE SIGN STA. 740+23 (2 SIGNS)	1	LS
645.502	REMOVE AND RESET MAINLINE SIGN STA. 7440+50 (1 SIGN)	1	LS
645.503	REMOVE AND RESET MAINLINE SIGN STA. 7467+00 (1 SIGN)	1	LS
645.504	REMOVE AND RESET MAINLINE SIGN STA. 7470+29 (1 SIGN)	1	LS
645.505	REMOVE AND RESET MAINLINE SIGN STA. 3488+27 (1 SIGN)	1	LS
645.506	REMOVE AND RESET MAINLINE SIGN STA. 4474+00 (1 SIGN)	1	LS
645.507	REMOVE AND RESET MAINLINE SIGN STA. 789+50 (1 SIGN)	1	LS
645.508	REMOVE AND RESET MAINLINE SIGN STA. 8492+00 (1 SIGN)	1	LS
645.509	REMOVE AND RESET MAINLINE SIGN STA. 7515+00 (1 SIGN)	1	LS
645.601	REMOVE AND STACK OVERHEAD SIGN STRUCTURE (NB ORT)	1	LS
645.602	REMOVE AND STACK OVERHEAD SIGN STRUCTURE (SB CASH)	1	LS
648.00	INSTALL FLAGPOLE	1	EA
652.30	FLASHING ARROW	1	EA
652.312	TYPE III BARRICADES	21	EA
652.33	DRUM	200	EA
652.34	CONE	50	EA
652.35	CONSTRUCTION SIGNS	4,100	SF
652.361	MAINTENANCE OF TRAFFIC CONTROL DEVICES	1	LS
652.38	FLAGGERS	100	HR
652.41	PORTABLE-CHANGEABLE MESSAGE SIGN	4	EA
652.45	TRUCK MOUNTED ATTENUATOR	540	CD
652.451	AUTOMATED TRAILER MOUNTED SPEED LIMIT SIGN	3	EA

ESTIMATED QUANTITIES			
ITEM NO.	DESCRIPTION	TOTAL QUANTITY	UNIT
652.46	TEMPORARY PORTABLE RUMBLE STRIP	50	UNIT
655.01	INSTALLATION OF ORT LANE CONTROLLER CABINET	2	EA
655.012	INSTALLATION OF CASH LANE CONTROLLER CABINET	6	EA
655.02	DVAs MOUNT INSTALLATION	10	EA
655.03	VCARS MOUNT INSTALLATION	16	EA
655.04	INSTALLATION OF SENSOR LOOPS	1	LS
655.05	INSTALLATION OF AVI ANTENNAS	20	EA
655.06	INSTALLATION OF AVI READERS	4	EA
655.07	TRAFFIC CONTROL PEDESTAL PREPARATION WORK	6	EA
655.08	OPUS MOUNT INSTALLATION	18	EA
655.09	ARMORED CABLE - 10/3	300	LF
655.1004	*4/0 AWG WIRE	2,000	LF
655.100	*2/0 AWG WIRE	2,900	LF
655.1001	*1/0 AWG WIRE	1,600	LF
655.101	*1 AWG WIRE	1,600	LF
655.102	*2 AWG WIRE	1,000	LF
655.104	*4 AWG WIRE	1,400	LF
655.106	*6 AWG WIRE	1,200	LF
655.108	*8 AWG WIRE	600	LF
655.11	*10 AWG WIRE	2,400	LF
655.12	*12 AWG WIRE	45,600	LF
655.14	4PR/24 (CATEGORY 5E) CABLE	31,200	LF
655.15	LMR 400 CABLE	2,040	LF
655.161	MULTIMODE FIBER OPTIC CABLE - 6 FIBER	2,000	LF
655.162	SINGLE MODE FIBER OPTIC CABLE - 6 FIBER	3,500	LF
655.17	IVIS HOMERUN LOOP CABLE (IMSA 50-2 *14)	7,140	LF
655.200	1 1/2" SCHEDULE 40 PVC CONDUIT	300	LF
655.2001	3/4" SCHEDULE 40 PVC CONDUIT	300	LF
655.2002	1" SCHEDULE 40 PVC CONDUIT	300	LF
655.2003	2" SCHEDULE 40 PVC CONDUIT	300	LF
655.201	3" SCHEDULE 40 PVC CONDUIT	240	LF
655.202	4" SCHEDULE 40 PVC CONDUIT	360	LF
655.2021	1" SCHEDULE 80 PVC CONDUIT	420	LF
655.203	1 1/2" SCHEDULE 80 PVC CONDUIT	840	LF
655.2031	2" SCHEDULE 80 PVC CONDUIT	480	LF
655.204	3" SCHEDULE 80 PVC CONDUIT	2,550	LF
655.205	4" SCHEDULE 80 PVC CONDUIT	360	LF
655.2051	6" SCHEDULE 80 PVC CONDUIT	240	LF
655.2052	5" SCHEDULE 80 PVC CONDUIT	240	LF
655.206	1" GALVANIZED RIGID METAL CONDUIT	720	LF
655.2061	3/4" GALVANIZED RIGID METAL CONDUIT	720	LF
655.207	1 1/2" GALVANIZED RIGID METAL CONDUIT	420	LF
655.2071	2" GALVANIZED RIGID METAL CONDUIT	660	LF
655.208	3" GALVANIZED RIGID METAL CONDUIT	720	LF
655.209	1/2" LIQUID TIGHT METALLIC FLEXIBLE CONDUIT	480	LF
655.210	3/4" LIQUID TIGHT METALLIC FLEXIBLE CONDUIT	360	LF
655.2101	1 1/2" LIQUID TIGHT METALLIC FLEXIBLE CONDUIT	120	LF
655.2102	2" LIQUID TIGHT METALLIC FLEXIBLE CONDUIT	120	LF
655.211	1 1/2" ELECTRICAL METALLIC TUBING CONDUIT	120	LF
655.212	2" ELECTRICAL METALLIC TUBING CONDUIT	120	LF
655.213	3" ELECTRICAL METALLIC TUBING CONDUIT	120	LF
655.214	4" ELECTRICAL METALLIC TUBING CONDUIT	120	LF
655.215	3/4" ELECTRICAL METALLIC TUBING CONDUIT	120	LF
655.221	TYPE A PULL BOX INSIDE	20	EA
655.222	TYPE C PULL BOX IN TUNNEL/BOOTH PIT	24	EA
655.223	TYPE D PULL BOX OUTDOOR CANOPY	6	EA
655.224	TYPE E PULL BOX STEEL IN BOOTH	12	EA
655.225	TYPE F PULL BOX OUTSIDE	24	EA
655.30	12" X 12" X 6" GALVANIZED JUNCTION BOX	90	EA
655.31	18" X 18" X 6" GALVANIZED JUNCTION BOX	6	EA
655.40	18" X 24" X 12" JUNCTION BOX	8	EA
655.42	36" X 30" X 20" NEMA 4X CABINET	6	EA
655.43	60 AMP 3 PHASE PANELBOARD CABINET	8	EA
655.44	100 AMP 3 PHASE PANELBOARD CABINET	2	EA
655.45	150 AMP 3 PHASE PANELBOARD CABINET	2	EA
655.50	2" SCHEDULE 80 PVC CONDUIT CONDULETS	10	EA
655.51	4" SCHEDULE 80 PVC CONDUIT CONDULETS	10	EA
655.511	3/4" RIGID METAL CONDUIT CONDULETS	10	EA
655.52	1" RIGID METAL CONDUIT CONDULETS	20	EA
655.53	1 1/2" RIGID METAL CONDUIT CONDULETS	50	EA
655.54	2" RIGID METAL CONDUIT CONDULETS	50	EA
655.55	3" ELECTRICAL METAL TUBING CONDULETS	40	EA
655.56	2" ELECTRICAL METAL TUBING CONDULETS	40	EA
655.57	1 1/2" ELECTRICAL METAL TUBING CONDULETS	40	EA

Scale:

No.	Revision	By	Date

Designed by:



CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

By	Date	By	Date
Designed	PLP 3\20\19	Checked	LEM 3\20\19
Drawn	EJB 3\20\19	In Charge of	GAE 3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 ESTIMATED QUANTITIES 2

SHEET NUMBER: QN-02
 CONTRACT: 2019.04
 3 OF 503

Date: 3/20/2019

Filename: ... \00\HIGHWAY\MSTA\... Estimate.dgn

ESTIMATED QUANTITIES			
ITEM NO.	DESCRIPTION	TOTAL QUANTITY	UNIT
655.58	3/4" ELECTRICAL METAL TUBING CONDULETS	40	EA
655.59	1" ELECTRICAL METAL TUBING CONDULETS	40	EA
655.63	4-INCH X 4-INCH PVC NEMA 3R WIREWAY	540	LF
655.64	6-INCH X 6-INCH PVC NEMA 3R WIREWAY	500	LF
655.65	8-INCH X 8-INCH PVC NEMA 3R WIREWAY	300	LF
655.65	12-INCH X 12-INCH PVC NEMA 3R WIREWAY	300	LF
655.75	CONCRETE ENCASED CONDUIT	135	CY
655.80	LIGHTNING SUPPRESSION SYSTEM	1	LS
655.81	KEY SWITCH	10	EA
655.82	DUPLEX RECEPTACLE	20	EA
655.83	NEMA L5-20R RECEPTACLE	10	EA
655.84	QUADPLEX RECEPTACLE	10	EA
655.90	SPACE FRAME LIGHTING	1	LS
655.92	LED CANOPY LIGHT FIXTURE	12	EA
655.99	LED BUMPER BEACON	6	EA
656.41	GRAVEL WETLAND FILTER 1	1	LS
656.42	GRAVEL WETLAND FILTER 2	1	LS
656.50	BALED HAY, IN-PLACE	140	EA
656.60	TEMPORARY BERMS	3,000	LF
656.62	TEMPORARY SLOPE DRAINS	500	LF
656.632	30" TEMPORARY SILT FENCE	26,000	LF
659.10	MOBILIZATION	1	LS
670.01	SEWAGE DISPOSAL SYSTEM	1	LS
800.01	TOLL ADMINISTRATION BUILDING	1	LS
800.10	ELECTRICAL DEMOLITION	1	LS
800.20	MECHANICAL DEMOLITION	1	LS
800.22	HVAC AND ELECTRICAL TUNNEL AND BOOTHS	1	LS
800.30	TOLL PLAZA & BRIDGE DEMOLITION	1	LS
800.40	NEW TOLL BOOTH INSTALLATION	1	LS
800.41	STAIR ENCLOSURE & INSTALLATION	1	LS
800.90	GENERATOR PAD	1	LS
800.91	TRANSFORMER PAD	1	LS
803.01	TEST PITS	3	EA
832.41	TYPE A STEEL SITE BOLLARD	8	EA

EARTHWORK SUMMARY

COMMON EXCAVATION FOR ESTIMATE

COMMON EXCAVATION (FROM CROSS SECTIONS)	80,749
GRUBBING IN FILL	4,206
MUCK EXCAVATION (FROM CROSS SECTIONS)	2,542
PAVEMENT SALVAGE IN FILL	0
TOTAL COMMON EXCAVATION	87,497

ITEM 203.20 - COMMON EXCAVATION

88,000

FILL FOR BORROW CALCULATIONS

COMMON FILL (FROM CROSS SECTIONS)	41,965
GRUBBING IN FILL	4,026
EXISTING TUNNEL VOID	300
EXISTING BUILDING FOUNDATION VOID	275
BACKFILL CATCH BASIN REMOVAL	33
MUCK EXCAVATION	2,542
TOTAL FILL	49,141

AVAILABLE COMMON EXCAVATION FOR BORROW CALCULATIONS

(1) TOTAL COMMON EXCAVATION 87,497

DEDUCTIONS:

GRUBBING IN CUT	6,939
GRUBBING IN FILL	4,026
PAVEMENT REMOVAL	726

(2) TOTAL DEDUCTIONS 11,691

TOTAL AVAILABLE COMMON EXCAVATION (1) MINUS (2)	75,806
TOTAL AVAILABLE STRUCTURAL EXCAVATIONS (PLAZA)	2,350
TOTAL AVAILABLE STRUCTURAL EXCAVATIONS (BUILDING)	815
TOTAL AVAILABLE NON-ROCK EXCAVATION	78,971

COMPUTATION FOR GRANULAR BORROW FOR ESTIMATE

GRANULAR BORROW TO REPLACE MUCK	2,542
GRANULAR BORROW IN LOW WET AREAS	0
GRANULAR BORROW TO UPGRADE EXCAVATION	0
GRANULAR BORROW TO MAINTAIN TRAFFIC	0
GRANULAR BORROW FOR UNDERCUTTING	0
GRANULAR BORROW =	2,542 X 1.00 = 2,542

COMPUTATION FOR COMMON BORROW FOR ESTIMATE

(3) TOTAL FILL 49,141

TOTAL AVAILABLE NON-ROCK EXCAVATION	78,971 x 0.90 = 71,074
(4) TOTAL AVAILABLE EXCAVATION	71,074

(4) - (3) SURPLUS MATERIAL = 21,933

Scale:			
No.	Revision	By	Date

Designed by:			
CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
	By	Date	
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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
ESTIMATED QUANTITIES 3 &
EARTHWORKS SUMMARY

SHEET NUMBER: QN-03
CONTRACT: 2019.04
4 OF 503

GENERAL

- ALL DETAILS SHALL BE IN CONFORMANCE WITH MAINE DEPARTMENT OF TRANSPORTATION (MaineDOT) STANDARD DETAILS HIGHWAYS AND BRIDGES 2014 WITH UPDATES AND MaineDOT BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL LATEST REVISION UNLESS OTHERWISE INCLUDED IN THESE PLANS OR PROJECT SPECIFICATIONS.
- ALL EXISTING ROADWAYS USED IN ACCESSING THE SITE SHALL REMAIN CLEAN IN ACCORDANCE WITH THE MaineDOT STANDARD SPECIFICATIONS.
- THE CONTRACTOR SHALL SUBMIT THE PROPOSED STAGING AREA(S) AND FIELD TRAILER LOCATION TO THE RESIDENT FOR APPROVAL PRIOR TO STARTING WORK.
- CONTRACTOR IS REQUIRED TO MAINTAIN SAFE ACCESS TO PARKING AREAS FOR MTA EMPLOYEES AT ALL TIMES DURING CONSTRUCTION.
- A COPY OF THE "GEOTECHNICAL REPORT" AND ENVIRONMENTAL PERMITS ARE AVAILABLE FOR DOWNLOAD AT MAINETURNPIKE.COM.
- DUST CONTROL IS INCIDENTAL TO CONTRACT.
- WASTE MATERIALS SHALL BE DISPOSED OF OFF THE PROJECT SITE, IN ACCORDANCE WITH CHAPTER 404, DEPARTMENT OF ENVIRONMENTAL PROTECTION SOLID WASTE MANAGEMENT RULES.
- CLEARING LIMITS ARE AS SHOWN ON THE PLANS UNLESS OTHERWISE AUTHORIZED BY THE RESIDENT. THE ACTUAL CLEARING LINES SHALL BE ESTABLISHED BY THE CONTRACTOR AND APPROVED BY THE RESIDENT PRIOR TO ANY CLEARING TAKING PLACE. NOTE ACTUAL TREE LINE MAY VARY DUE TO CLEARING EFFORTS BY THE AUTHORITY. TREE CLEARING MAY NOT OCCUR IN JUNE AND JULY.
- GEOTECHNICAL INFORMATION FURNISHED OR REFERRED TO IN THIS PLAN SET IS FOR THE USE OF THE BIDDERS AND THE CONTRACTOR. NO ASSURANCE IS GIVEN THAT THE INFORMATION OR INTERPRETATIONS WILL BE REPRESENTATIVE OF ACTUAL SUBSURFACE CONDITIONS OF THE CONSTRUCTION SITE. THE MTA WILL NOT BE RESPONSIBLE FOR THE BIDDERS' OR CONTRACTOR'S INTERPRETATIONS OF, OR CONCLUSIONS DRAWN FROM, THE GEOTECHNICAL INFORMATION.
- THE CONTRACTOR WILL BE REQUIRED TO SUBMIT AS BUILT PLANS.
- BASELINE IS BASED ON FIELD SURVEY AND DOES NOT MATCH ORIGINAL I-95 CONSTRUCTION BASELINE FROM THE 1940'S. THE FIELD SURVEY BASELINE IS A BEST FIT CENTERLINE.
- THE CONTRACTOR SHALL NOTE THAT THE TOLL PLAZA AREA WITHIN THE PROJECT LIMITS RECEIVED A PAVEMENT OVERLAY IN 2018. THE WORK LIKELY INCLUDED PAVEMENT MILLING AND SHIMMING NECESSARY FOR TRADITIONAL PAVEMENT PRESERVATION WORK. PLEASE NOTE THAT THE TOPOGRAPHIC FIELD SURVEY WAS COLLECTED PRIOR TO THE PAVEMENT OVERLAY AND THEREFORE **THE OVERLAY IS NOT REFLECTIVE IN THE EXISTING PAVEMENT CONDITIONS SURVEY. CONTRACTOR SHALL PLAN HIS WORK ACCORDINGLY.**

EARTHWORK

- EXCAVATIONS ACCOMPLISHED AS PART OF THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH OSHA SUBPART P OF 29 CFR PART 1926.650-652 (CONSTRUCTION STANDARDS FOR EXCAVATION).
- THE NORMAL GRUBBING WIDTH IN THE FILLS SHALL BE VARIABLE WHEN SUBGRADE IS LESS THAN 5' ABOVE OLD GROUND. THE GRUBBING DEPTH HAS BEEN ESTIMATED AS 6" IN FIELD AREAS AND 12" IN WOODED AREAS.
- EXISTING INSLOPES STEEPER THAN 2:1 IN PROPOSED FILL AREAS SHALL BE BENCH AS SHOWN IN THE DETAILS OR AS DIRECTED BY THE RESIDENT.
- GRANULAR BORROW USED TO BACKFILL MUCK EXCAVATION OR IN LOW WET AREAS, TO 1' ABOVE WATER OR OLD GROUND SHALL MEET REQUIREMENTS FOR GRANULAR BORROW UNDERWATER BACKFILL.

UTILITY

- EXISTING UTILITIES ON THESE PLANS WERE COMPILED FROM FIELD SURVEY AND VARIOUS OTHER SOURCES. LOCATIONS ARE NOT GUARANTEED TO BE ACCURATE NOR IS IT GUARANTEED THAT ALL UTILITIES ARE SHOWN. NO SEPARATE OR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR DUE TO ANY VARIANCE BETWEEN THE DATA SHOWN ON THE PLANS AND THE ACTUAL FIELD CONDITIONS ENCOUNTERED. NO WORK SHALL BE STARTED UNTIL THE OWNERS OF THE VARIOUS UTILITIES ARE NOTIFIED BY THE CONTRACTOR OF THE PROPOSED CONSTRUCTION. THE CONTRACTOR IS ALSO REQUIRED TO CALL DIG SAFE AT 1-888-344-7233 PRIOR TO THE START OF THE WORK.
- THE UTILITIES INVOLVED IN THIS CONTRACT ARE:
MAINE TURNPIKE AUTHORITY
CENTRAL MAINE POWER
FAIRPOINT/CONSOLIDATED COMMUNICATIONS
GARDNER WATER DISTRICT
MAINEDOT
SPECTRUM/CHARTER COMMUNICATIONS
- THE CONTRACTOR SHALL NOTIFY THE RESIDENT 10 DAYS PRIOR TO CONSTRUCTION SO THE RESIDENT CAN ARRANGE FOR MAINE TURNPIKE UNDERGROUND UTILITY LOCATION. ALL PROPOSED SIGN LOCATIONS AND EXCAVATION LOCATIONS SHALL BE MARKED AT THE NOTIFICATION TIME. EXCAVATION WILL NOT BE PERMITTED UNTIL THE AUTHORITY HAS LOCATED AND MARKED ITS UNDERGROUND UTILITIES, OR NOTIFIED THE RESIDENT THERE ARE NO UNDERGROUND UTILITIES IN THE MARKED AREAS. THE AUTHORITY HAS PROGRAMMED TWO FIELD VISITS FOR MAINE TURNPIKE UTILITY COORDINATION ON THIS PROJECT. SHOULD THE CONTRACTOR NEED ADDITIONAL SIGN LOCATIONS AND/OR ADDITIONAL EXCAVATION LOCATIONS MARKED, OR SHOULD THE CONTRACTOR FAIL TO MAINTAIN THE AUTHORITY'S PREVIOUSLY ESTABLISHED DIG SAFE MARKS, THE AUTHORITY SHALL DEDUCT THE ADDED MARKING COSTS FROM THE CONTRACTOR'S PAYMENTS.

EROSION CONTROL

- THE ANTICIPATED EROSION CONTROL DEVICES ARE SHOWN ON THE PLANS. THE CONTRACTOR SHALL PROPOSE ACTUAL TYPE AND LOCATION OF DEVICES FOR APPROVAL BY THE RESIDENT. ADDITIONAL MEASURES MAY BE PROPOSED BY THE CONTRACTOR DUE TO SITE OR WEATHER CONDITIONS. THE RESIDENT MAY DIRECT THE CONTRACTOR TO IMPLEMENT ADDITIONAL MEASURES. ANY ADDITIONAL MEASURES APPROVED BY THE RESIDENT WILL BE MEASURED FOR PAYMENT.
- 4' LOAM HAS BEEN ESTIMATED FOR 100% OF THE DISTURBED SLOPE AREA UNLESS OTHERWISE SPECIFIED ON THE PLANS. ACTUAL PLACEMENT OF THE LOAM SHALL BE AS DESIGNATED BY THE RESIDENT.
- NEWLY DISTURBED EARTH SHALL BE MULCHED PRIOR TO A RAIN EVENT. THIS WORK SHALL BE PAID FOR UNDER ITEM 619.1202 TEMPORARY MULCH.
- ALL TEMPORARY AND PERMANENT EROSION CONTROL DEVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE MAINE DEPARTMENT OF TRANSPORTATION BEST MANAGEMENT PRACTICES.
- TEMPORARY SEED SHALL BE APPLIED TO ALL DISTURBED AREAS THAT WILL NOT BE COMPLETED WITHIN 30 DAYS. TEMPORARY SEED SHALL BE INCIDENTAL TO THE 618 ITEMS.
- TEMPORARY EROSION CONTROL BLANKET, ITEM 613.319 SHALL BE INSTALLED IN ALL DITCHES AND 2:1 SLOPES FROM TOP TO TOE OF SLOPE. LOAM AND SEED SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE EROSION CONTROL BLANKET. LIMITS OF THE EROSION CONTROL BLANKET IN DITCHES SHALL BE 8' WIDE OR AS DESIGNATED BY THE RESIDENT. SEEDING METHOD NO. 2 SHALL BE USED BEHIND GUARDRAIL AND SEEDING METHOD NO. 1 SHALL BE USED AT AND AROUND THE PLAZA BUILDING, TEMPORARY SIDESLOPES AND ALL OTHER AREAS.

SIGN STRUCTURES / FOUNDATIONS

- THESE NOTES APPLY ONLY TO OVERHEAD SIGN STRUCTURES. FOR ORT SPACE FRAME NOTES, SEE PLAZA STRUCTURAL PLANS. FOR ADDITIONAL OVERHEAD SIGN SUPPORT STRUCTURE FOUNDATIONS NOTES, SEE ADDENDUM.
- AT LOCATIONS WHERE A MEDIAN FOUNDATION IS REQUIRED, THE FOOTPRINT OF THE DESIGNED FOUNDATION SHALL FIT ENTIRELY WITHIN THE EXISTING MEDIAN. THE CONSTRUCTION OF THE MEDIAN FOUNDATION(S) SHALL NOT EXTEND INTO THE SHOULDER OF THE ROADWAY AND SHALL INCLUDE APPROPRIATE SUPPORT OF EXCAVATION TO PROTECT THE PAVEMENT SURFACE.
- IF TEMPORARY SHEETING IS REQUIRED, IT SHALL BE INCIDENTAL TO THE SIGN SUPPORT STRUCTURE ITEM, AND SHALL MEET THE REQUIREMENTS UNDER SECTION 511. TEMPORARY SHEETING IS REQUIRED IF THE EXCAVATED SLOPES CAN NOT MEET OSHA REQUIREMENTS. NO DISTURBING OR UNDERMINING OF PAVEMENT SHALL BE ALLOWED. CONTRACTOR SHALL SUBMIT WORKING DRAWINGS FOR THE SUPPORT OF EXCAVATION TO THE AUTHORITY FOR REVIEW AND APPROVAL.
- THE TOP OF SIGN STRUCTURE FOUNDATIONS SHALL BE AT LEAST 3 INCHES BUT NO MORE THAN 18 INCHES ABOVE THE GRADE. MEASUREMENT SHALL BE MADE FROM THE HORIZONTAL FOUNDATION FACE LEAST EXPOSED ON A SLOPE.
- NEW FOUNDATIONS SHALL BE INSTALLED SUCH THAT THE MINIMUM VERTICAL CLEARANCE TO THE BOTTOM OF THE LOWEST SIGN IS AT LEAST 18'-0" FROM THE HIGH POINT OF THE ROADWAY INCLUDING SHOULDERS. THE MAXIMUM VERTICAL CLEARANCE TO THE BOTTOM OF THE LOWEST SIGN SHALL NOT EXCEED 20'-6".
- SOIL AND ROCK EXCAVATION, DEWATERING AND FINE GRADING AROUND SIGN STRUCTURE FOUNDATIONS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCIDENTAL TO THE SECTION 645 SIGN SUPPORT PAY ITEMS.
- IF SOIL CONDITIONS DIFFER MATERIALLY FROM THOSE SHOWN ON THE GEOTECHNICAL BORING LOGS, THE CONTRACTOR SHALL STOP WORK ON THAT STRUCTURE'S FOUNDATIONS AND CONTACT THE RESIDENT.

PAVING

- WHERE PROPOSED PAVEMENT JOINS EXISTING PAVEMENT, THE EXISTING PAVEMENT SHALL BE SAW CUT ALONG A SMOOTH LINE TO A NEAT, EVEN AND VERTICAL JOINT, AS DIRECTED BY THE RESIDENT. SAWCUTTING SHALL BE PAID FOR UNDER ITEM 419.30. ALL WORK NECESSARY FOR THE PREPARATION OF THE JOINT SHALL BE CONSIDERED INCIDENTAL TO THE RELATED CONTRACT ITEMS.
- ALL JOINTS BETWEEN EXISTING AND PROPOSED HOT BITUMINOUS PAVEMENT SHALL BE BUTTED.
- ANY NECESSARY CLEANING OF EXISTING PAVEMENT PRIOR TO PAVING SHALL BE CONSIDERED INCIDENTAL TO THE RELATED PAVING ITEMS.
- A COATING OF HOT RUBBERIZED ASPHALT (SS-S-140IC) SHALL BE APPLIED TO ALL TRANSVERSE BUTT JOINTS AND LONGITUDINAL JOINTS IN THE TOP LIFT OF PAVEMENT ALONG THE MAINE TURNPIKE AND RAMPS.
- A BITUMINOUS TACK COAT IS REQUIRED BETWEEN ALL PAVEMENT LIFTS.
- PLACEMENT OF THE FINAL LIFTS OF PAVEMENT SHALL BE DONE IN A MANNER TO LIMIT PAVEMENT JOINTS. CONTRACTOR SHALL COORDINATE WITH RESIDENT.

DRAINAGE

- NO EXISTING DRAINAGE SHALL BE ABANDONED, REMOVED OR PLUGGED WITHOUT PRIOR APPROVAL OF THE RESIDENT.
- INLETS AND OUTLETS OF ALL CULVERTS SHALL BE RIPRAPPED UNLESS OTHERWISE NOTED ON THE PLANS OR DIRECTED BY THE RESIDENT.
- ALL DITCH ELEVATIONS AND OFFSETS SHOWN ON THE CROSS SECTIONS ARE FOR THE FINISHED DITCH FLOW LINE.
- ANY NECESSARY CUTTING OF EXISTING PIPES TO FIT IN AREAS OF PROPOSED CATCH BASINS AND MANHOLES WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE PROPOSED CATCH BASIN AND MANHOLE ITEMS.
- ANY NECESSARY CUTTING OF EXISTING CATCH BASINS TO TAKE A PROPOSED PIPE WILL NOT BE PAID FOR SEPARATELY AND SHALL BE CONSIDERED INCIDENTAL TO THE PROPOSED CULVERT ITEMS.
- EXISTING CULVERTS TO REMAIN SHALL BE CLEANED AS DIRECTED BY THE RESIDENT. PAYMENT WILL BE MADE UNDER ITEM 631.32 CULVERT CLEANER (INCLUDING OPERATORS).
- ONE GREEN DELINEATOR POST SHALL BE INSTALLED AT ALL UNDERDRAIN AND STORM DRAIN OUTLETS.

GUARDRAIL


- AT THE END OF THE WORK DAY, EVERYDAY, THE CONTRACTOR IS REQUIRED TO HAVE AN APPROVED CRASHWORTHY END TREATMENT ON ALL GUARDRAIL WITHIN ALL WORK AREAS THAT ARE ACCESSIBLE TO TRAFFIC.
- CONNECTIONS FOR PROPOSED GUARDRAIL TO EXISTING GUARDRAIL SHALL BE INCIDENTAL TO THE PROPOSED GUARDRAIL ITEMS.
- ALL PROPOSED AND RESET GUARDRAIL SHALL BE INSTALLED IN A MANNER TO AVOID ALL EXISTING OR PROPOSED SUBSURFACE FEATURES.
- CONTRACTOR SHALL STOCKPILE ALL GUARDRAIL TO BE REMOVED, MODIFIED AND RESET UNTIL GUARDRAIL INSTALLATION HAS BEEN COMPLETE. STOCKPILING THE GUARDRAIL SHALL BE INCIDENTAL TO ITEM 606.3605. GUARDRAIL - REMOVE, MODIFY, AND RESET SINGLE RAIL. ALL USABLE EXISTING GUARDRAIL SHALL BE MODIFIED AND RESET PRIOR TO BEING PAID TO INSTALL NEW GUARDRAIL.
- GUARDRAIL WHICH IS REMOVED AND NOT REUSED ON THE PROJECT SHALL BECOME PROPERTY OF THE CONTRACTOR. REMOVAL AND DISPOSAL SHALL BE CONSIDERED INCIDENTAL TO THE GUARDRAIL ITEMS.
- DAMAGE TO EXISTING PAVEMENT OR NEW PAVEMENT DUE TO THE INSTALLATION OF NEW OR RESET GUARDRAIL SHALL BE REPAIRED WITH NO ADDITIONAL COMPENSATION TO THE CONTRACTOR.
- ONE GUARDRAIL DELINEATOR POST SHALL BE INSTALLED AT EACH GUARDRAIL TERMINAL. TWO GUARDRAIL DELINEATOR POSTS SHALL BE INSTALLED AT EACH LEADING GUARDRAIL END.
- IN NON-GUARDRAIL AREAS, DELINEATORS SHALL BE SPACED AT 264' ON MAINLINE AND AT 50' ON RAMPS AND ACCESS ROAD. THE CONTRACTOR SHALL CONFIRM THE DELINEATOR LAYOUT WITH THE RESIDENT.

Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\...Gen_Notes.dgn

Scale:			
No.	Revision	By	Date

Designed by:



CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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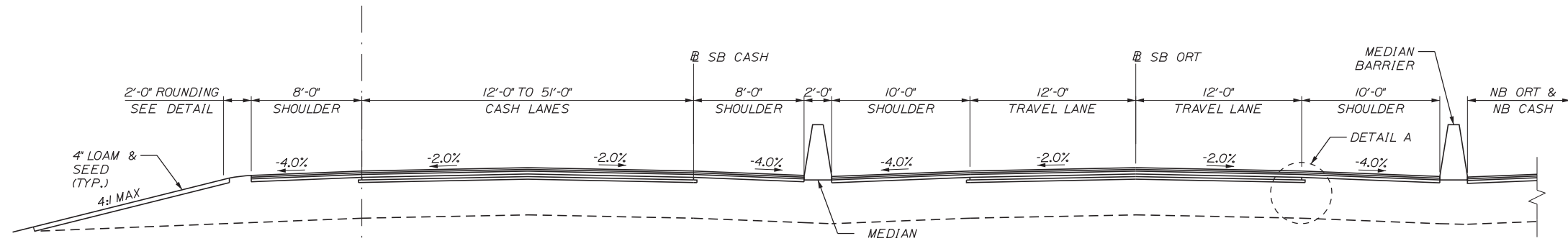
**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

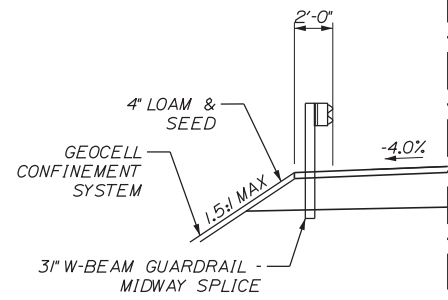
INTERCHANGE 103
ORT CONVERSION
GENERAL
NOTES

SHEET NUMBER: GN-01
CONTRACT: 2019.04
5 OF 503

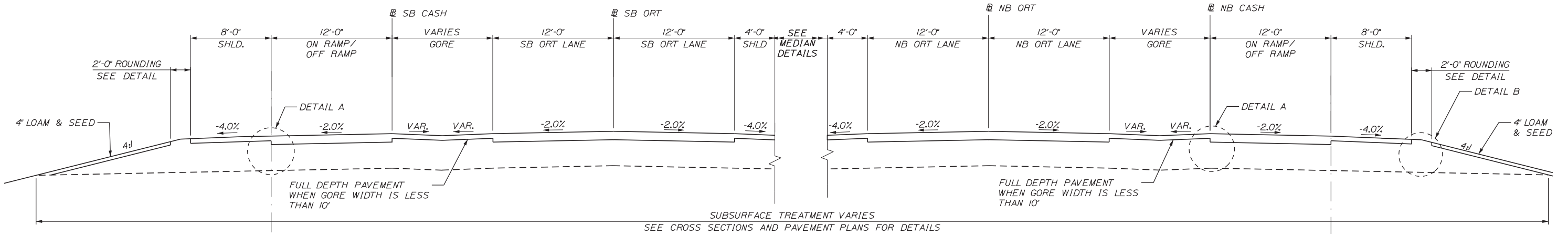
Date: 3/20/2019



SOUTHBOUND ORT AND CASH LANES
 STA. 4490+00 TO STA. 4499+16.17
 STA. 4500+87.5 TO STA. 4508+00
NORTHBOUND ORT AND CASH LANES (MIRRORED)
 STA. 7490+00 TO STA. 7499+12.50
 STA. 7500+75 TO STA. 7513+00

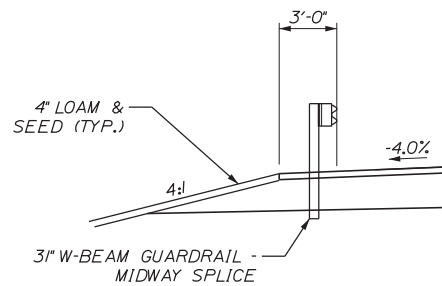


SB CASH APPROACH
 4502+00 TO 4507+00 LT.

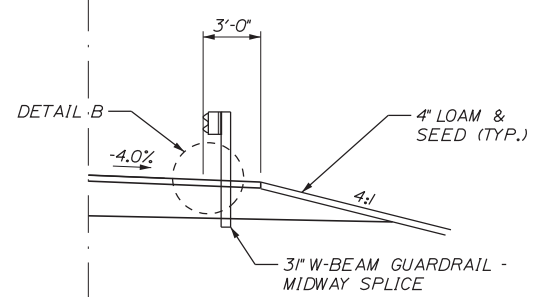


SB ORT/CASH RAMP
 4481+50 TO 4490+00

NB ORT/CASH RAMP
 STA. 7481+52 TO 7490+00

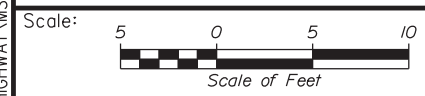


CASH RAMP
 4481+50 TO 4484+00



CASH RAMP
 STA. 7481+52 TO 7484+50

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No.	Revision	By	Date

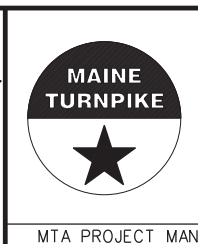
Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

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**THE GOLD STAR
 MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

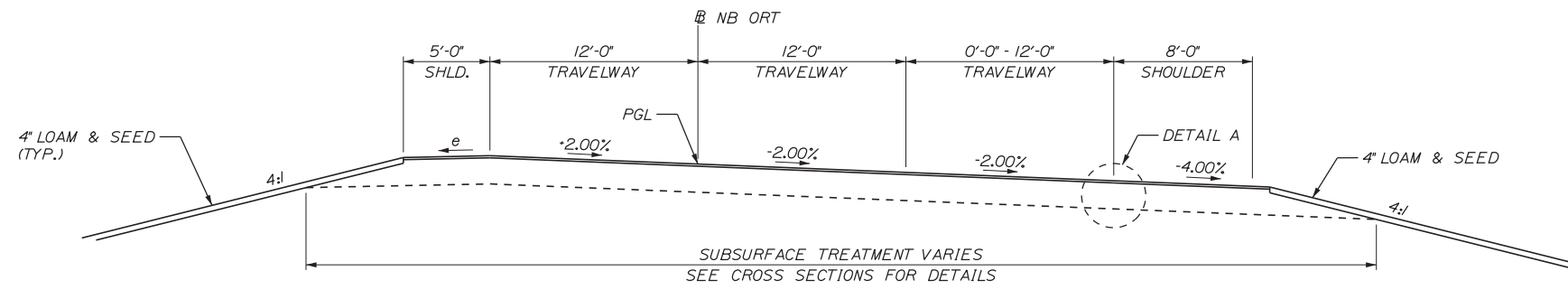
EXIT 103
 ORT CONVERSION
 TYPICAL SECTIONS 2

SHEET NUMBER: TS-2
 7 OF 503

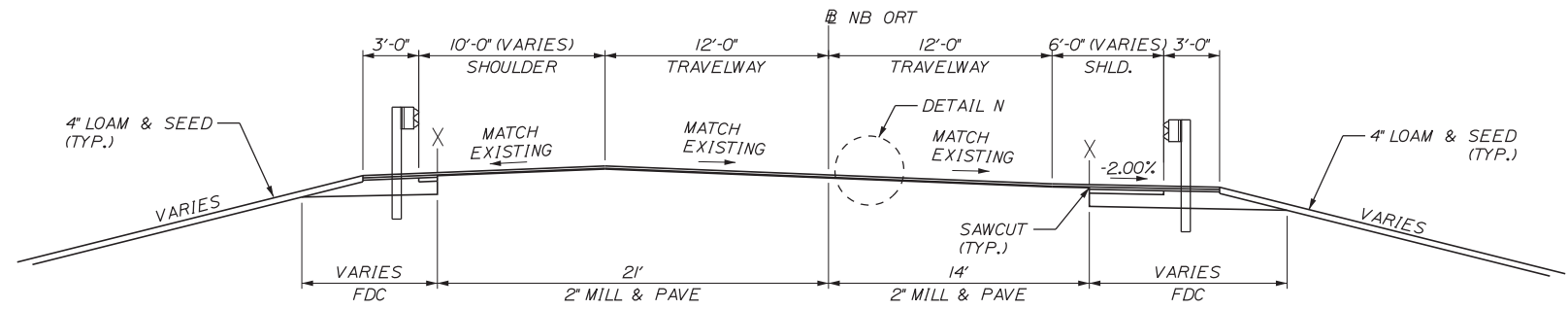
CONTRACT: 2019.04

Date: 3/20/2019

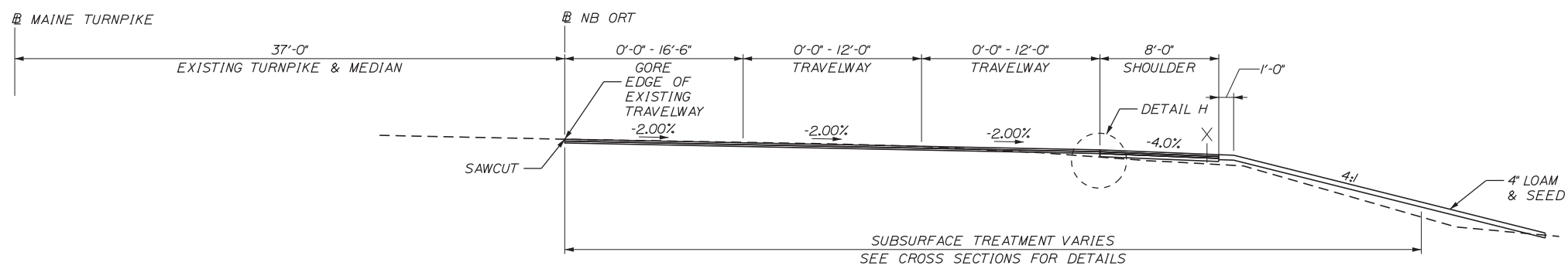
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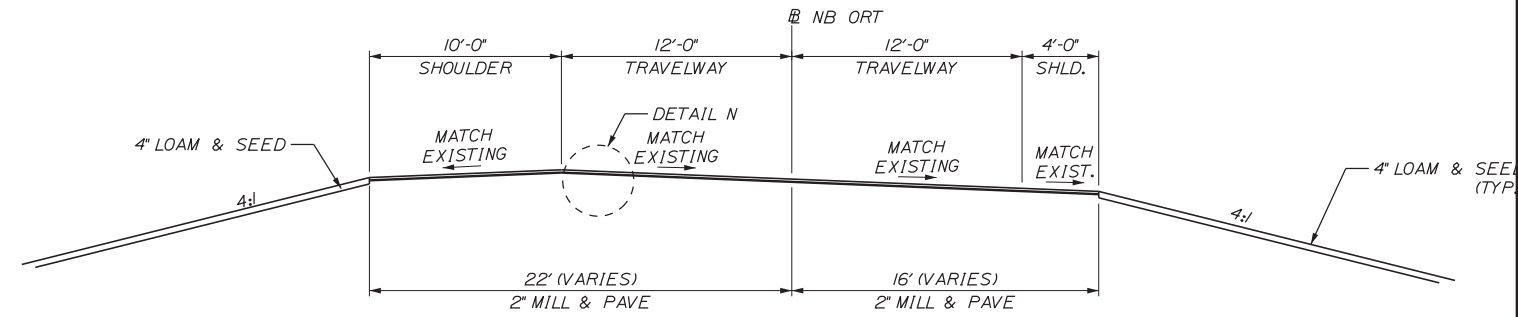
EXIT 103 NB ON RAMP
STA. 7513+00 TO STA. 7523+70



EXIT 103 SB OFF RAMP
STA. 42+00 TO STA. 46+00



EXIT 103 NB ON RAMP TO I-95 LANE REDUCTIONS
STA. 7523+70 TO STA. 5280+60



EXIT 103 SB OFF RAMP
STA. 28+50 TO STA. 42+00
STA. 46+00 TO STA. 47+00



No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

STANTEC CONSULTING SERVICES INC.
482 PAYNE ROAD
SCARBOROUGH, ME 04074
TEL (207) 887-3448
FAX (207) 883-3376

MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

EXIT 103
ORT CONVERSION

TYPICAL SECTIONS 3

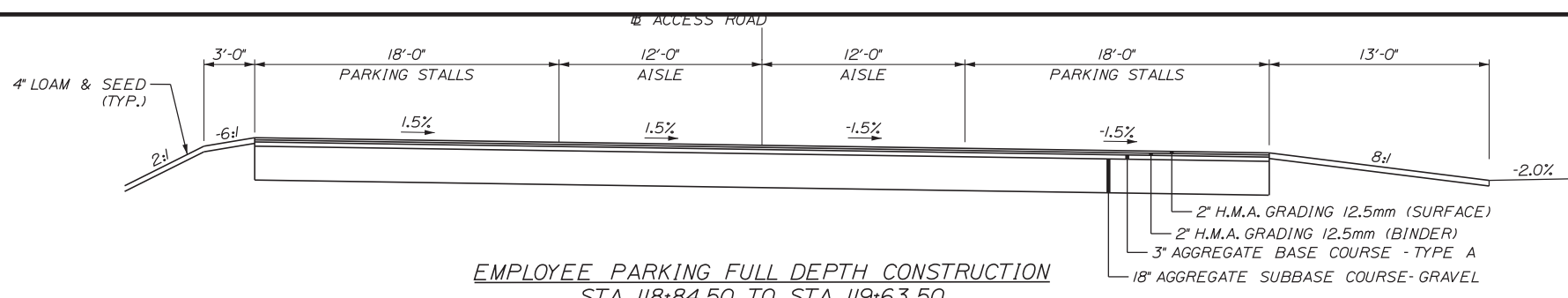
SHEET NUMBER: TS-3

CONTRACT: 2019.04

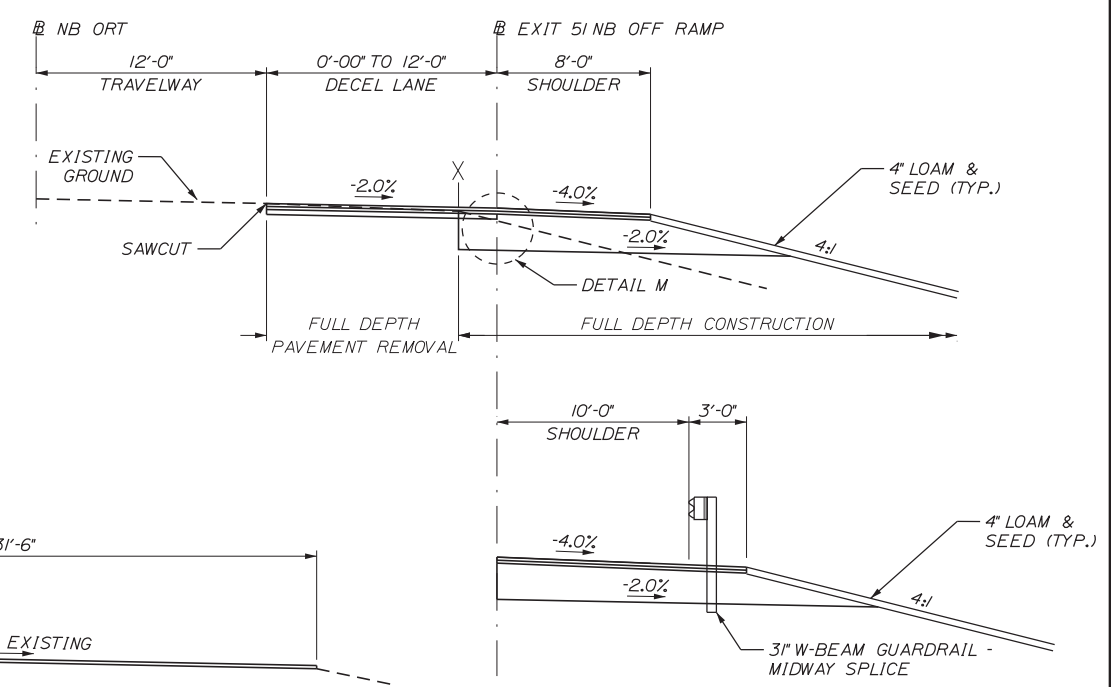
8 OF 503

Date: 3/20/2019

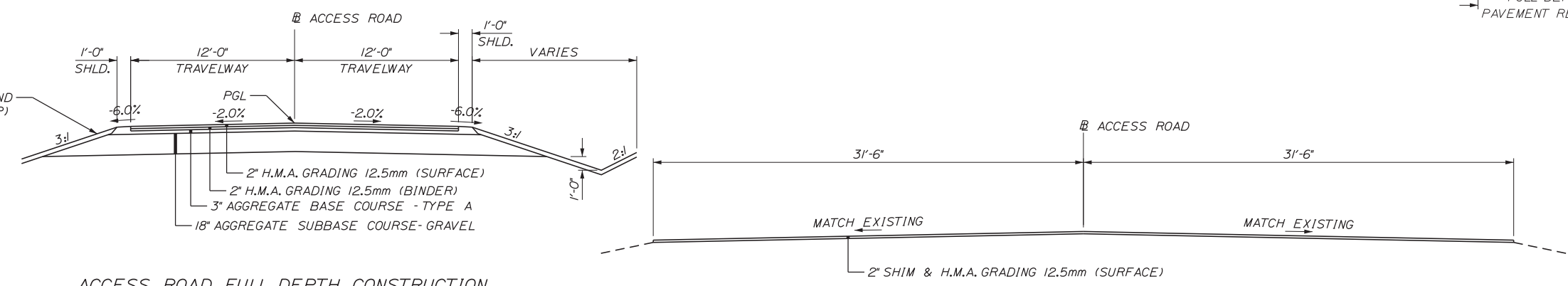
Filename: ...000\HIGHWAY\MSTA\..._Typ-XX.dgn



EMPLOYEE PARKING FULL DEPTH CONSTRUCTION
STA. 118+84.50 TO STA. 119+63.50

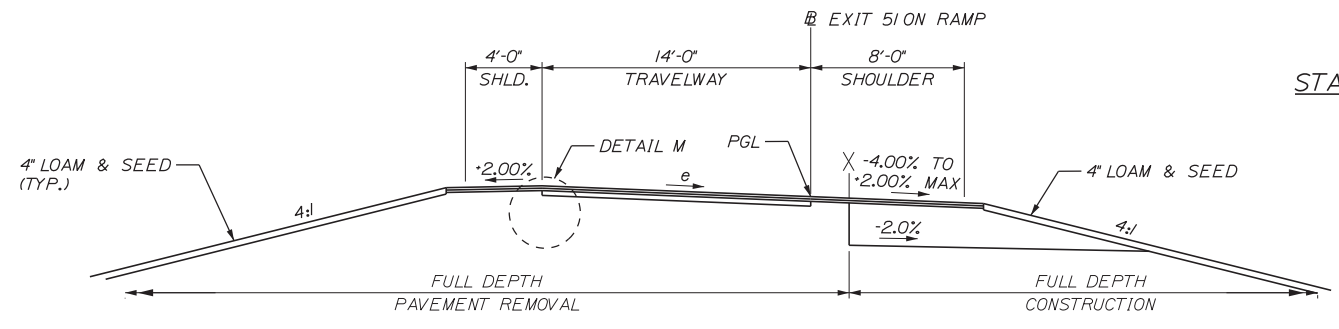


EXIT 51 NB OFF RAMP
STA. 7460+00 TO STA. 7467+00

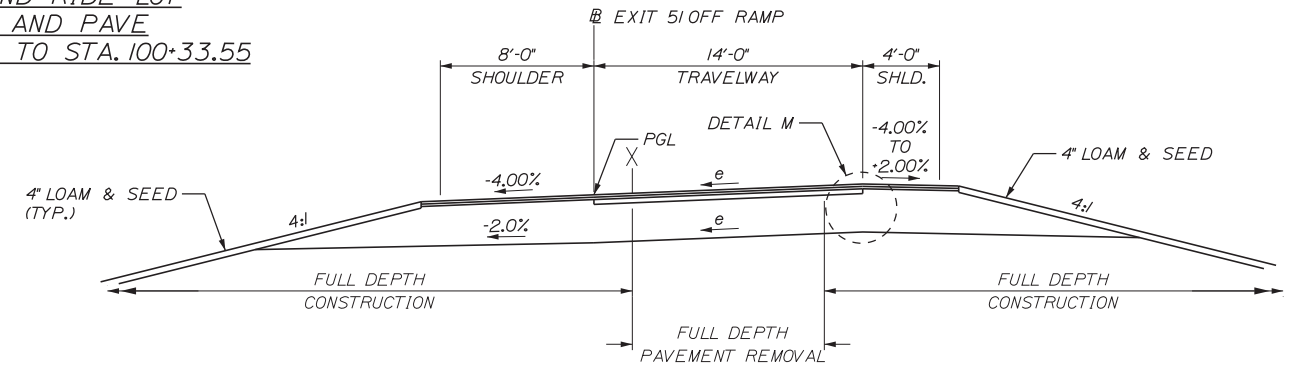


ACCESS ROAD FULL DEPTH CONSTRUCTION
STA. 100+33.55 TO STA. 118+84.50

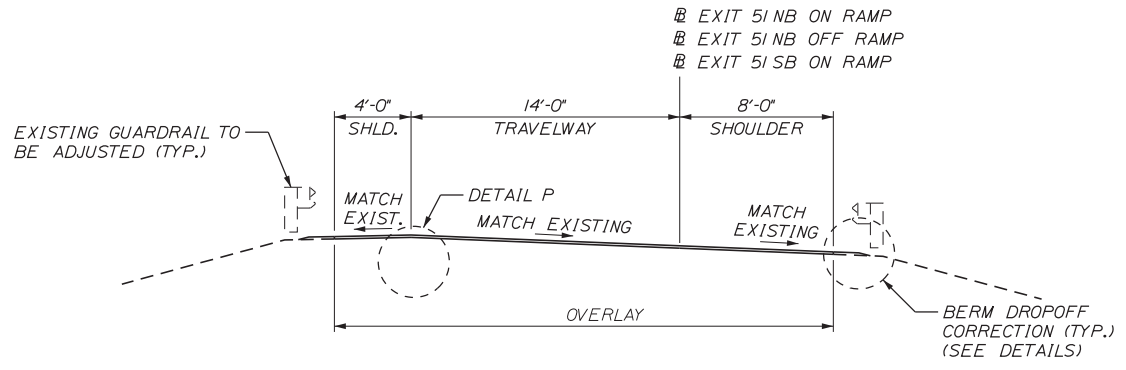
PARK AND RIDE LOT SHIM AND PAVE
STA. 97+72.55 TO STA. 100+33.55



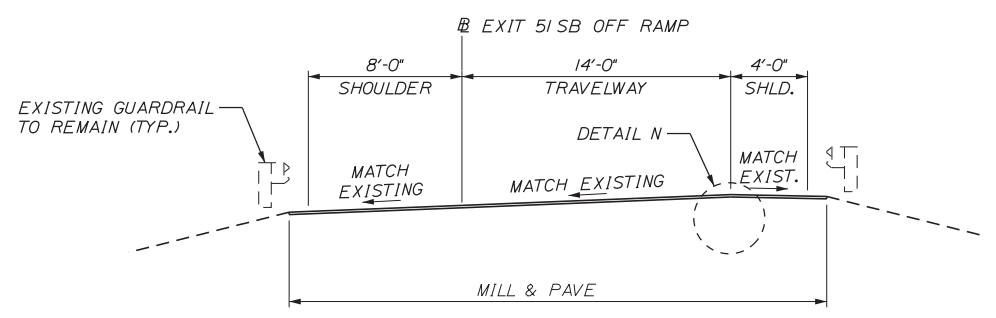
EXIT 51 NB ON RAMP - SUPERELEVATED
STA. 886+00 TO STA. 878+80



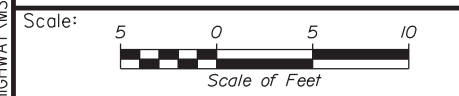
EXIT 51 SB OFF RAMP - SUPERELEVATED
STA. 785+25 TO STA. 788+40



EXIT 51 NB ON RAMP, SB ON RAMP & NB OFF RAMP OVERLAY
STA. 881+54 TO STA. 886+00
STA. 1459+31 TO STA. 1480+54 (MIRRORED)
STA. 2464+31 TO STA. 2482+62



EXIT 51 SB OFF RAMP MILL & PAVE
STA. 779+35 TO STA. 785+25



No.	Revision	By	Date

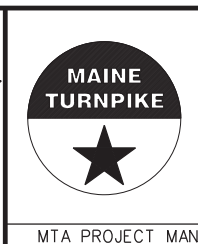
Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
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THE GOLD STAR MEMORIAL HIGHWAY

EXIT 103
ORT CONVERSION
TYPICAL SECTIONS 4

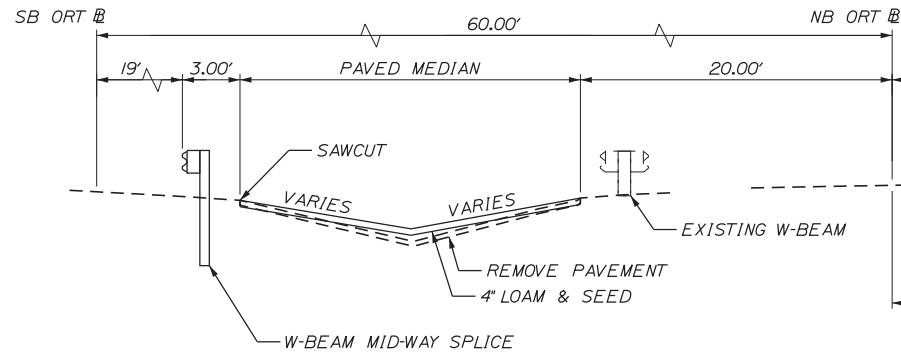
SHEET NUMBER: TS-4

CONTRACT: 2019.04

9 OF 503

Date: 3/20/2019

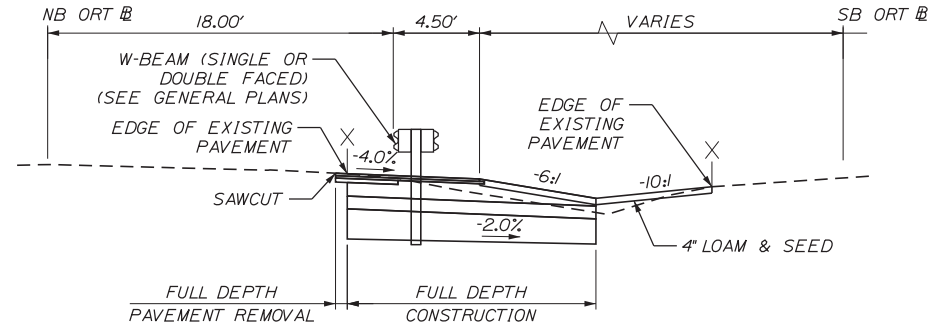
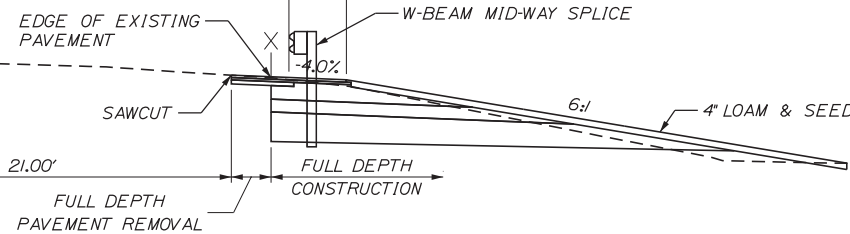
Filename: ...00\HIGHWAY\MSTA\...Typ-XX.dgn



STA. 4449+73 TO STA. 4450+40 LT.

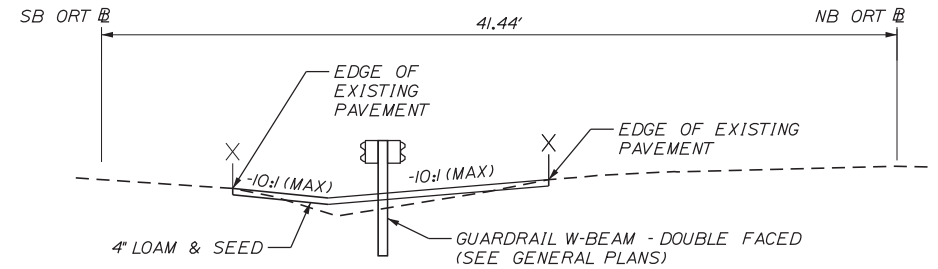
MEDIAN AND NORTHBOUND GUARDRAIL INSTALLATION

STA. 7448+83 TO STA. 7450+14 RT.

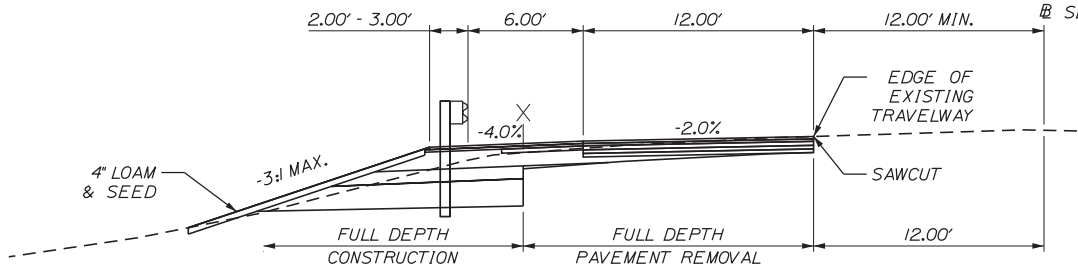


STA. 4462+87 TO STA. 4467+06 RT.
STA. 4477+10 TO STA. 4479+65 RT.

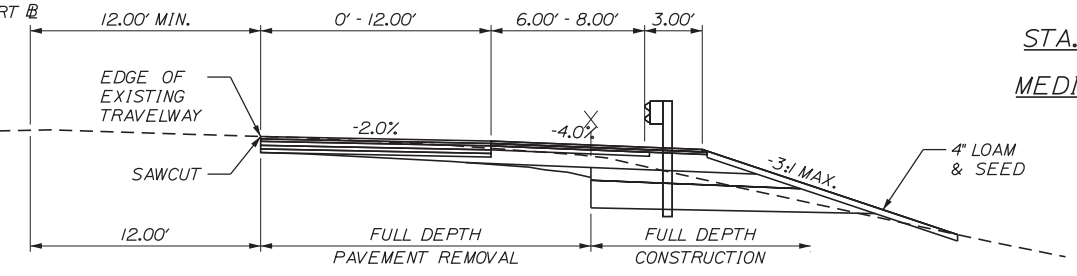
MEDIAN GUARDRAIL INSTALLATION



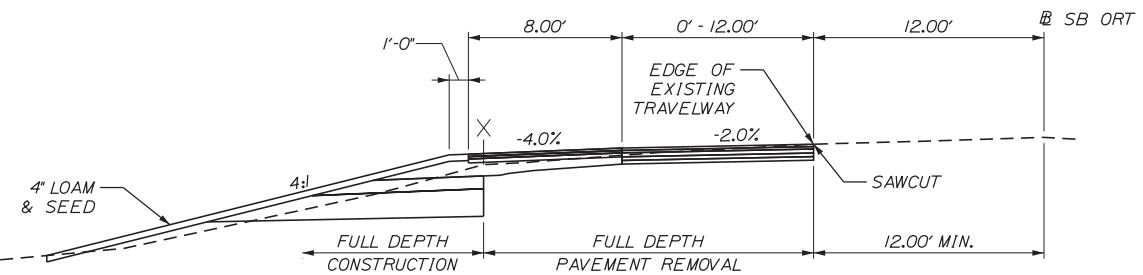
STA. 4467+06 TO STA. 4471+90 RT.
MEDIAN GUARDRAIL INSTALLATION



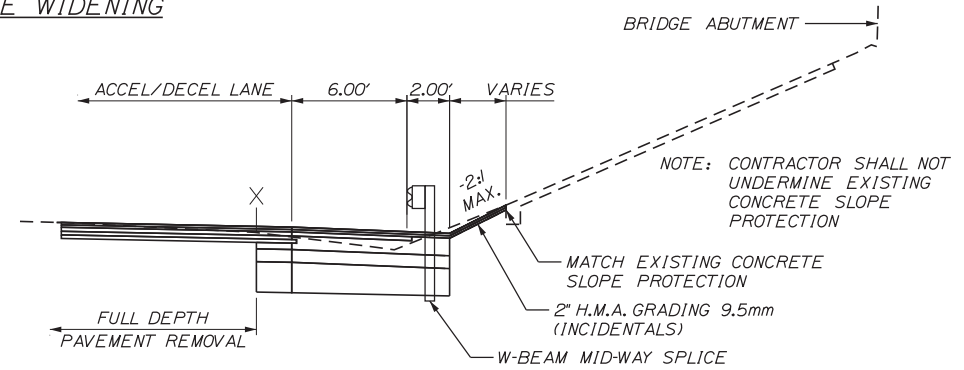
STA. 4474+30 TO STA. 4477+48 LT.



STA. 7474+30 TO STA. 7480+96 RT.
NB I-295 MAINLINE WIDENING

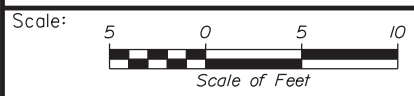


STA. 4477+48 TO STA. 7480+64 LT.
SB I-295 MAINLINE WIDENING



STA. 4480+64 TO STA. 4481+19 LT.
STA. 7480+96 TO STA. 7481+52 RT.

WIDENING UNDER ROUTE 126 BRIDGE



No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

EXIT 103
ORT CONVERSION
TYPICAL SECTIONS 5

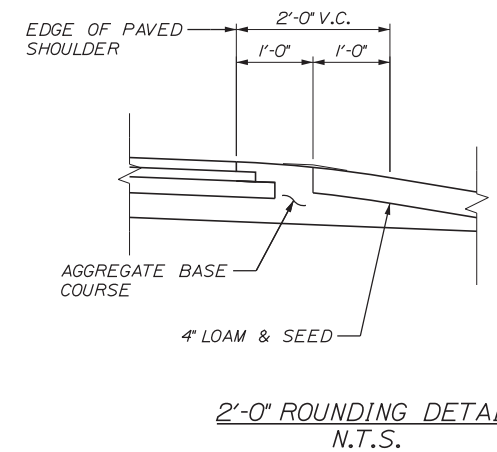
SHEET NUMBER: TS-5
CONTRACT: 2019.04
10 OF 50.3

SOUTHBOUND ORT @ SUPERELEVATION TABLE

LEFT SHOULDER	LEFT TRAVELWAY	LEFT GORE	STATION	RIGHT TRAVELWAY	RIGHT SHOULDER
MATCH EXISTING					
-4.00%	-2.00%	-	4482+00	-1.60%	-4.00%
-4.00%	-2.00%	-	4482+50	-2.00%	-4.00%
-4.00%	-2.00%	-2.00%	4483+00	-2.00%	-4.00%
NORMAL CROWN					
-4.00%	-2.00%	-2.00%	4488+00	-2.00%	-4.00%
-3.00%	-	-	4488+50	-2.00%	-4.00%
-4.00%	-	-	4489+00	-2.00%	-4.00%
NORMAL CROWN					
-4.00%	-	-	4498+50	-2.00%	-4.00%
-2.00%	-	-	4499+00	-2.00%	-2.00%
-2.00%	-	-	4500+00	-2.00%	-2.00%
-4.00%	-	-	4500+50	-2.00%	-4.00%
NORMAL CROWN					
-4.00%	-	-	4504+65	-2.00%	-4.00%
-3.30%	-	-	4505+00	-2.00%	-4.00%
-3.00%	-	-	4505+15	-2.00%	-4.00%
-2.30%	-	-	4505+50	-2.00%	-4.00%
-4.00%	-2.00%	-2.00%	4505+65	-2.00%	-4.00%
NORMAL CROWN					
-4.00%	-2.00%	-2.00%	4507+00	-2.00%	-4.00%
-4.00%	-3.80%	-2.00%	4507+50	-2.00%	-4.00%
FULL SUPER					
-4.00%	-3.80%	-2.00%	4507+50	-2.00%	-4.00%
-4.00%	-2.00%	-2.00%	4508+00	-2.00%	-4.00%
NORMAL CROWN					
-4.00%	-2.00%	-2.00%	4509+00	-2.00%	-4.00%
MATCH EXIT 103 SB BRIDGE					

NORTHBOUND ORT @ SUPERELEVATION TABLE

LEFT SHOULDER	LEFT TRAVELWAY	STATION	RIGHT GORE	RIGHT TRAVELWAY	RIGHT SHOULDER
MATCH EXISTING					
-2.30%	-1.30%	7484+50	-2.00%	-2.00%	-4.00%
-4.00%	-2.00%	7485+00	-2.00%	-2.00%	-4.00%
NORMAL CROWN					
-4.00%	-2.00%	7487+80	-2.00%	-2.00%	-4.00%
-4.00%	-2.00%	7488+00	-	-	-2.40%
-4.00%	-2.00%	7488+30	-	-	-3.00%
-4.00%	-2.00%	7488+50	-	-	-3.40%
-4.00%	-2.00%	7488+80	-	-	-4.00%
NORMAL CROWN					
-4.00%	-2.00%	7499+50	-	-	-4.00%
-2.00%	-2.00%	7500+00	-	-	-2.00%
-2.00%	-2.00%	7501+00	-	-	-2.00%
-4.00%	-0.30%	7501+50	-	-	-4.00%
-4.00%	1.40%	7502+00	-	-	-4.00%
-4.00%	2.00%	7502+17.65	-	-	-4.00%
-2.90%	3.10%	7502+50	-	-	-4.00%
-2.00%	4.00%	7502+76.47	-	-	-4.00%
-2.00%	4.80%	7503+00	-	-	-4.00%
FULL SUPER					
-2.00%	4.80%	7510+50	-	-	-4.80%
-2.00%	4.10%	7511+00	-	-	-4.10%
-2.00%	4.00%	7511+07.14	-	-	-4.00%
-4.00%	3.40%	7511+50	-	-	-4.00%
-4.00%	2.70%	7512+00	-	-	-4.00%
-4.00%	2.00%	7512+50	-	-	-4.00%
REMOVE CROWN					
-4.00%	2.00%	7521+00	-	-	-4.00%
-2.00%	2.00%	7521+50	-	-	-4.00%
0.00%	2.00%	7522+00	-	-	-4.00%
2.00%	2.00%	7522+50	-	-	-4.00%
2.00%	2.00%	7538+87.61	-	-	-4.00%

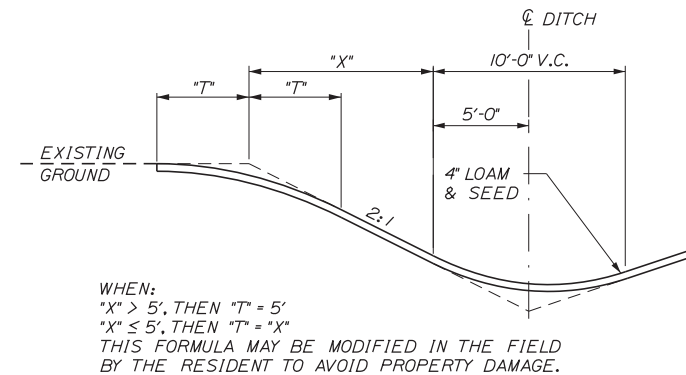


SOUTHBOUND CASH @ SUPERELEVATION TABLE

LEFT SHOULDER	LEFT TRAVELWAY	STATION	RIGHT SHOULDER
-4.00%	-2.00%	3499+50	2.00%
-4.00%	0.00%	3500+00	0.00%
-4.00%	0.00%	3501+00	0.00%
-4.00%	-2.00%	3501+50	2.00%

NORTHBOUND CASH @ SUPERELEVATION TABLE

LEFT SHOULDER	STATION	RIGHT TRAVELWAY	RIGHT SHOULDER
2.00%	8498+50	-2.00%	-4.00%
0.00%	8499+00	0.00%	-4.00%
0.00%	8500+00	0.00%	-4.00%
2.00%	8500+50	-2.00%	-4.00%



WHEN:
 "X" > 5', THEN "T" = 5'
 "X" ≤ 5', THEN "T" = "X"
 THIS FORMULA MAY BE MODIFIED IN THE FIELD BY THE RESIDENT TO AVOID PROPERTY DAMAGE.

EXIT 51 SB OFF RAMP @ SUPERELEVATION TABLE

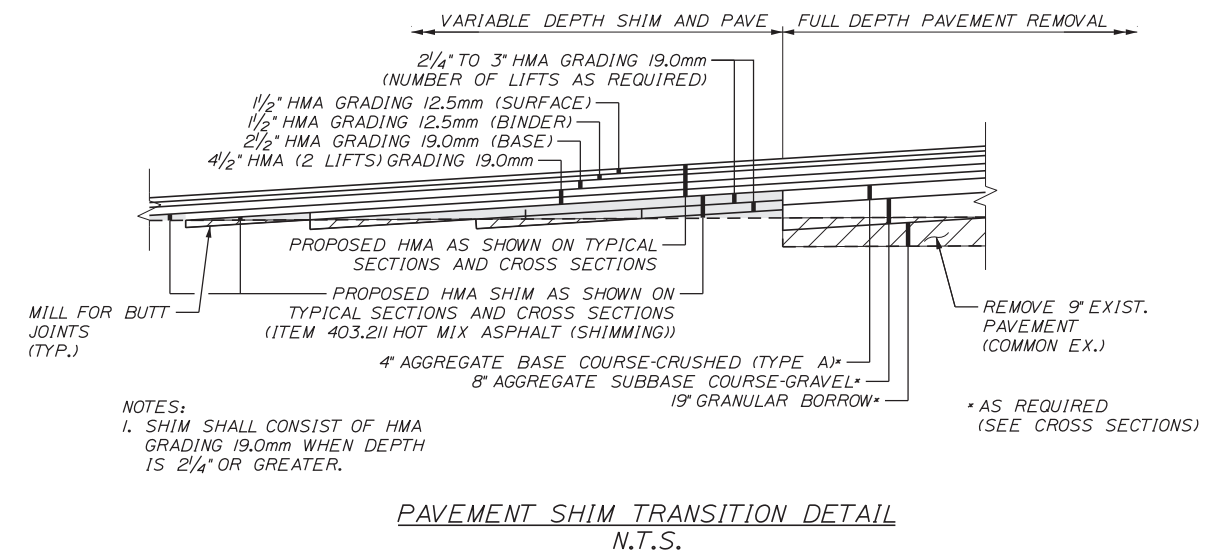
LEFT SHOULDER	STATION	RIGHT TRAVELWAY	RIGHT SHOULDER
MATCH EXISTING			
-4.00%	785+24.65	3.70%	-5.80%
-4.00%	785+50	3.80%	-4.00%
FULL SUPER			
-4.00%	787+00	3.80%	-4.00%
-4.00%	787+50	3.80%	-2.00%
-4.00%	788+00	3.80%	0.00%
-4.00%	788+50	3.80%	2.00%
FULL SUPER			
-4.00%	790+50	3.80%	-
-4.00%	4491+00*	2.90%	-
-4.00%	4491+50*	2.00%	-

* SB ORT BASELINE

EXIT 51 NB ON RAMP @ SUPERELEVATION TABLE

LEFT SHOULDER	STATION	RIGHT TRAVELWAY	RIGHT SHOULDER
MATCH EXISTING			
-1.90%	3.00%	886+00	-5.00%
-2.00%	3.80%	886+50	-4.60%
-2.00%	4.60%	887+00	-4.60%
FULL SUPER			
-2.00%	4.60%	888+50	-4.60%
0.00%	4.60%	889+00	-4.60%
-	4.60%	889+50	4.60%
-	3.30%	7490+00*	-4.00%
-	2.00%	7490+50*	-4.00%

* NB ORT BASELINE



NOTES:
 1. SHIM SHALL CONSIST OF HMA GRADING 19.0mm WHEN DEPTH IS 2/4" OR GREATER.

Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA..._Typ-XX.dgn

Scale: 5 0 5 10
Scale of Feet

No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

By	Date	By	Date
Designed	PLP 3\20\19	Checked	LEM 3\20\19
Drawn	EJB 3\20\19	In Charge of	GAE 3\20\19

STANTEC CONSULTING SERVICES INC.
 482 PAYNE ROAD
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THE GOLD STAR MEMORIAL HIGHWAY

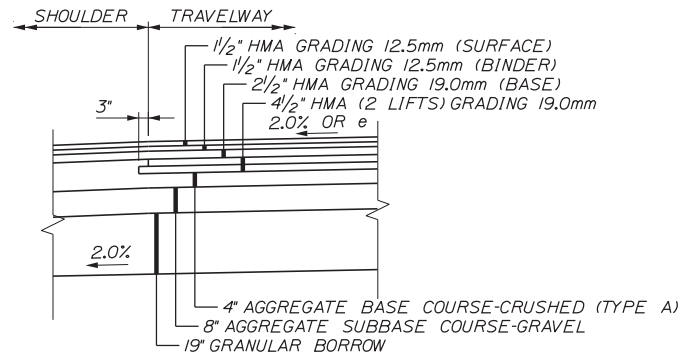
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

EXIT 103 ORT CONVERSION
 TYPICAL SECTIONS 6

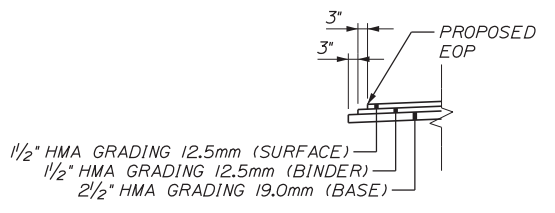
SHEET NUMBER: TS-6
 CONTRACT: 2019.04
 11 OF 503

Date: 3/20/2019

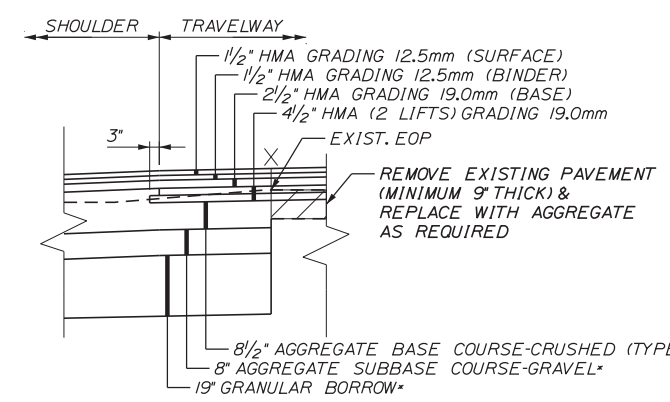
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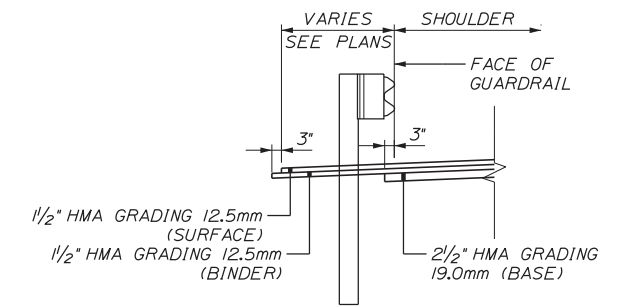
DETAIL A
N.T.S.



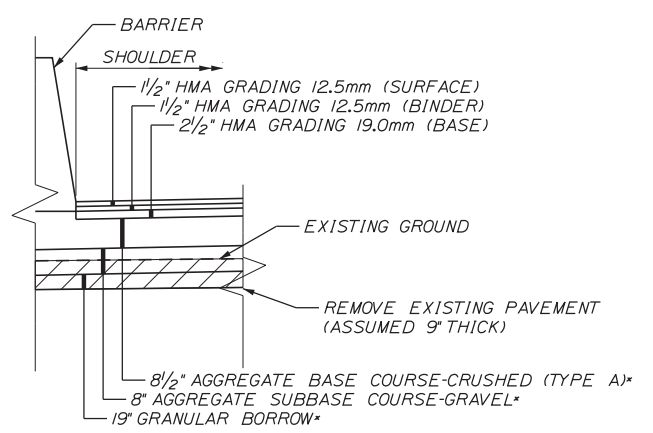
DETAIL B
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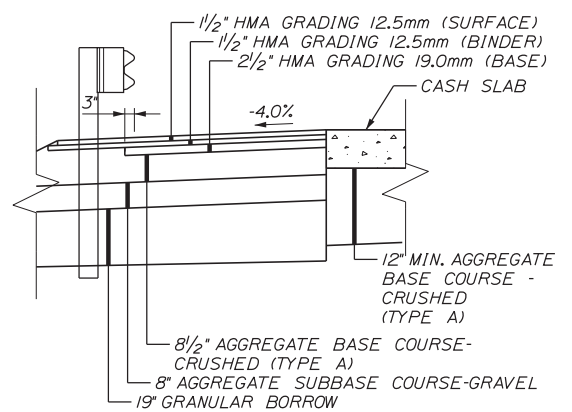
DETAIL C
N.T.S. * AS REQUIRED (SEE CROSS SECTIONS)



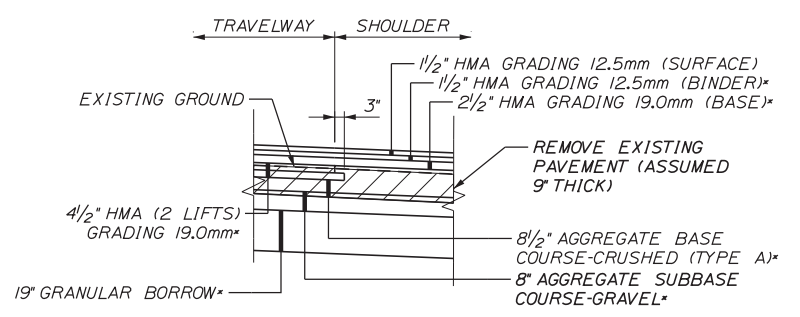
DETAIL D
N.T.S.



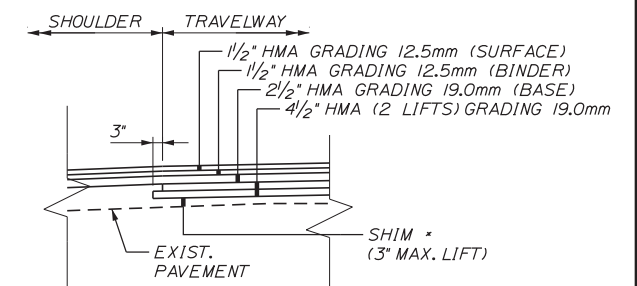
DETAIL E
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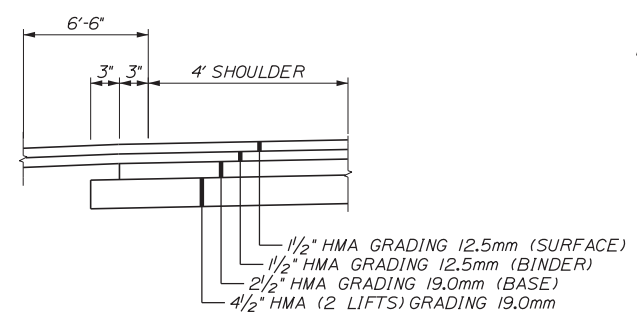
DETAIL F
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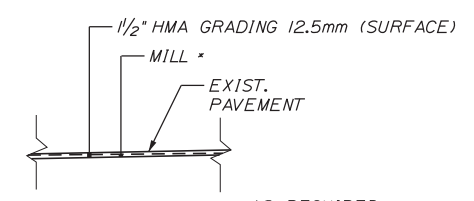
DETAIL G
N.T.S. * AS REQUIRED (SEE CROSS SECTIONS)



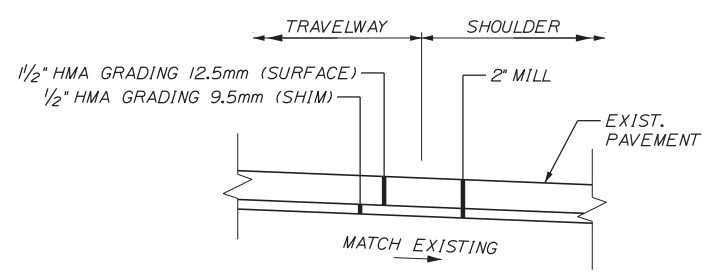
DETAIL H
N.T.S. * AS REQUIRED (SEE CROSS SECTIONS)



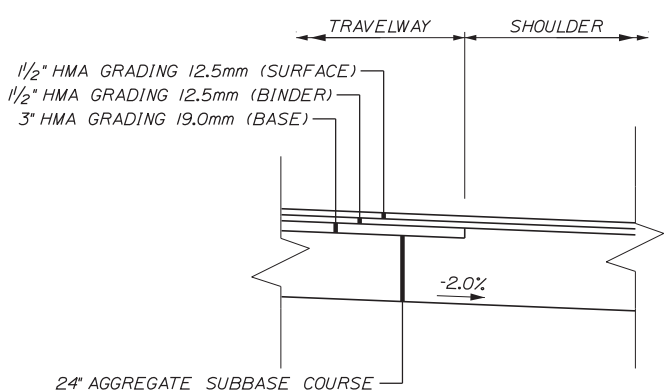
DETAIL J
N.T.S.



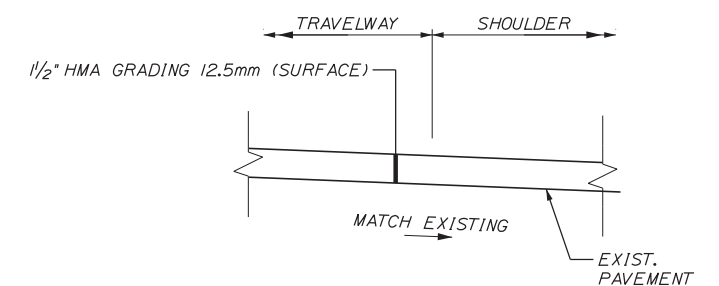
DETAIL I
N.T.S. * AS REQUIRED (SEE CROSS SECTIONS)



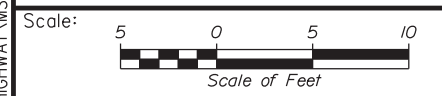
DETAIL N
N.T.S.



DETAIL M
N.T.S.



DETAIL P
N.T.S.



No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

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MAINE TURNPIKE

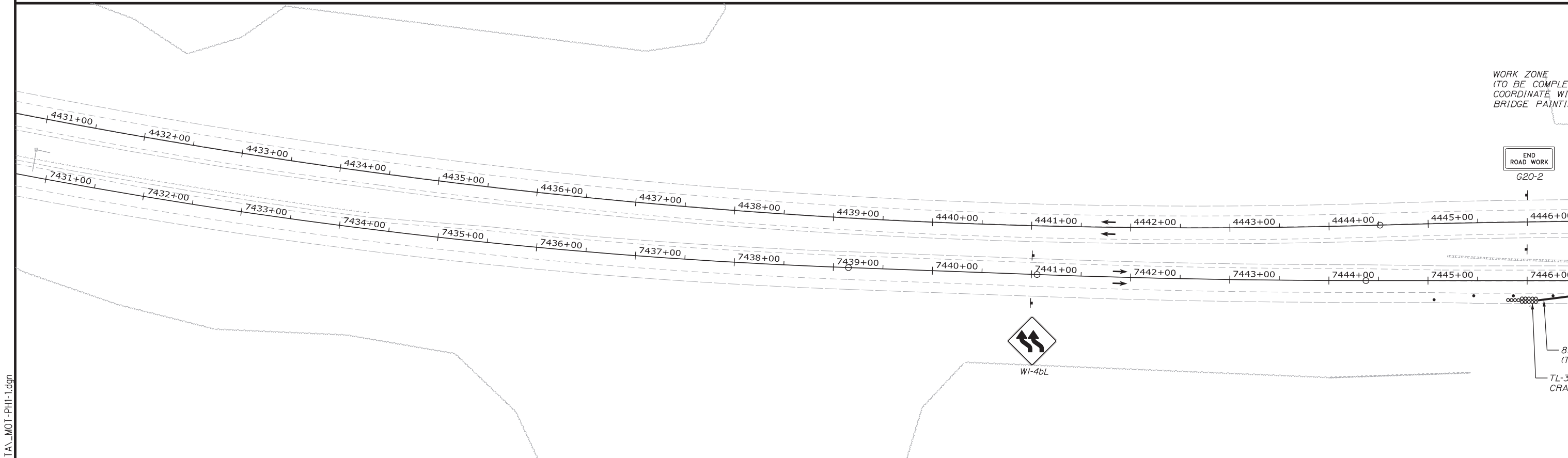
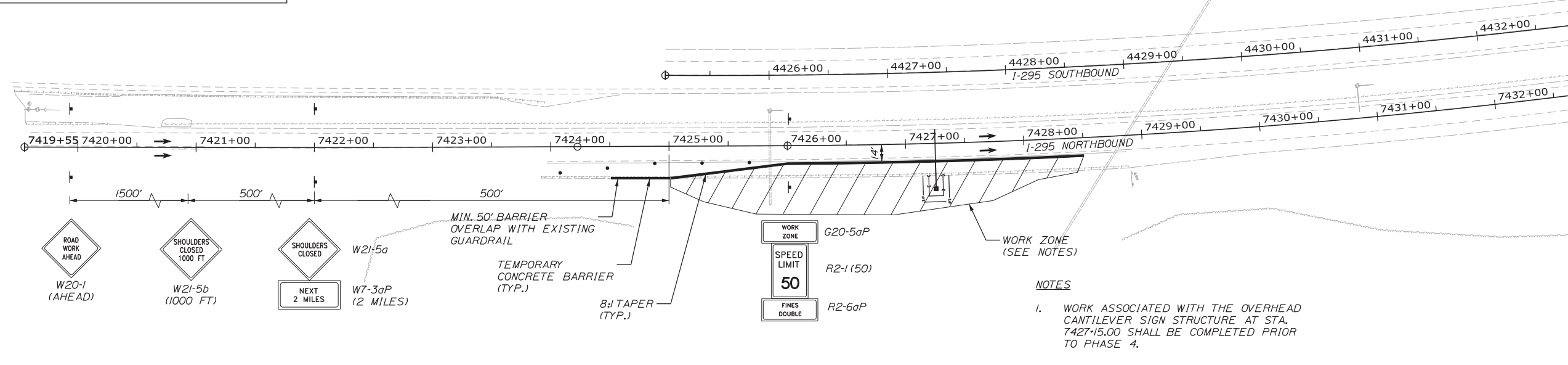
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

EXIT 103
ORT CONVERSION
TYPICAL SECTIONS 7

SHEET NUMBER: TS-7
CONTRACT: 2019.04
12 OF 503

WINTER TIME REQUIREMENTS:
 CONTRACTOR SHALL SUFFICIENTLY COMPLETE CONSTRUCTION ON I-296 NORTHBOUND AND SOUTHBOUND PRIOR TO NOVEMBER 16, 2019 AND NOVEMBER 16, 2020 SO AS ALL RIGHT SIDE TEMPORARY CONCRETE BARRIER IS REMOVED OR THE TEMPORARY CONCRETE BARRIER IS RELOCATED TO PROVIDE A RIGHT SHOULDER 8 FEET WIDE OR GREATER, FROM NOVEMBER 15, 2019 TO APRIL 15, 2020 AND FROM NOVEMBER 16, 2020 THROUGH APRIL 15 2021, EXCEPT AS NOTED ON THESE PLANS OR SPECIAL PROVISIONS.



Scale: 50 0 50 100
 Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date	By	Date	
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 1
 MAINTENANCE OF TRAFFIC PLAN 1

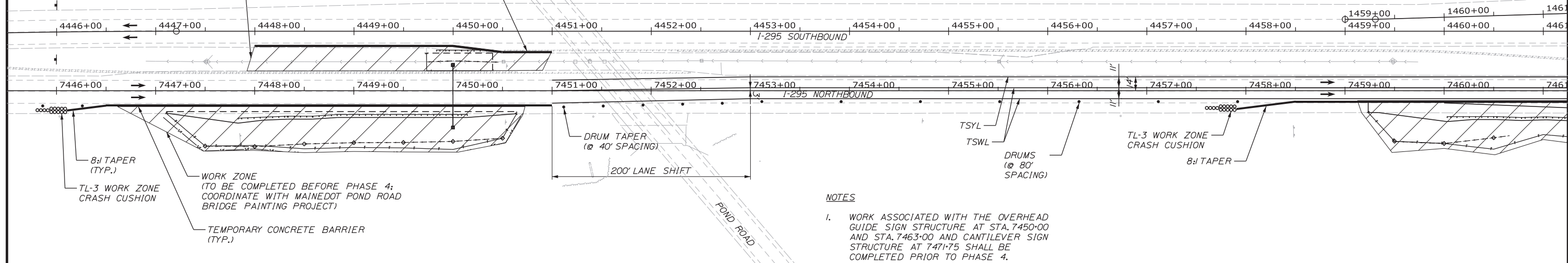
SHEET NUMBER: MOT1-01
 CONTRACT: 2019.04
 13 OF 503

Filename: ...\\00\HIGHWAY\MSTA\MOT-PH1-1.dgn

WORK ZONE
(TO BE COMPLETED BEFORE PHASE 4;
COORDINATE WITH MAINEDOT POND ROAD
BRIDGE PAINTING PROJECT)

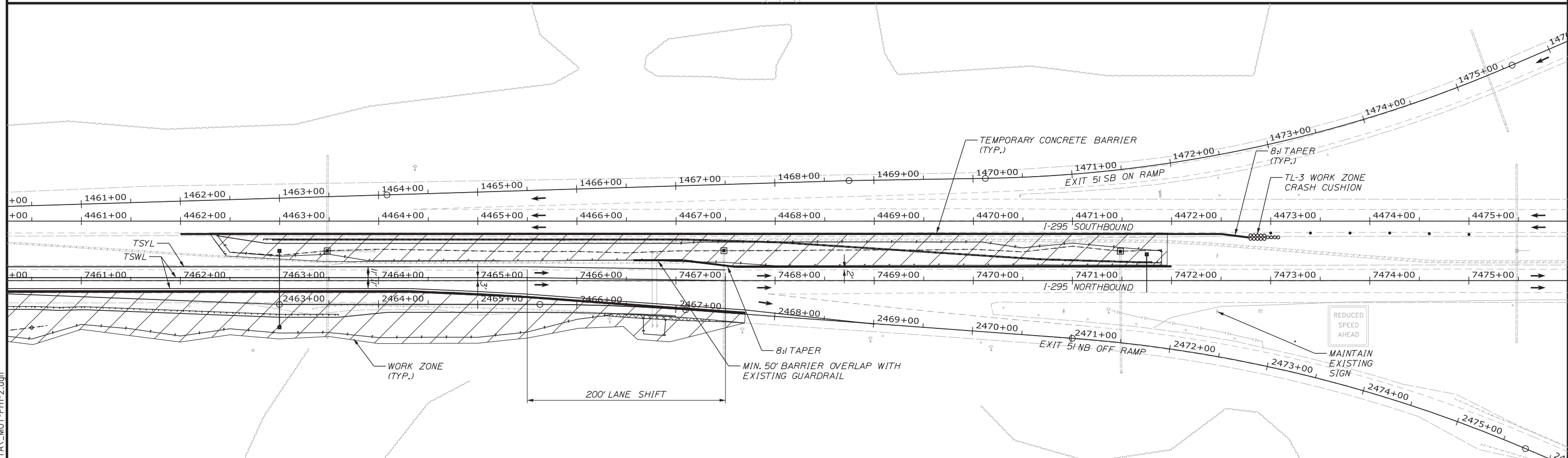
END
ROAD WORK
G20-2

MIN. 50' BARRIER OVERLAP WITH
EXISTING GUARDRAIL

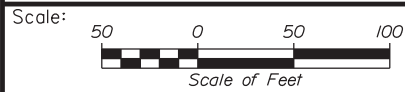


NOTES

1. WORK ASSOCIATED WITH THE OVERHEAD GUIDE SIGN STRUCTURE AT STA. 7450+00 AND STA. 7463+00 AND CANTILEVER SIGN STRUCTURE AT 7471+75 SHALL BE COMPLETED PRIOR TO PHASE 4.



Filename: ...:\00.HIGHWAY\MSTA\MOT-PH1-2.dgn



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**THE GOLD STAR
MEMORIAL HIGHWAY**

**INTERCHANGE 103
ORT CONVERSION
PHASE 1
MAINTENANCE OF TRAFFIC PLAN 2**

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE					
	By	Date	Checked	By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

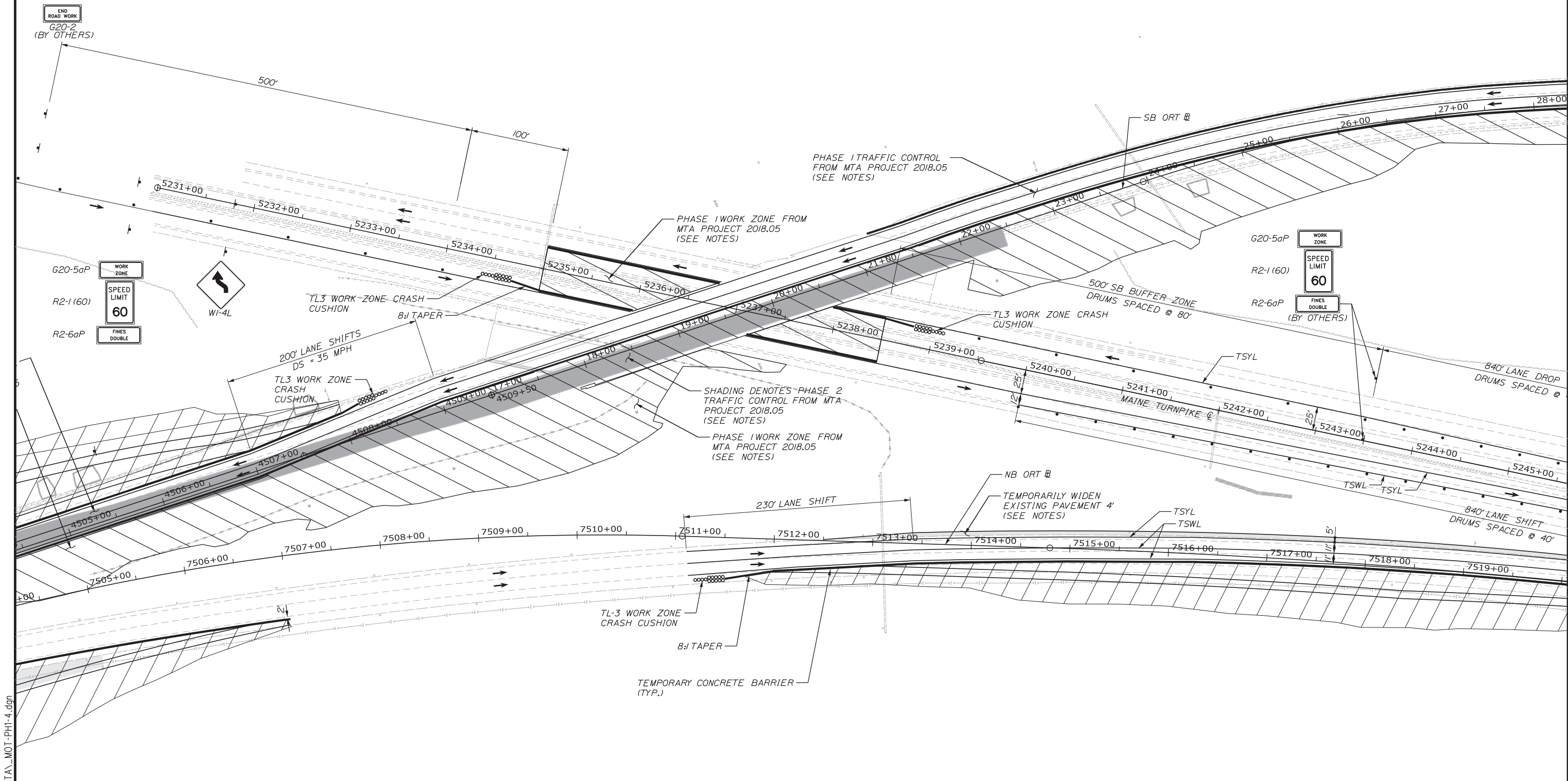
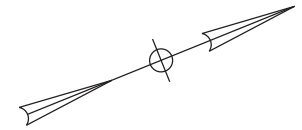
CONTRACT:2019.04

SHEET NUMBER: MOT1-01

14 OF 503

NOTES:

1. CONTRACTOR SHALL COORDINATE WITH BRIDGE CONTRACTOR FOR MTA PROJECT 2018.05. TRAFFIC IS ASSUMED TO BE IN PHASE 1 OF BRIDGE CONSTRUCTION AT THE BEGINNING OF PHASE 1. CONTRACTOR SHALL COORDINATE WITH BRIDGE CONTRACTOR WHEN PROJECT 2018.05 TRANSITIONS TO PHASE 2.
2. TEMPORARY WIDENING OF PAVEMENT SHALL BE DONE BEHIND DAILY CLOSURES WITH DRUMS. WORK SHALL BE COMPLETED PRIOR TO IMPLEMENTING TRAFFIC CONTROL FROM STA. 7510+00 TO END OF PROJECT.



Filename: ...:\00.HIGHWAY\MSTA_MOT-PH1-4.dgn

Scale: 50 0 50 100
Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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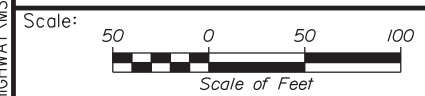
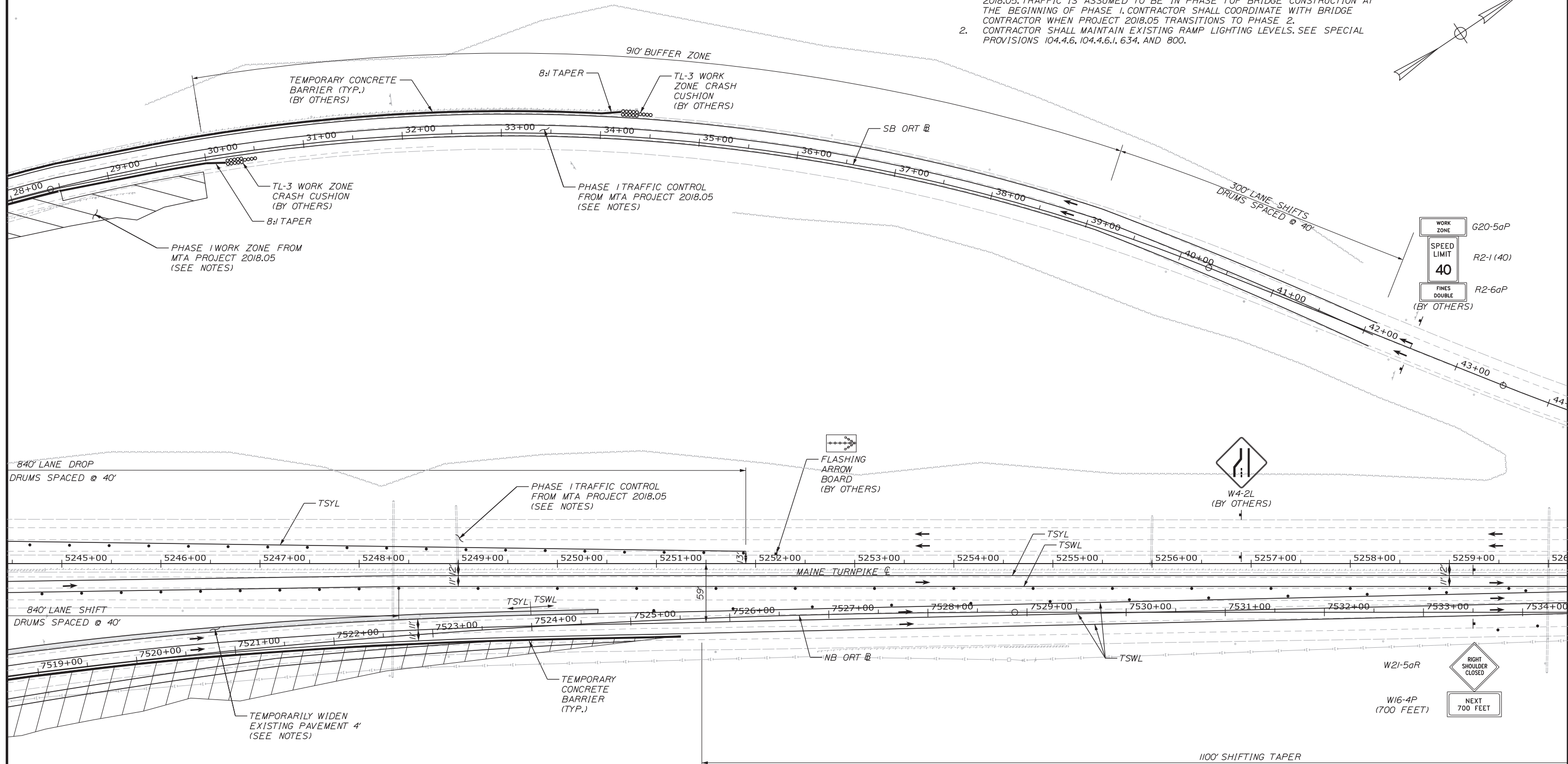
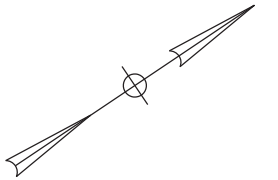
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 1
MAINTENANCE OF TRAFFIC PLAN 4

SHEET NUMBER: MOT1-02
CONTRACT: 2019.04
16 OF 503

NOTES:

1. CONTRACTOR SHALL COORDINATE WITH BRIDGE CONTRACTOR FOR MTA PROJECT 2018.05. TRAFFIC IS ASSUMED TO BE IN PHASE 1 OF BRIDGE CONSTRUCTION AT THE BEGINNING OF PHASE 1. CONTRACTOR SHALL COORDINATE WITH BRIDGE CONTRACTOR WHEN PROJECT 2018.05 TRANSITIONS TO PHASE 2.
2. CONTRACTOR SHALL MAINTAIN EXISTING RAMP LIGHTING LEVELS. SEE SPECIAL PROVISIONS 104.4.6, 104.4.6.1, 634, AND 800.



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No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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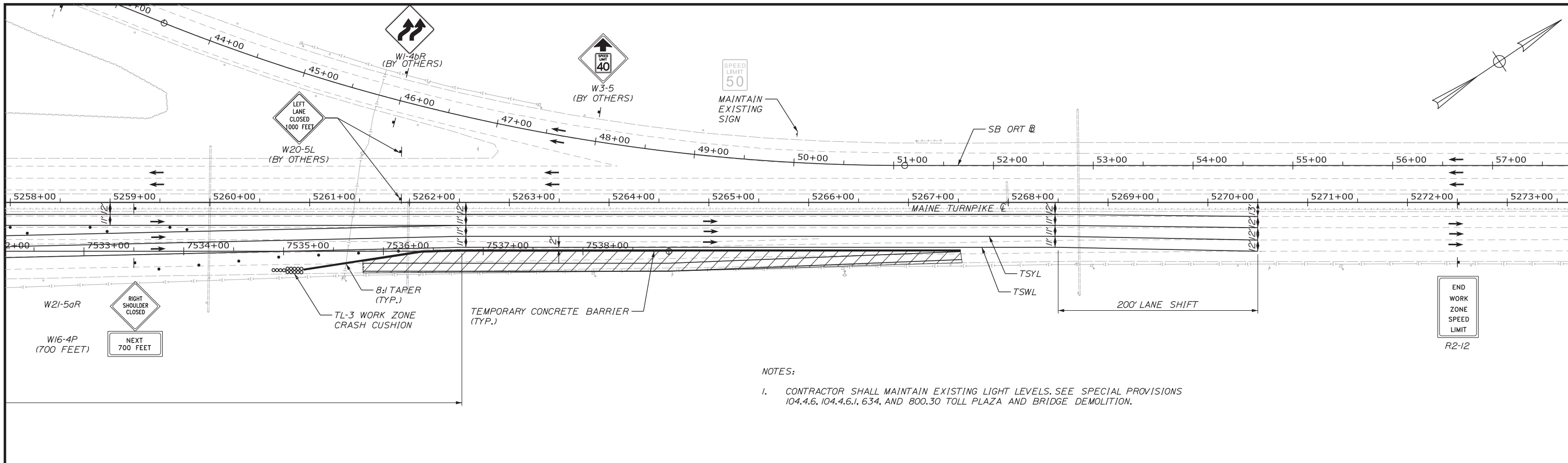


THE GOLD STAR MEMORIAL HIGHWAY

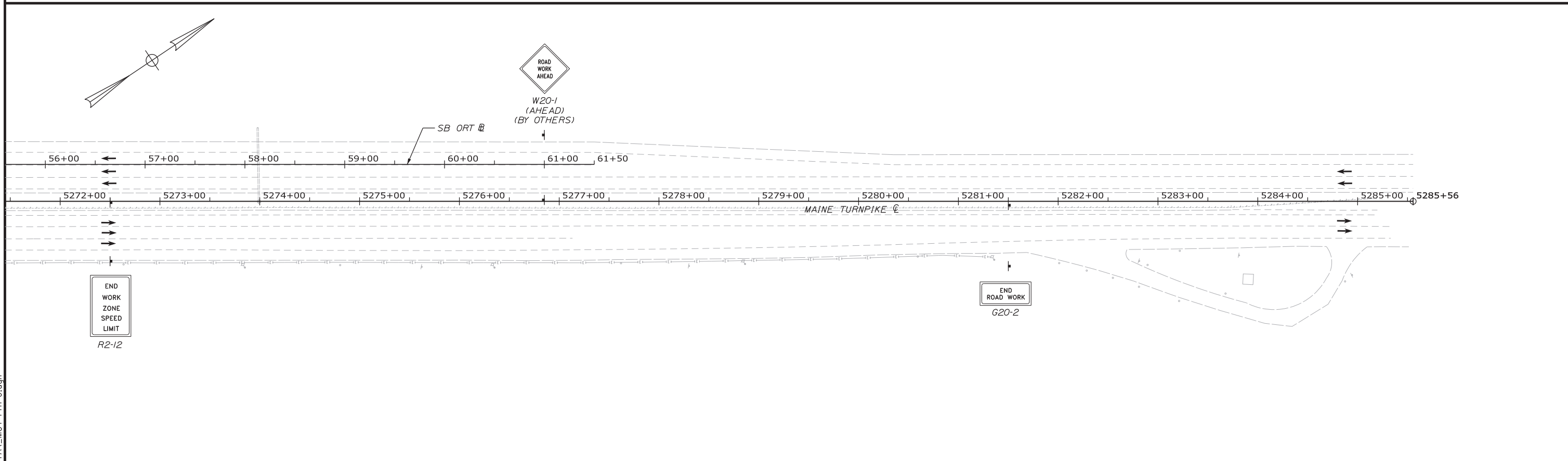
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 1
 MAINTENANCE OF TRAFFIC PLAN 5

SHEET NUMBER: MOT1-03
 CONTRACT: 2019.04
 17 OF 503



NOTES:
 1. CONTRACTOR SHALL MAINTAIN EXISTING LIGHT LEVELS. SEE SPECIAL PROVISIONS 104.4.6, 104.4.6.1, 634, AND 800.30 TOLL PLAZA AND BRIDGE DEMOLITION.



Filename: ...:\00\HIGHWAY\MSTA\MOT-PH1-6.dgn

Scale: Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
Designed	By	Date	Checked
Drawn	By	Date	In Charge of
	PLP	3\20\19	LEM
	EJB	3\20\19	GAE

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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

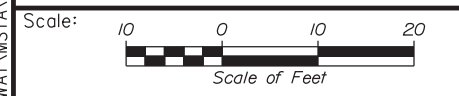
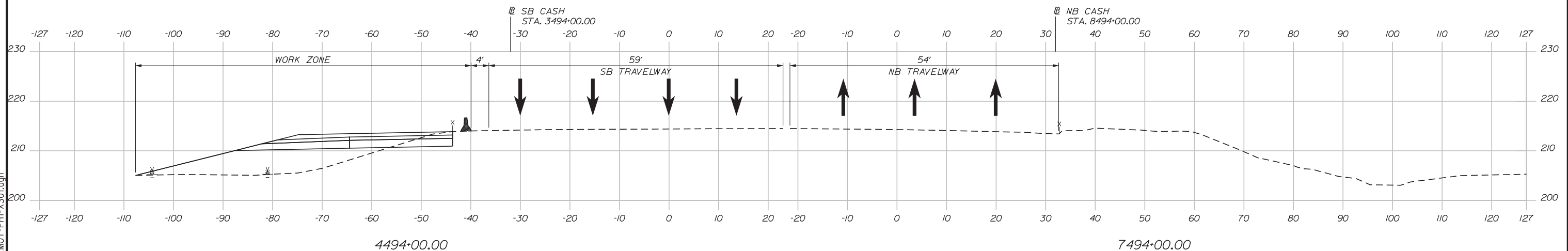
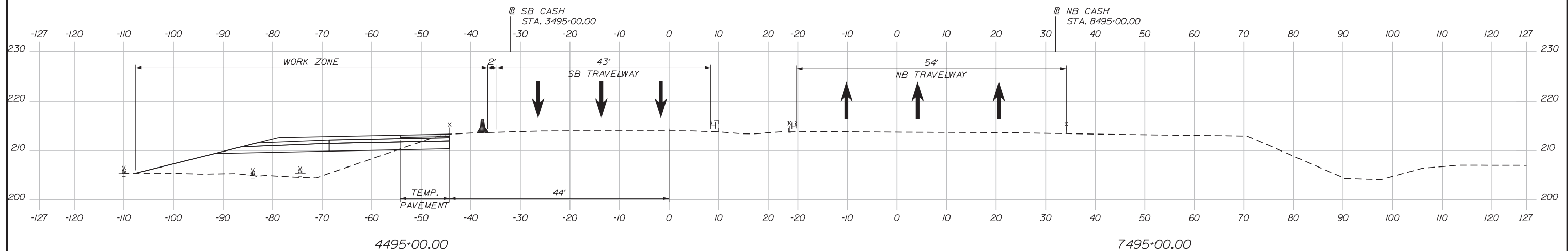
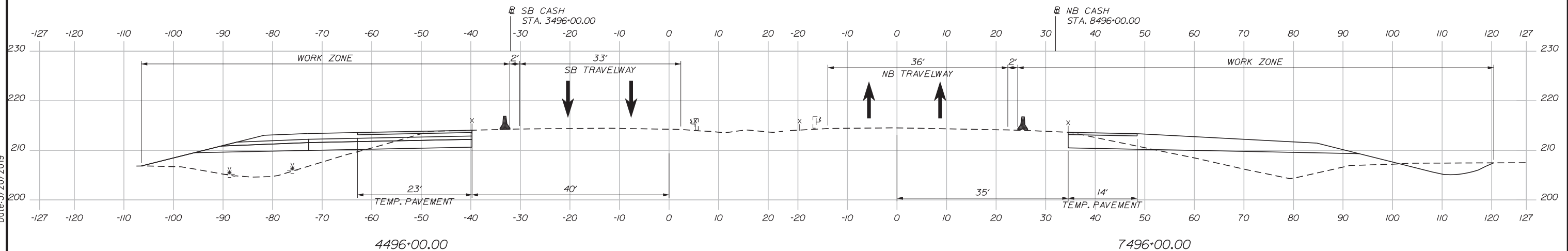
INTERCHANGE 103
 ORT CONVERSION
 PHASE 1
 MAINTENANCE OF TRAFFIC PLAN 6

SHEET NUMBER: MOT1-05
 18 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ... \HIGHWAY\MSTA\MOT-PTH-XS01.dgn



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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

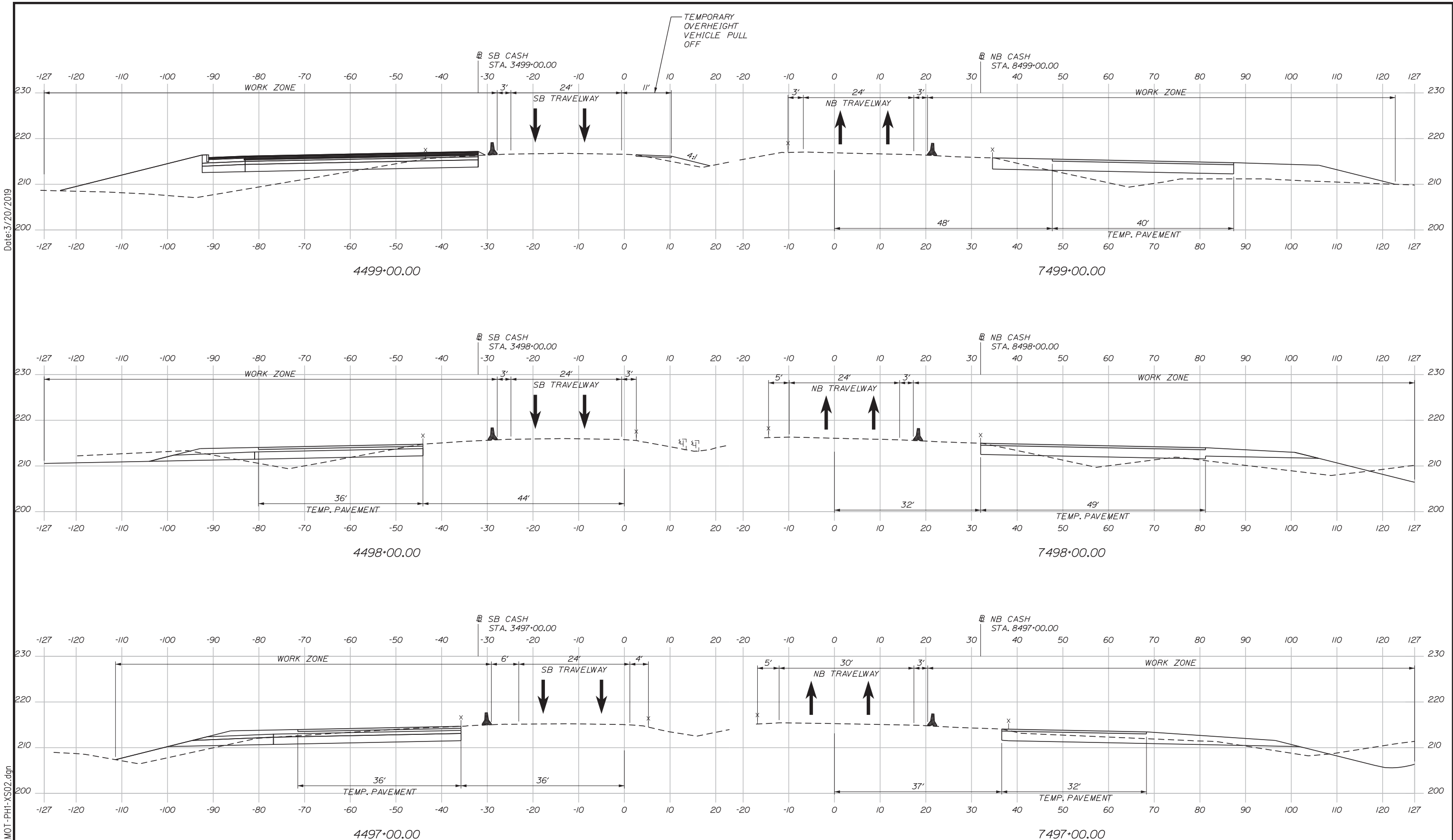
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 1
 MAINTENANCE OF TRAFFIC SECTION 1

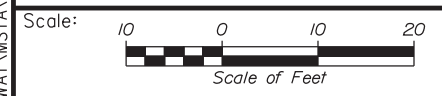
SHEET NUMBER: MOTXS-01
 19 OF 503

CONTRACT: 2019.04

Date: 3/20/2019



Filename: ... \HIGHWAY\MSTA\...MOT-PH1-XS02.dgn



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THE GOLD STAR
MEMORIAL HIGHWAY

INTERCHANGE 103
ORT CONVERSION
PHASE 1
MAINTENANCE OF TRAFFIC SECTION 2

No.	Revision	By	Date

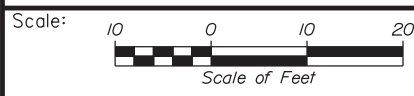
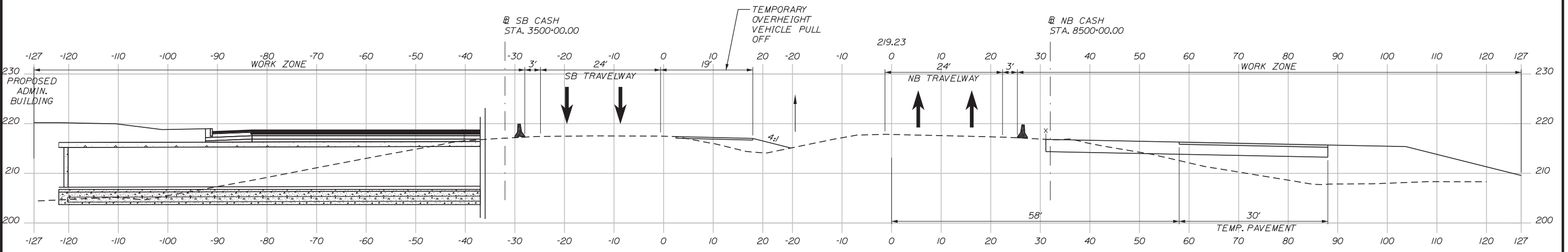
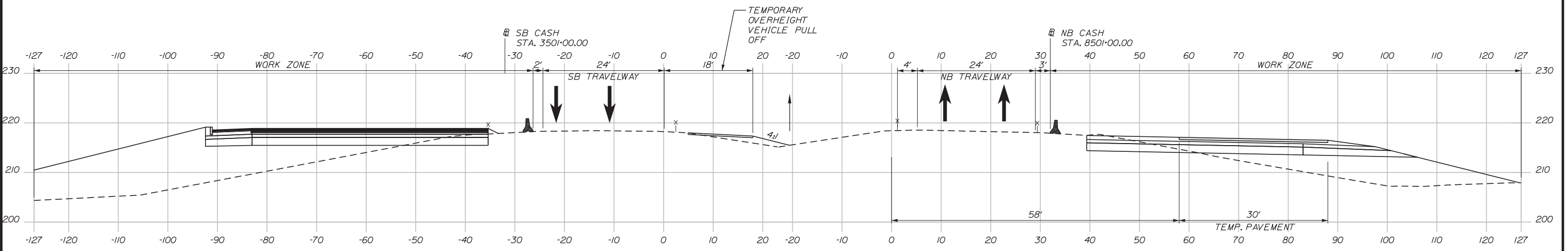
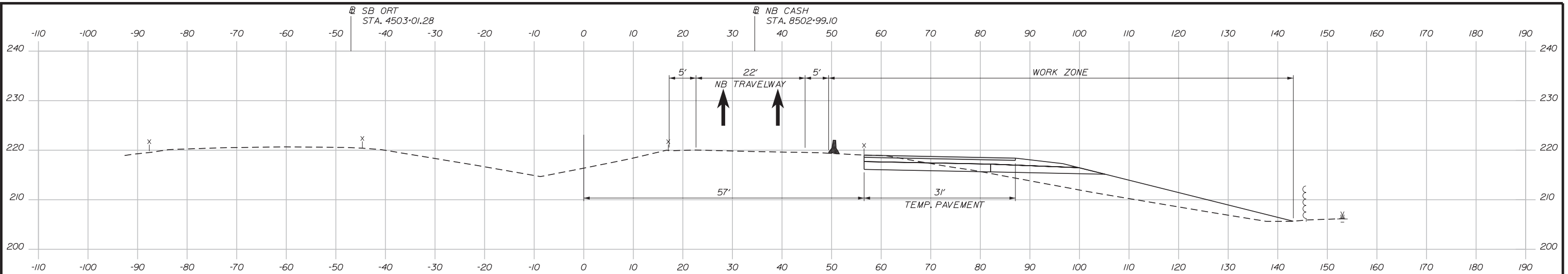
CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
Designed	By	Date	Checked
	PLP	3\20\19	LEM
Drawn	By	Date	In Charge of
	EJB	3\20\19	GAE

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

CONTRACT: 2019.04

SHEET NUMBER: MOTXS-02
20 OF 503

Date: 3/20/2019



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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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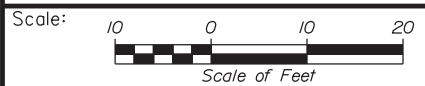
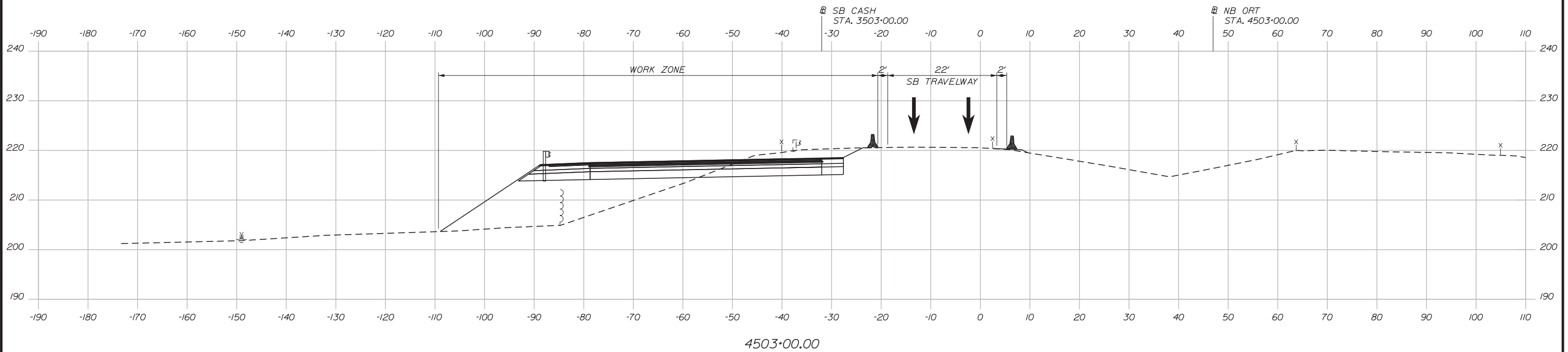
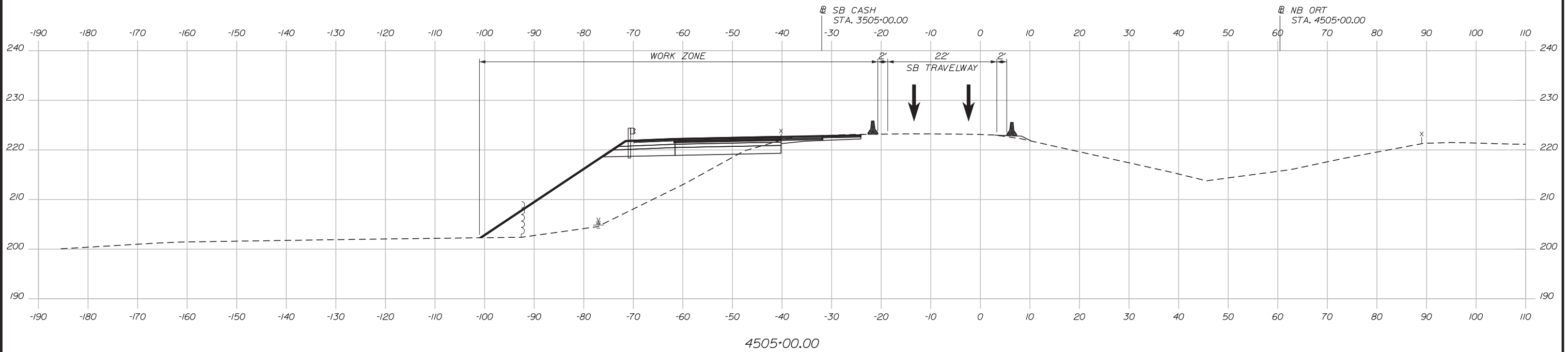


INTERCHANGE 103
 ORT CONVERSION
 PHASE 1
 MAINTENANCE OF TRAFFIC SECTION 3

Filename: ... \HIGHWAY\MSTA_MOT-PR1-XS03.dgn

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

Date: 3/20/2019



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**THE GOLD STAR
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INTERCHANGE 103
 ORT CONVERSION
 PHASE 1
 MAINTENANCE OF TRAFFIC SECTION 4

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
Designed	By	Date	Checked
	PLP	3\20\19	LEM
Drawn	By	Date	In Charge of
	EJB	3\20\19	GAE

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

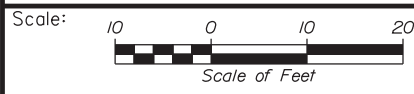
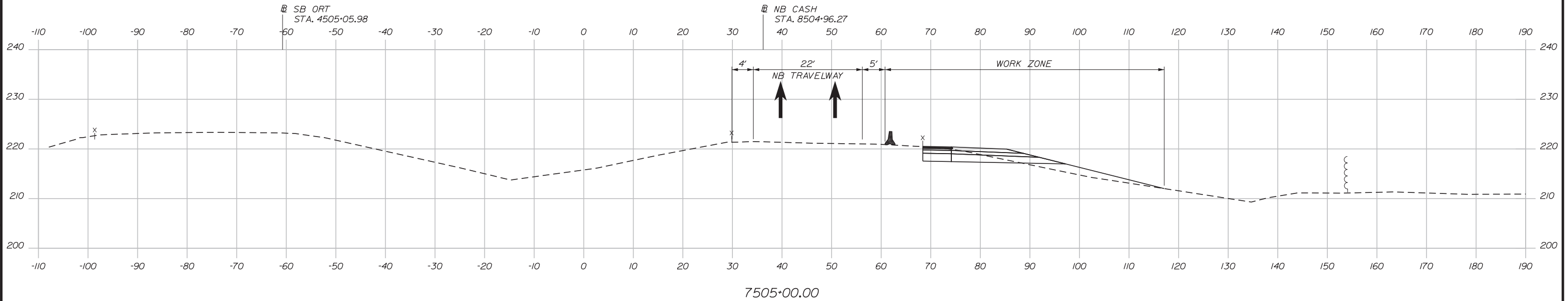
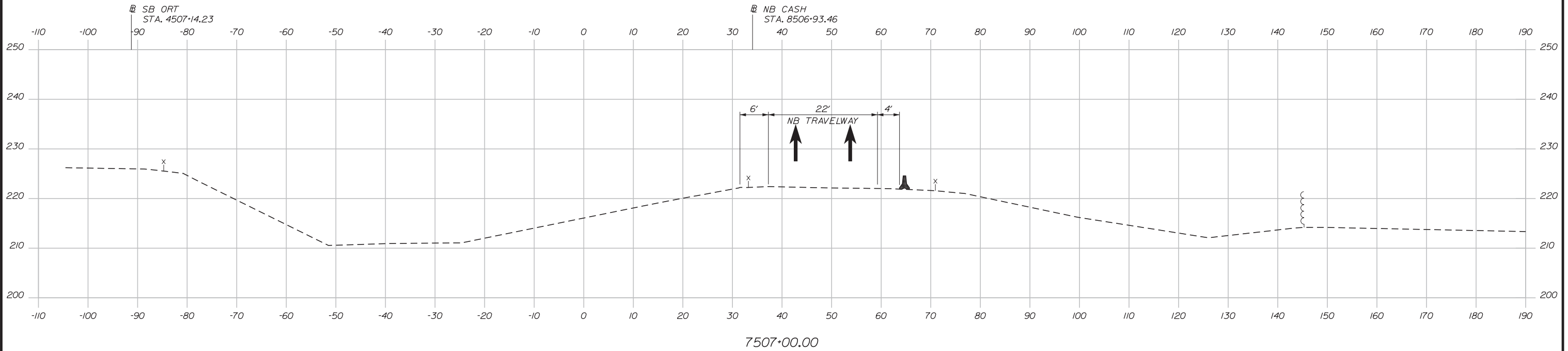
CONTRACT: 2019.04

SHEET NUMBER: MOTXS-04

22 OF 503

Filename: ...HIGHWAY\MSTA\MOT-Ph1-XS04.dgn

Date: 3/20/2019



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INTERCHANGE 103
 ORT CONVERSION
 PHASE 1
 MAINTENANCE OF TRAFFIC SECTION 5

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
Designed	By	Date	Checked
	PLP	3\20\19	LEM
Drawn	By	Date	In Charge of
	EJB	3\20\19	GAE

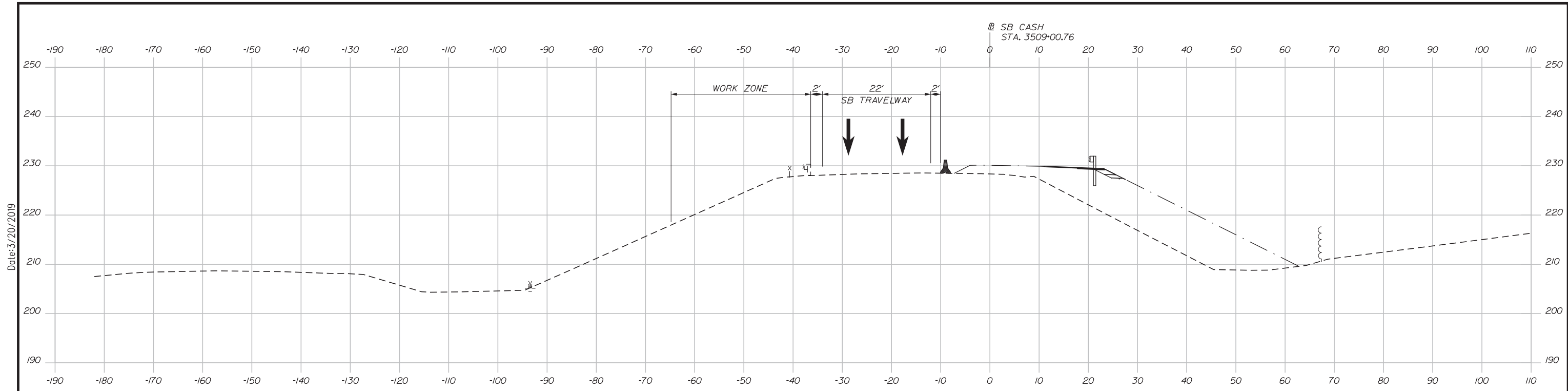
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

CONTRACT: 2019.04

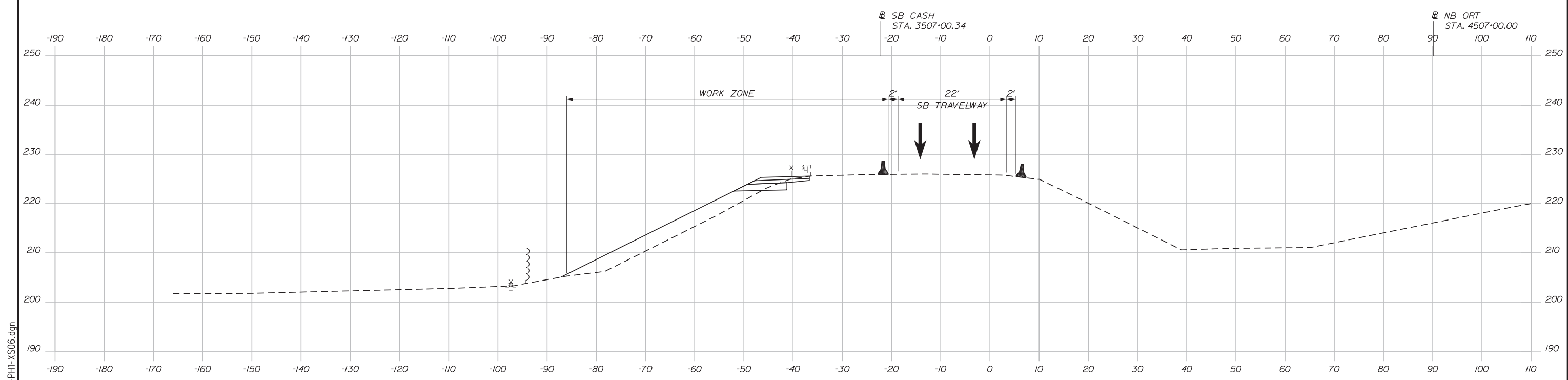
SHEET NUMBER: MOTXS-05

23 OF 503

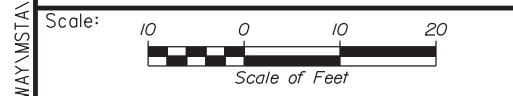
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4509+00.00



4507+00.00



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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 1
 MAINTENANCE OF TRAFFIC SECTION 6

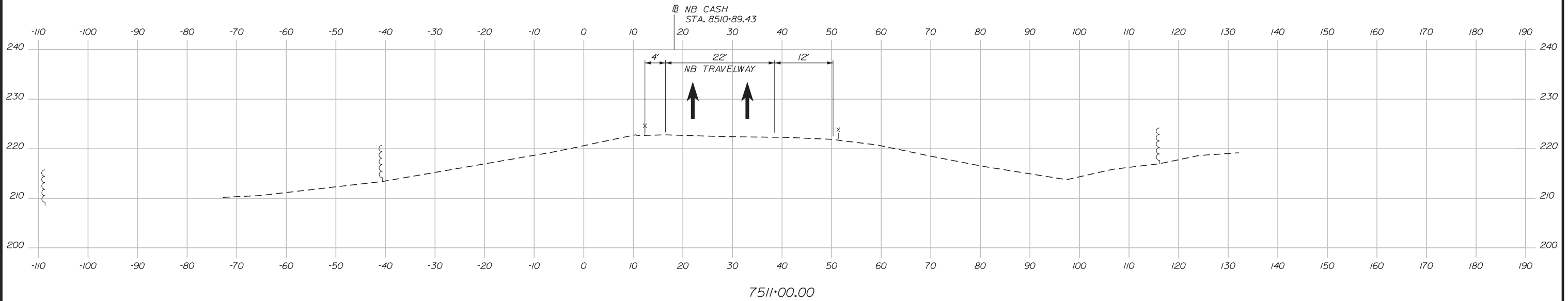
SHEET NUMBER: MOTXS-06
 24 OF 503

CONTRACT: 2019.04

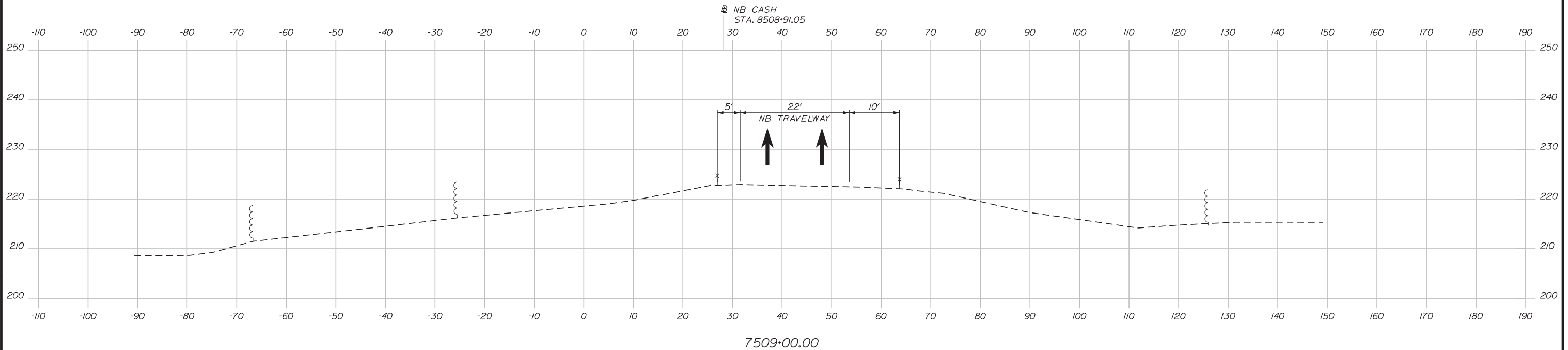
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Date: 3/20/2019

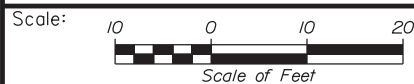
Date: 3/20/2019



7511+00.00



7509+00.00



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THE GOLD STAR
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INTERCHANGE 103
ORT CONVERSION
PHASE 1
MAINTENANCE OF TRAFFIC SECTION 7

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
Designed	By	Date	Checked
	PLP	3\20\19	LEM
Drawn	By	Date	In Charge of
	EJB	3\20\19	GAE

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

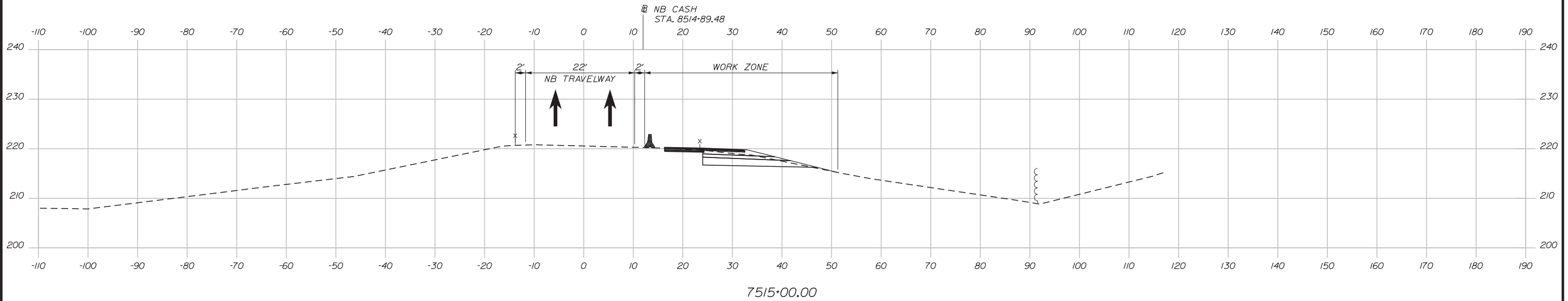
CONTRACT: 2019.04

SHEET NUMBER: MOTXS-07

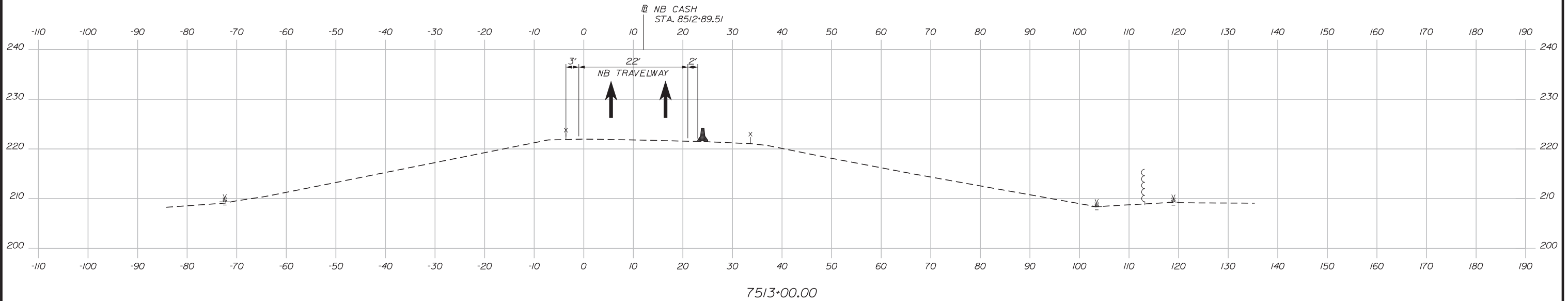
25 OF 503

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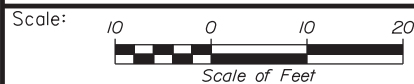
Date: 3/20/2019



7515+00.00



7513+00.00



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THE GOLD STAR
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INTERCHANGE 103
ORT CONVERSION
PHASE 1
MAINTENANCE OF TRAFFIC SECTION 8

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
	By	Date	
Designed	PLP	3\20\19	Checked LEM 3\20\19
Drawn	EJB	3\20\19	In Charge of GAE 3\20\19

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

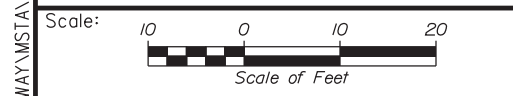
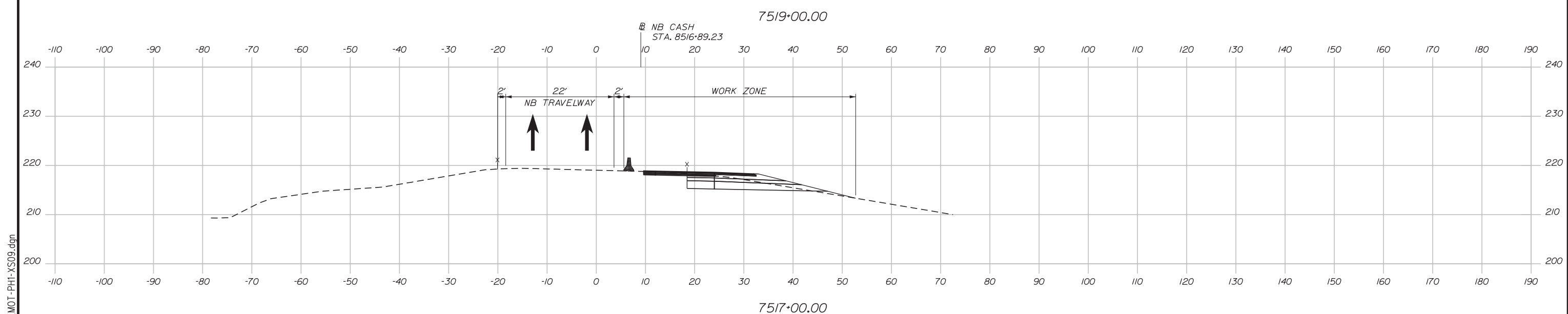
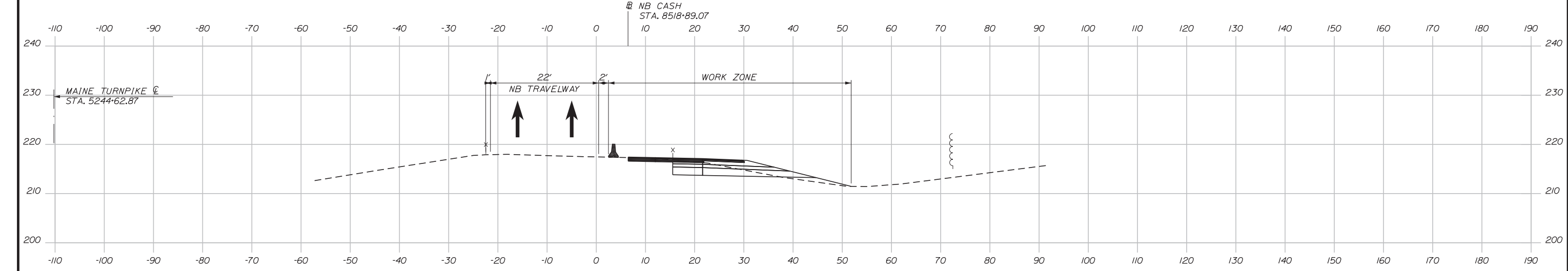
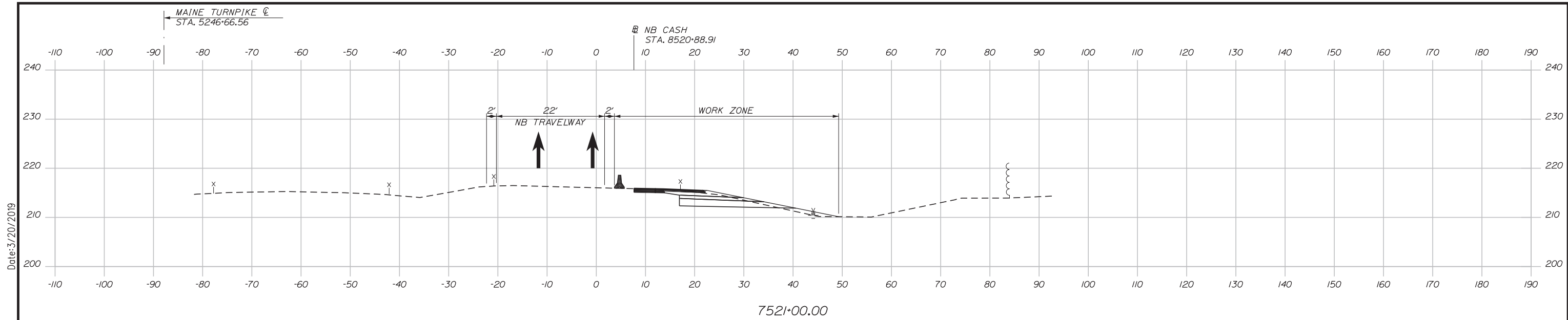
CONTRACT: 2019.04

SHEET NUMBER: MOTXS-08

26 OF 503

Filename: ... \HIGHWAY\MSTA\MOT-Ph1-XS08.dgn

Date: 3/20/2019



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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

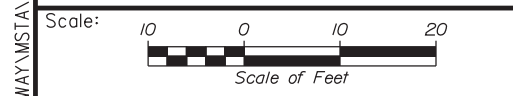
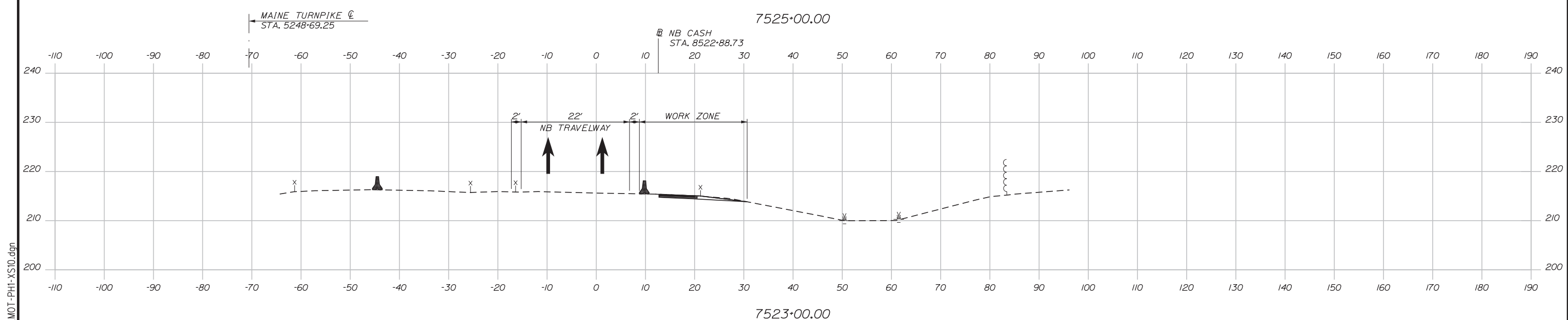
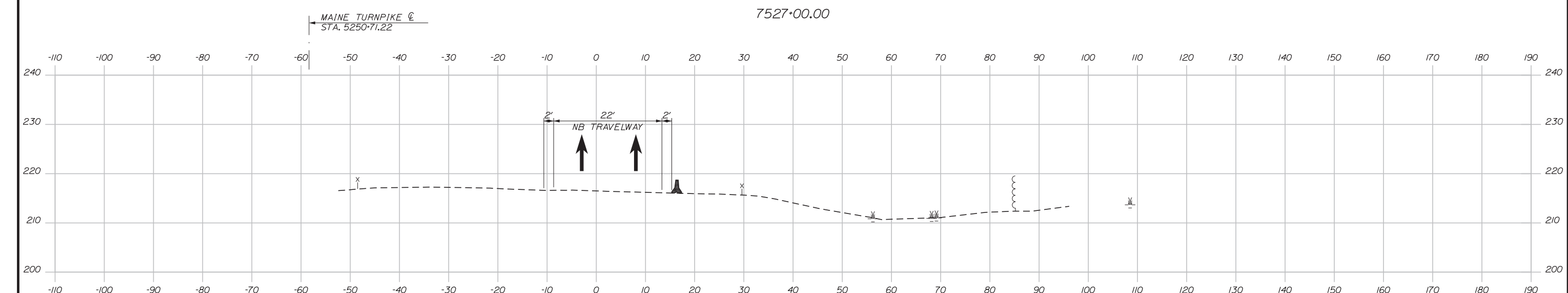
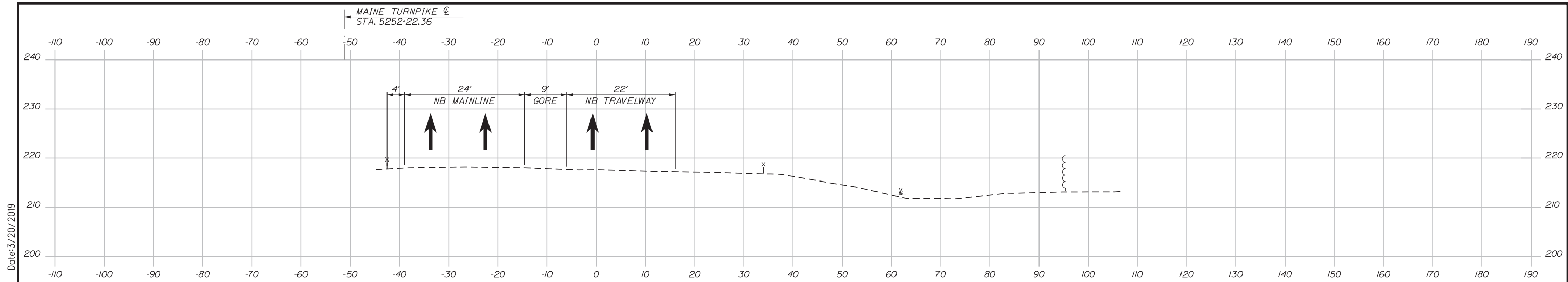
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 1
 MAINTENANCE OF TRAFFIC SECTION 9

SHEET NUMBER: MOTXS-09
 CONTRACT: 2019.04
 27 OF 503

Filename: ... \HIGHWAY\MSTA\...MOT-Ph1-XS09.dgn

Date: 3/20/2019



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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 1
 MAINTENANCE OF TRAFFIC SECTION 10

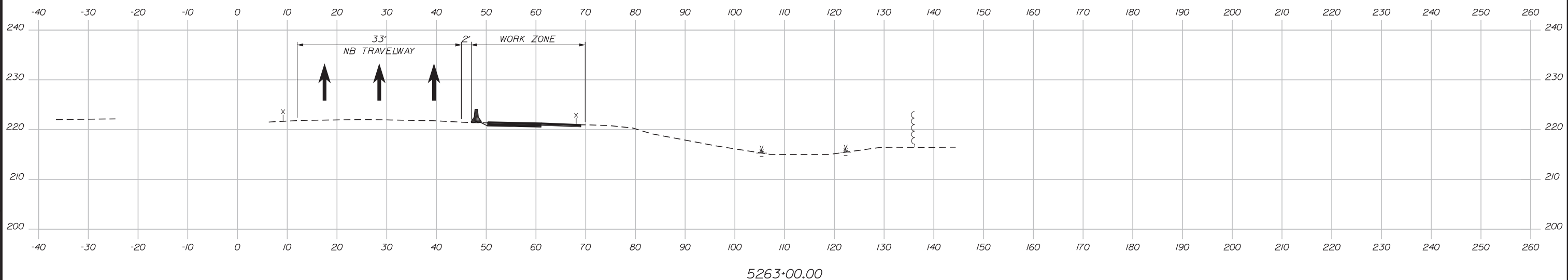
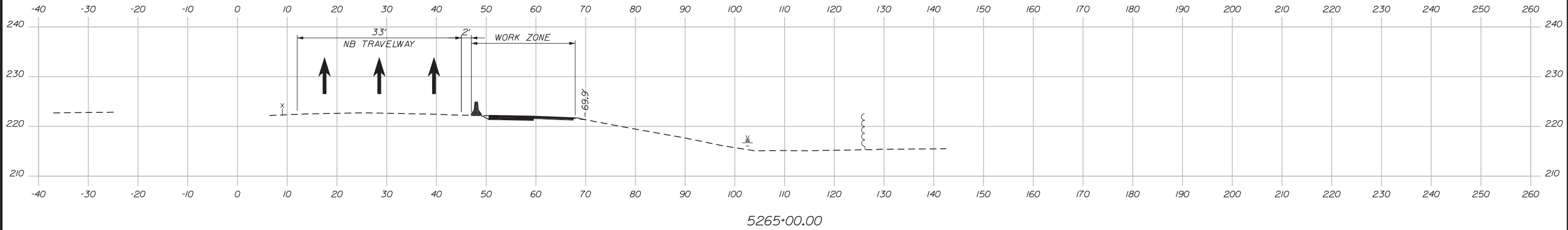
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 28 OF 503

CONTRACT: 2019.04

Filename: ... \HIGHWAY\MSTA\...MOT-PH1-XS10.dgn

Date: 3/20/2019

Filename: ... \HIGHWAY\MSTA\MOT-Ph1-XS11.dgn



Scale: Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date	Checked	By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
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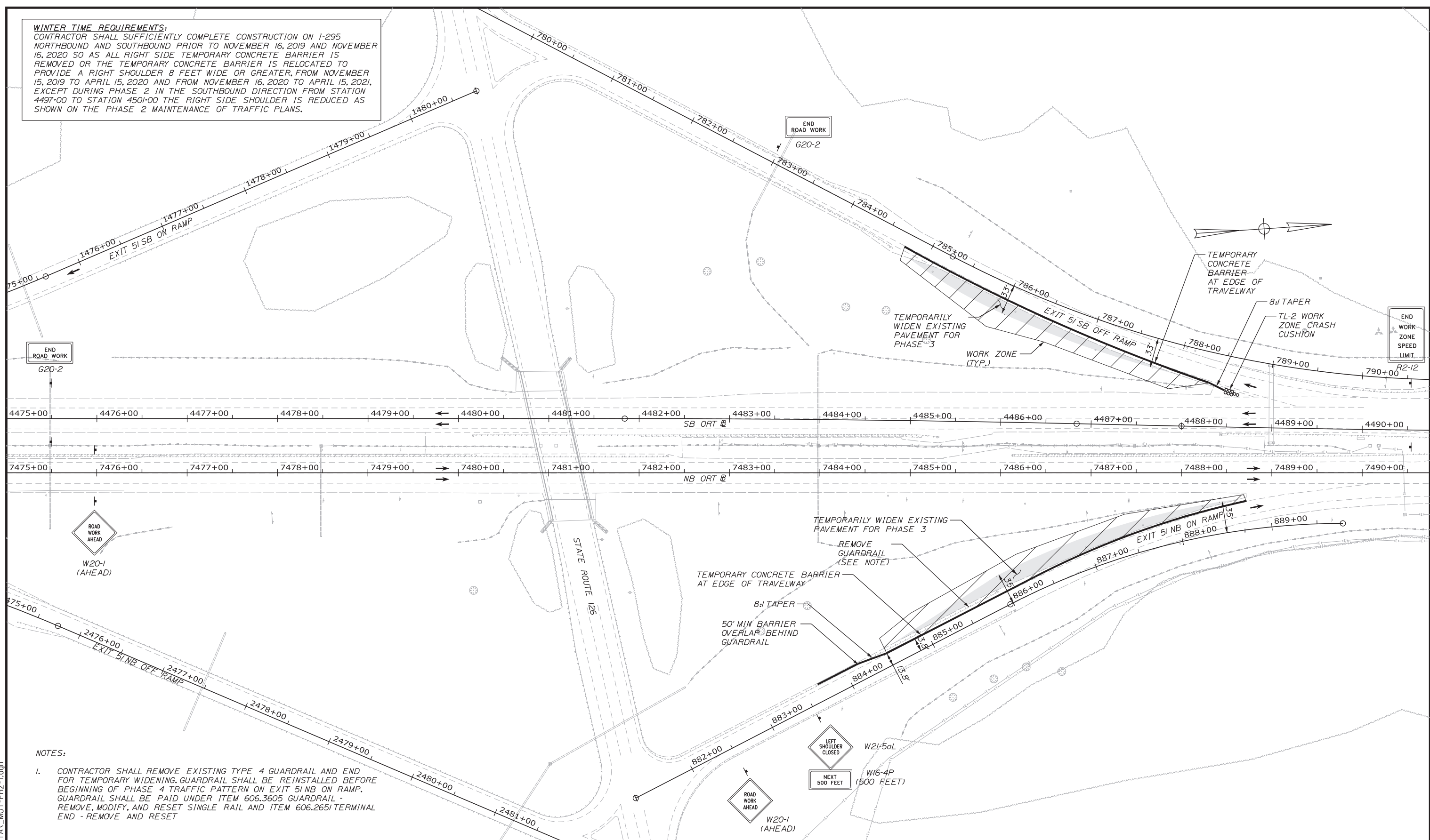
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 1
 MAINTENANCE OF TRAFFIC SECTION 11

SHEET NUMBER: MOTXS-11
 CONTRACT: 2019.04
 29 OF 503

WINTER TIME REQUIREMENTS:

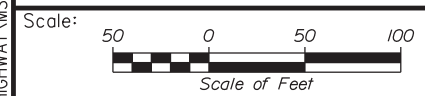
CONTRACTOR SHALL SUFFICIENTLY COMPLETE CONSTRUCTION ON I-295 NORTHBOUND AND SOUTHBOUND PRIOR TO NOVEMBER 16, 2019 AND NOVEMBER 16, 2020 SO AS ALL RIGHT SIDE TEMPORARY CONCRETE BARRIER IS REMOVED OR THE TEMPORARY CONCRETE BARRIER IS RELOCATED TO PROVIDE A RIGHT SHOULDER 8 FEET WIDE OR GREATER, FROM NOVEMBER 15, 2019 TO APRIL 15, 2020 AND FROM NOVEMBER 16, 2020 TO APRIL 15, 2021, EXCEPT DURING PHASE 2 IN THE SOUTHBOUND DIRECTION FROM STATION 4497+00 TO STATION 4501+00 THE RIGHT SIDE SHOULDER IS REDUCED AS SHOWN ON THE PHASE 2 MAINTENANCE OF TRAFFIC PLANS.



NOTES:

- CONTRACTOR SHALL REMOVE EXISTING TYPE 4 GUARDRAIL AND END FOR TEMPORARY WIDENING. GUARDRAIL SHALL BE REINSTALLED BEFORE BEGINNING OF PHASE 4 TRAFFIC PATTERN ON EXIT 51 NB ON RAMP. GUARDRAIL SHALL BE PAID UNDER ITEM 606.3605 GUARDRAIL - REMOVE, MODIFY, AND RESET SINGLE RAIL AND ITEM 606.2651 TERMINAL END - REMOVE AND RESET

Filename: ...00\HIGHWAY\MSTA\MOT-PH2-1.dgn



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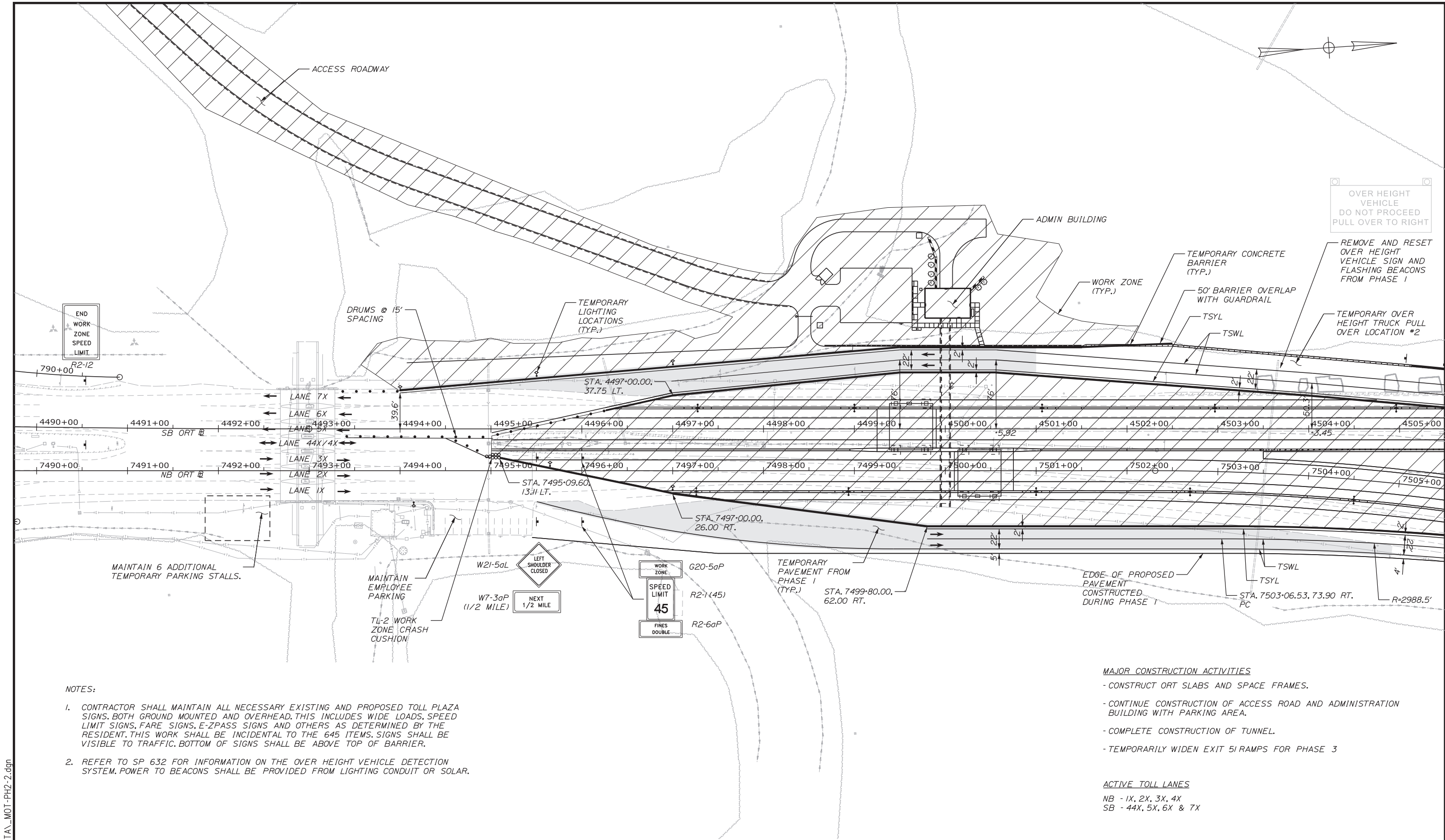


THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 2
 MAINTENANCE OF TRAFFIC PLAN 1

SHEET NUMBER: MOT2-01
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 30 OF 503



NOTES:

- CONTRACTOR SHALL MAINTAIN ALL NECESSARY EXISTING AND PROPOSED TOLL PLAZA SIGNS, BOTH GROUND MOUNTED AND OVERHEAD. THIS INCLUDES WIDE LOADS, SPEED LIMIT SIGNS, FARE SIGNS, E-ZPASS SIGNS AND OTHERS AS DETERMINED BY THE RESIDENT. THIS WORK SHALL BE INCIDENTAL TO THE 645 ITEMS. SIGNS SHALL BE VISIBLE TO TRAFFIC. BOTTOM OF SIGNS SHALL BE ABOVE TOP OF BARRIER.
- REFER TO SP 632 FOR INFORMATION ON THE OVER HEIGHT VEHICLE DETECTION SYSTEM. POWER TO BEACONS SHALL BE PROVIDED FROM LIGHTING CONDUIT OR SOLAR.

- MAJOR CONSTRUCTION ACTIVITIES**
- CONSTRUCT ORT SLABS AND SPACE FRAMES.
 - CONTINUE CONSTRUCTION OF ACCESS ROAD AND ADMINISTRATION BUILDING WITH PARKING AREA.
 - COMPLETE CONSTRUCTION OF TUNNEL.
 - TEMPORARILY WIDEN EXIT 5I RAMP FOR PHASE 3

ACTIVE TOLL LANES
 NB - 1X, 2X, 3X, 4X
 SB - 44X, 5X, 6X & 7X

Scale: 50 0 50 100
Scale of Feet

No.	Revision	By	Date

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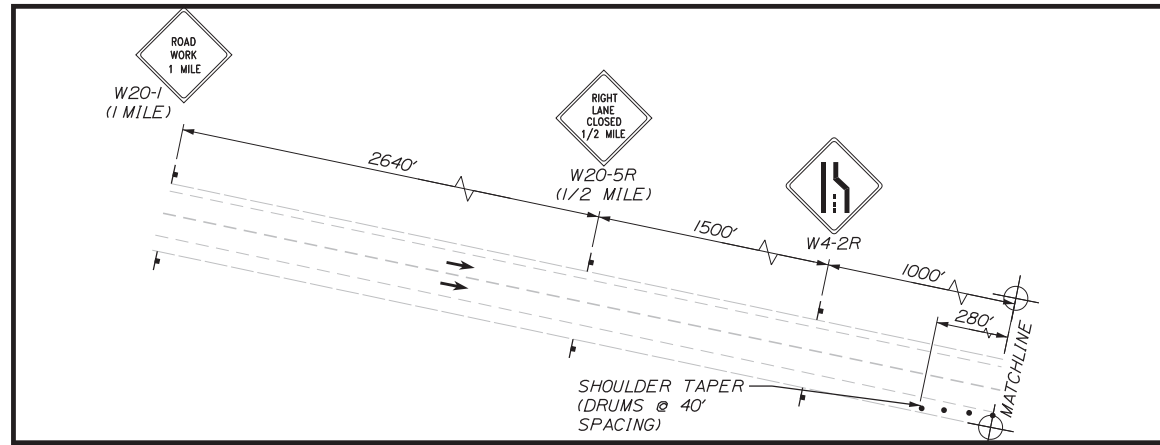
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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

**INTERCHANGE 103
 ORT CONVERSION
 PHASE 2
 MAINTENANCE OF TRAFFIC PLAN 2**

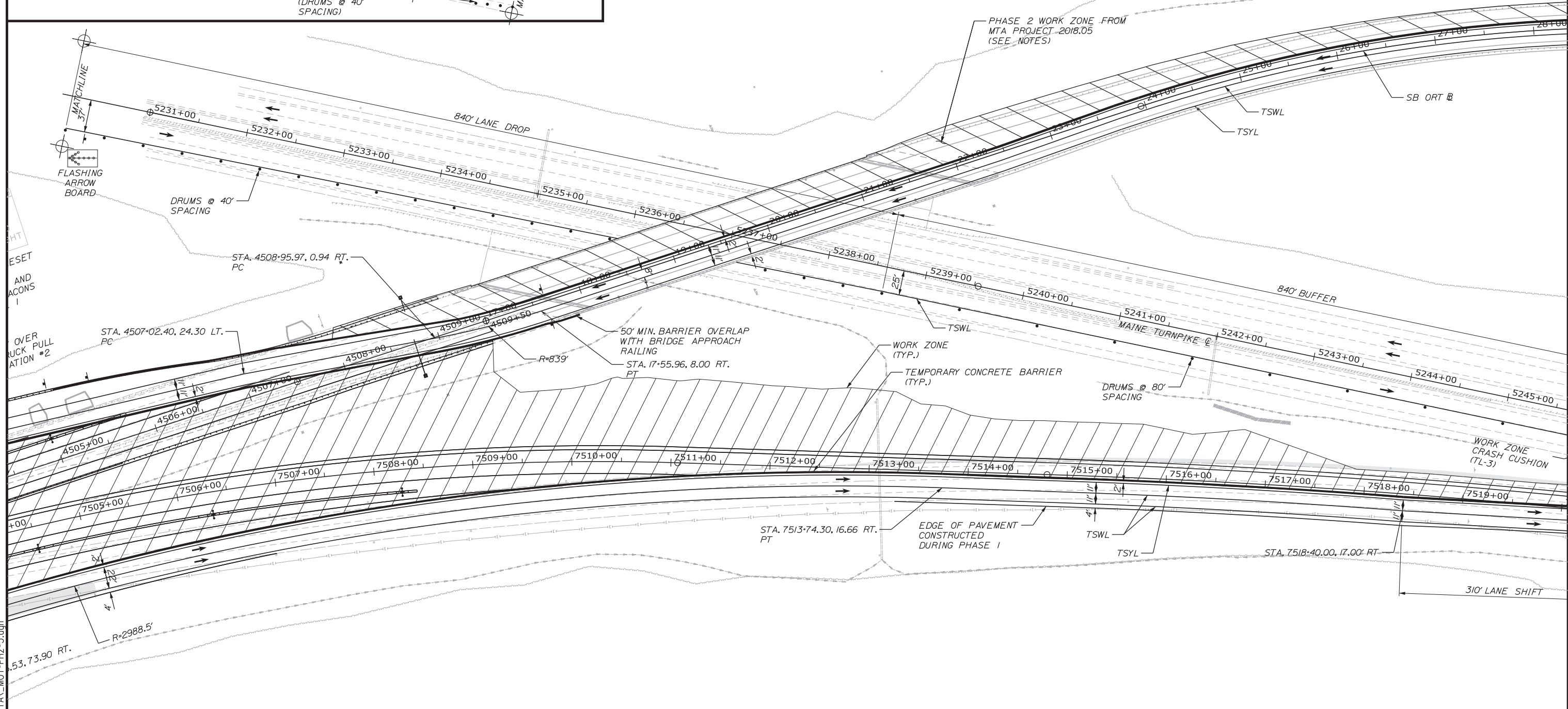
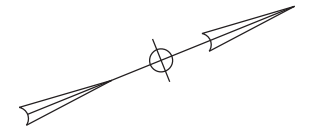
SHEET NUMBER: MOT2-01
 CONTRACT: 2019.04
 31 OF 503

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NOTES:

1. THE MTA PROJECT 2018.05 CONTRACT IS SCHEDULED TO BE SUBSTANTIALLY COMPLETE BY NOVEMBER 18, 2019. THIS INCLUDES BEING ABLE TO BE FULLY OPENED TO TRAFFIC INCLUDING SHOULDERS, GUARDRAIL, SURFACE PAVEMENT AND SIGNAGE. PRIOR TO SUBSTANTIAL COMPLETION THE 2018.05 CONTRACTOR WILL BE REMOVING THEIR MAINTENANCE OF TRAFFIC PACKAGE AND REMOVING THE TEMPORARY TRAFFIC BARRIER FROM STATION 4508+00 TO STATION 29+50. THE 2019.04 CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS TO THE MAINTENANCE OF TRAFFIC DEVICES AND TEMPORARY BARRIER TO MEET THE CONTRACT REQUIREMENTS INCLUDING WINTER TIME REQUIREMENTS. SEE THE PHASE 3 MAINTENANCE OF TRAFFIC PLANS FOR THE BRIDGE LANE CONFIGURATION ONCE THE 2018.05 CONTRACTOR IS SUBSTANTIALLY COMPLETE.
2. CONTRACTOR SHALL COORDINATE WITH BRIDGE CONTRACTOR FOR MTA PROJECT 2018.05. TRAFFIC IS ASSUMED TO BE IN PHASE 1 OF BRIDGE CONSTRUCTION AT THE BEGINNING OF PHASE 1. CONTRACTOR SHALL COORDINATE WITH BRIDGE CONTRACTOR WHEN PROJECT 2018.05 TRANSITIONS TO PHASE 2.



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Scale: 1" = 50'

No.	Revision	By	Date

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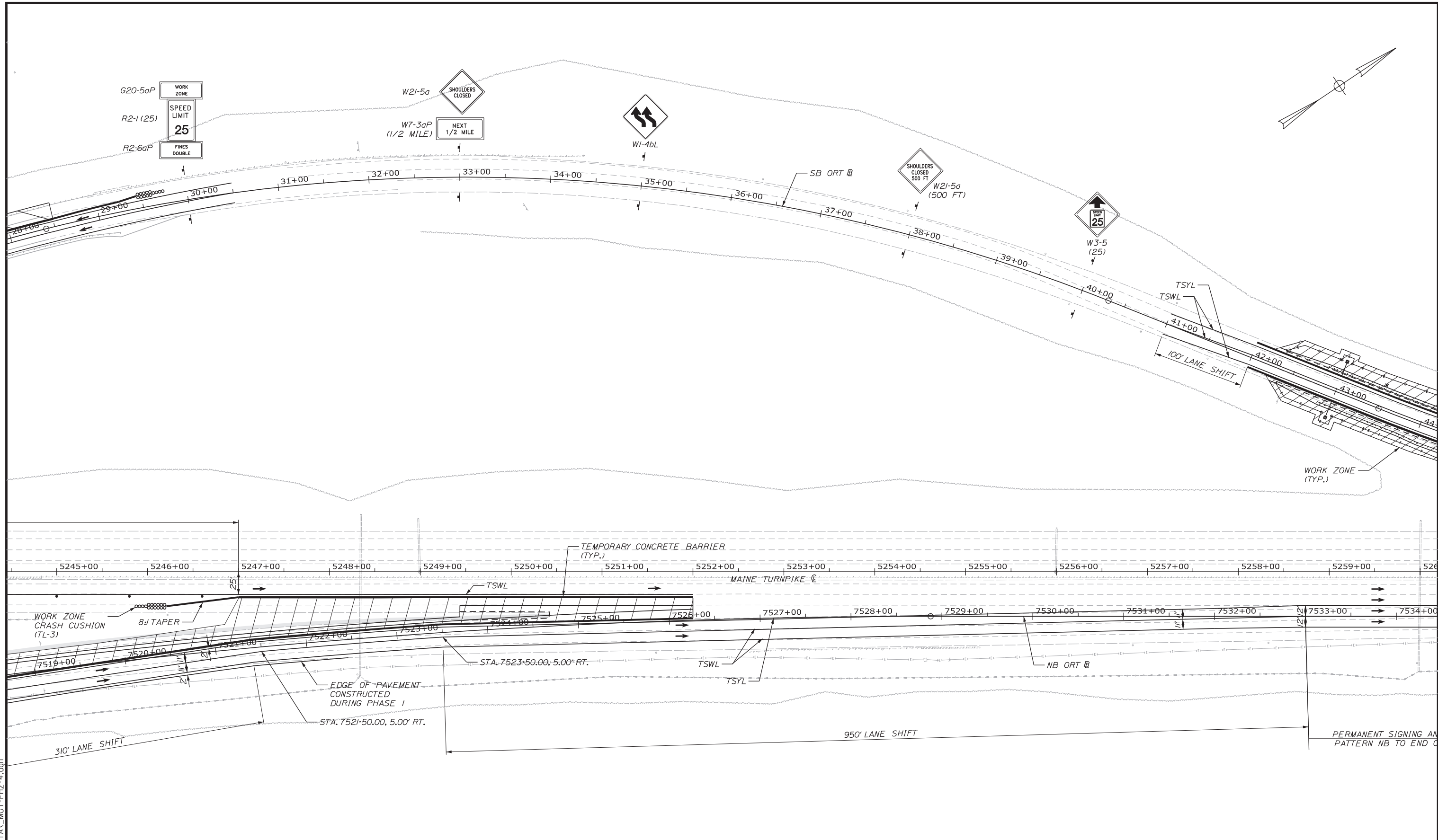
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

**INTERCHANGE 103
 ORT CONVERSION
 PHASE 2
 MAINTENANCE OF TRAFFIC PLAN 3**

SHEET NUMBER: MOT2-02
 CONTRACT: 2019.04
 32 OF 503

Filename: ...:\00\HIGHWAY\MSTA_MOT-PH2-4.dgn



Scale: 50 0 50 100
Scale of Feet

No.	Revision	By	Date

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Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

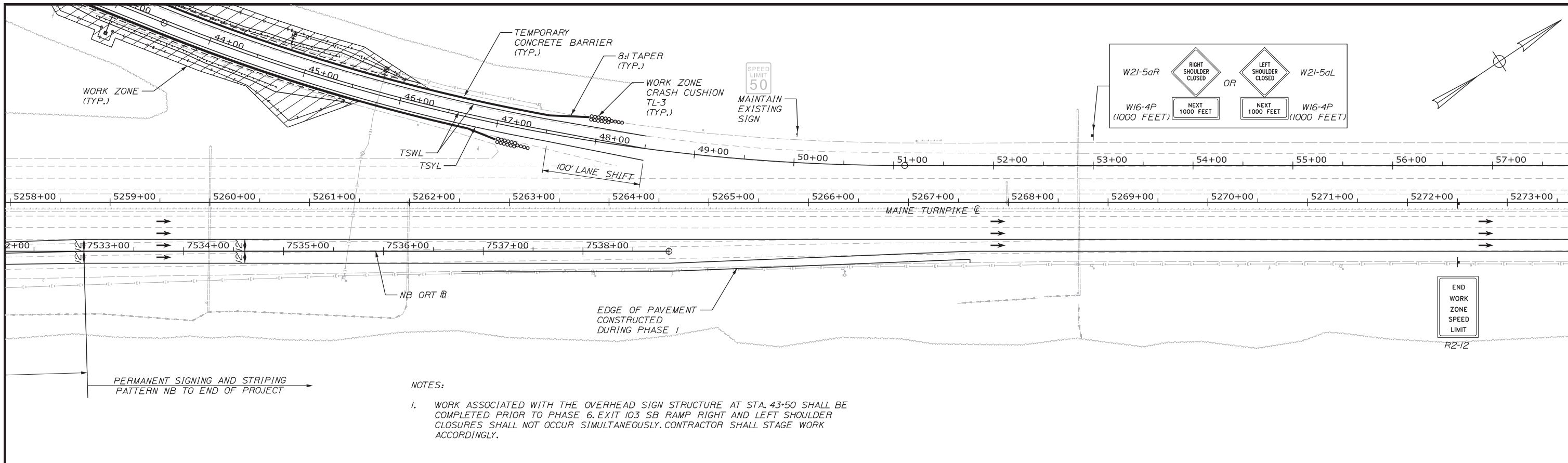
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INTERCHANGE 103
ORT CONVERSION
PHASE 2
MAINTENANCE OF TRAFFIC PLAN 4

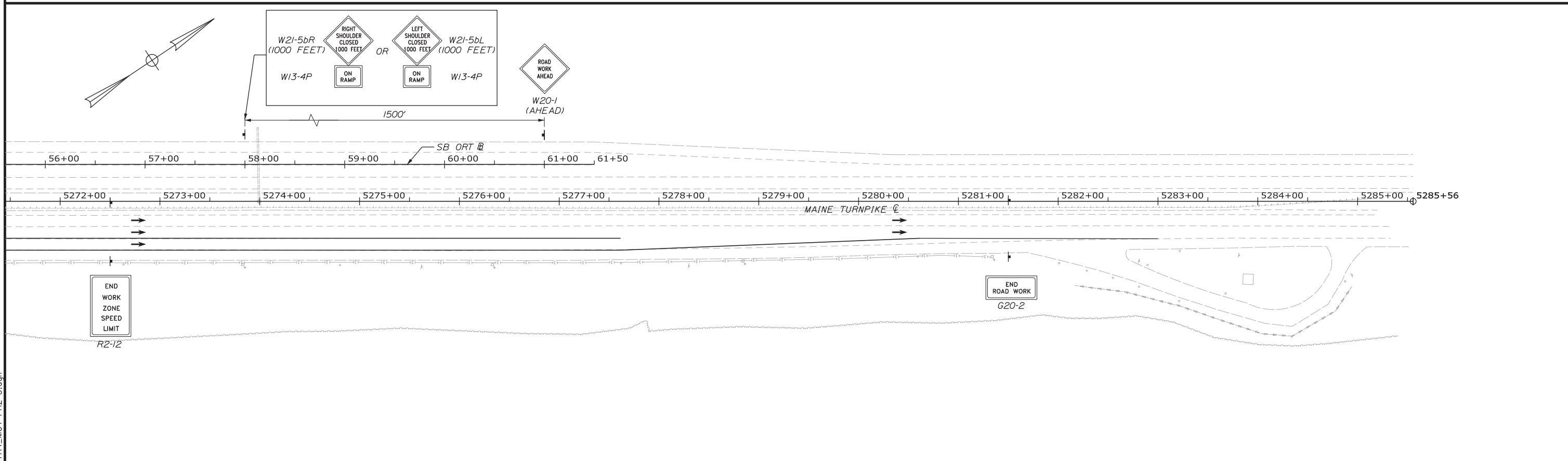
SHEET NUMBER: MOT2-03
CONTRACT: 2019.04
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PERMANENT SIGNING AND STRIPING PATTERN NB TO END OF PROJECT

NOTES:

1. WORK ASSOCIATED WITH THE OVERHEAD SIGN STRUCTURE AT STA. 43+50 SHALL BE COMPLETED PRIOR TO PHASE 6. EXIT 103 SB RAMP RIGHT AND LEFT SHOULDER CLOSURES SHALL NOT OCCUR SIMULTANEOUSLY. CONTRACTOR SHALL STAGE WORK ACCORDINGLY.



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Scale: 50 0 50 100
Scale of Feet

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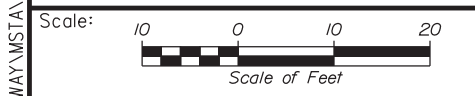
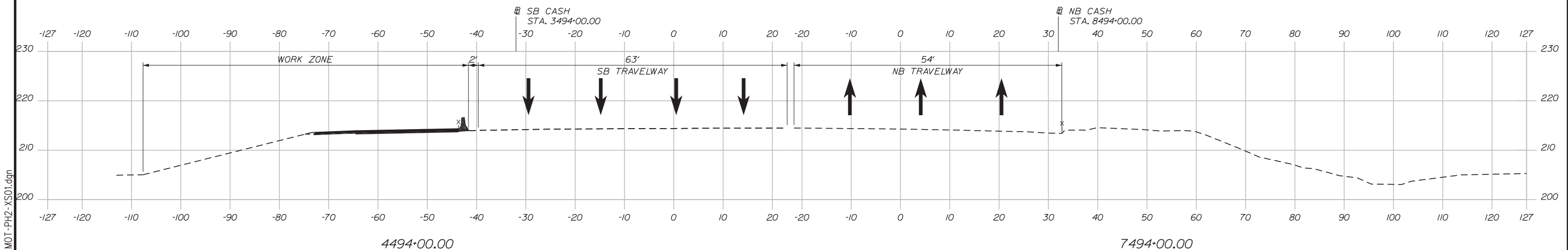
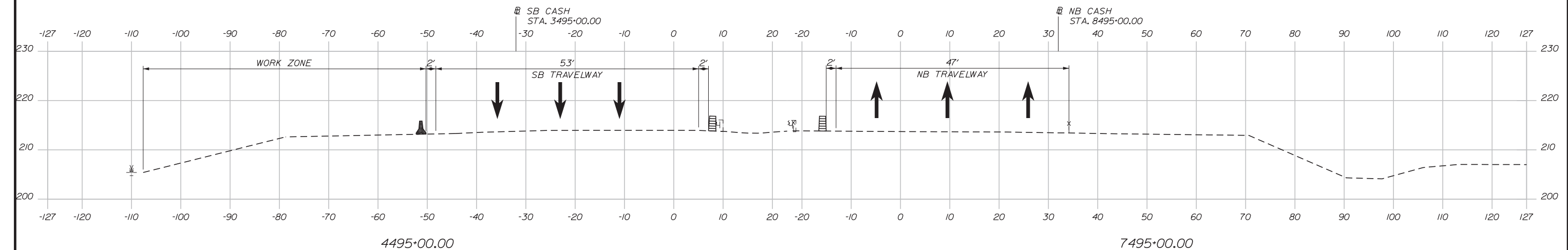
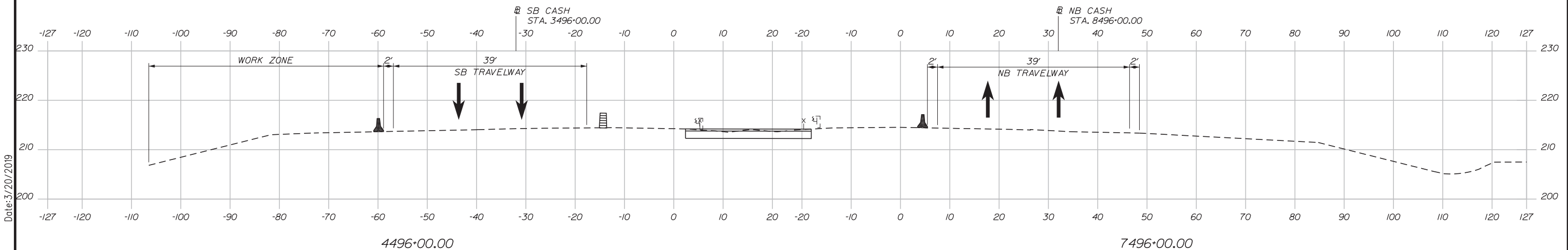
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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 2
MAINTENANCE OF TRAFFIC PLAN 5

SHEET NUMBER: MOT2-04
CONTRACT: 2019.04
34 OF 503

Date: 3/20/2019



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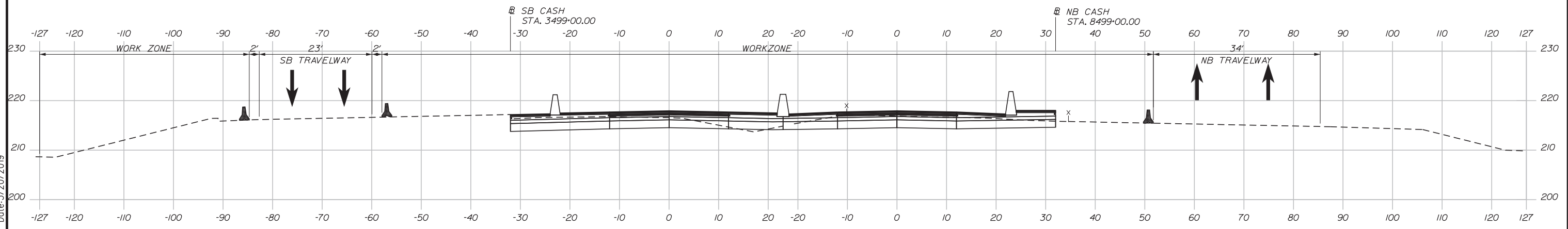
INTERCHANGE 103
ORT CONVERSION
PHASE 2
MAINTENANCE OF TRAFFIC SECTION 1

SHEET NUMBER: MOTXS-12
CONTRACT: 2019.04
35 OF 503

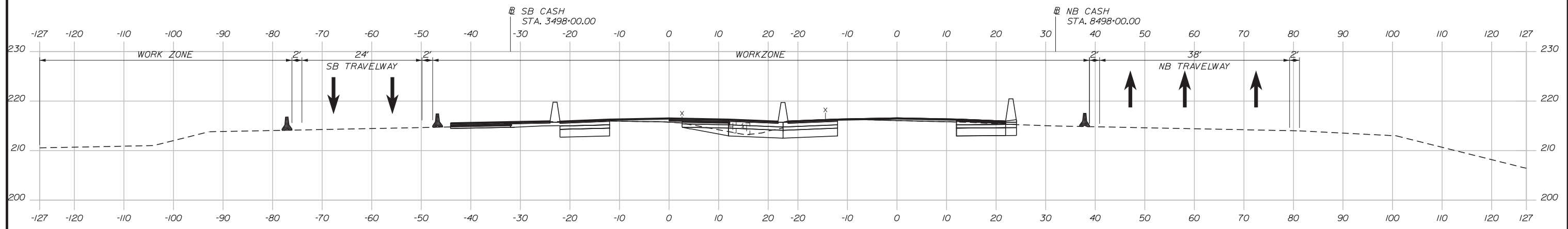
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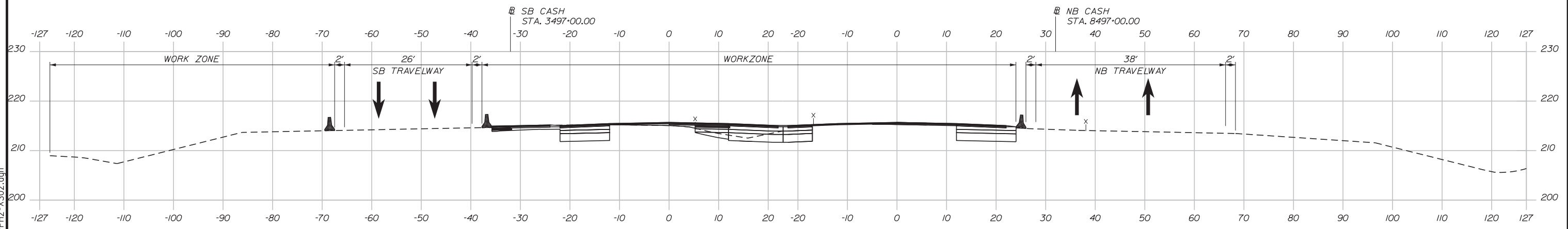
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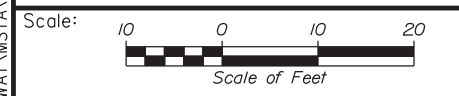
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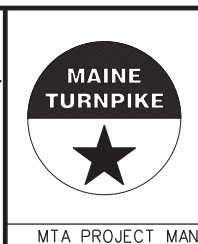
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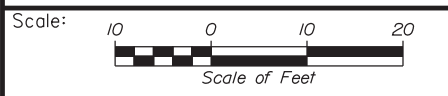
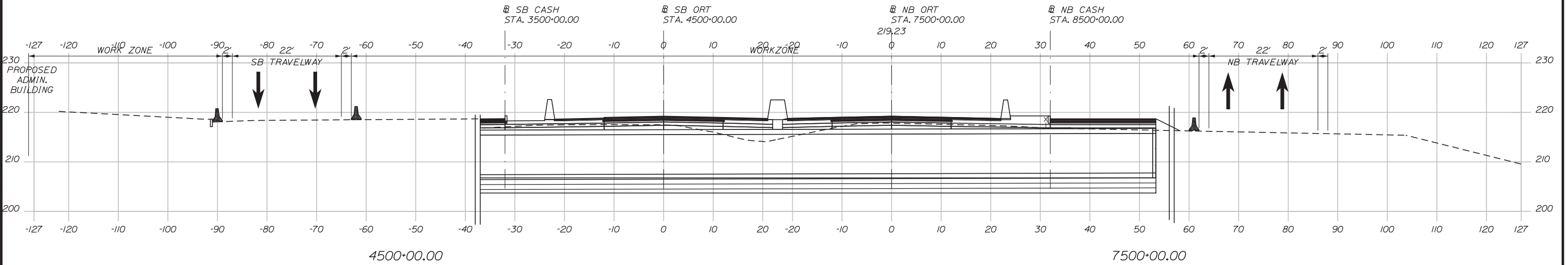
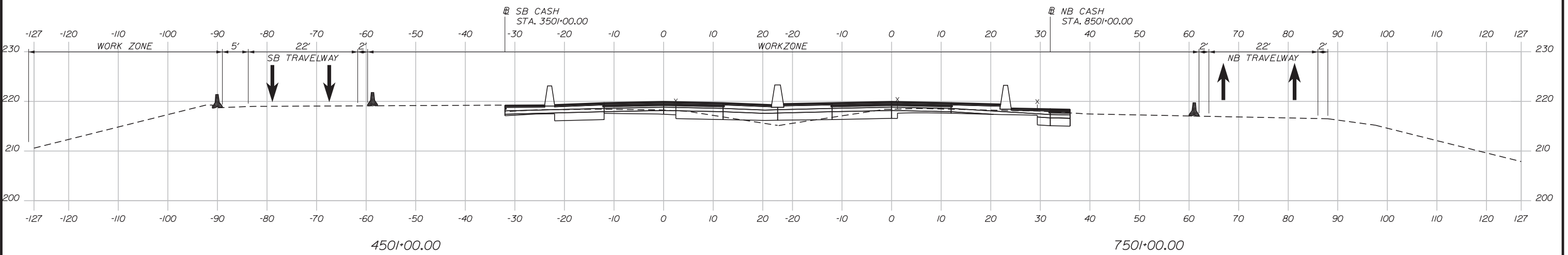
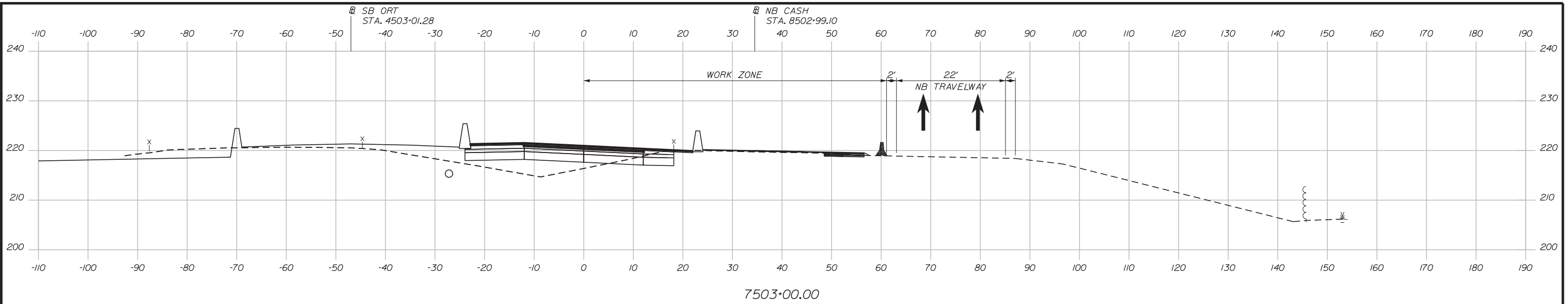


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INTERCHANGE 103
ORT CONVERSION
PHASE 2
MAINTENANCE OF TRAFFIC SECTION 2

SHEET NUMBER: MOTXS-13
CONTRACT: 2019.04
36 OF 503

Date: 3/20/2019



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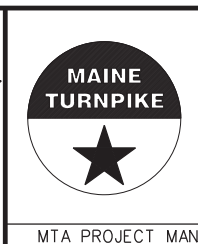
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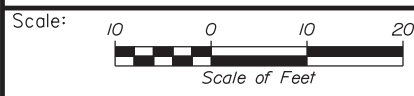
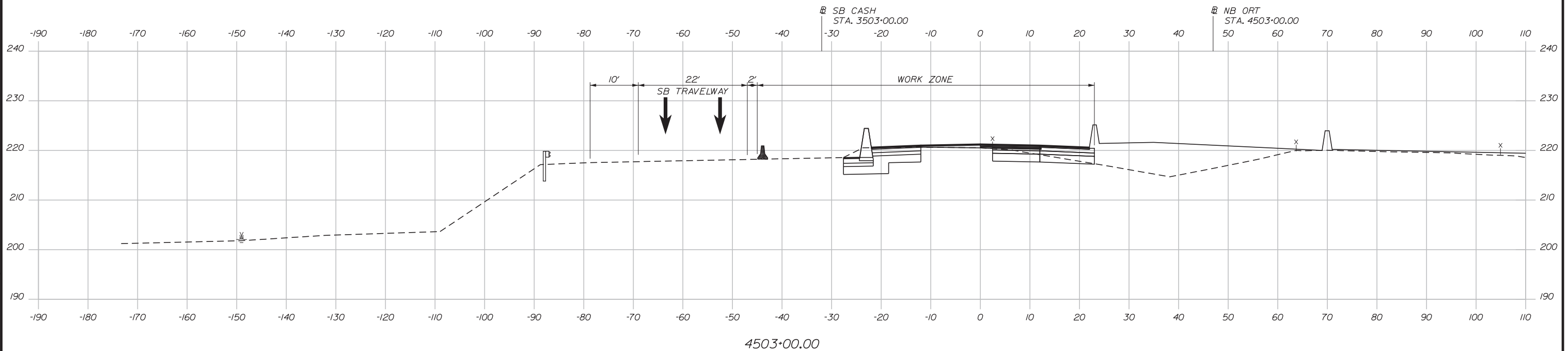
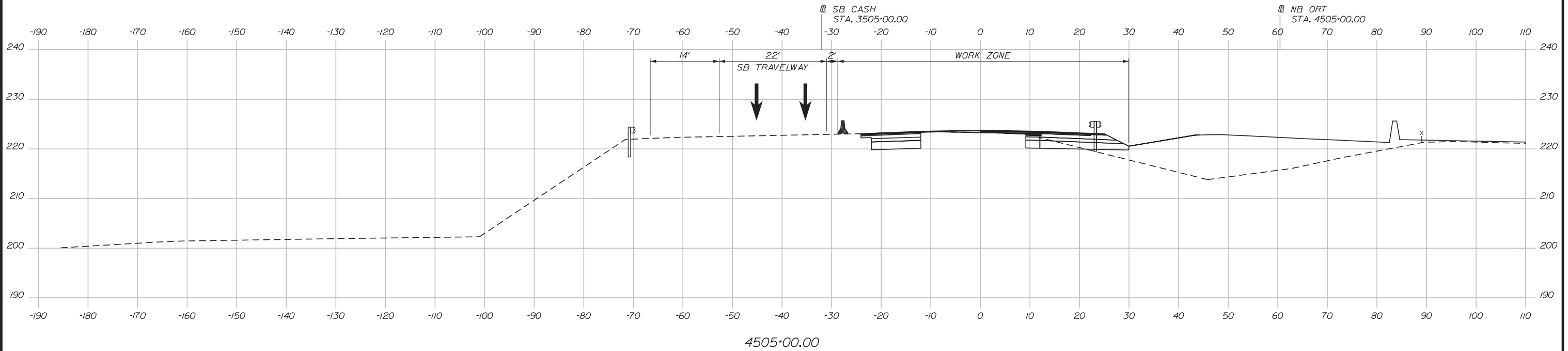
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 MEMORIAL HIGHWAY**

INTERCHANGE 103
 ORT CONVERSION
 PHASE 2
 MAINTENANCE OF TRAFFIC SECTION 3

SHEET NUMBER: MOTXS-14
 CONTRACT: 2019.04
 37 OF 503

Filename: ...HIGHWAY\MSTA\MOT-PH2-XS03.dgn

Date: 3/20/2019



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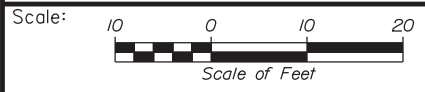
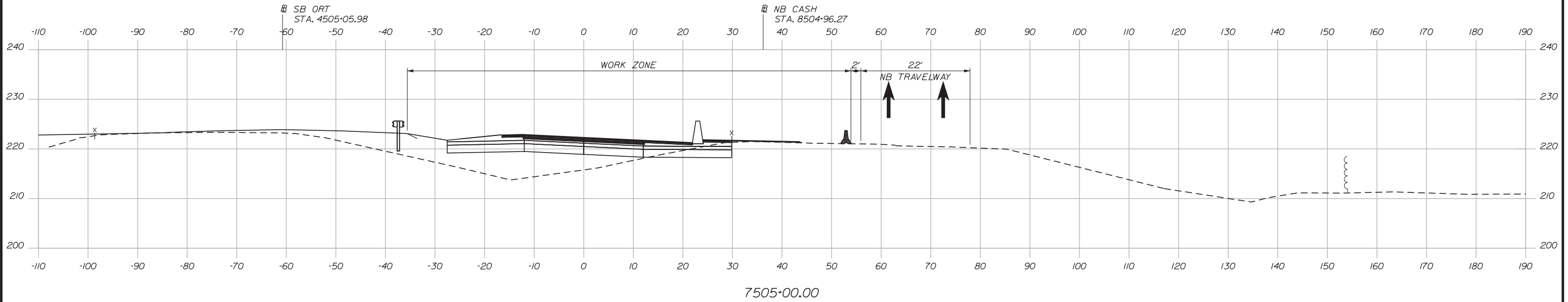
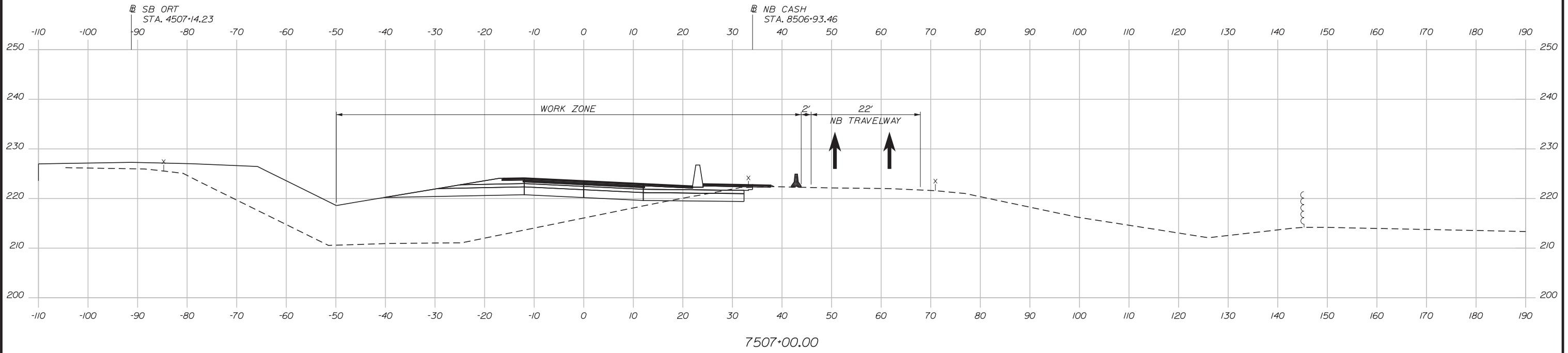
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 2
 MAINTENANCE OF TRAFFIC SECTION 4

SHEET NUMBER: MOTXS-15
 CONTRACT: 2019.04
 38 OF 503

Filename: ... \HIGHWAY\MSTA\MOT-PH2-XS04.dgn

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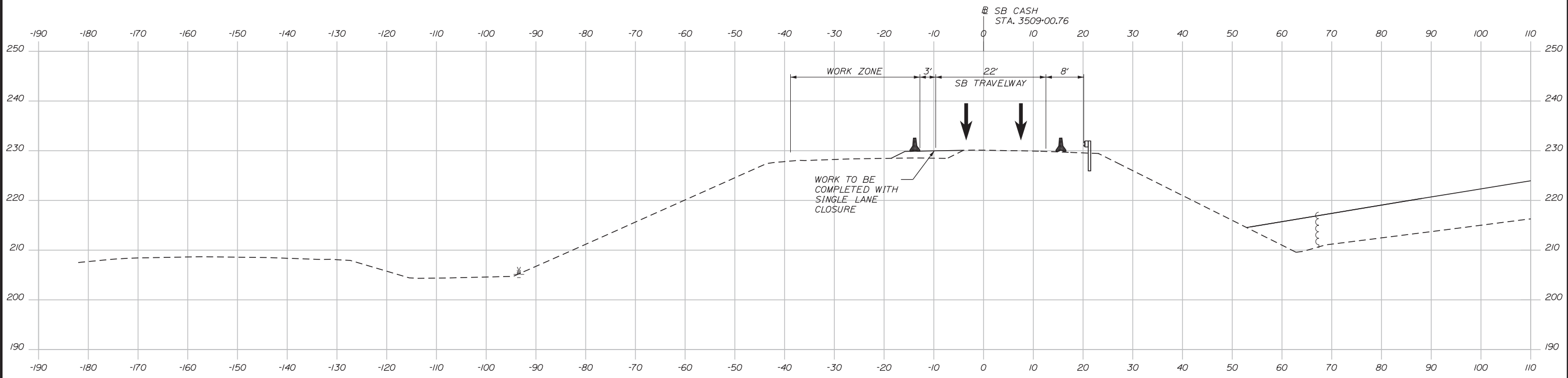
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INTERCHANGE 103
 ORT CONVERSION
 PHASE 2
 MAINTENANCE OF TRAFFIC SECTION 5

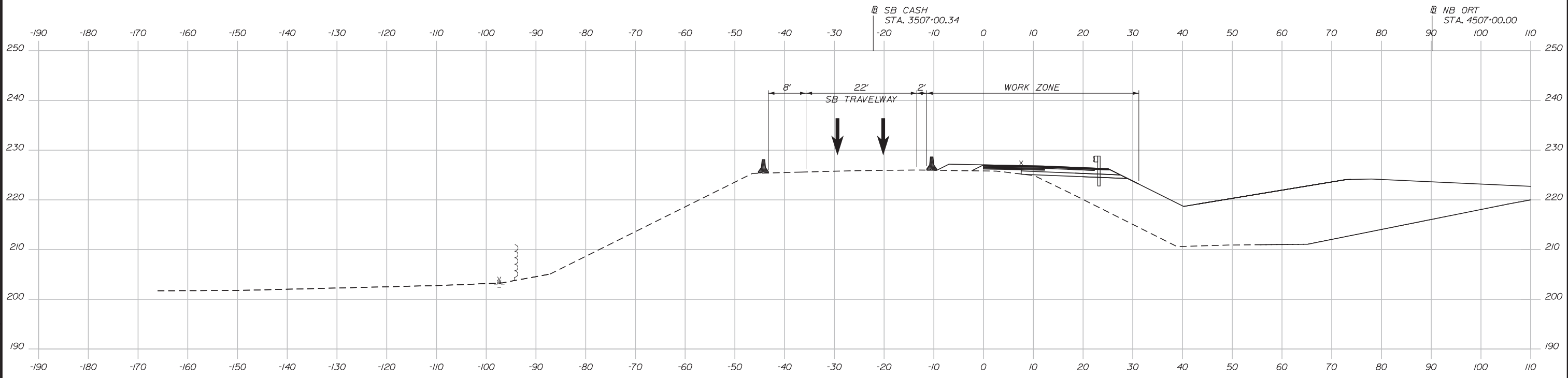
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 CONTRACT: 2019.04
 39 OF 503

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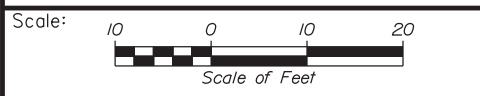
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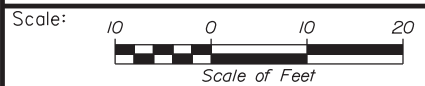
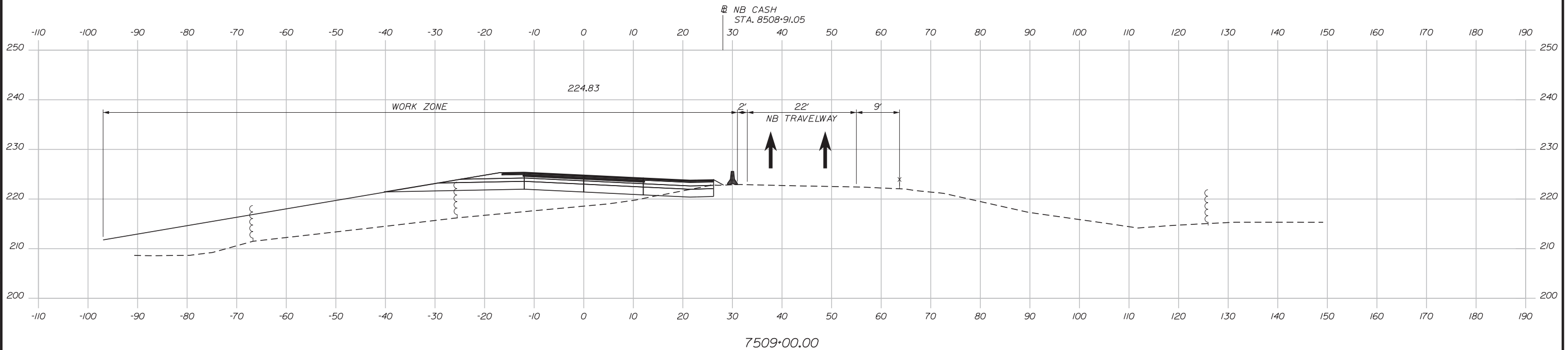
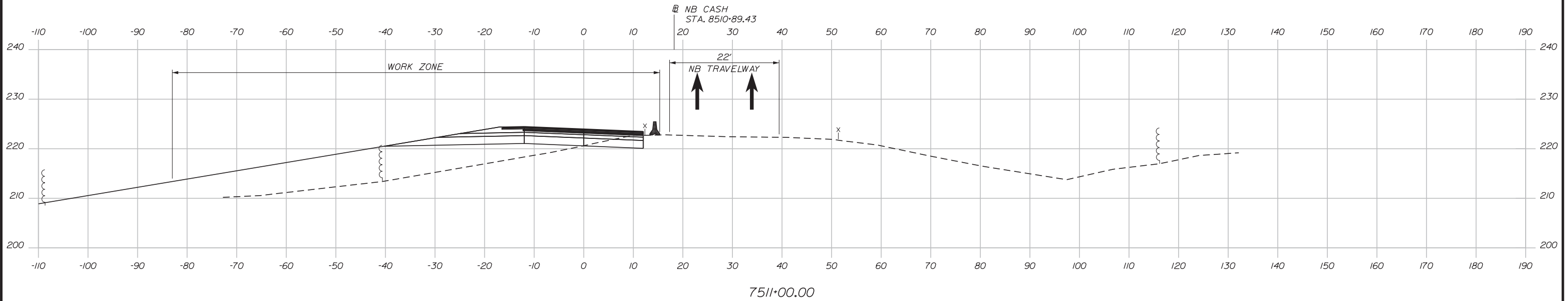
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 2
MAINTENANCE OF TRAFFIC SECTION 6

SHEET NUMBER: MOTXS-17
CONTRACT: 2019.04
40 OF 503

Filename: ...HIGHWAY\MSTA\MOT-PH2-XS06.dgn

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INTERCHANGE 103
 ORT CONVERSION
 PHASE 2
 MAINTENANCE OF TRAFFIC SECTION 7

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
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Drawn	By	Date	In Charge of
	EJB	3\20\19	GAE

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

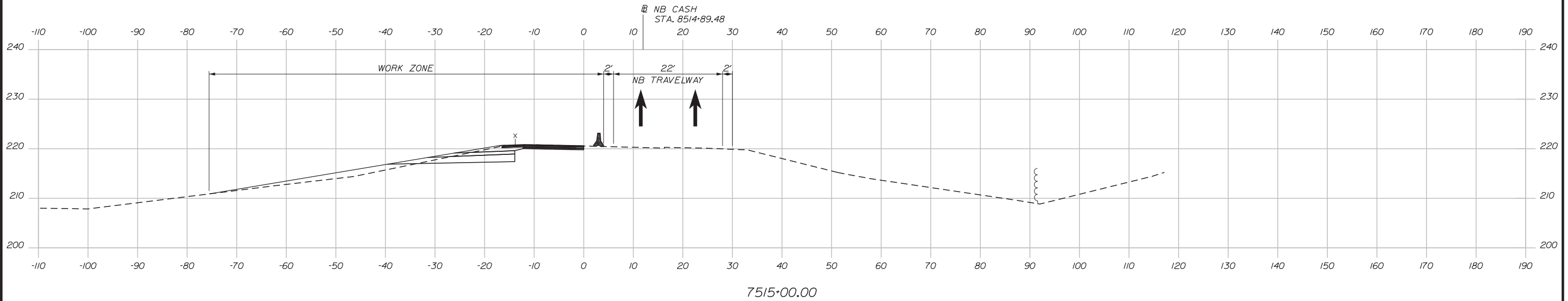
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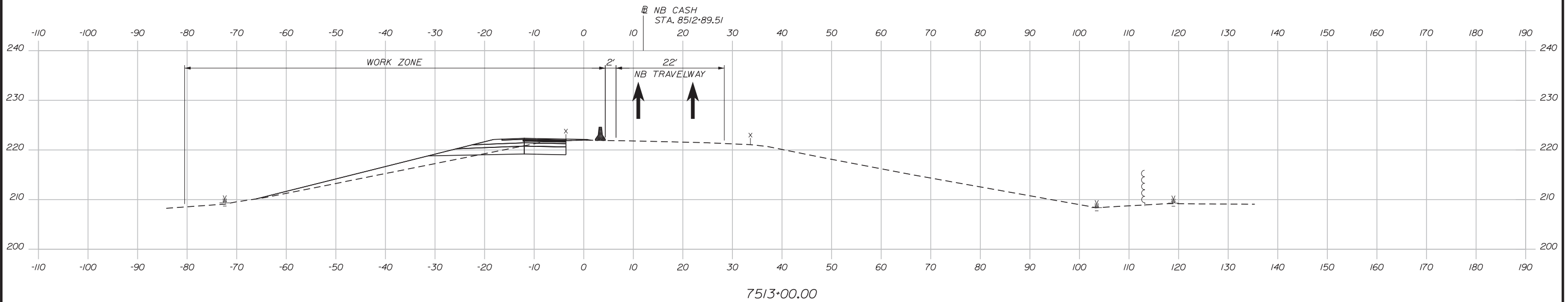
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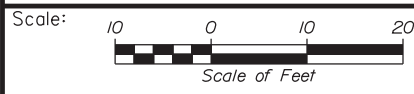
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THE GOLD STAR
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INTERCHANGE 103
ORT CONVERSION
PHASE 2
MAINTENANCE OF TRAFFIC SECTION 8

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
	By	Date	
Designed	PLP	3\20\19	Checked LEM 3\20\19
Drawn	EJB	3\20\19	In Charge of GAE 3\20\19

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

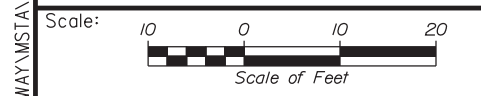
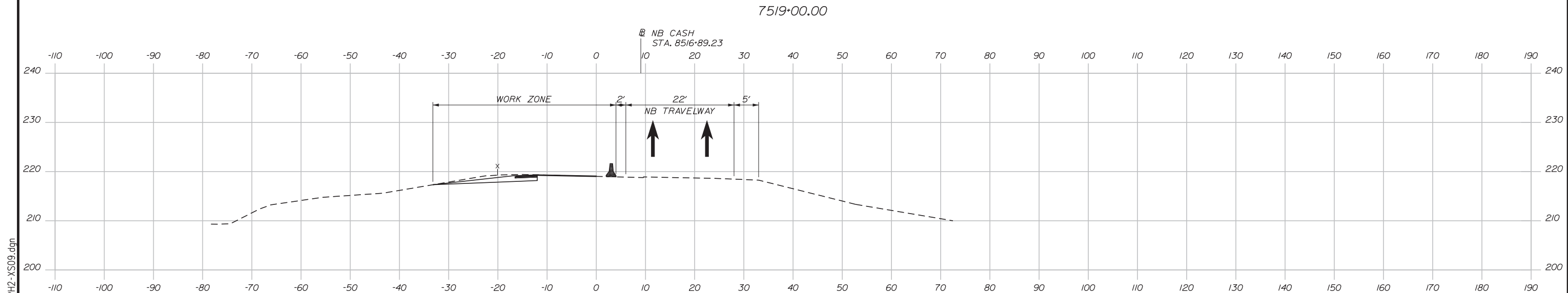
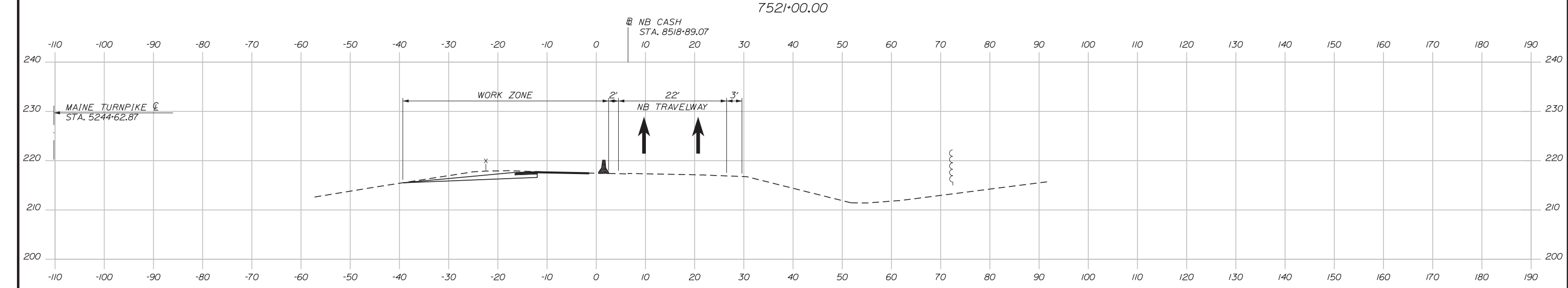
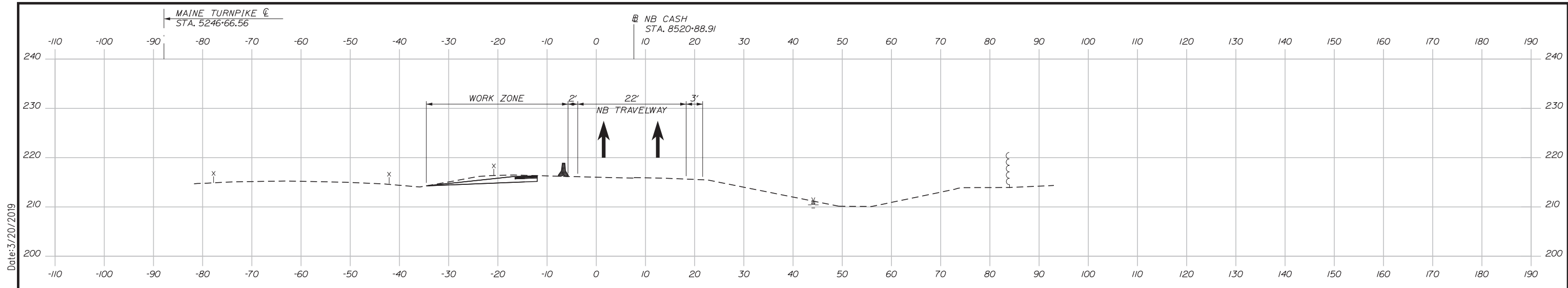
CONTRACT: 2019.04

SHEET NUMBER: MOTXS-19

42 OF 503

Filename: ... \HIGHWAY\MSTA\MOT-PH2-XS08.dgn

Date: 3/20/2019



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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

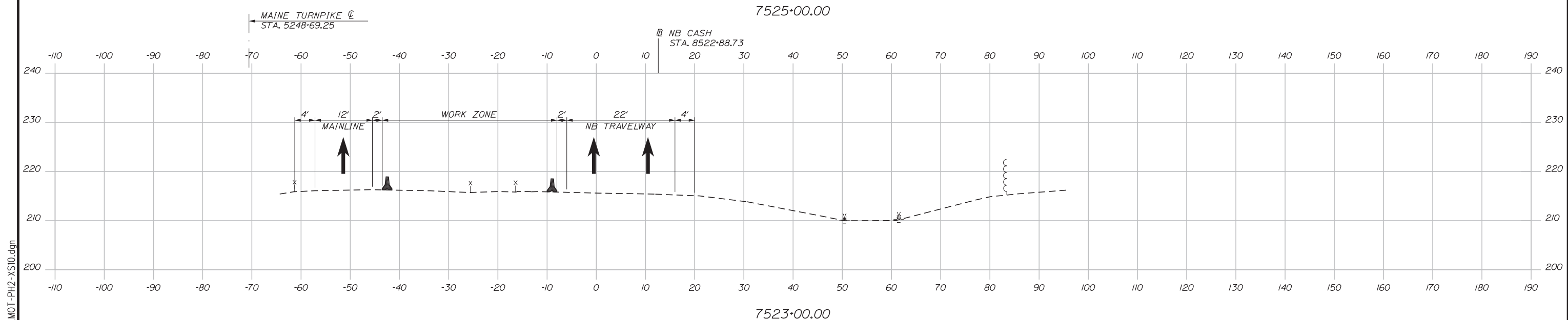
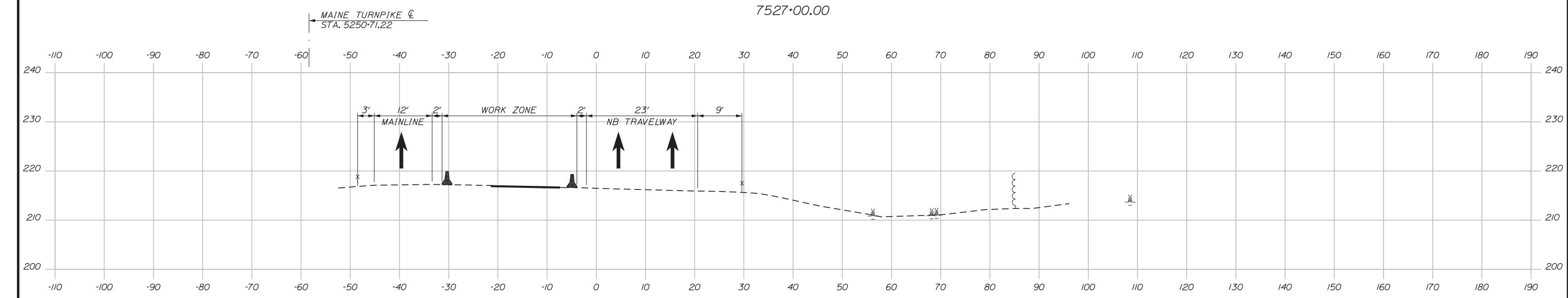
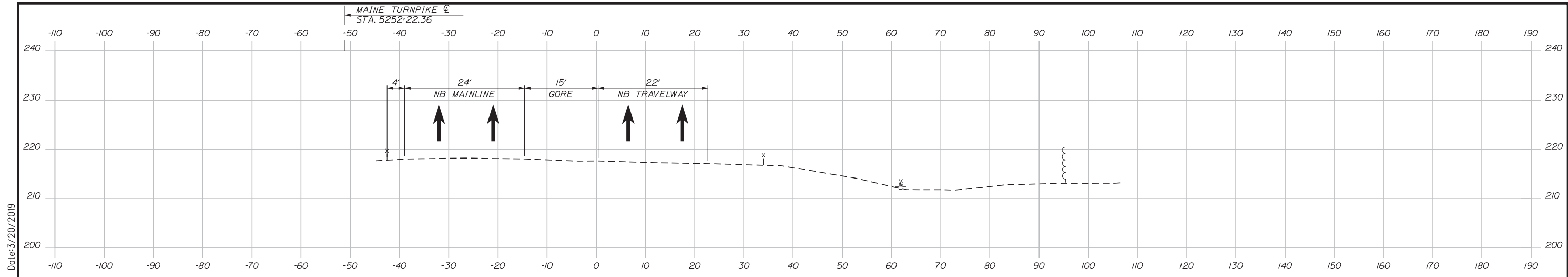
INTERCHANGE 103
 ORT CONVERSION
 PHASE 2
 MAINTENANCE OF TRAFFIC SECTION 9

SHEET NUMBER: MOTXS-20
 43 OF 503

CONTRACT: 2019.04

Filename: ...HIGHWAY\MSTA\MOT-PH2-XS09.dgn

Date: 3/20/2019



Scale: 10 0 10 20
Scale of Feet

No.	Revision	By	Date

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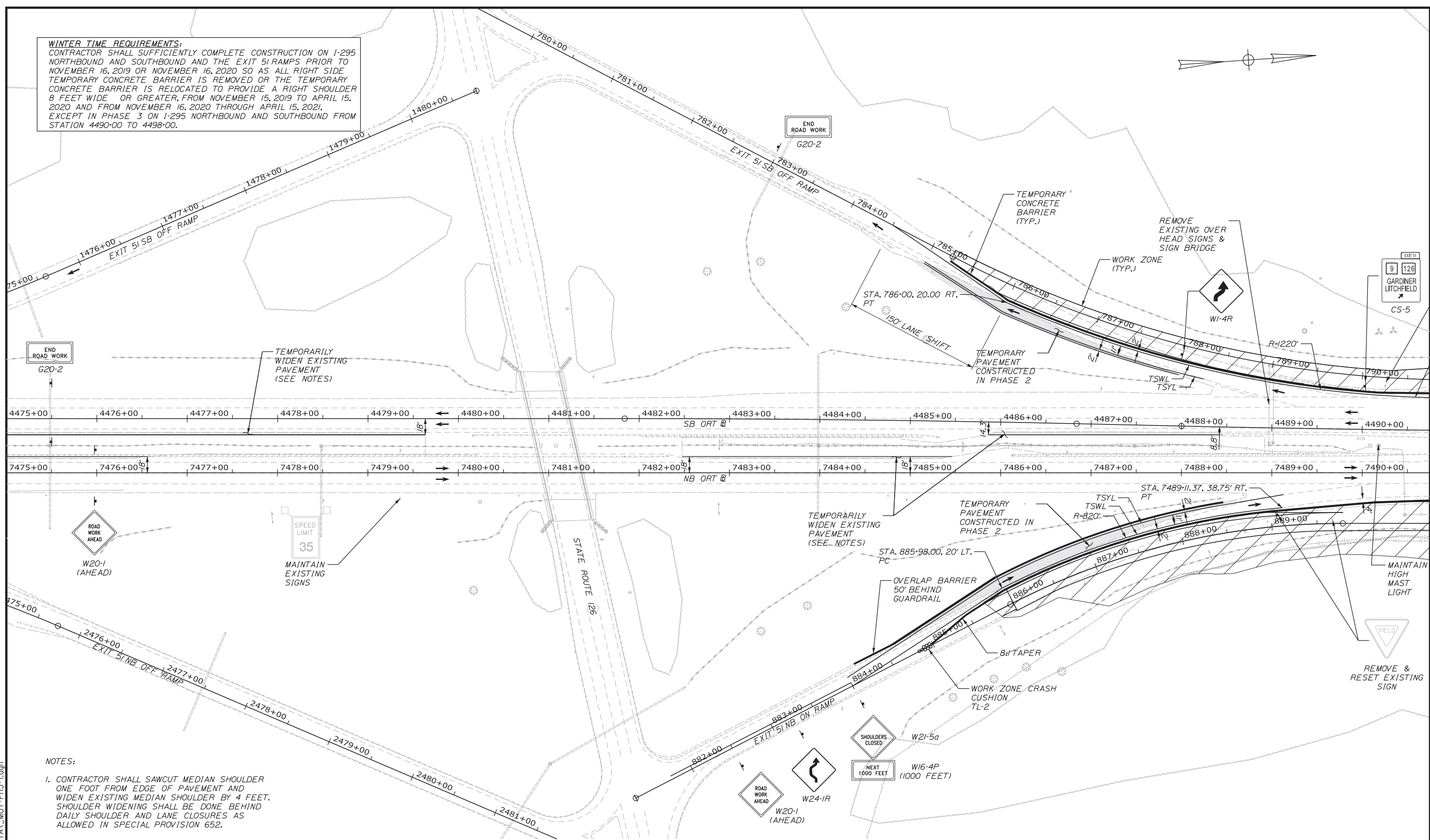
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 2
MAINTENANCE OF TRAFFIC SECTION 10

SHEET NUMBER: MOTXS-21
CONTRACT: 2019.04
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Filename: ... \HIGHWAY\MSTA\...MOT-PH2-XS10.dgn

WINTER TIME REQUIREMENTS:
 CONTRACTOR SHALL SUFFICIENTLY COMPLETE CONSTRUCTION ON I-295 NORTHBOUND AND SOUTHBOUND AND THE EXIT 5I RAMP PRIOR TO NOVEMBER 16, 2019 OR NOVEMBER 16, 2020 SO AS ALL RIGHT SIDE TEMPORARY CONCRETE BARRIER IS REMOVED OR THE TEMPORARY CONCRETE BARRIER IS RELOCATED TO PROVIDE A RIGHT SHOULDER 8 FEET WIDE OR GREATER, FROM NOVEMBER 15, 2019 TO APRIL 15, 2020 AND FROM NOVEMBER 16, 2020 THROUGH APRIL 15, 2021, EXCEPT IN PHASE 3 ON I-295 NORTHBOUND AND SOUTHBOUND FROM STATION 4490+00 TO 4498+00.



NOTES:
 1. CONTRACTOR SHALL SAWCUT MEDIAN SHOULDER ONE FOOT FROM EDGE OF PAVEMENT AND WIDEN EXISTING MEDIAN SHOULDER BY 4 FEET. SHOULDER WIDENING SHALL BE DONE BEHIND DAILY SHOULDER AND LANE CLOSURES AS ALLOWED IN SPECIAL PROVISION 652.

Scale: 50 0 50 100
Scale of Feet

No.	Revision	By	Date

Designed by: **Stantec**

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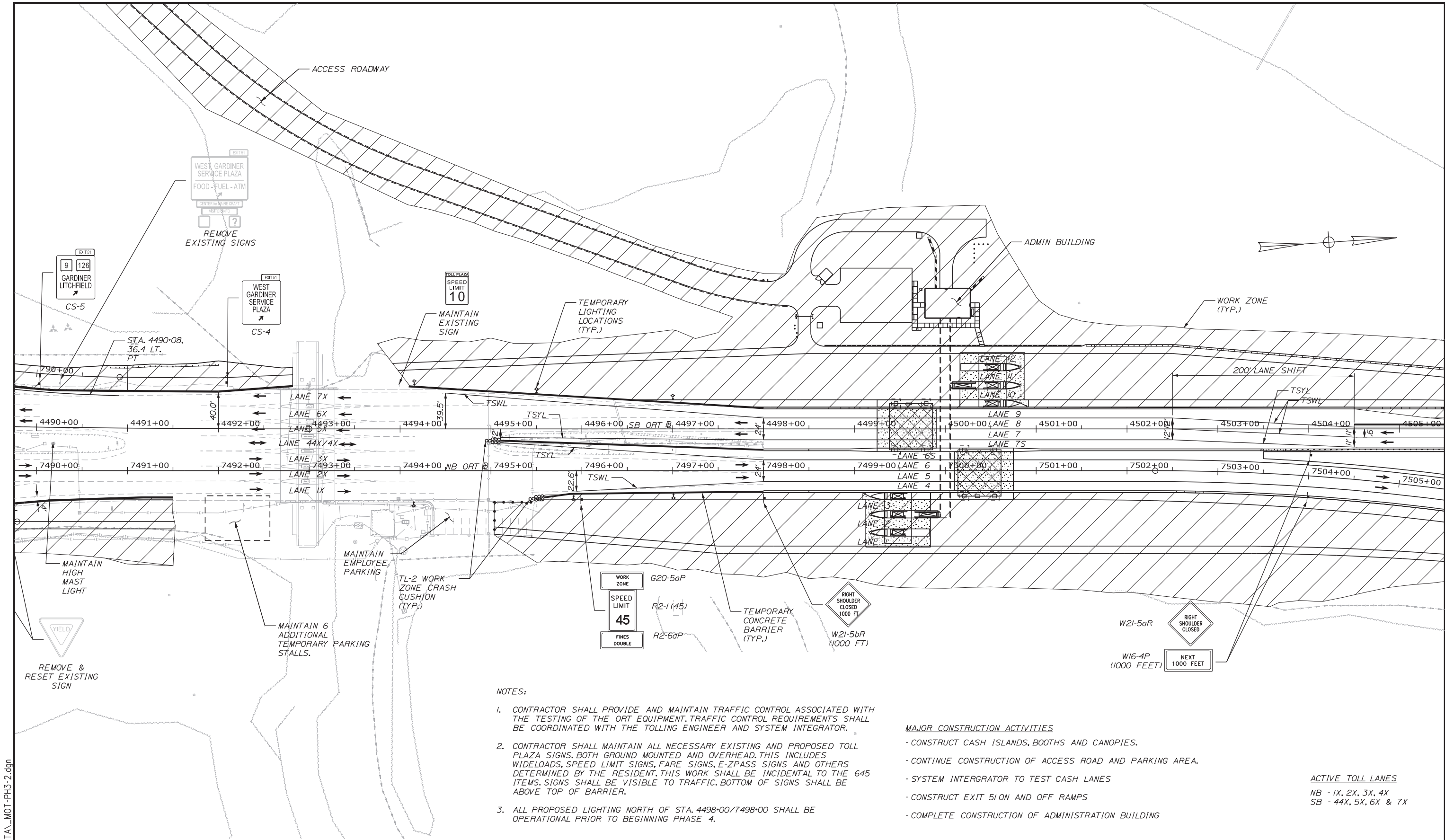
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 3
 MAINTENANCE OF TRAFFIC PLAN 1

SHEET NUMBER: MOT3-01
 CONTRACT: 2019.04
 45 OF 503

Filename: ...00\HIGHWAY\MSTA\MOT-PH3-1.dgn



NOTES:

1. CONTRACTOR SHALL PROVIDE AND MAINTAIN TRAFFIC CONTROL ASSOCIATED WITH THE TESTING OF THE ORT EQUIPMENT. TRAFFIC CONTROL REQUIREMENTS SHALL BE COORDINATED WITH THE TOLLING ENGINEER AND SYSTEM INTEGRATOR.
2. CONTRACTOR SHALL MAINTAIN ALL NECESSARY EXISTING AND PROPOSED TOLL PLAZA SIGNS, BOTH GROUND MOUNTED AND OVERHEAD. THIS INCLUDES WIDELoads, SPEED LIMIT SIGNS, FARE SIGNS, E-ZPASS SIGNS AND OTHERS DETERMINED BY THE RESIDENT. THIS WORK SHALL BE INCIDENTAL TO THE 645 ITEMS. SIGNS SHALL BE VISIBLE TO TRAFFIC. BOTTOM OF SIGNS SHALL BE ABOVE TOP OF BARRIER.
3. ALL PROPOSED LIGHTING NORTH OF STA. 4498+00/7498+00 SHALL BE OPERATIONAL PRIOR TO BEGINNING PHASE 4.

MAJOR CONSTRUCTION ACTIVITIES

- CONSTRUCT CASH ISLANDS, BOOTHS AND CANOPIES.
- CONTINUE CONSTRUCTION OF ACCESS ROAD AND PARKING AREA.
- SYSTEM INTERGRATOR TO TEST CASH LANES
- CONSTRUCT EXIT 51 ON AND OFF RAMP
- COMPLETE CONSTRUCTION OF ADMINISTRATION BUILDING

ACTIVE TOLL LANES

- NB - 1X, 2X, 3X, 4X
- SB - 44X, 5X, 6X & 7X

Scale: 50 0 50 100
Scale of Feet

No.	Revision	By	Date

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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

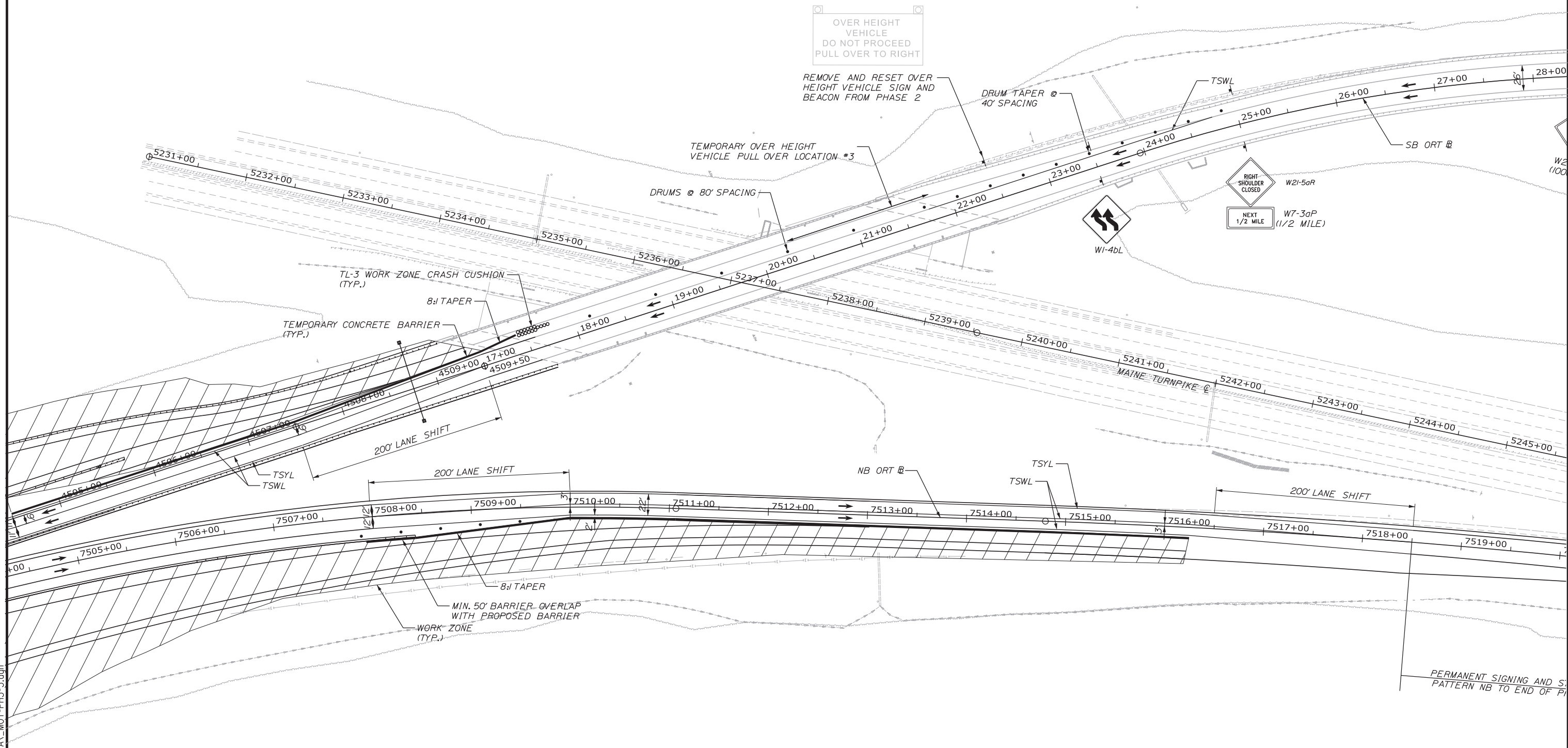
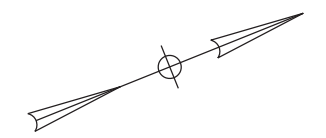
**INTERCHANGE 103
ORT CONVERSION
PHASE 3
MAINTENANCE OF TRAFFIC PLAN 2**

SHEET NUMBER: MOT3-02
CONTRACT: 2019.04
46 OF 503

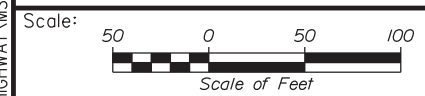
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NOTES:

- REFER TO SP 632 FOR INFORMATION ON THE OVER HEIGHT VEHICLE DETECTION SYSTEM. POWER TO BEACONS SHALL BE SOLAR.
- WORK ASSOCIATED WITH OVERHEAD SIGN STRUCTURE AT 4508-75 SHALL BE COMPLETED PRIOR TO PHASE 6.



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Designed	PLP	3\20\19	Checked	LEM	3\20\19
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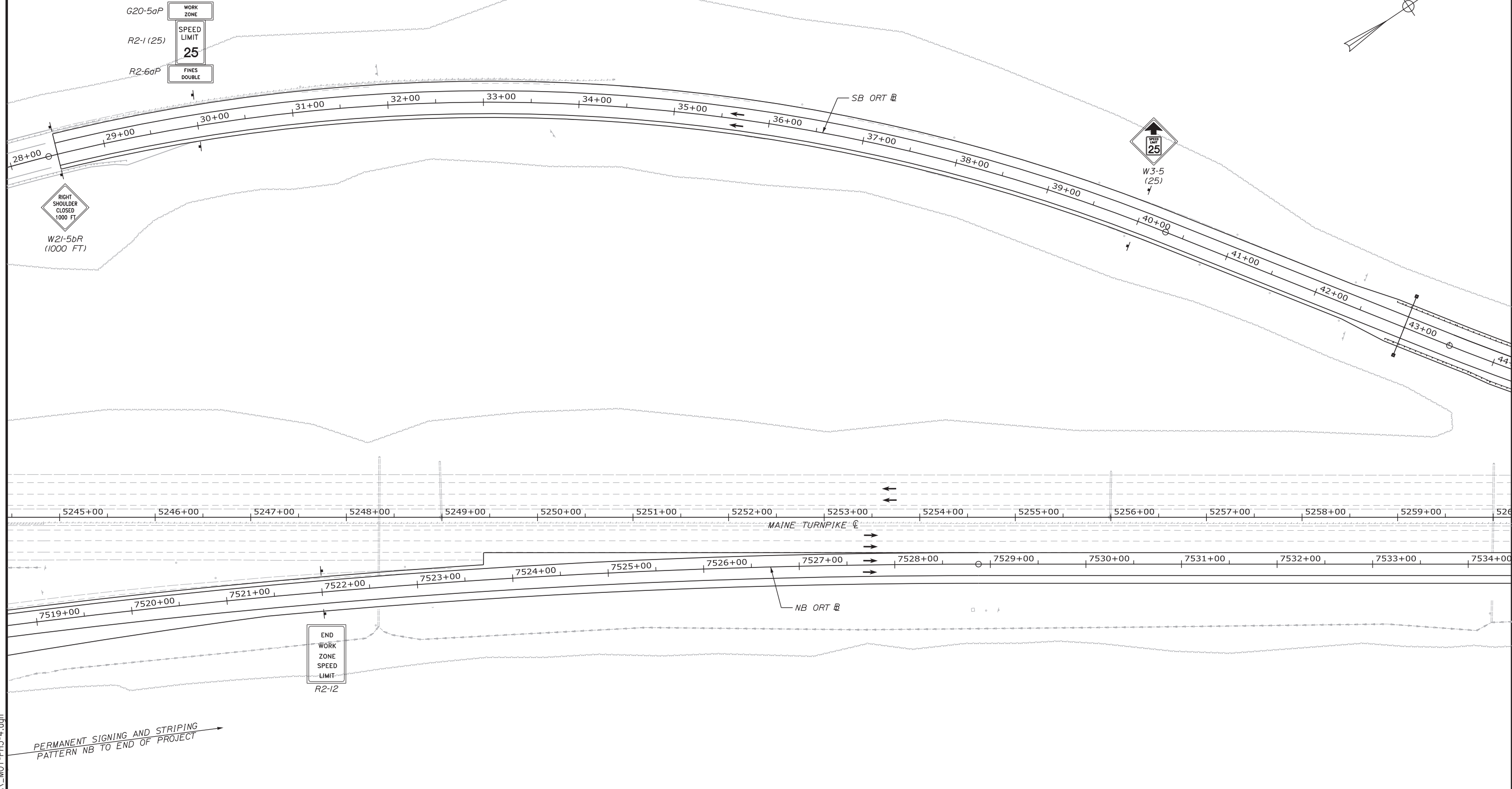
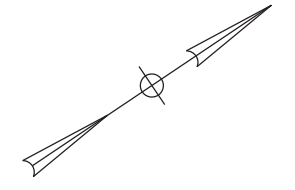
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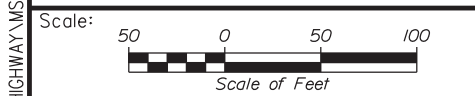
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 3
 MAINTENANCE OF TRAFFIC PLAN 3

SHEET NUMBER: MOT3-03
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PERMANENT SIGNING AND STRIPING
PATTERN NB TO END OF PROJECT



No.	Revision	By	Date

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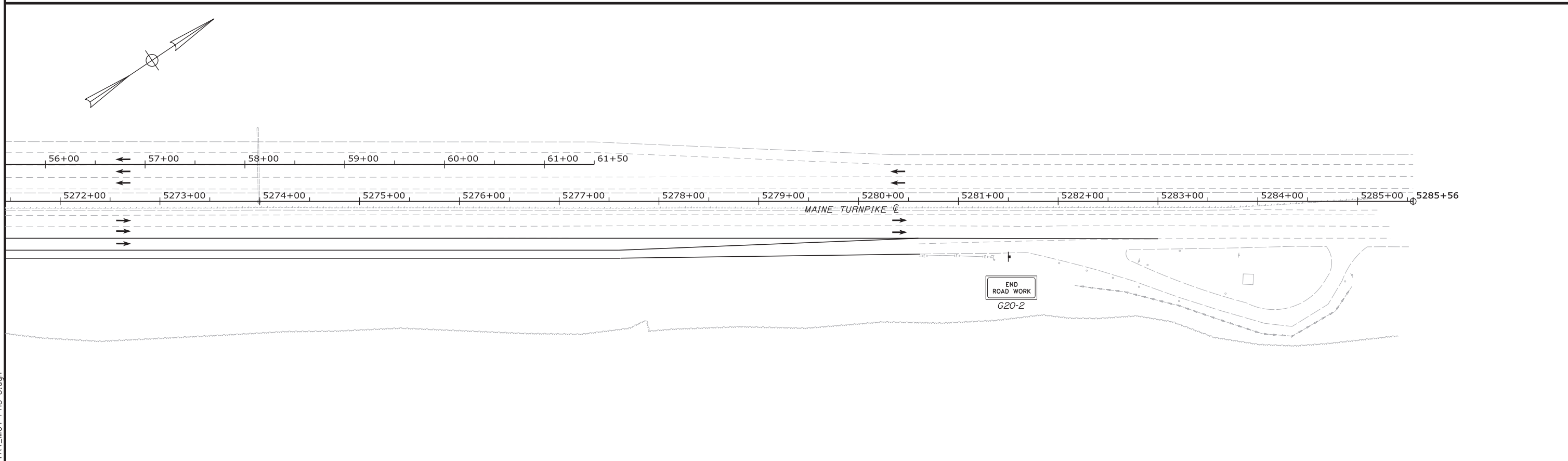
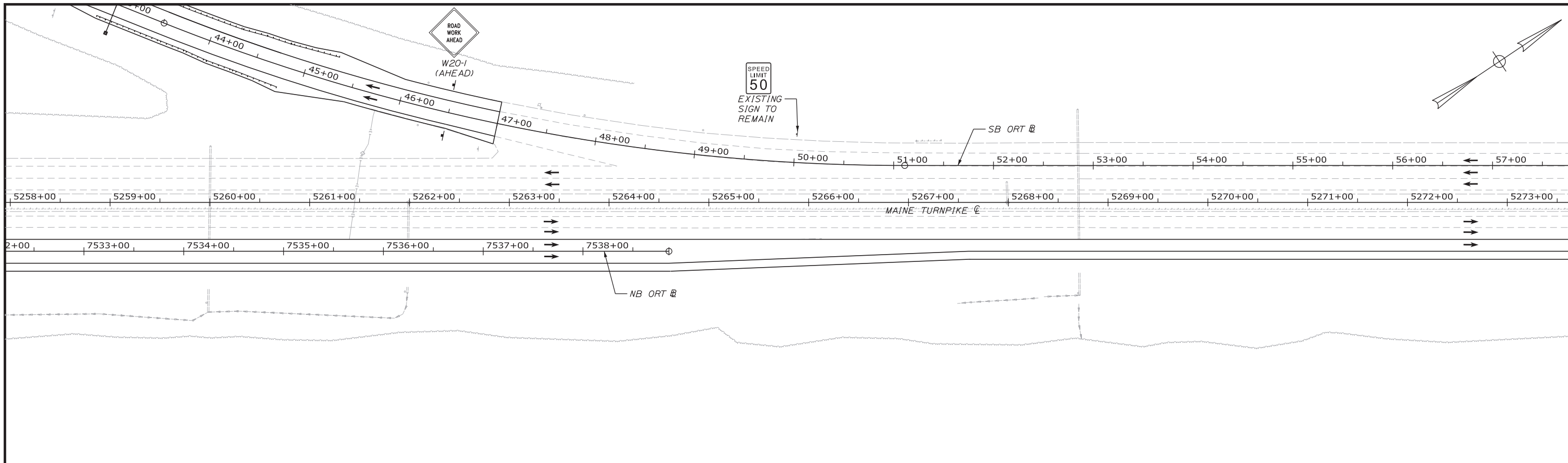
**THE GOLD STAR
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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 3
MAINTENANCE OF TRAFFIC PLAN 4

SHEET NUMBER: MOT3-04
CONTRACT: 2019.04
48 OF 503

Filename: ...00.HIGHWAY.MSTA\MOT-PH3-4.dgn



Filename: ...:\00\HIGHWAY\MSTA\MOT-PH3-5.dgn

Scale: Scale of Feet

No.	Revision	By	Date

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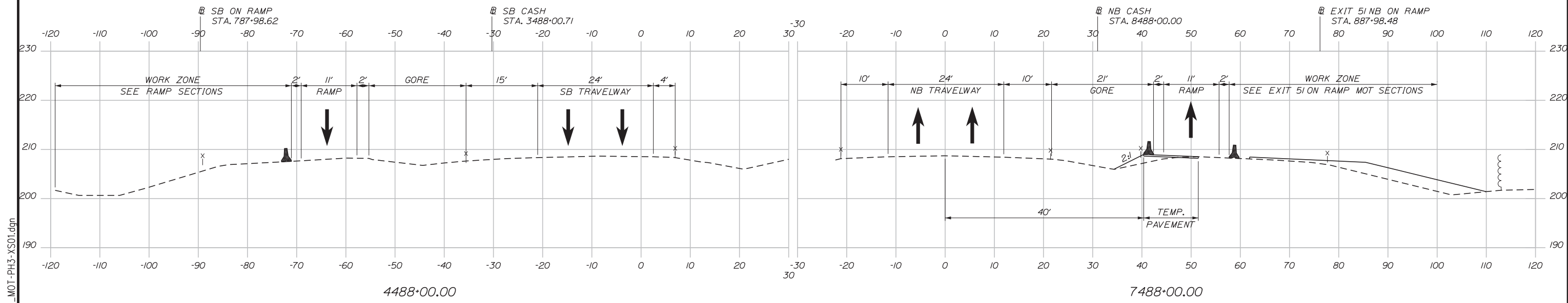
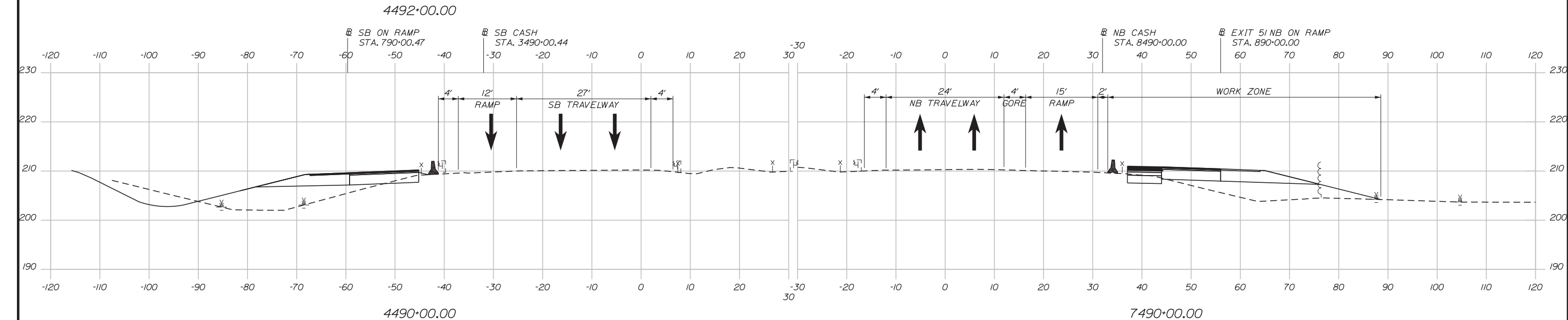
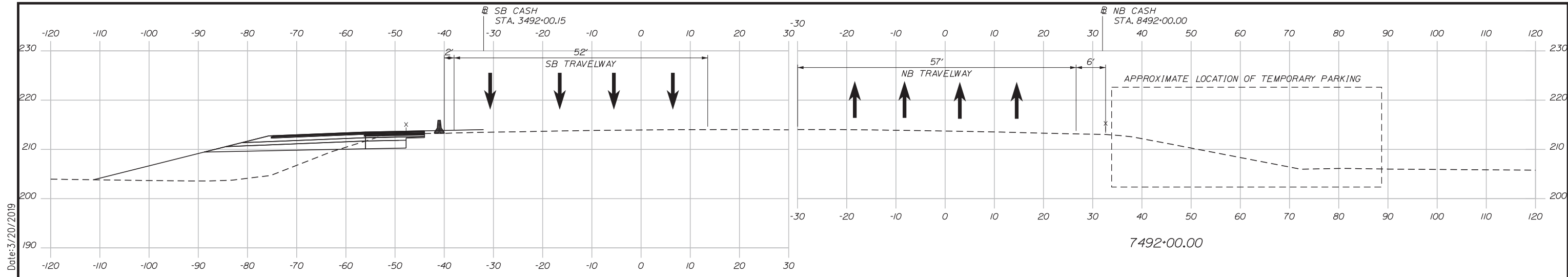
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

**INTERCHANGE 103
 ORT CONVERSION
 PHASE 3
 MAINTENANCE OF TRAFFIC PLAN 5**

SHEET NUMBER: MOT3-05
 CONTRACT: 2019.04
 49 OF 503

Date: 3/20/2019



Scale: 10 0 10 20
Scale of Feet

No.	Revision	By	Date

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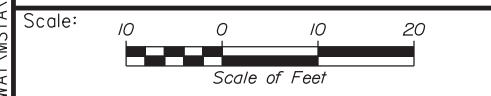
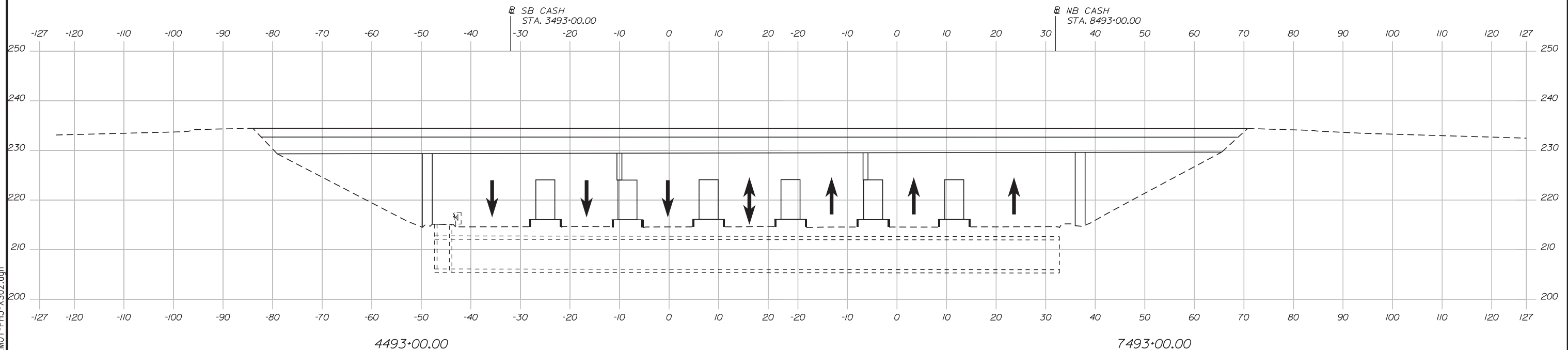
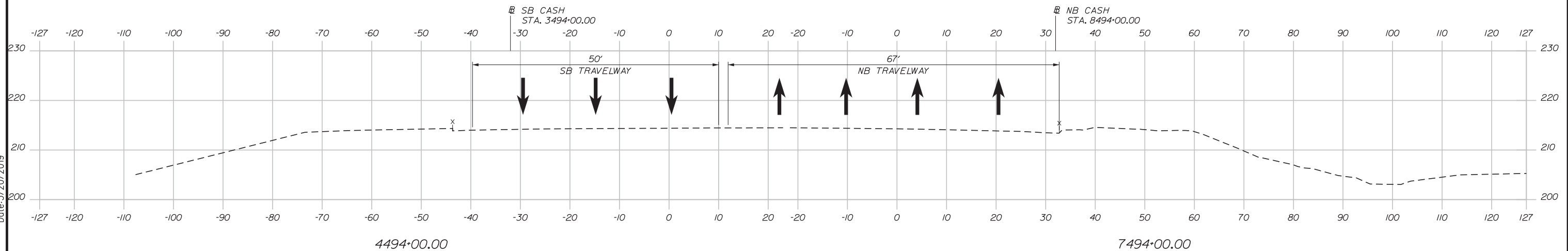
**THE GOLD STAR
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INTERCHANGE 103
ORT CONVERSION
PHASE 3
MAINTENANCE OF TRAFFIC SECTION 1

SHEET NUMBER: MOTXS-22
CONTRACT: 2019.04
50 OF 503

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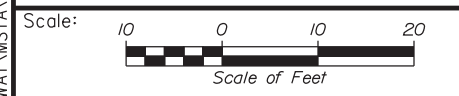
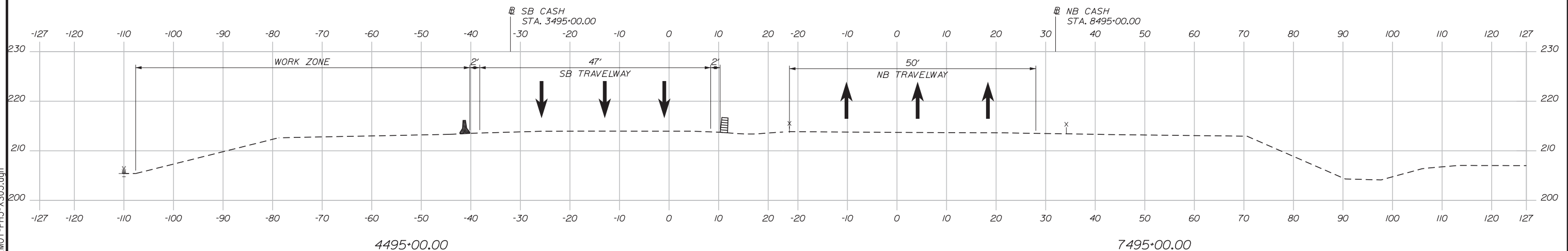
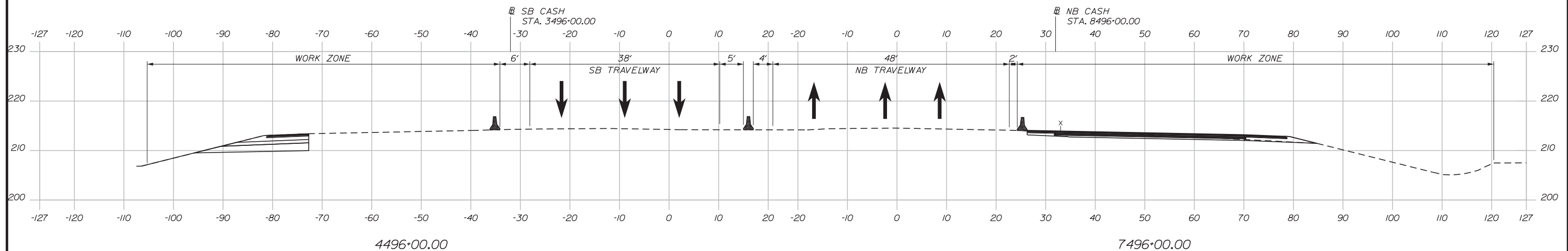
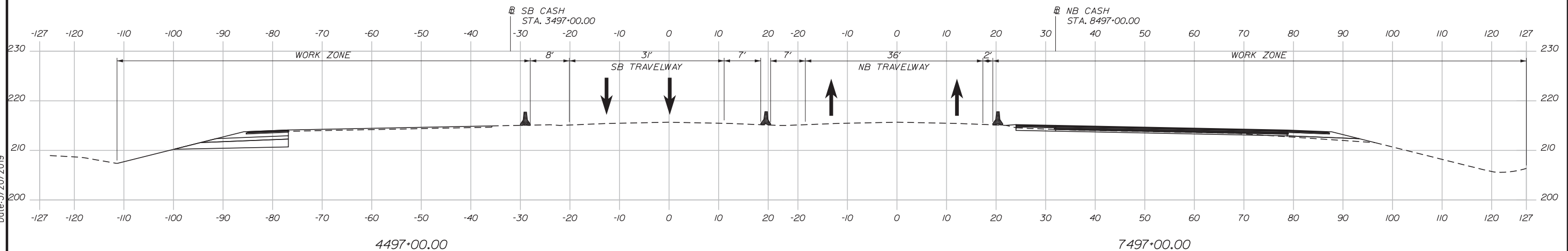
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ORT CONVERSION
PHASE 3
MAINTENANCE OF TRAFFIC SECTION 2

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CONTRACT: 2019.04
51 OF 503

Filename: ... \HIGHWAY\MSTA\...MOT-PH3-XS02.dgn

Date: 3/20/2019

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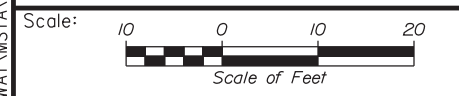
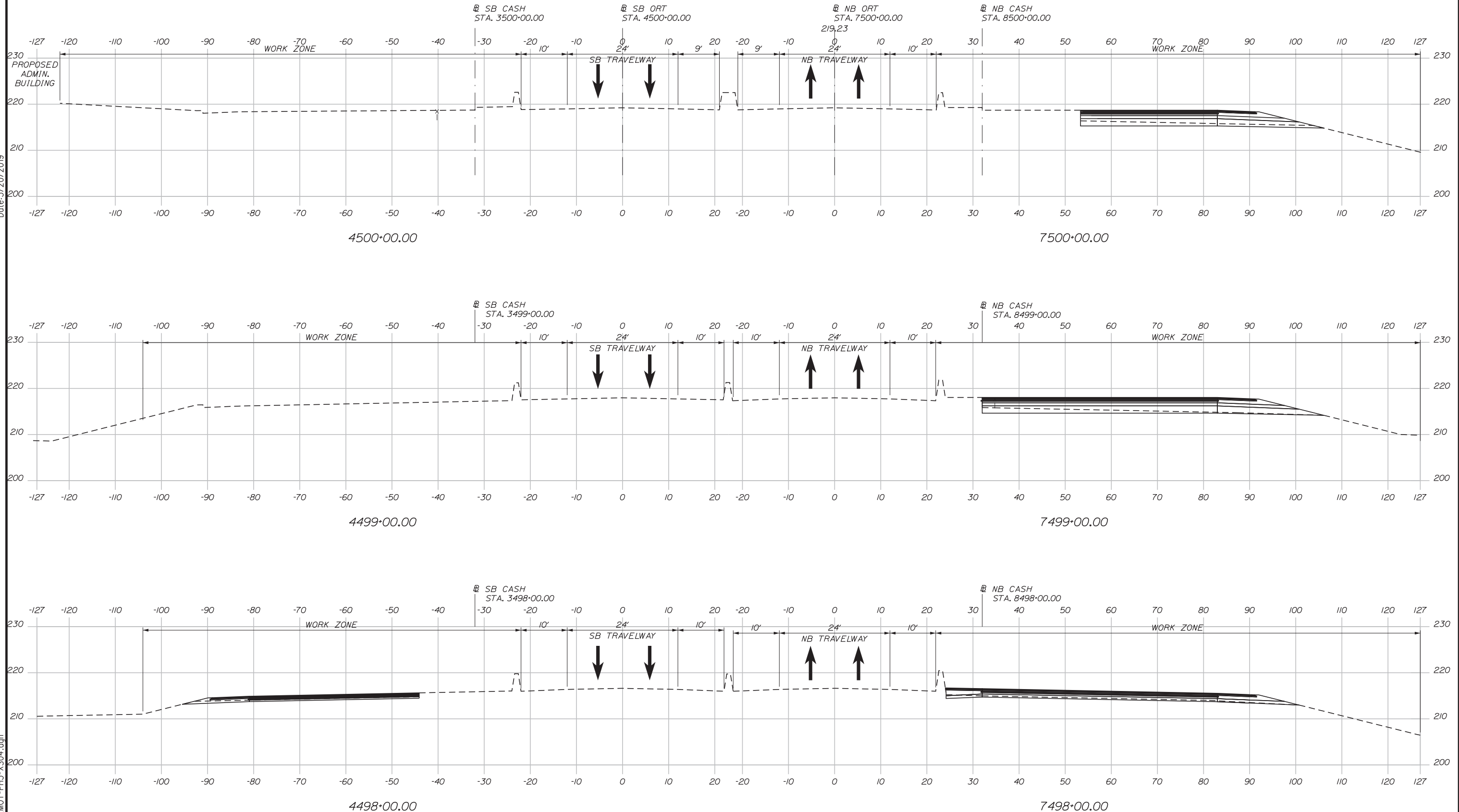
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 3
MAINTENANCE OF TRAFFIC SECTION 3

SHEET NUMBER: MOTXS-24
CONTRACT: 2019.04
52 OF 503

Date: 3/20/2019

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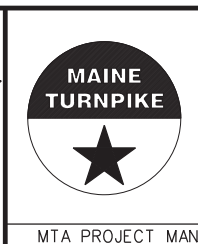
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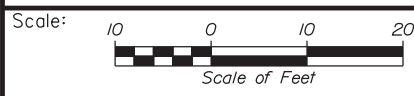
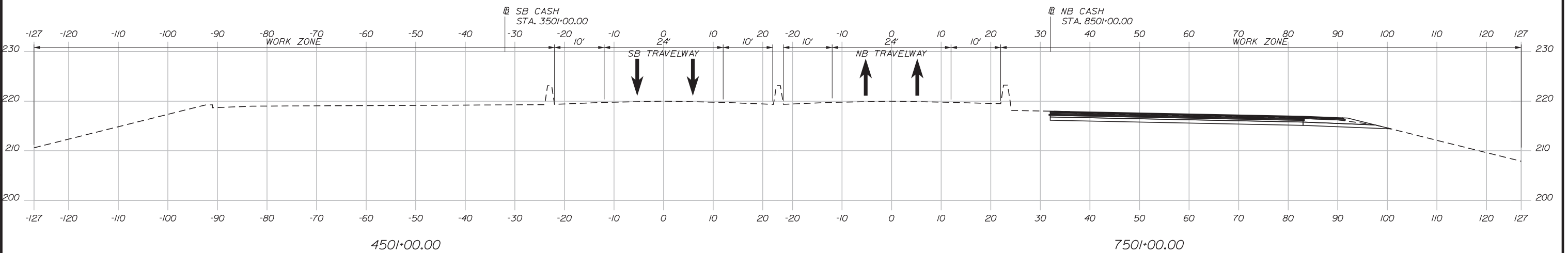
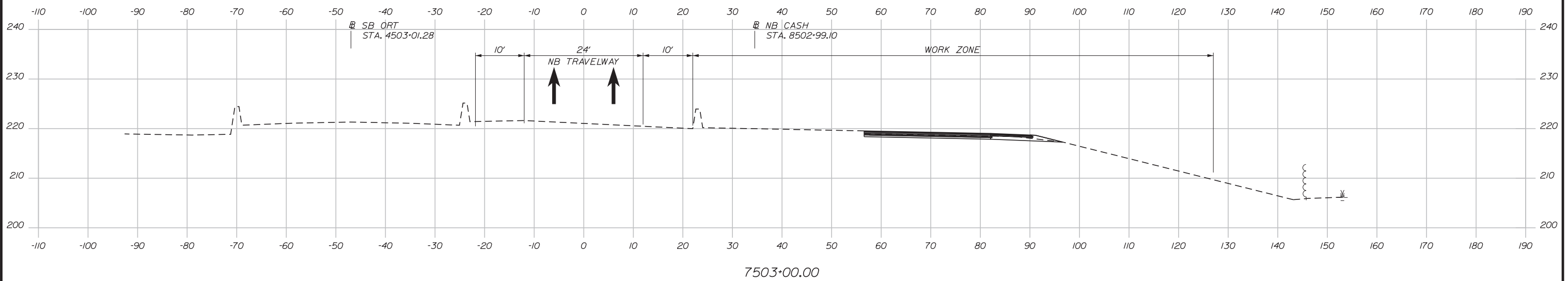
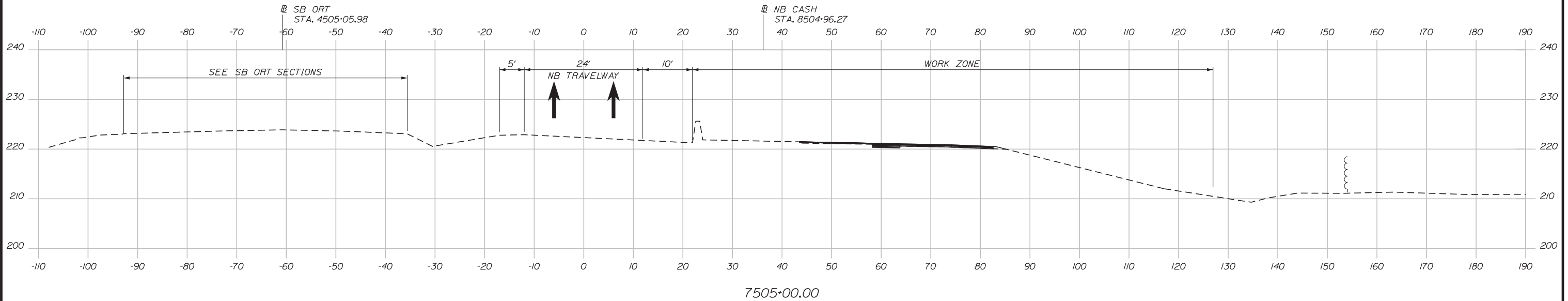


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INTERCHANGE 103
ORT CONVERSION
PHASE 3
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SHEET NUMBER: MOTXS-25
CONTRACT: 2019.04
53 OF 503

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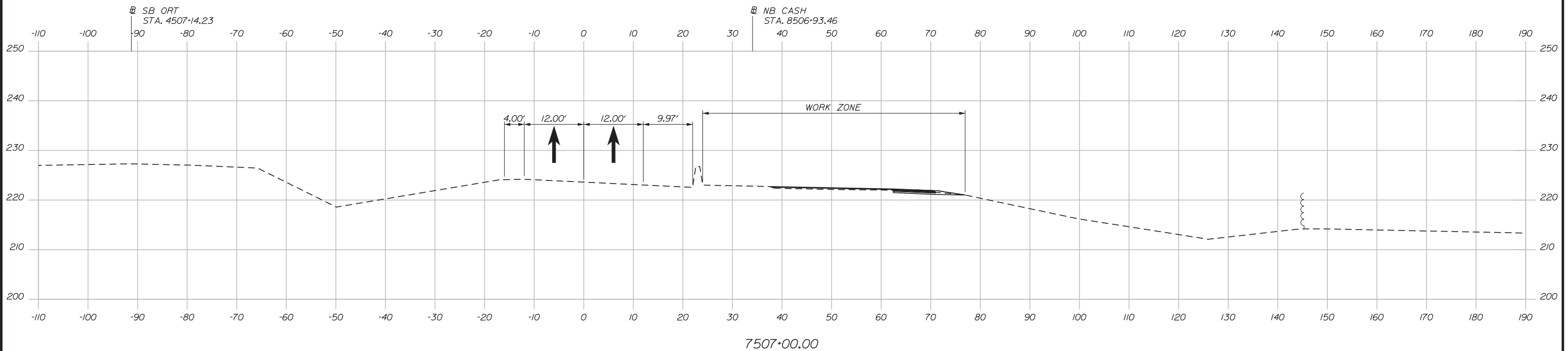
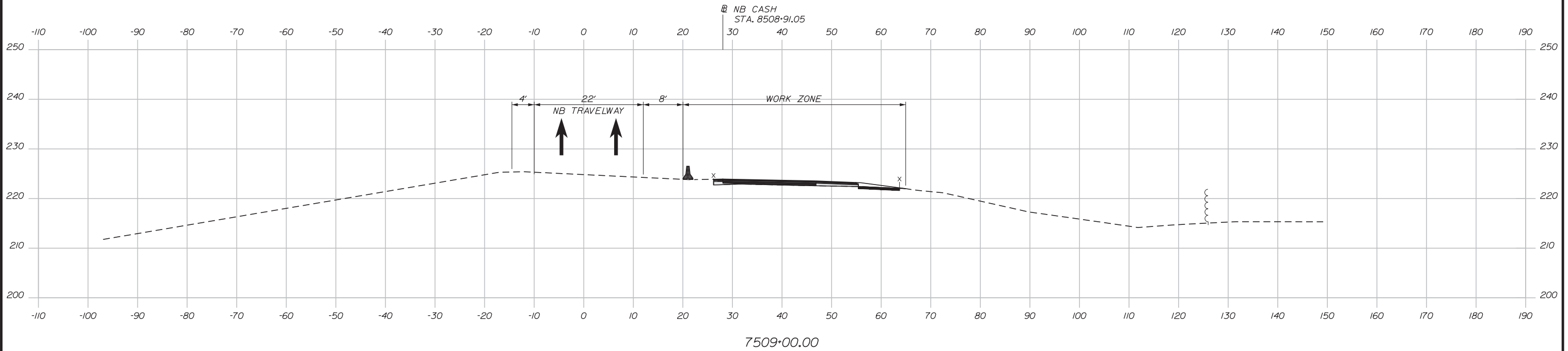
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 3
 MAINTENANCE OF TRAFFIC SECTION 5

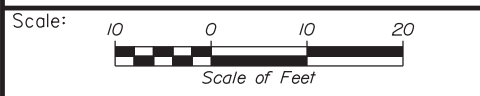
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 CONTRACT: 2019.04
 54 OF 503

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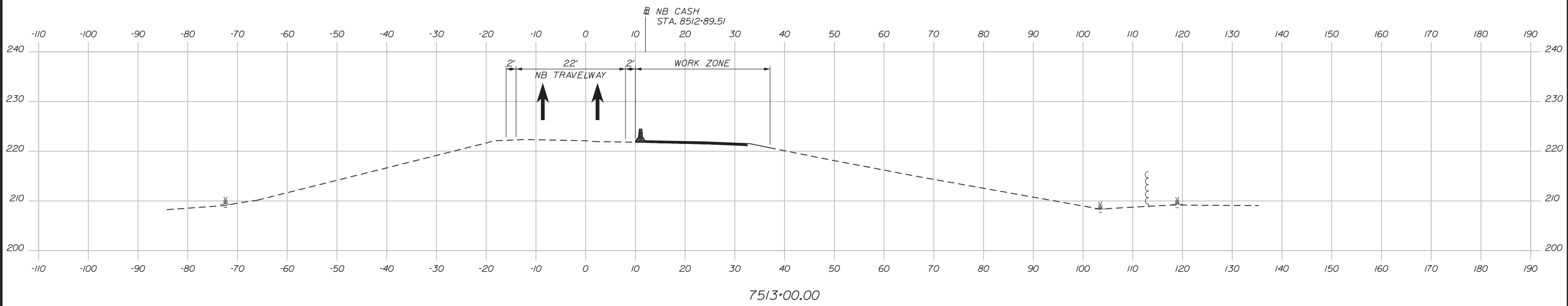
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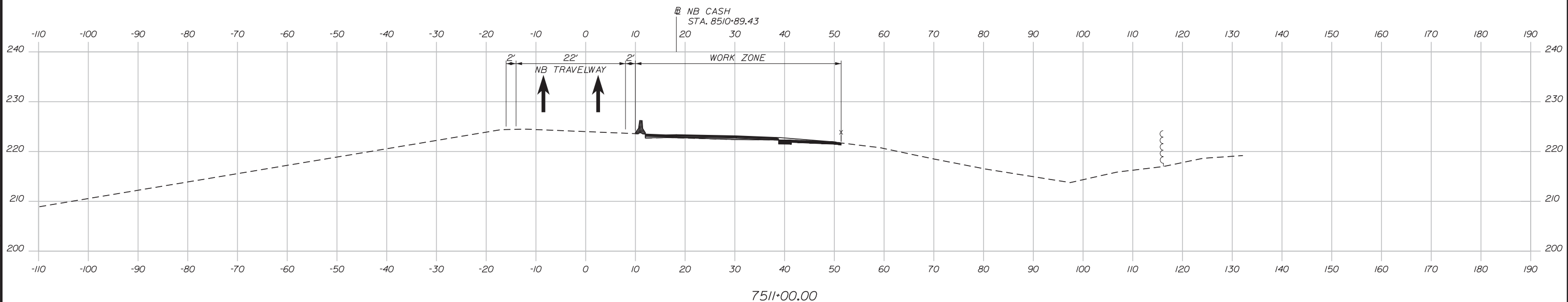
INTERCHANGE 103
ORT CONVERSION
PHASE 3
MAINTENANCE OF TRAFFIC SECTION 6

SHEET NUMBER: MOTXS-27
CONTRACT: 2019.04
55 OF 503

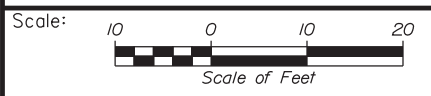
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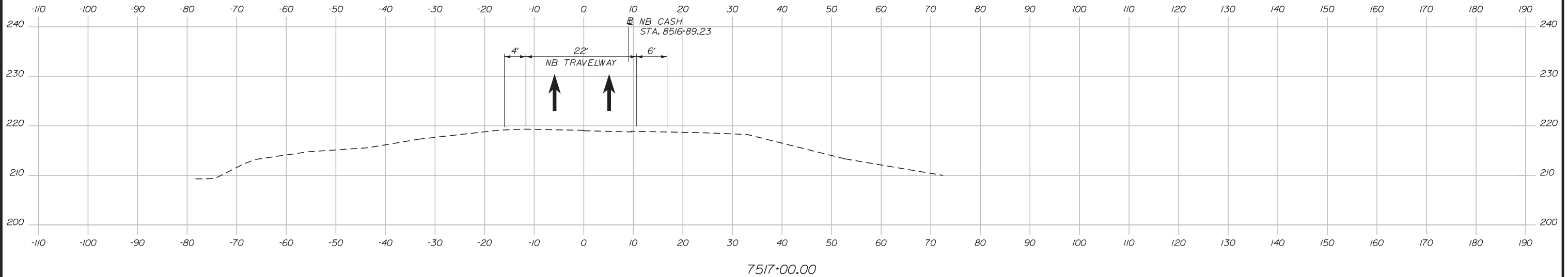
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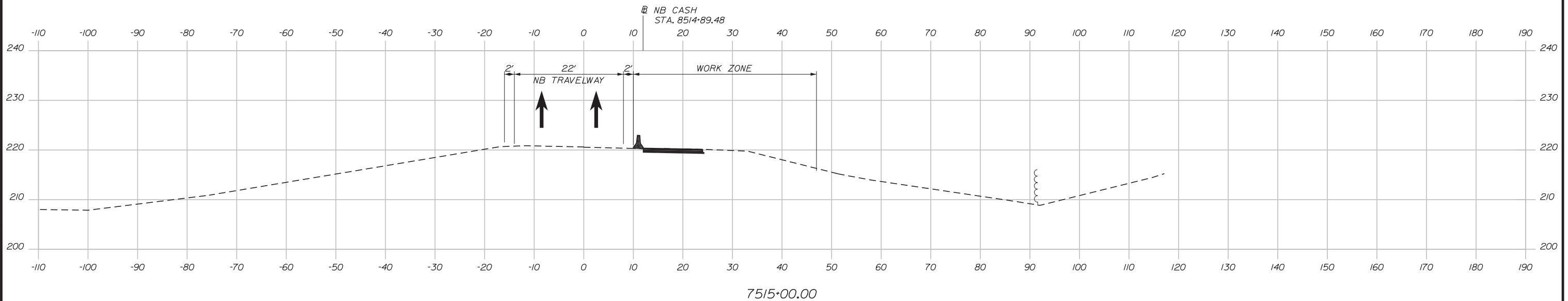
INTERCHANGE 103
 ORT CONVERSION
 PHASE 3
 MAINTENANCE OF TRAFFIC SECTION 7

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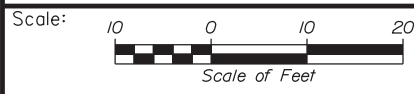
Date: 3/20/2019



7517+00.00



7515+00.00



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THE GOLD STAR
MEMORIAL HIGHWAY

INTERCHANGE 103
ORT CONVERSION
PHASE 3
MAINTENANCE OF TRAFFIC SECTION 8

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
	By	Date	
Designed	PLP	3\20\19	Checked LEM 3\20\19
Drawn	EJB	3\20\19	In Charge of GAE 3\20\19

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

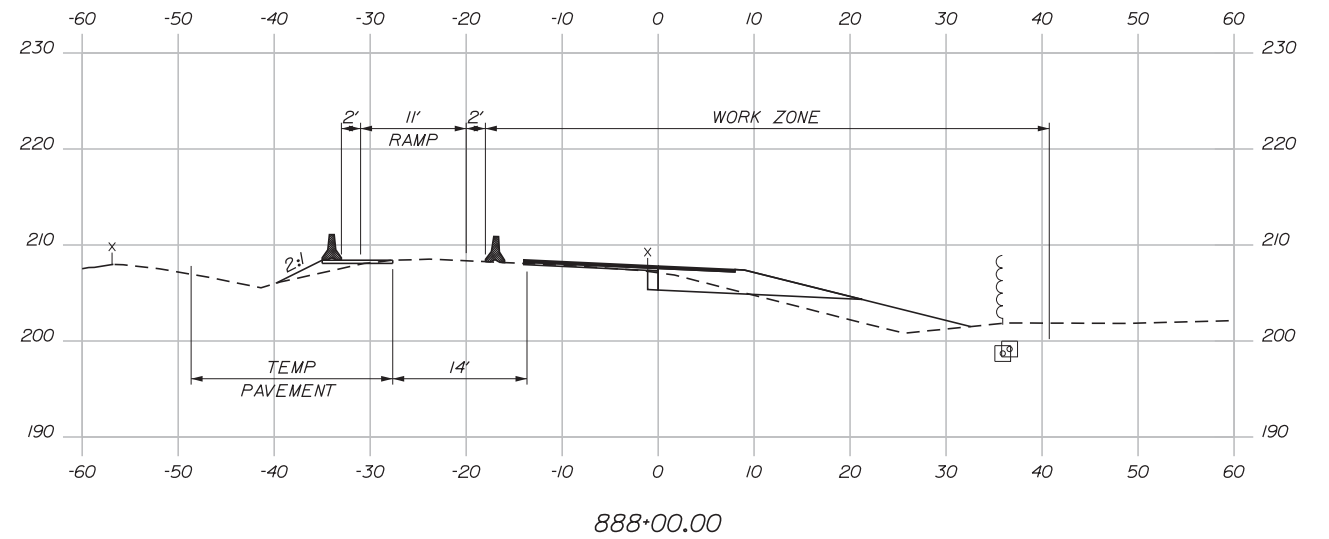
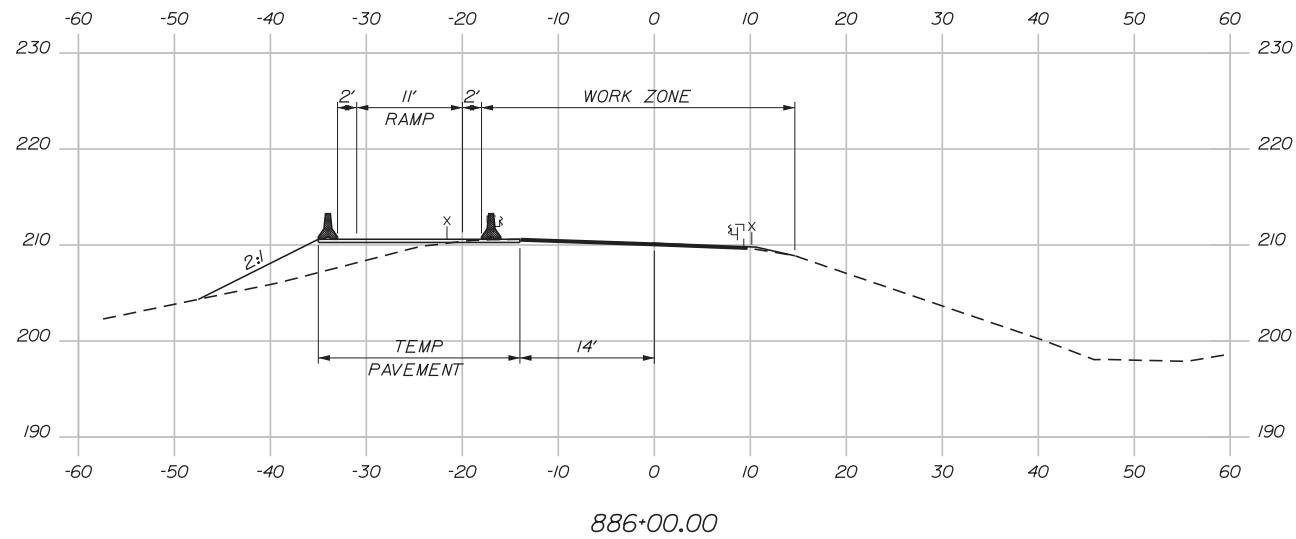
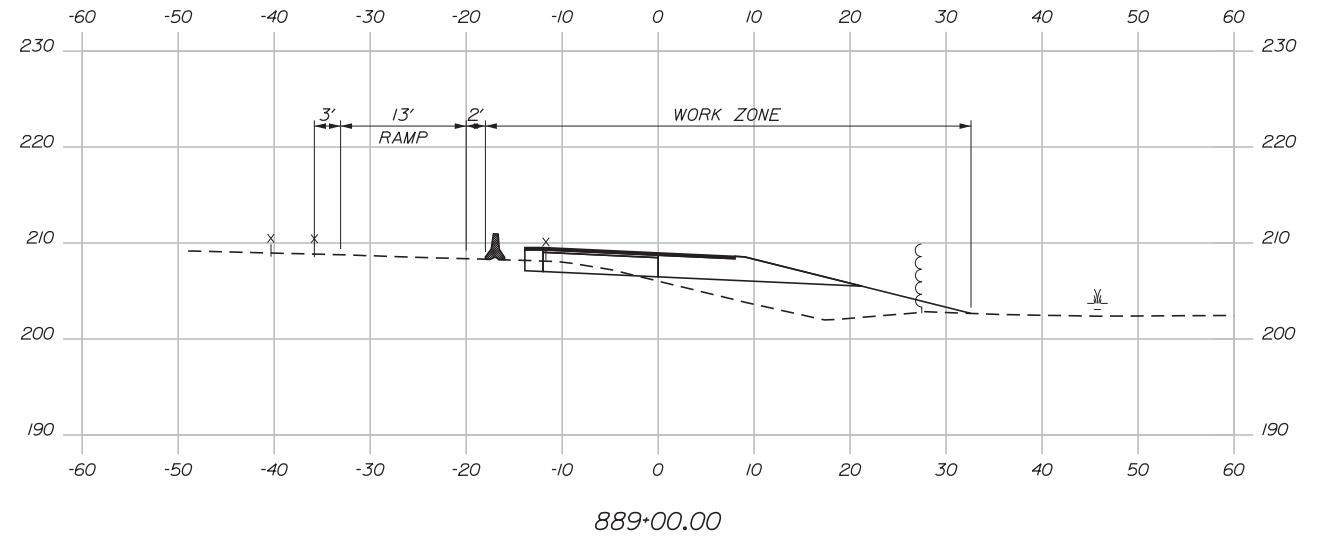
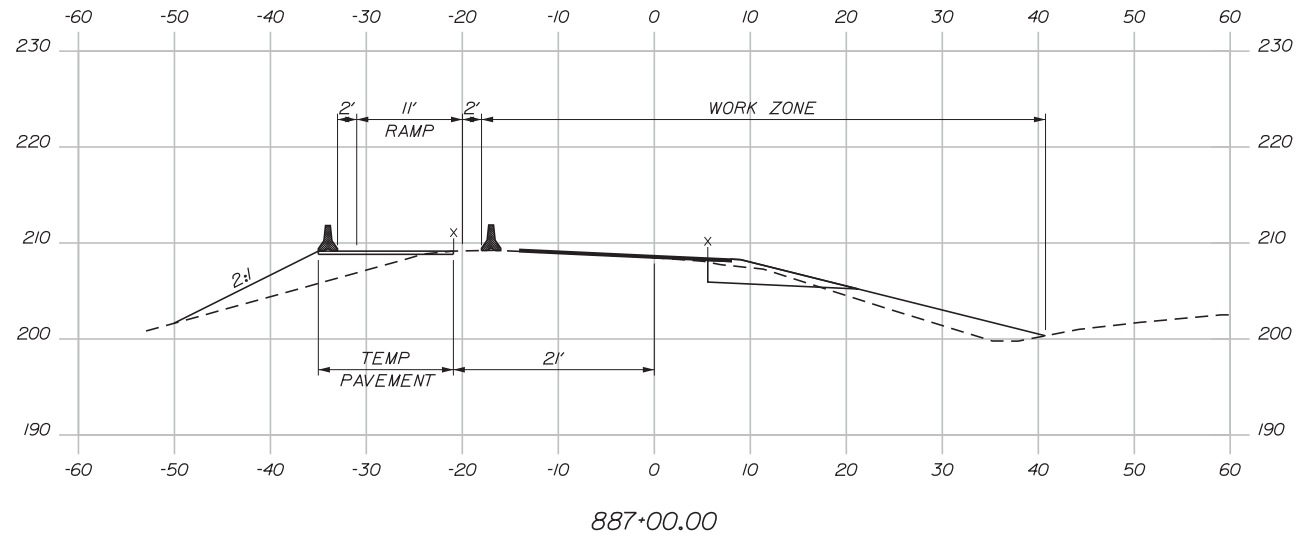
CONTRACT: 2019.04

SHEET NUMBER: MOTXS-29

57 OF 503

Filename: ... \HIGHWAY\MSTA\MOT-PH3-XS08.dgn

Date: 3/20/2019



Filename: ... \HIGHWAY\MSTA\MOT-PT-3-XS09.dgn



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INTERCHANGE 103
 ORT CONVERSION
 PHASE 3
 MAINTENANCE OF TRAFFIC SECTION 9

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
Designed	By	Date	Checked
	PLP	3\20\19	LEM
Drawn	By	Date	In Charge of
	EJB	3\20\19	GAE

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

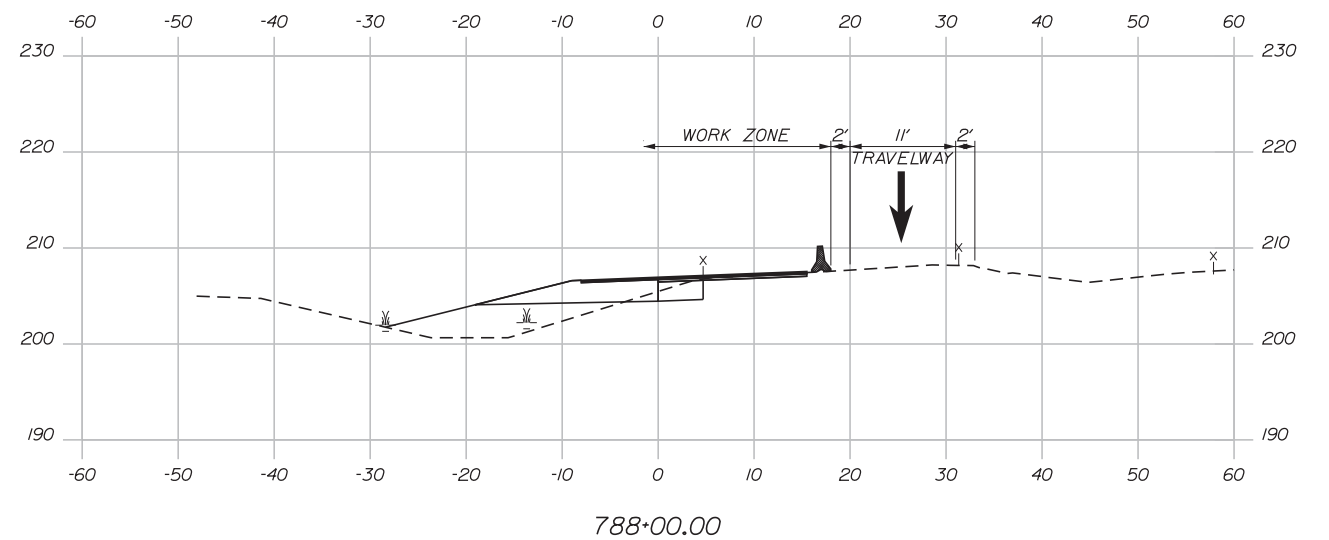
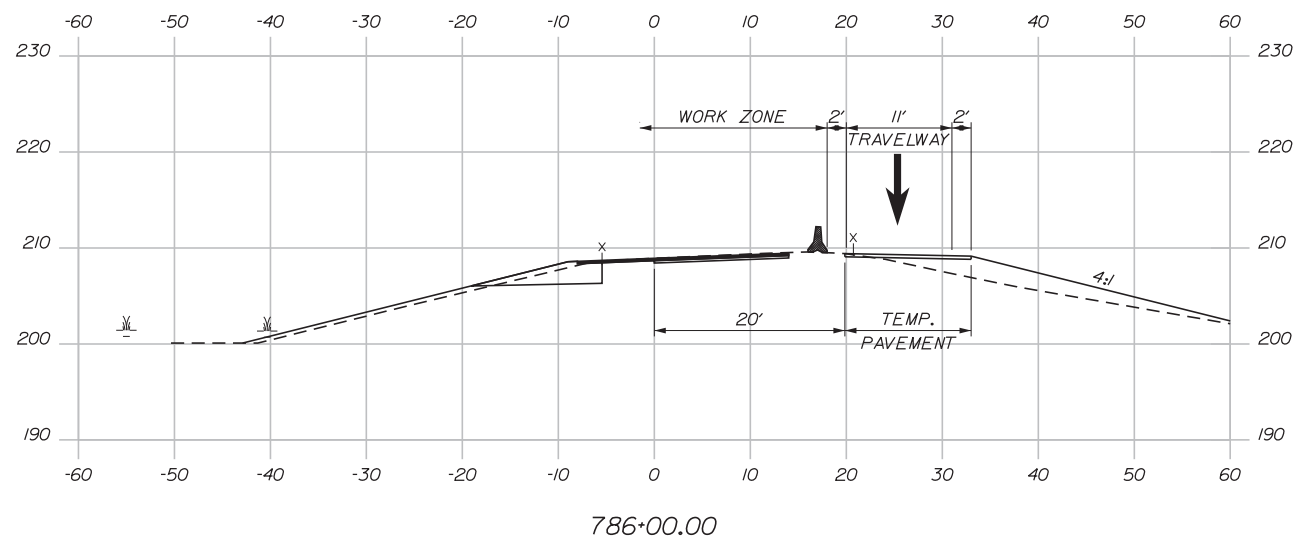
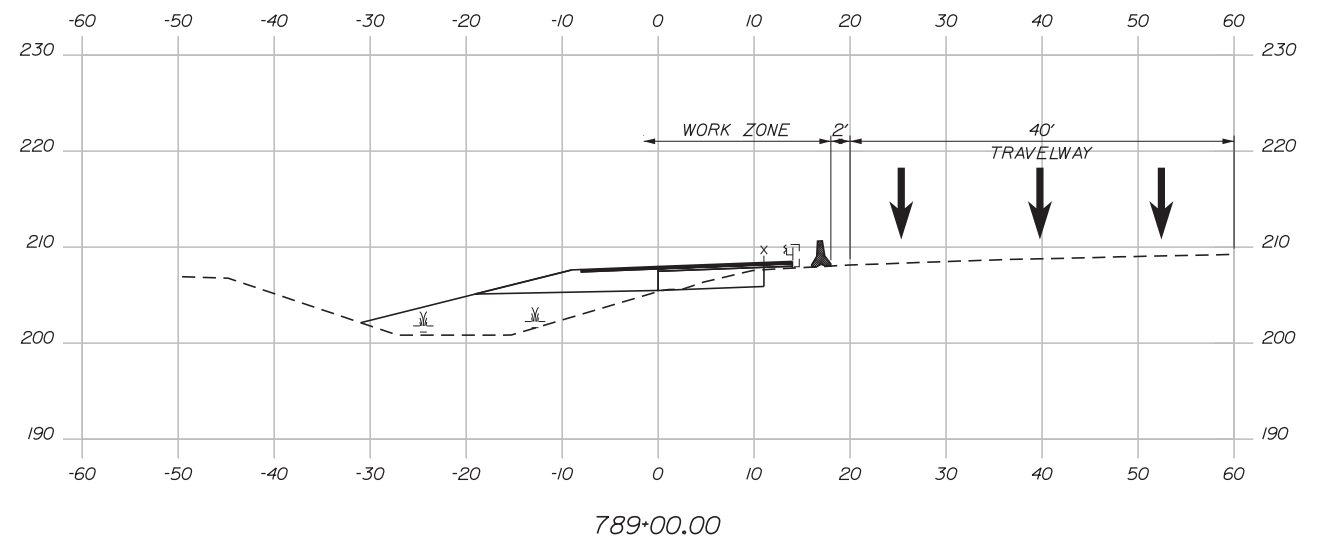
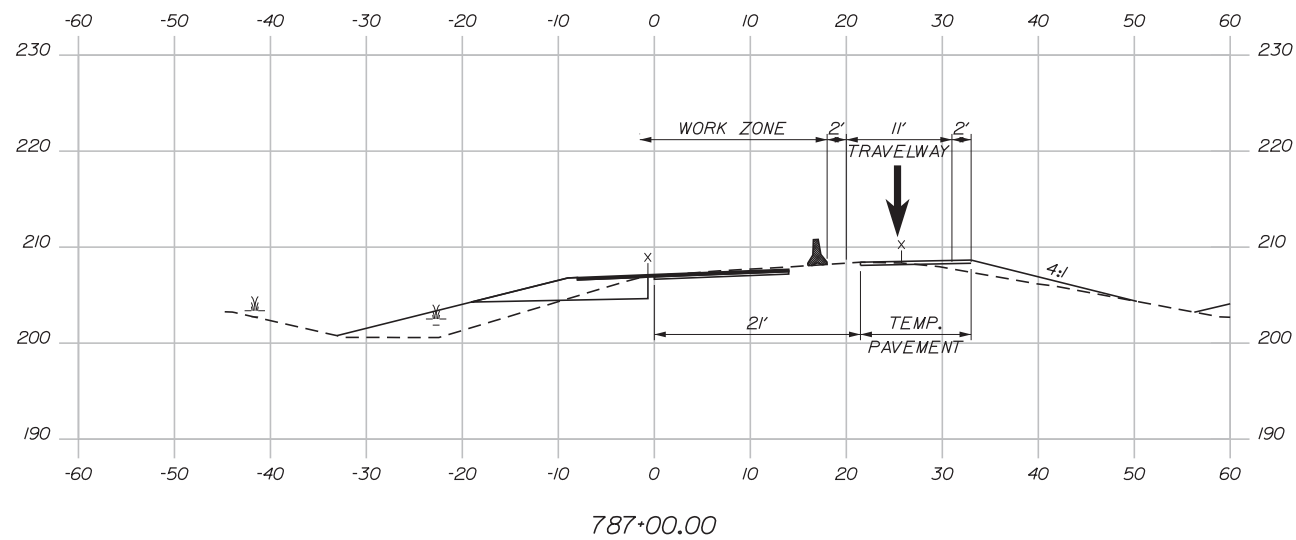
CONTRACT: 2019.04

SHEET NUMBER: MOTXS-30

58 OF 503

Date: 3/20/2019

Filename: ... \HIGHWAY\MSTA\MOT-PH3-XS10.dgn



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INTERCHANGE 103
ORT CONVERSION
PHASE 3
MAINTENANCE OF TRAFFIC SECTION 10

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
	By	Date	
Designed	PLP	3\20\19	Checked LEM 3\20\19
Drawn	EJB	3\20\19	In Charge of GAE 3\20\19

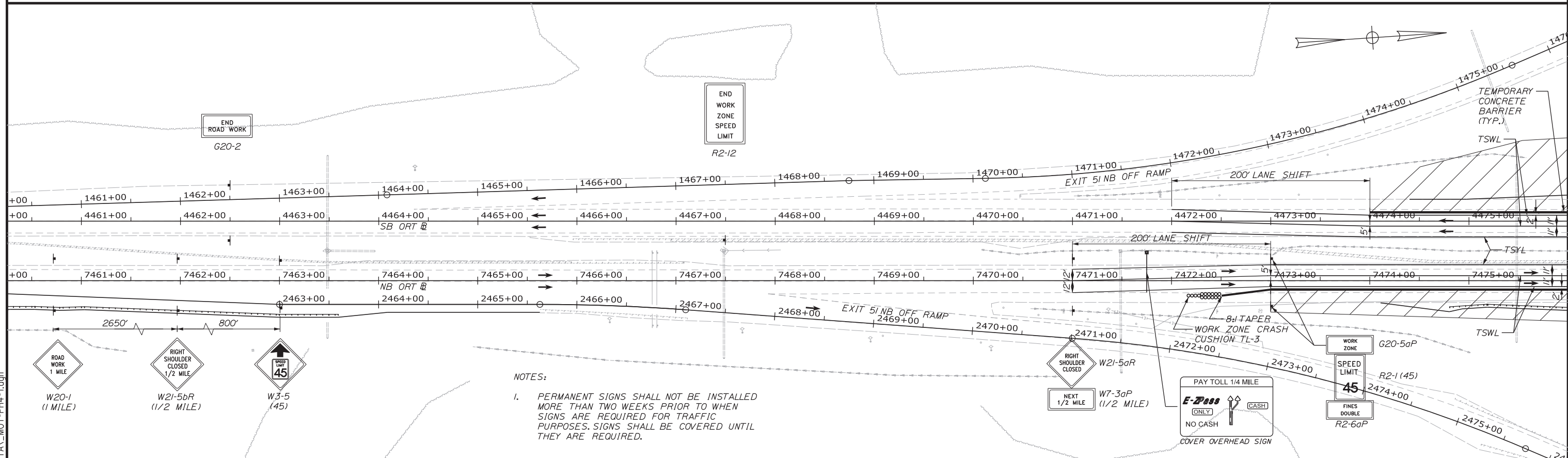
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

CONTRACT: 2019.04

SHEET NUMBER: MOTXS-31

59 OF 503

WINTER TIME REQUIREMENTS:
 CONTRACTOR SHALL SUFFICIENTLY COMPLETE CONSTRUCTION ON I-295 NORTHBOUND AND SOUTHBOUND PRIOR TO NOVEMBER 16, 2019, OR NOVEMBER 16, 2020 SO ALL RIGHT SIDE TEMPORARY CONCRETE BARRIER IS REMOVED OR THE TEMPORARY CONCRETE BARRIER IS RELOCATED TO PROVIDE A RIGHT SHOULDER 8 FEET WIDE OR GREATER, FROM NOVEMBER 15, 2019 TO APRIL 15, 2020 AND FROM NOVEMBER 16, 2020 THROUGH APRIL 15, 2021, EXCEPT DURING PHASE 4 IN THE SOUTHBOUND DIRECTION FROM STATION 4506+00 TO STATION 4509+00 THE RIGHT SIDE SHOULDER IS REDUCED AS SHOWN ON THE PHASE 4 MAINTENANCE OF TRAFFIC PLANS.



NOTES:
 1. PERMANENT SIGNS SHALL NOT BE INSTALLED MORE THAN TWO WEEKS PRIOR TO WHEN SIGNS ARE REQUIRED FOR TRAFFIC PURPOSES. SIGNS SHALL BE COVERED UNTIL THEY ARE REQUIRED.

Filename: ...:\00\HIGHWAY\MSTA\MOT-PH4-1.dgn

Scale: 50 0 50 100
 Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
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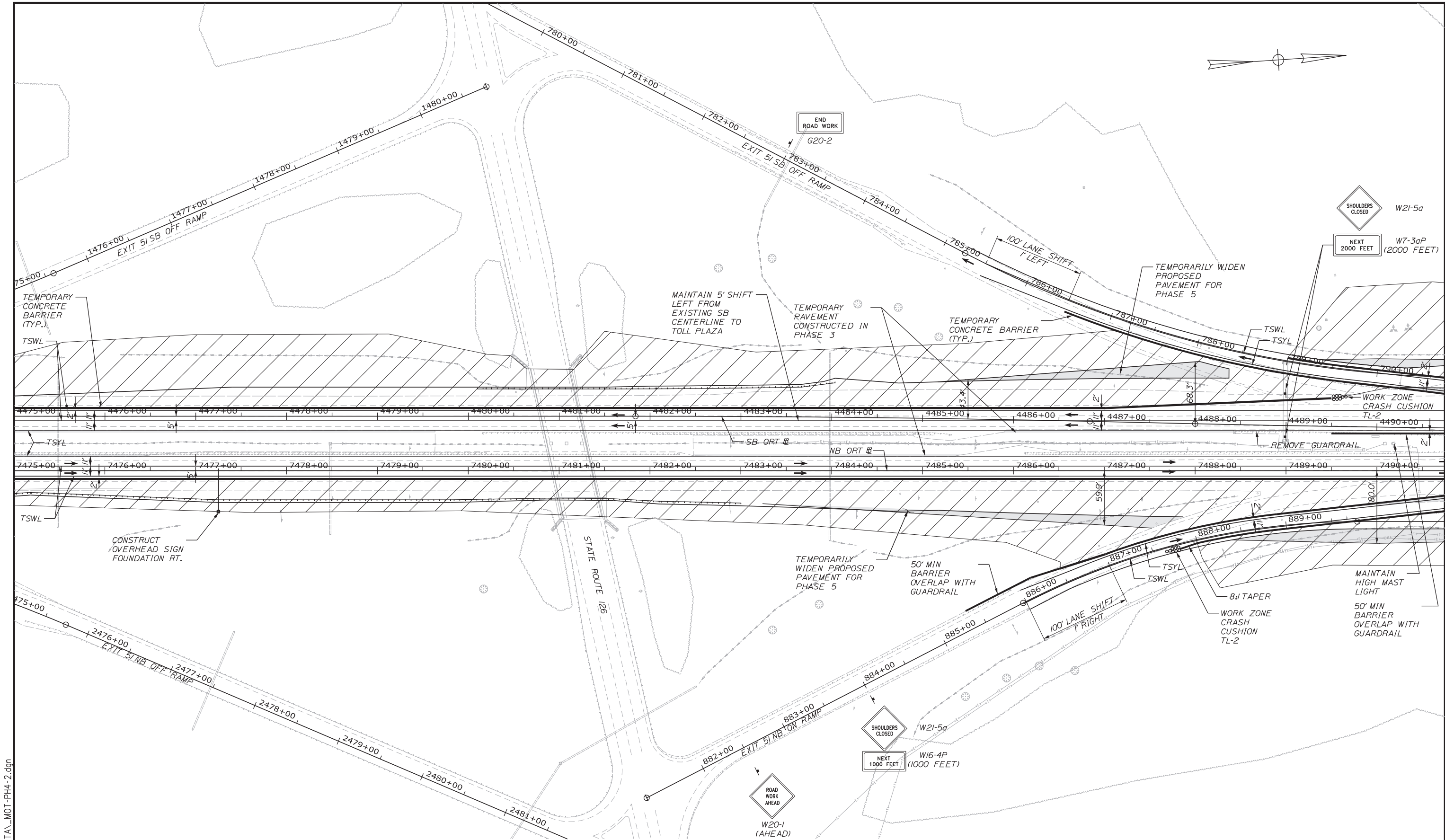
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 4
 MAINTENANCE OF TRAFFIC PLAN 1

SHEET NUMBER: MOT4-01
 60 OF 503

CONTRACT: 2019.04



Filename: ...:\00.HIGHWAY\MSTA\MOT-PH4-2.dgn

Scale: Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

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Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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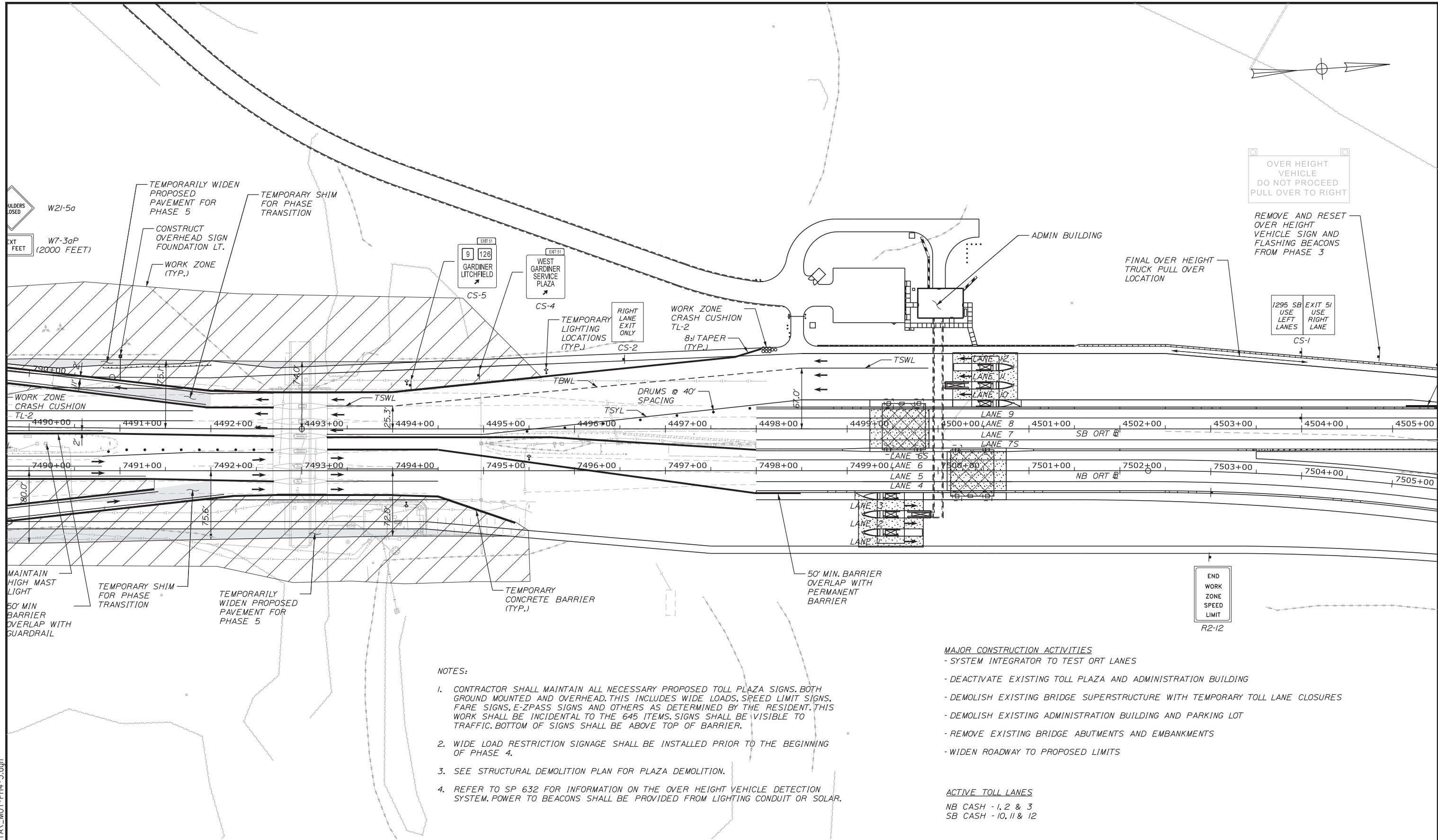
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

**INTERCHANGE 103
 ORT CONVERSION
 PHASE 4
 MAINTENANCE OF TRAFFIC PLAN 2**

SHEET NUMBER: MOT4-02
61 OF 503

CONTRACT: 2019.04



W2-5a
W7-3aP
(2000 FEET)

TEMPORARILY WIDEN PROPOSED PAVEMENT FOR PHASE 5
CONSTRUCT OVERHEAD SIGN FOUNDATION LT.
WORK ZONE (TYP.)
TEMPORARY SHIM FOR PHASE TRANSITION

EXIT 51
9 126
GARDINER LITCHFIELD
CS-5
EXIT 51
WEST GARDINER SERVICE PLAZA
CS-4

RIGHT LANE EXIT ONLY
TEMPORARY LIGHTING LOCATIONS (TYP.)
CS-2
WORK ZONE CRASH CUSHION TL-2
8:1 TAPER (TYP.)

ADMIN BUILDING

FINAL OVER HEIGHT TRUCK PULL OVER LOCATION

OVER HEIGHT VEHICLE
DO NOT PROCEED
PULL OVER TO RIGHT

REMOVE AND RESET OVER HEIGHT VEHICLE SIGN AND FLASHING BEACONS FROM PHASE 3

1295 SB USE LEFT LANES
EXIT 51 USE RIGHT LANE
CS-1

WORK ZONE CRASH CUSHION TL-2

DRUMS @ 40' SPACING

LANE 12
LANE 11
LANE 10
LANE 9
LANE 8
LANE 7
LANE 6S
LANE 6
LANE 5
LANE 4

SB ORT

NB ORT

MAINTAIN HIGH MAST LIGHT
60' MIN BARRIER OVERLAP WITH GUARDRAIL

TEMPORARY SHIM FOR PHASE TRANSITION

TEMPORARILY WIDEN PROPOSED PAVEMENT FOR PHASE 5

TEMPORARY CONCRETE BARRIER (TYP.)

50' MIN. BARRIER OVERLAP WITH PERMANENT BARRIER

END WORK ZONE SPEED LIMIT R2-12

NOTES:

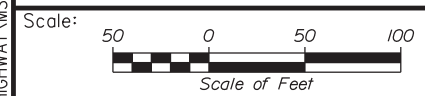
- CONTRACTOR SHALL MAINTAIN ALL NECESSARY PROPOSED TOLL PLAZA SIGNS, BOTH GROUND MOUNTED AND OVERHEAD. THIS INCLUDES WIDE LOADS, SPEED LIMIT SIGNS, FARE SIGNS, E-ZPASS SIGNS AND OTHERS AS DETERMINED BY THE RESIDENT. THIS WORK SHALL BE INCIDENTAL TO THE 645 ITEMS. SIGNS SHALL BE VISIBLE TO TRAFFIC. BOTTOM OF SIGNS SHALL BE ABOVE TOP OF BARRIER.
- WIDE LOAD RESTRICTION SIGNAGE SHALL BE INSTALLED PRIOR TO THE BEGINNING OF PHASE 4.
- SEE STRUCTURAL DEMOLITION PLAN FOR PLAZA DEMOLITION.
- REFER TO SP 632 FOR INFORMATION ON THE OVER HEIGHT VEHICLE DETECTION SYSTEM. POWER TO BEACONS SHALL BE PROVIDED FROM LIGHTING CONDUIT OR SOLAR.

MAJOR CONSTRUCTION ACTIVITIES

- SYSTEM INTEGRATOR TO TEST ORT LANES
- DEACTIVATE EXISTING TOLL PLAZA AND ADMINISTRATION BUILDING
- DEMOLISH EXISTING BRIDGE SUPERSTRUCTURE WITH TEMPORARY TOLL LANE CLOSURES
- DEMOLISH EXISTING ADMINISTRATION BUILDING AND PARKING LOT
- REMOVE EXISTING BRIDGE ABUTMENTS AND EMBANKMENTS
- WIDEN ROADWAY TO PROPOSED LIMITS

ACTIVE TOLL LANES

NB CASH - 1, 2 & 3
SB CASH - 10, 11 & 12



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THE GOLD STAR
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INTERCHANGE 103
ORT CONVERSION
PHASE 4
MAINTENANCE OF TRAFFIC PLAN 3

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
	By	Date	
Designed	PLP	3\20\19	Checked LEM 3\20\19
Drawn	EJB	3\20\19	In Charge of GAE 3\20\19

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

CONTRACT:2019.04

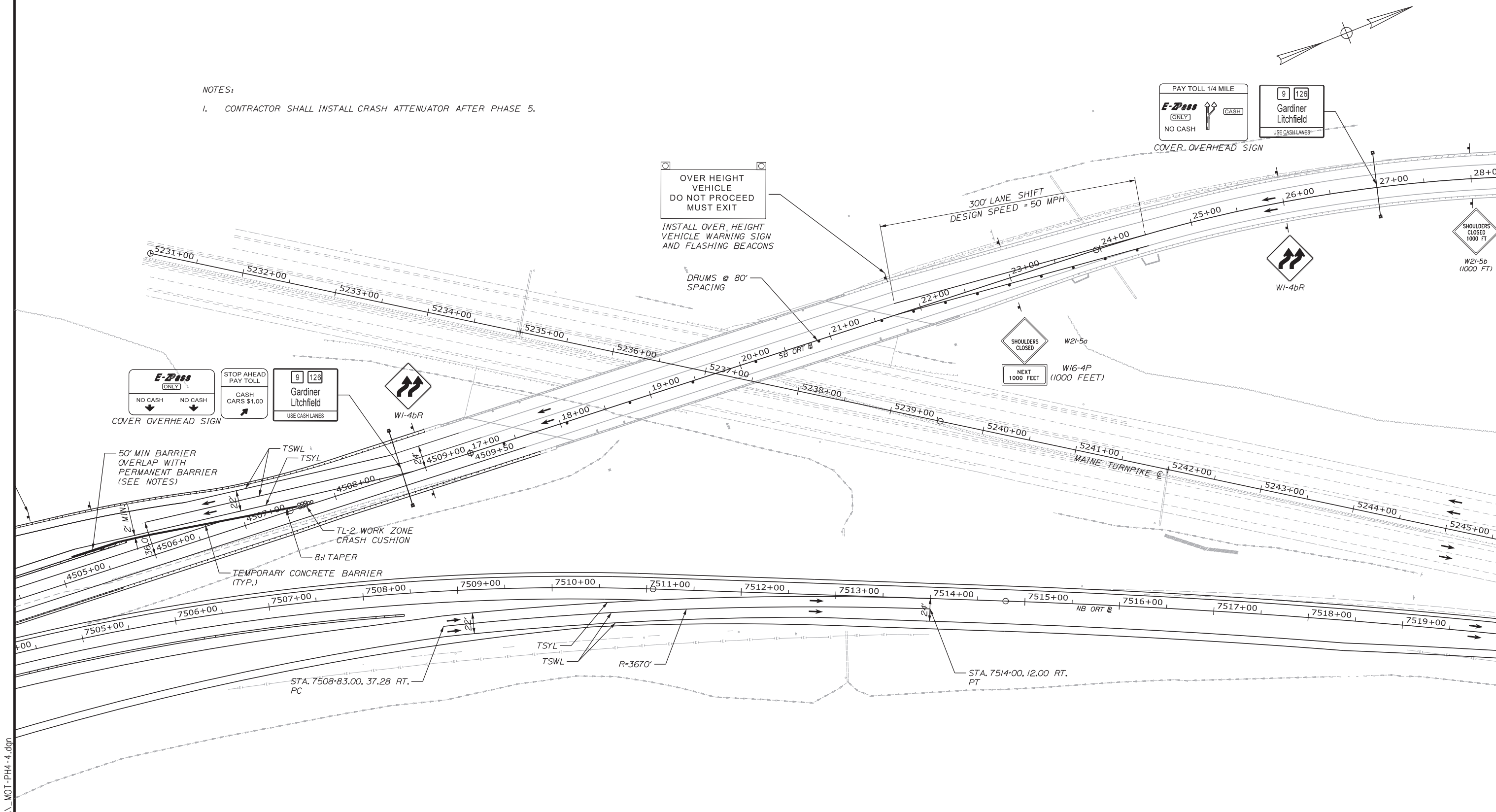
SHEET NUMBER: MOT4-02

62 OF 503

Filename: ...00.HIGHWAY\MSTA\MOT-PH4-3.dgn

NOTES:

1. CONTRACTOR SHALL INSTALL CRASH ATTENUATOR AFTER PHASE 5.



Filename: ...:\00.HIGHWAY\MSTA\MOT-PH4-4.dgn

Scale:

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

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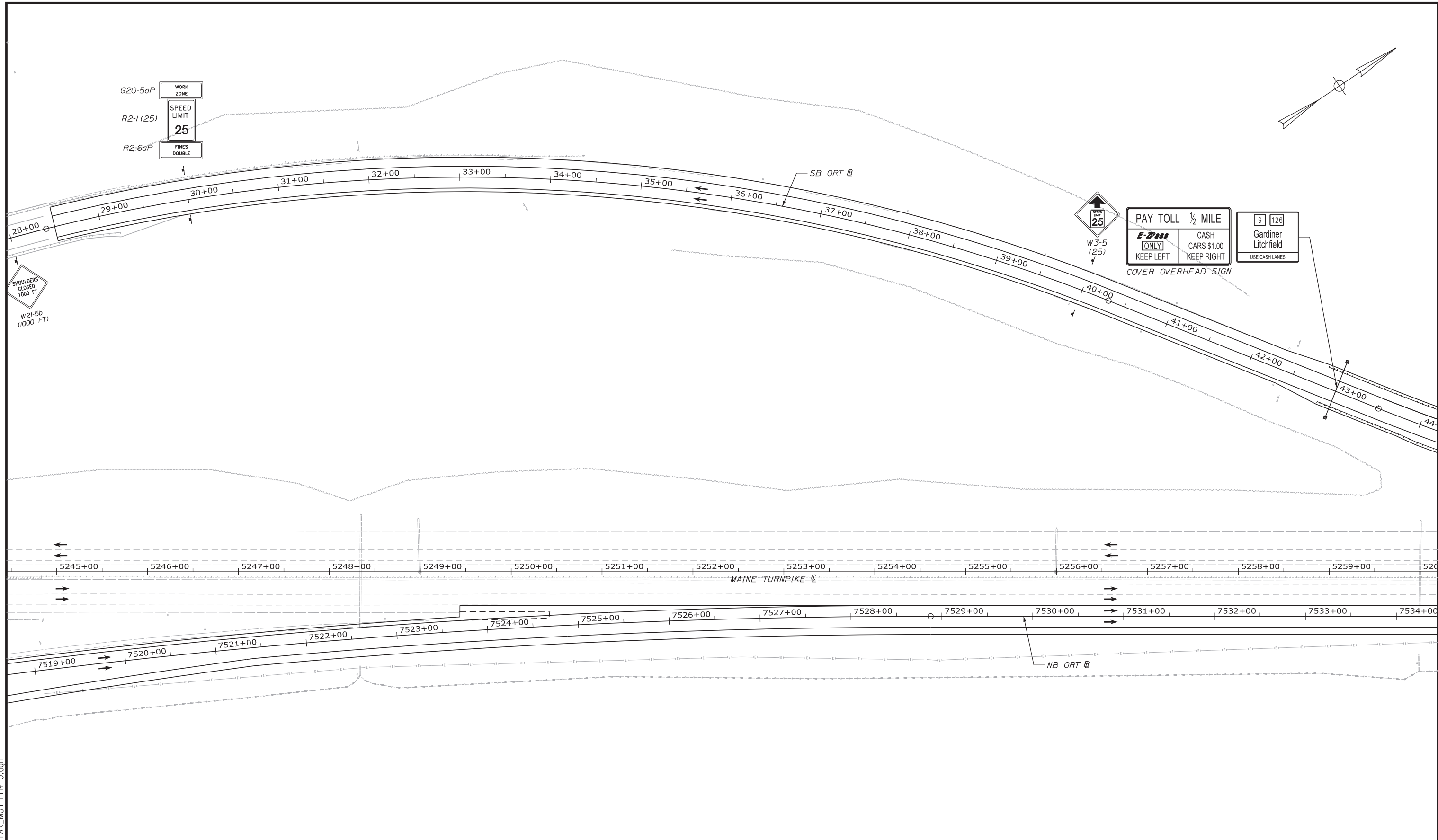
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 4
 MAINTENANCE OF TRAFFIC PLAN 4

SHEET NUMBER: MOT4-03
 CONTRACT: 2019.04
 63 OF 503

Filename: ...:\00.HIGHWAY\MSTA\MOT-PH4-5.dgn



Scale: 50 0 50 100
Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

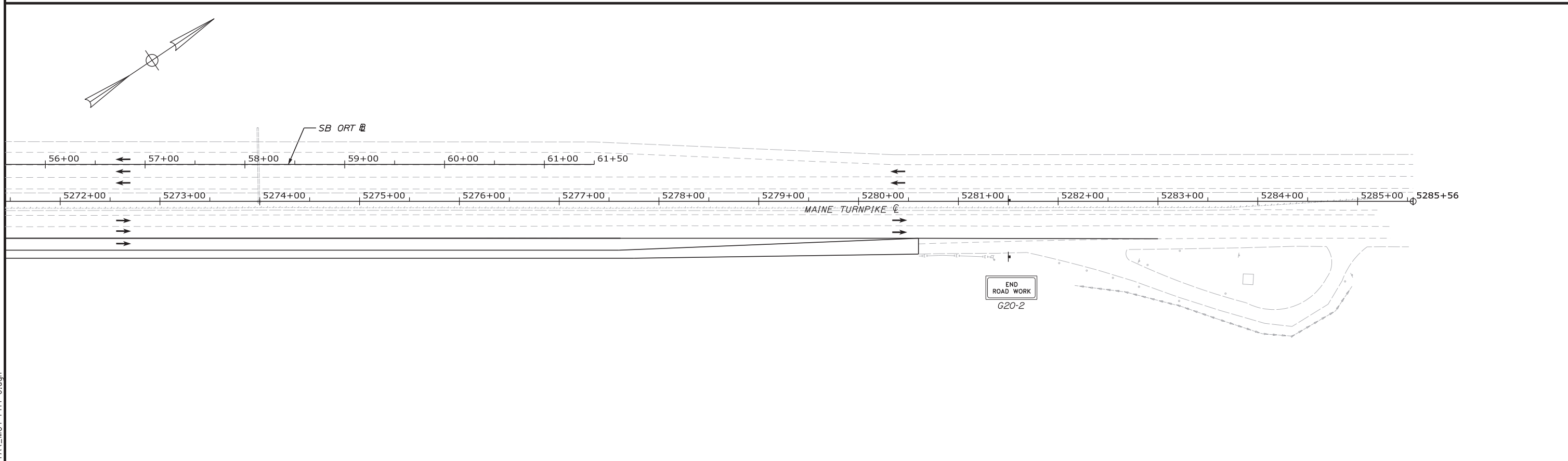
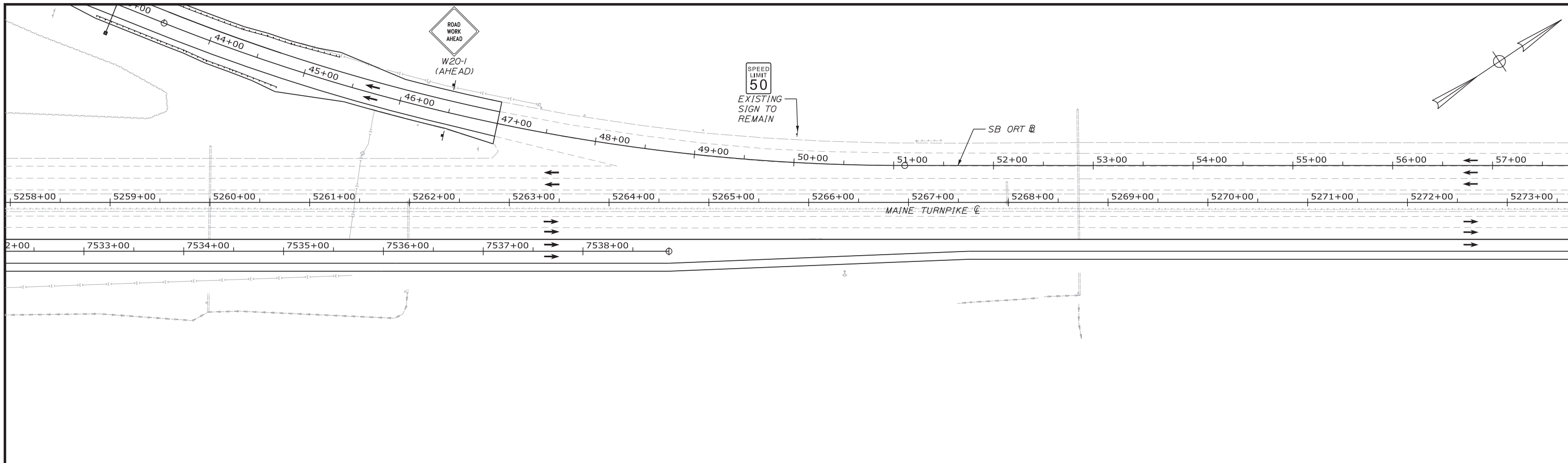
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**THE GOLD STAR
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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 4
MAINTENANCE OF TRAFFIC PLAN 5

SHEET NUMBER: MOT4-04
CONTRACT: 2019.04
64 OF 503



Filename: ...:\00\HIGHWAY\MSTA\MOT-PH4-6.dgn

Scale: Scale of Feet

No.	Revision	By	Date

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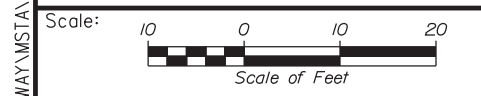
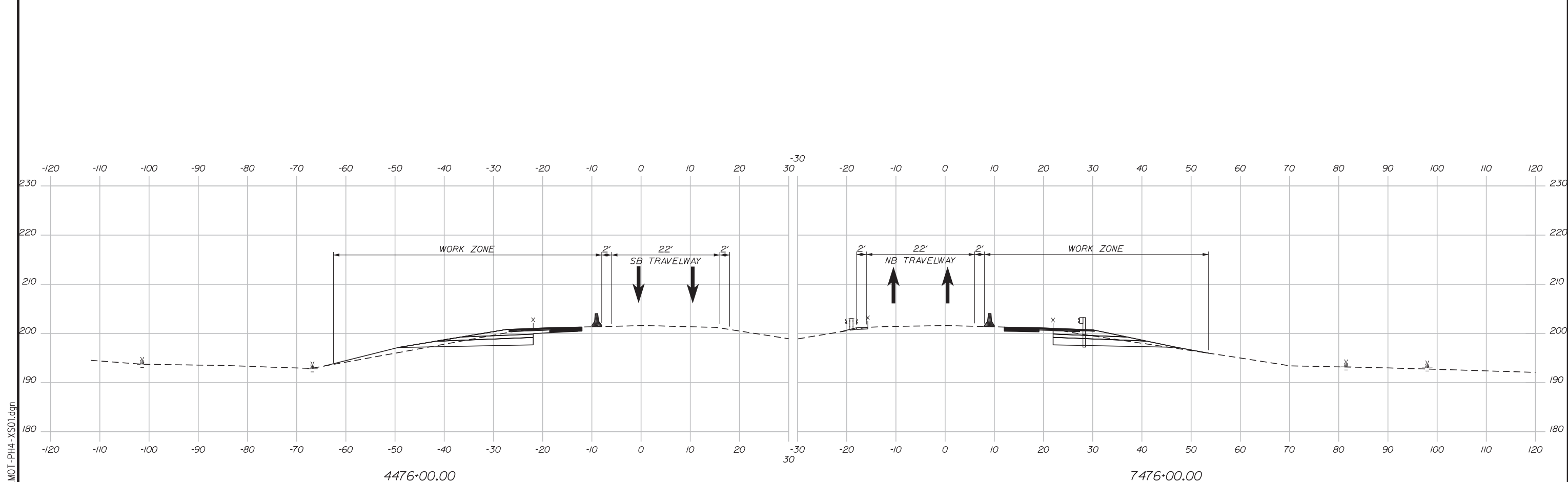
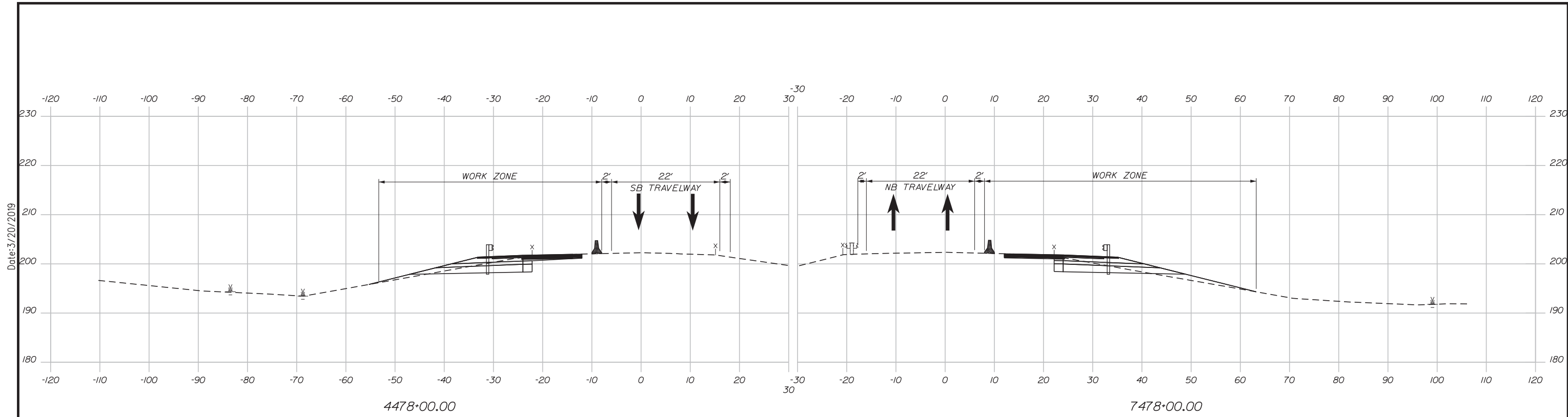


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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 4
 MAINTENANCE OF TRAFFIC PLAN 6

SHEET NUMBER: MOT4-05
 CONTRACT: 2019.04
 65 OF 503



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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 4
MAINTENANCE OF TRAFFIC SECTION 1

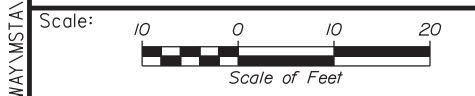
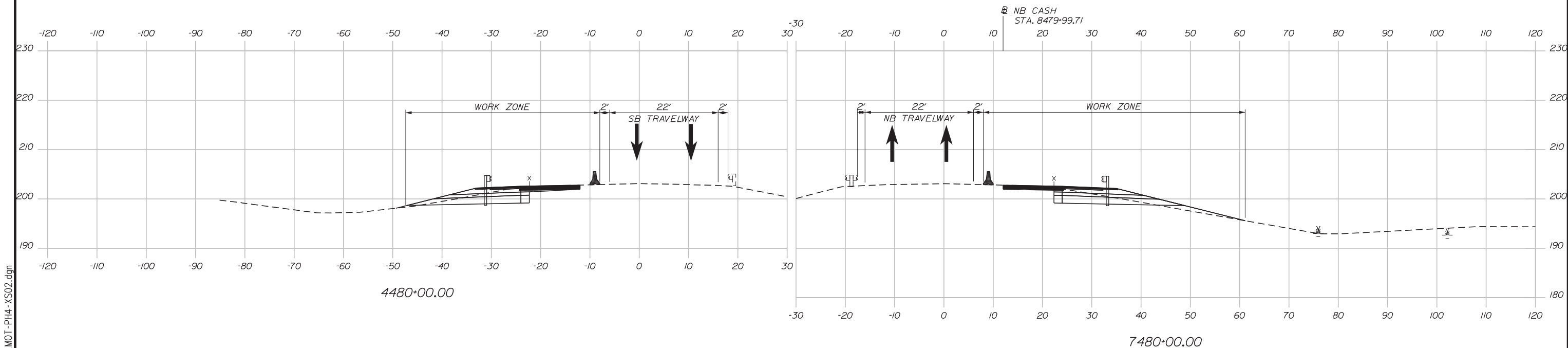
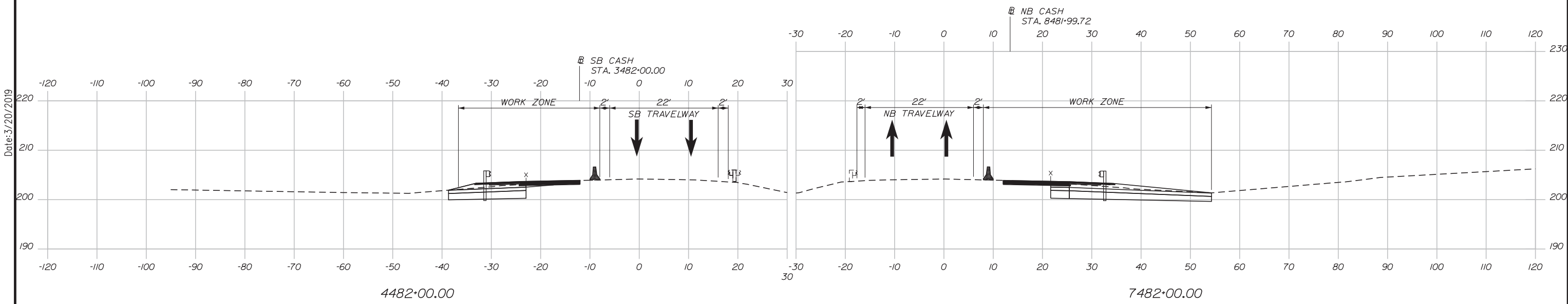
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66 OF 503

CONTRACT: 2019.04

Filename: ... \HIGHWAY\MSTA\MOT-Ph4-XS01.dgn

Date: 3/20/2019

Date: 3/20/2019



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No.	Revision	By	Date

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THE GOLD STAR MEMORIAL HIGHWAY

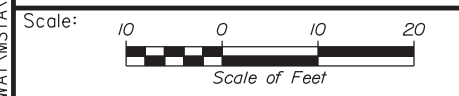
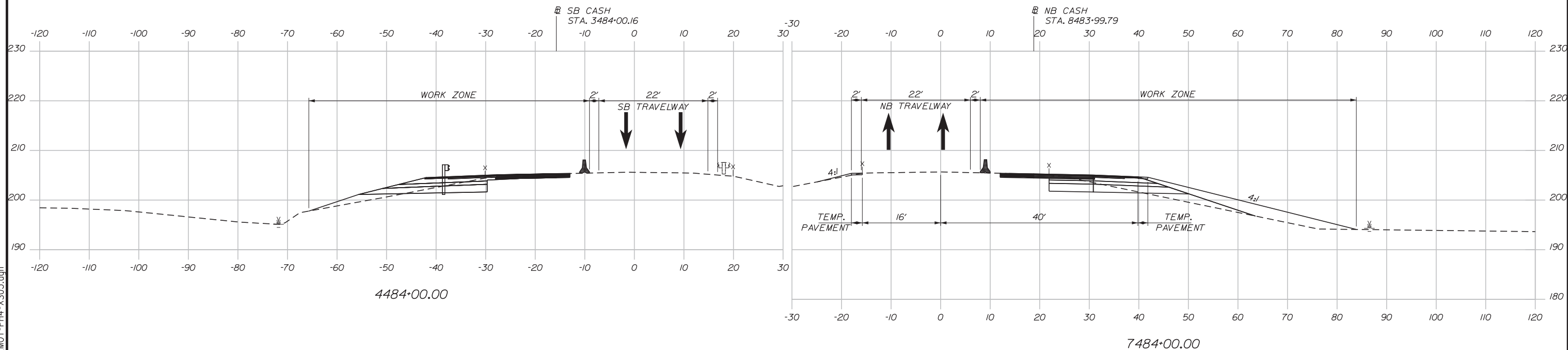
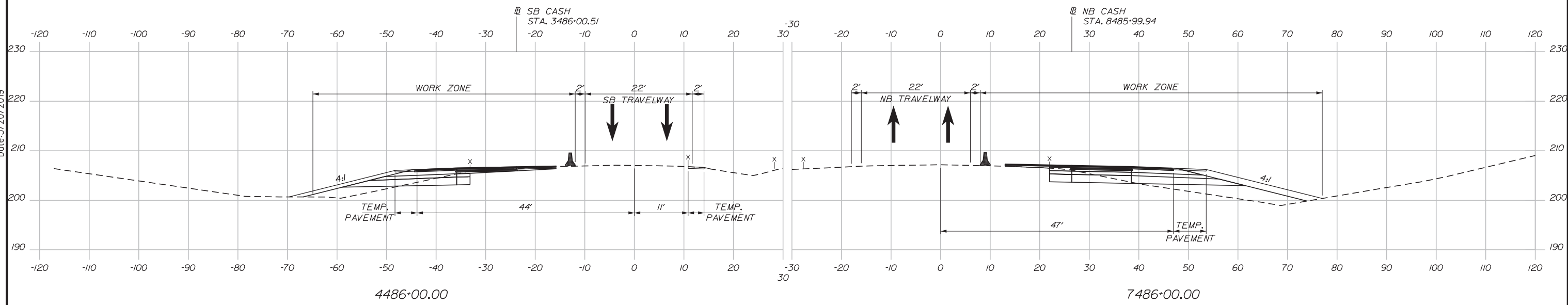
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 4
MAINTENANCE OF TRAFFIC SECTION 2

SHEET NUMBER: MOTXS-34
CONTRACT: 2019.04
67 OF 503

Filename: ... \HIGHWAY\MSTA\...MOT-Ph4-XS02.dgn

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INTERCHANGE 103
 ORT CONVERSION
 PHASE 4
 MAINTENANCE OF TRAFFIC SECTION 3

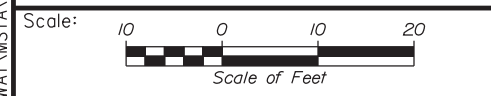
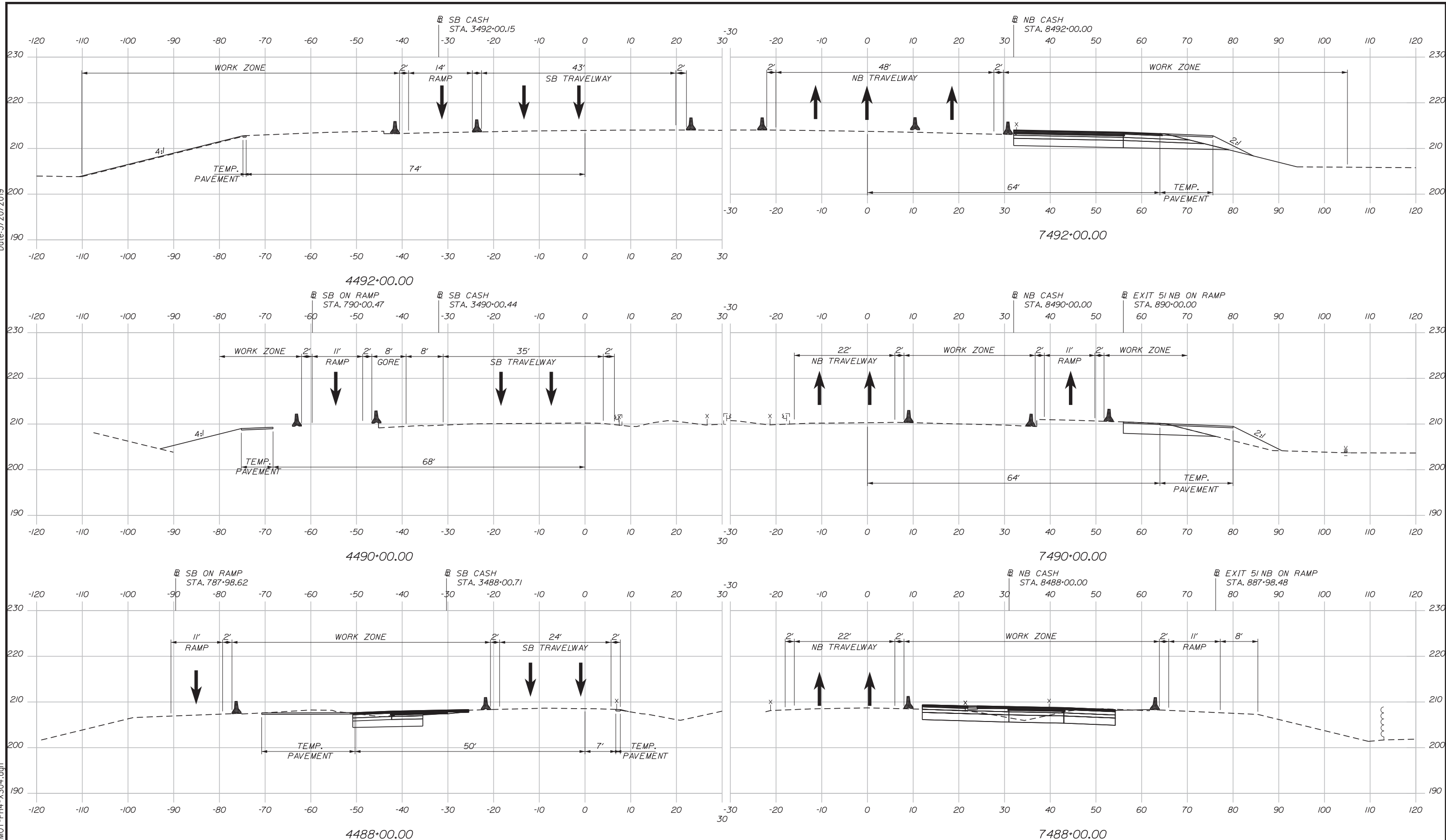
SHEET NUMBER: MOTXS-35
 68 OF 503

CONTRACT: 2019.04

Filename: ... \HIGHWAY\MSTA\...MOT-Ph4-XS03.dgn

Date: 3/20/2019

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No.	Revision	By	Date

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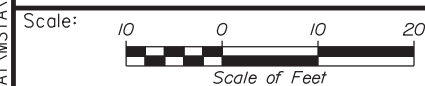
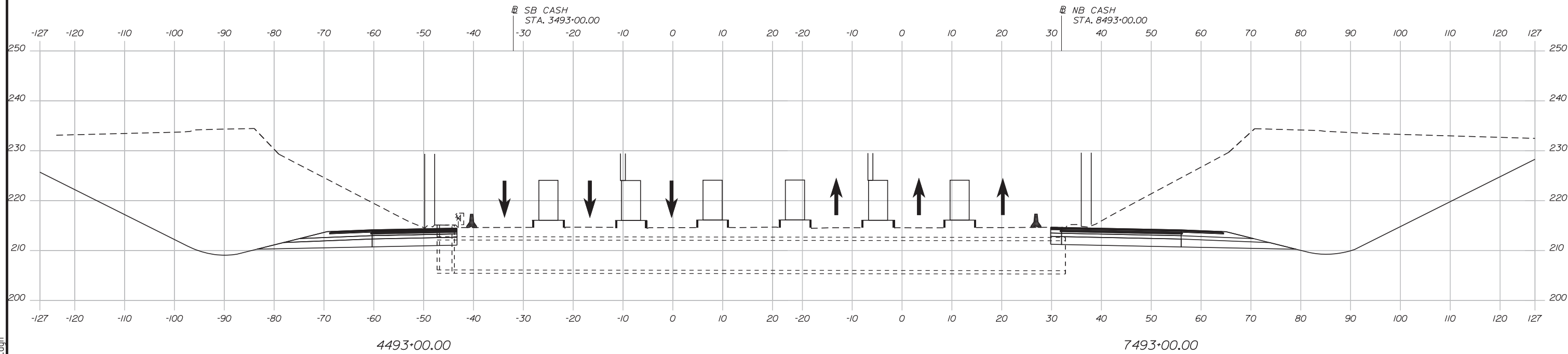
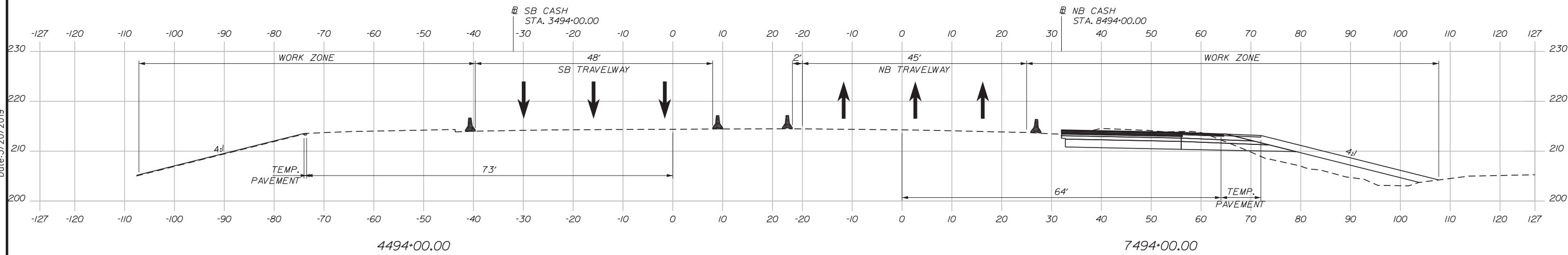
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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 4
MAINTENANCE OF TRAFFIC SECTION 4

SHEET NUMBER: MOTXS-36
CONTRACT: 2019.04
9 OF 50.3

Date: 3/20/2019



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No.	Revision	By	Date

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Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
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**INTERCHANGE 103
 ORT CONVERSION
 PHASE 4
 MAINTENANCE OF TRAFFIC SECTION 5**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

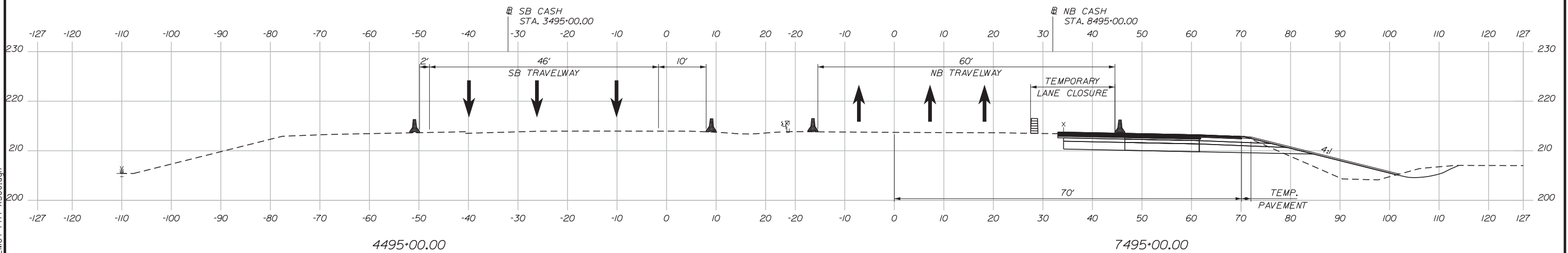
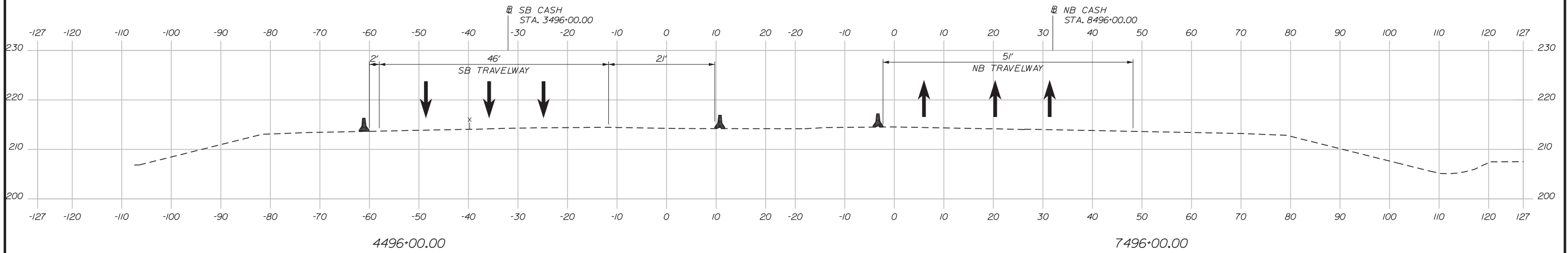
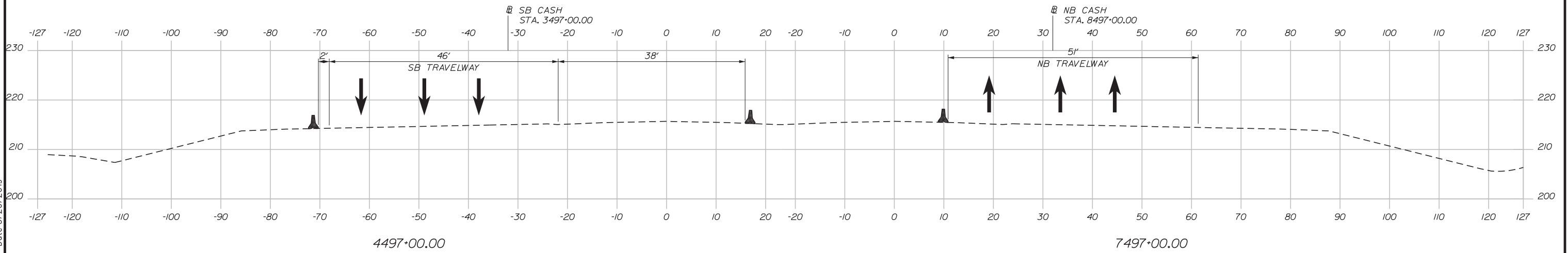
CONTRACT: 2019.04

SHEET NUMBER: MOTXS-37

70 OF 503

Filename: ... \HIGHWAY\MSTA\...MOT-Ph4-XS05.dgn

Date: 3/20/2019



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No.	Revision	By	Date

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 4
 MAINTENANCE OF TRAFFIC SECTION 6

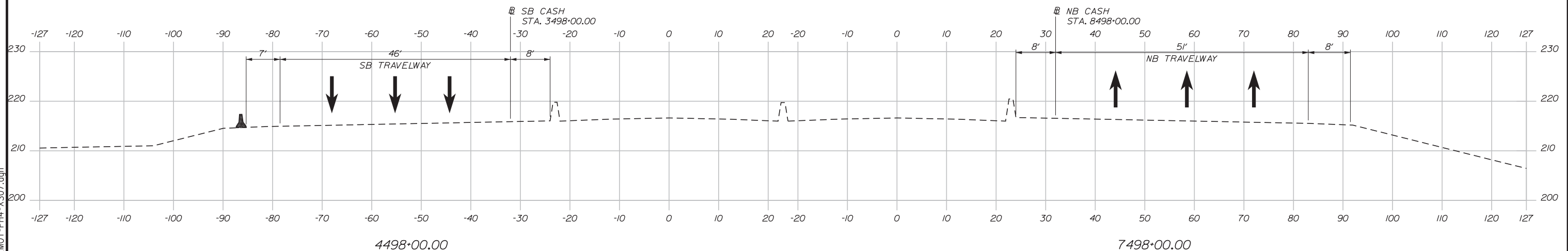
SHEET NUMBER: MOTXS-38
 71 OF 503

CONTRACT: 2019.04

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Date: 3/20/2019

Filename: ... \HIGHWAY\MSTA\...MOT-PH4-XS07.dgn



Scale: 10 0 10 20
Scale of Feet

No.	Revision	By	Date

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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

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Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

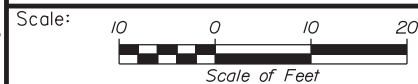
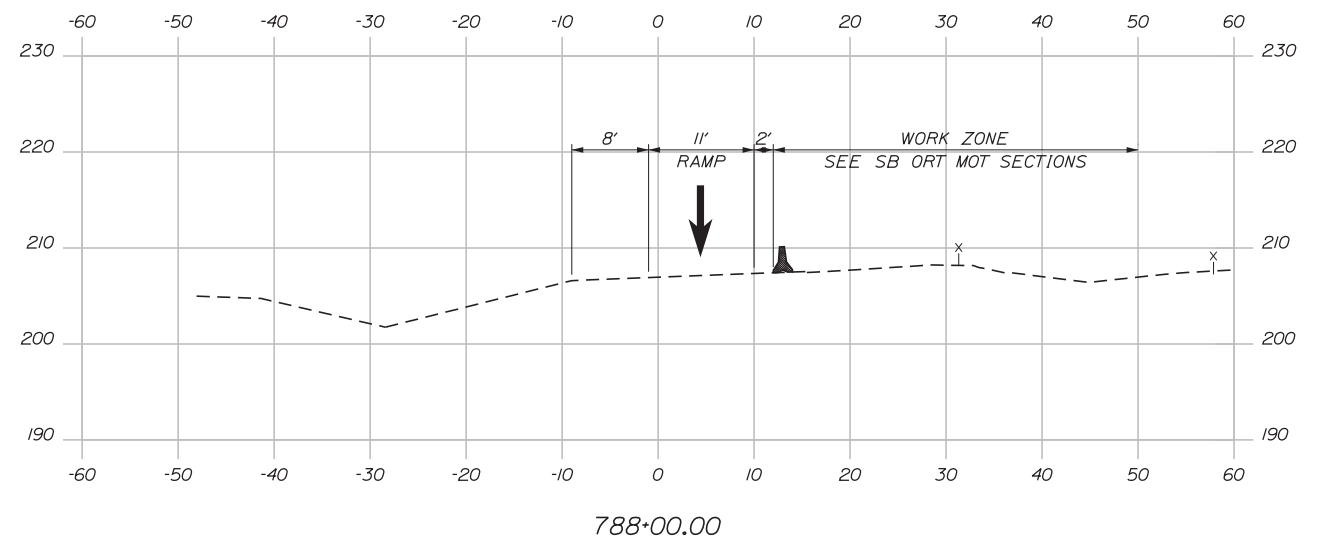
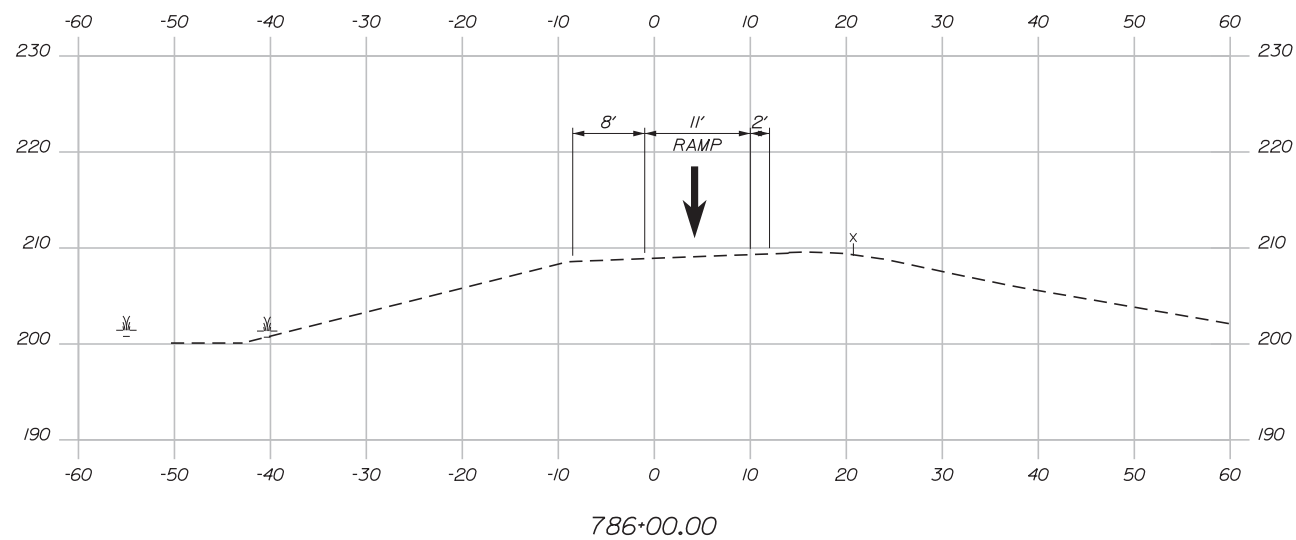
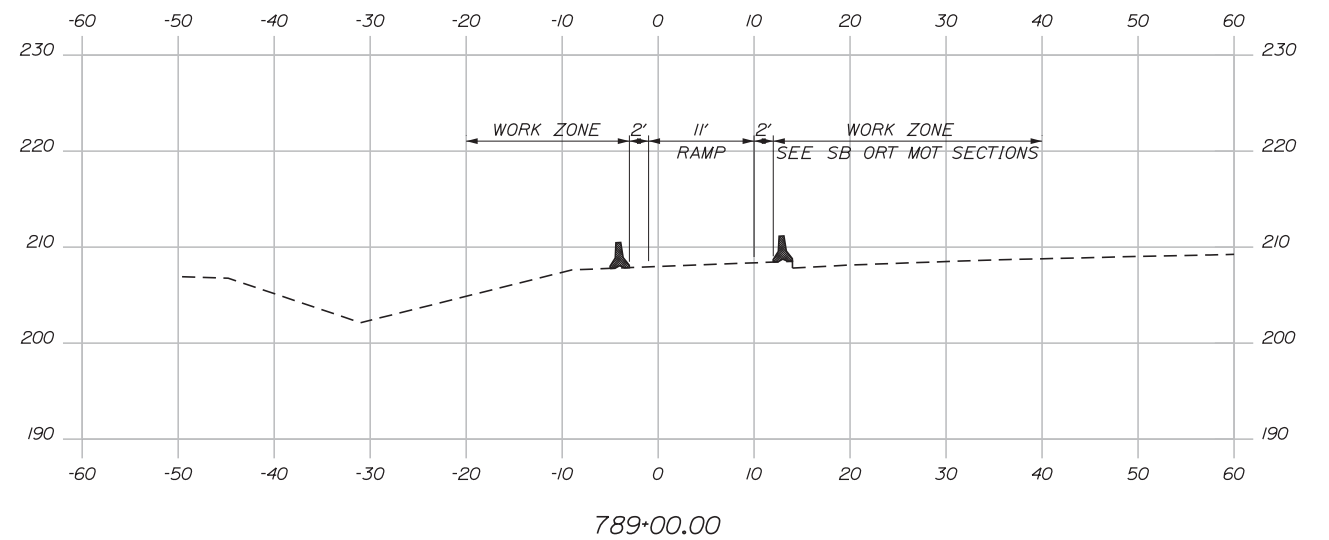
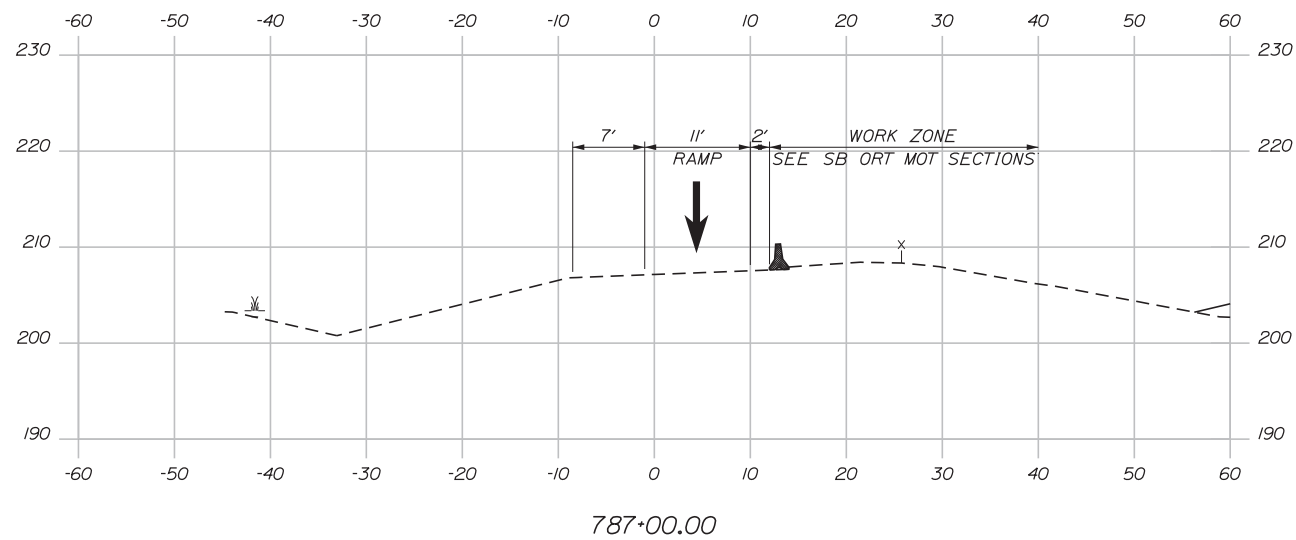
INTERCHANGE 103
ORT CONVERSION
PHASE 4
MAINTENANCE OF TRAFFIC SECTION 7

SHEET NUMBER: MOTXS-39
72 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ... \HIGHWAY\MSTA\MOT-Ph4-XS08.dgn



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INTERCHANGE 103
 ORT CONVERSION
 PHASE 4
 MAINTENANCE OF TRAFFIC SECTION 8

No.	Revision	By	Date

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	By	Date	
Designed	PLP	3\20\19	Checked LEM 3\20\19
Drawn	EJB	3\20\19	In Charge of GAE 3\20\19

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

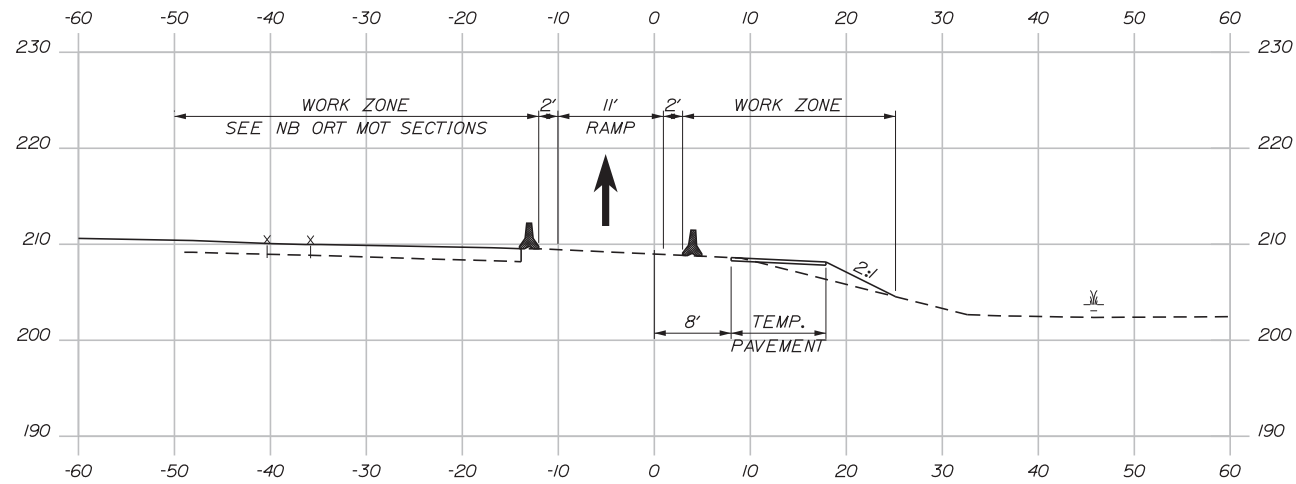
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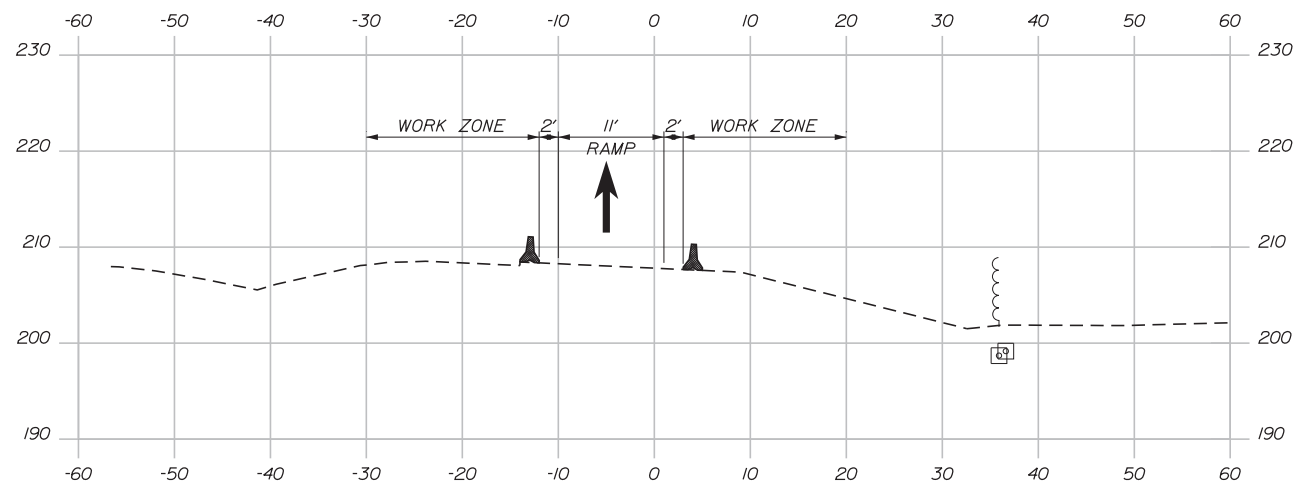
73 OF 503

Date: 3/20/2019

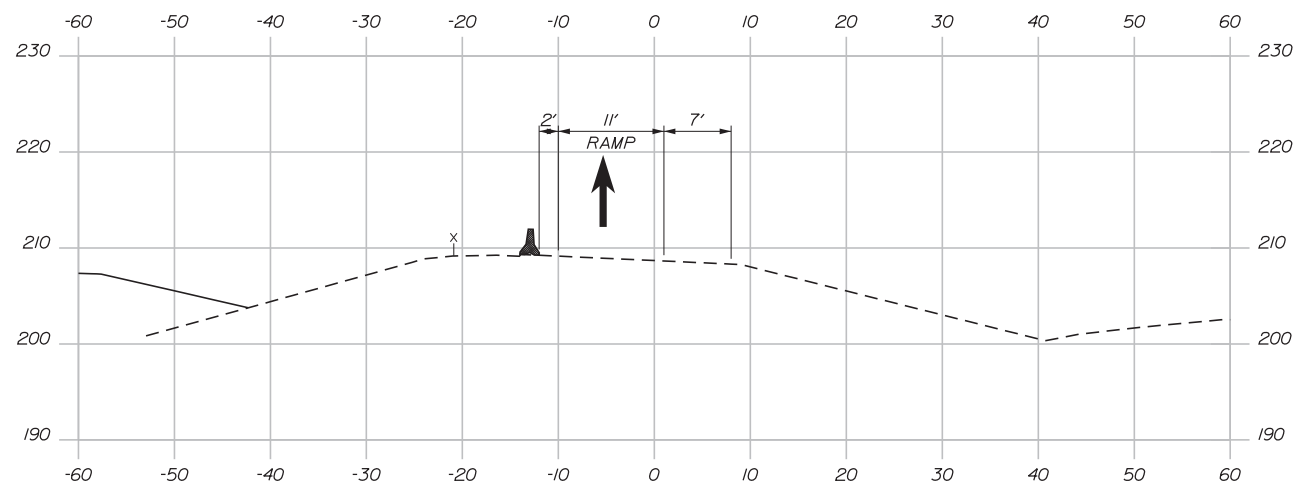
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889+00.00



888+00.00



887+00.00



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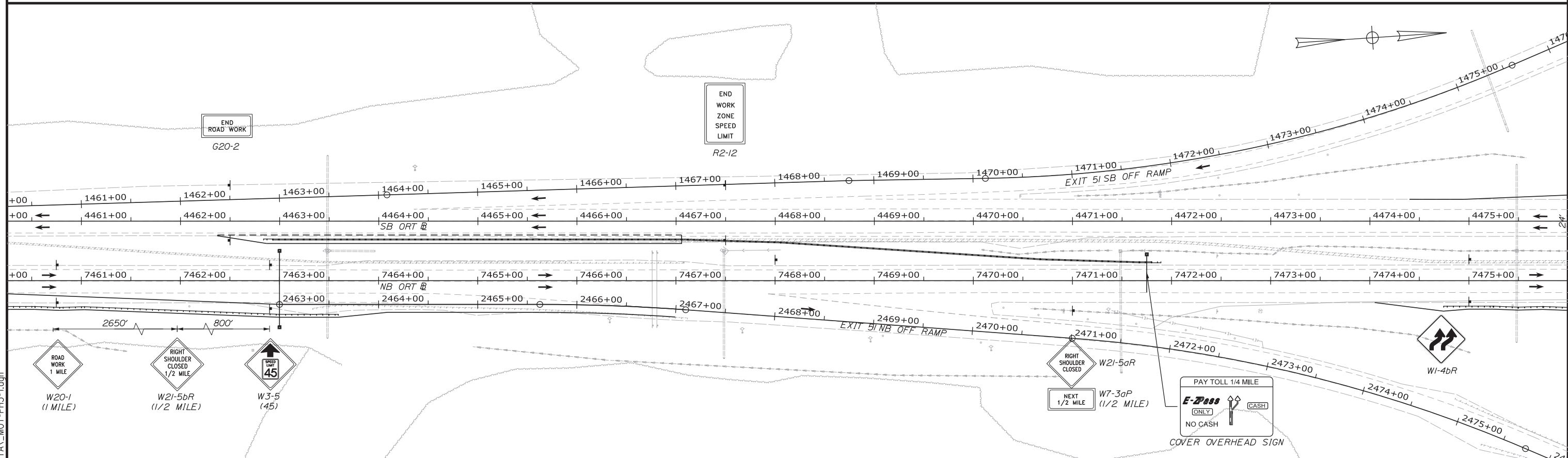
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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 4
 MAINTENANCE OF TRAFFIC SECTION 9

SHEET NUMBER: MOTXS-41
 CONTRACT: 2019.04
 74 OF 503

Filename: ...:\00\HIGHWAY\MSTA\MOT-PH5-1.dgn



Scale: 50 0 50 100
Scale of Feet

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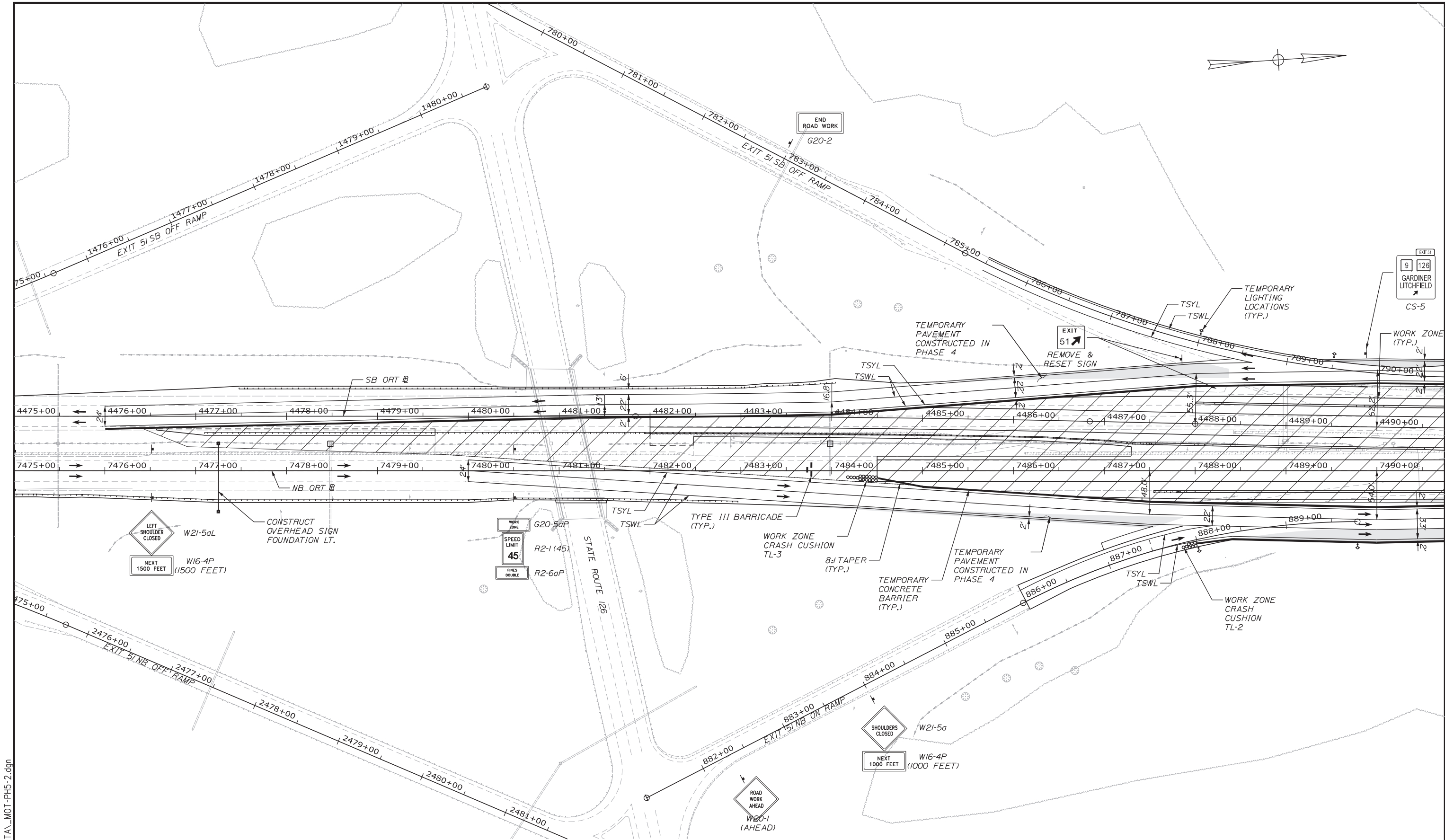
**THE GOLD STAR
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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 5
MAINTENANCE OF TRAFFIC PLAN 1

SHEET NUMBER: MOT5-01
75 OF 503

CONTRACT: 2019.04



Scale: 50 0 50 100
Scale of Feet

No.	Revision	By	Date

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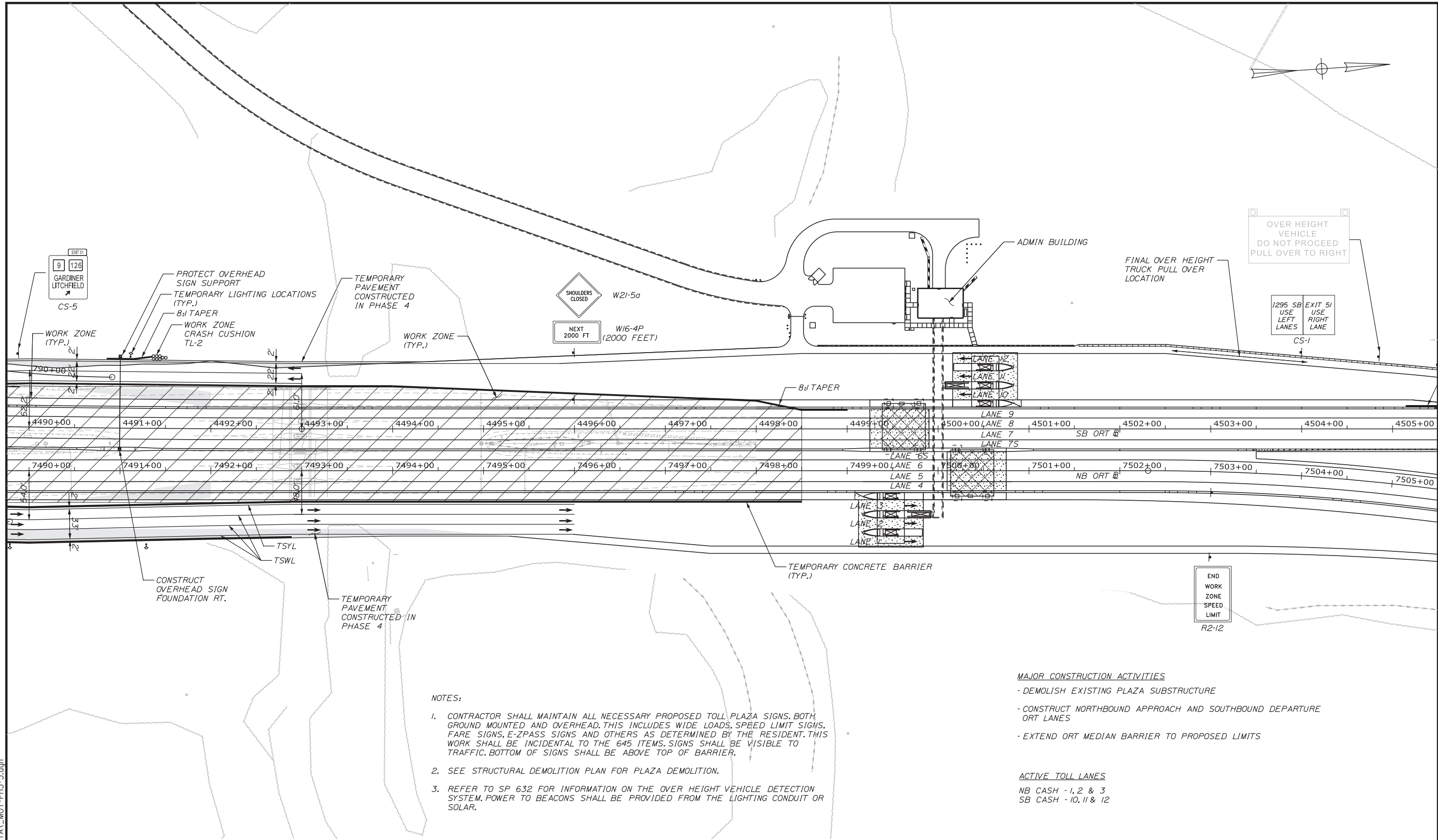
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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 5
MAINTENANCE OF TRAFFIC PLAN 2

SHEET NUMBER: MOT5-02
9 OF 128
CONTRACT: 2019.04
76 OF 503

Filename: ...:\00\HIGHWAY\MSTA\MOT-PH5-2.dgn



NOTES:

1. CONTRACTOR SHALL MAINTAIN ALL NECESSARY PROPOSED TOLL PLAZA SIGNS, BOTH GROUND MOUNTED AND OVERHEAD, THIS INCLUDES WIDE LOADS, SPEED LIMIT SIGNS, FARE SIGNS, E-ZPASS SIGNS AND OTHERS AS DETERMINED BY THE RESIDENT. THIS WORK SHALL BE INCIDENTAL TO THE 645 ITEMS. SIGNS SHALL BE VISIBLE TO TRAFFIC. BOTTOM OF SIGNS SHALL BE ABOVE TOP OF BARRIER.
2. SEE STRUCTURAL DEMOLITION PLAN FOR PLAZA DEMOLITION.
3. REFER TO SP 632 FOR INFORMATION ON THE OVER HEIGHT VEHICLE DETECTION SYSTEM. POWER TO BEACONS SHALL BE PROVIDED FROM THE LIGHTING CONDUIT OR SOLAR.

MAJOR CONSTRUCTION ACTIVITIES

- DEMOLISH EXISTING PLAZA SUBSTRUCTURE
- CONSTRUCT NORTHBOUND APPROACH AND SOUTHBOUND DEPARTURE ORT LANES
- EXTEND ORT MEDIAN BARRIER TO PROPOSED LIMITS

ACTIVE TOLL LANES

- NB CASH - 1, 2 & 3
- SB CASH - 10, 11 & 12

Scale: 50 0 50 100
Scale of Feet

No.	Revision	By	Date

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By	Date	By	Date
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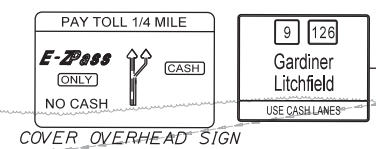
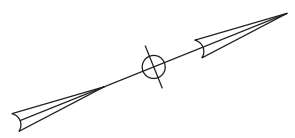
**THE GOLD STAR
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INTERCHANGE 103
ORT CONVERSION
PHASE 5
MAINTENANCE OF TRAFFIC PLAN 3

SHEET NUMBER: MOT5-02
CONTRACT: 2019.04
77 OF 503

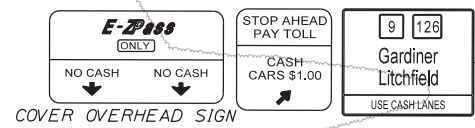
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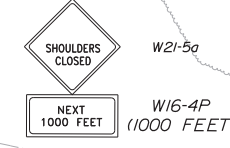
OVER HEIGHT VEHICLE DO NOT PROCEED MUST EXIT

300' LANE SHIFT DESIGN SPEED = 50 MPH

DRUMS @ 80' SPACING



COVER OVERHEAD SIGN



50' MIN BARRIER OVERLAP WITH PERMANENT BARRIER (SEE NOTES)

TSWL

TL-2 WORK ZONE CRASH CUSHION

TEMPORARY CONCRETE BARRIER (TYP.)

8:1 TAPER

TSYL

TSWL

R=3670'

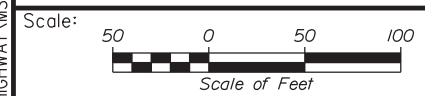
STA. 7514+00, 12.00 RT. PT

STA. 7508+83.00, 37.28 RT. PC

END ROAD WORK G20-2

NOTES:
1. WORK ASSOCIATED WITH THE OVERHEAD SIGN STRUCTURES AT STA. 4508+75 AND STA. 27+00 SHALL BE COMPLETED PRIOR TO PHASE 6.

Filename: ...:\00.HIGHWAY\MSTA\MOT-PH5-4.dgn



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	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19



THE GOLD STAR MEMORIAL HIGHWAY

INTERCHANGE 103
ORT CONVERSION
PHASE 5
MAINTENANCE OF TRAFFIC PLAN 4

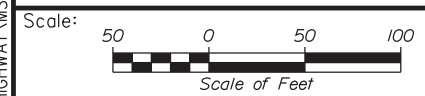
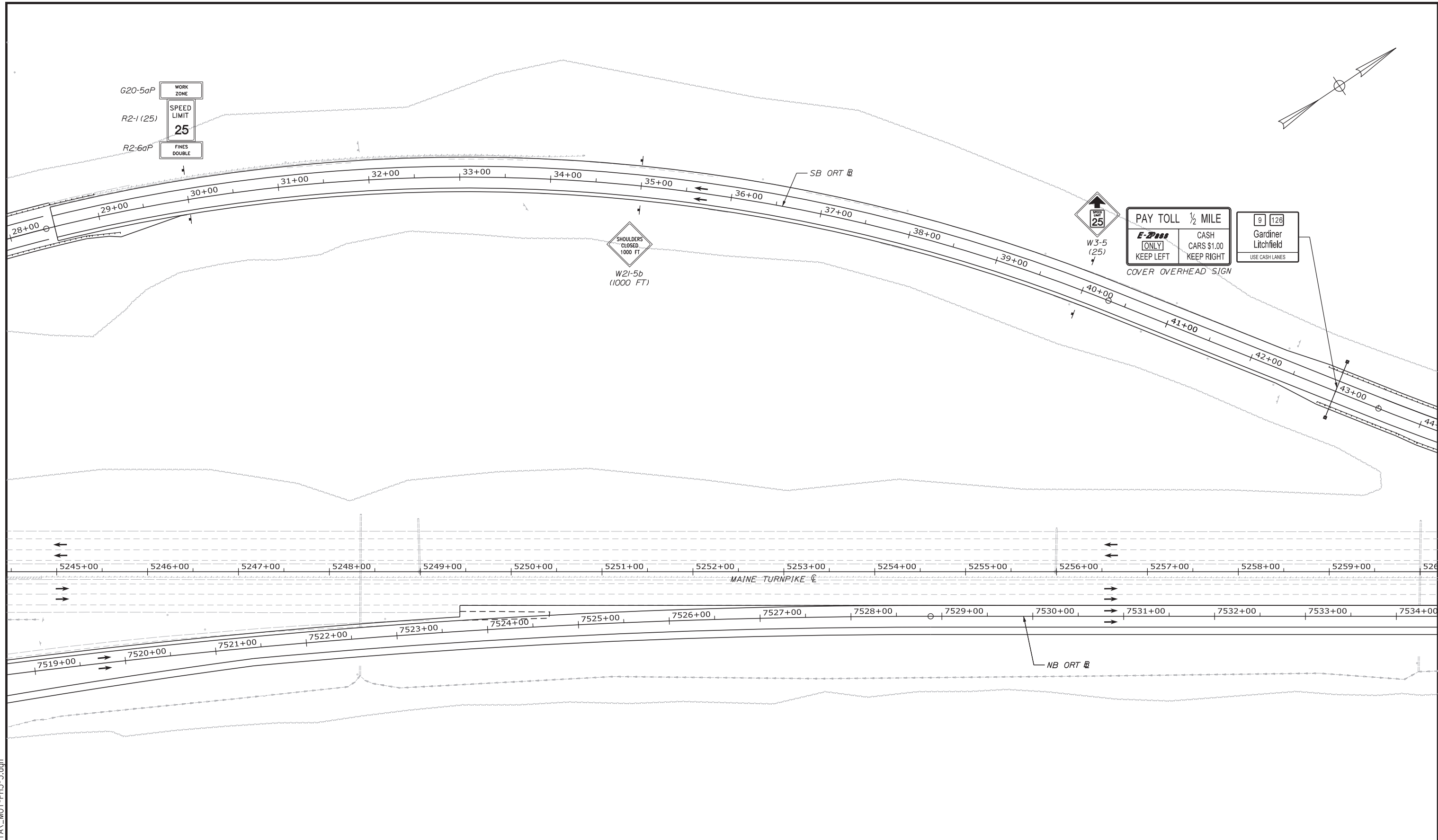
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

CONTRACT:2019.04

SHEET NUMBER: MOT5-03

78 OF 503

Filename: ...\\00.HIGHWAY\MSTA\MOT-PH5-5.dgn



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Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

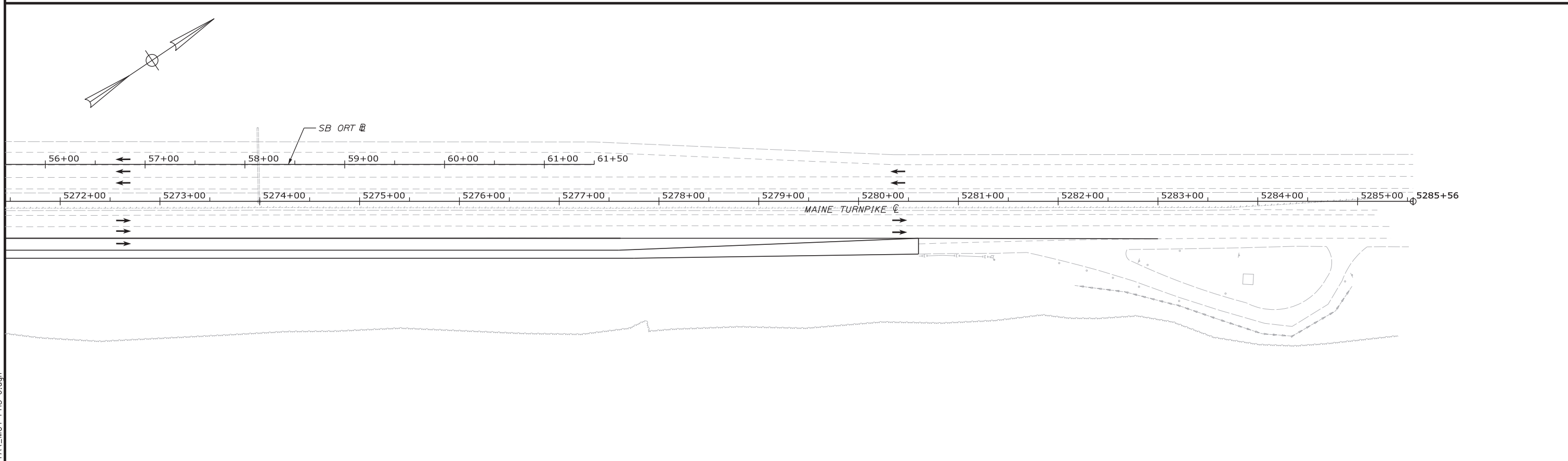
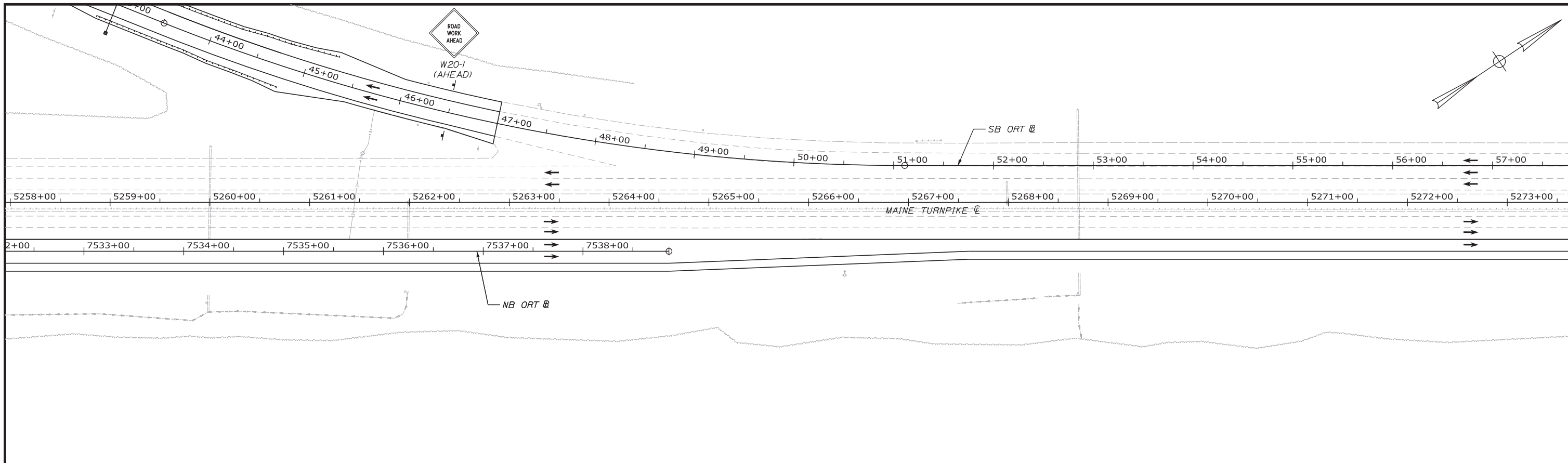
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**THE GOLD STAR
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INTERCHANGE 103
 ORT CONVERSION
 PHASE 5
 MAINTENANCE OF TRAFFIC PLAN 5

SHEET NUMBER: MOT5-04
 CONTRACT:2019.04
 79 OF 503



Filename: ...:\00\HIGHWAY\MSTA\MOT-PH5-6.dgn

Scale: Scale of Feet

No.	Revision	By	Date

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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
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Designed	PLP 3\20\19	Checked	LEM 3\20\19
Drawn	EJB 3\20\19	In Charge of	GAE 3\20\19

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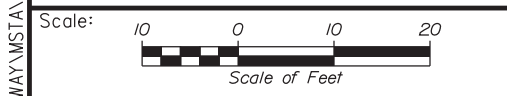
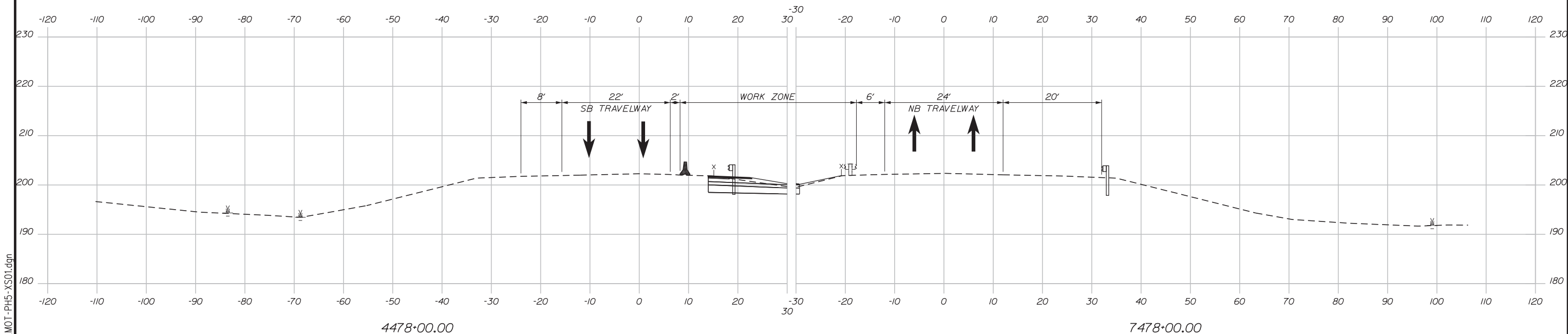
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 5
 MAINTENANCE OF TRAFFIC PLAN 6

SHEET NUMBER: MOT5-05
 80 OF 503

CONTRACT: 2019.04

Date: 3/20/2019



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No.	Revision	By	Date

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Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 5
MAINTENANCE OF TRAFFIC SECTION 1

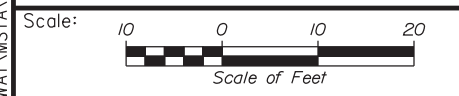
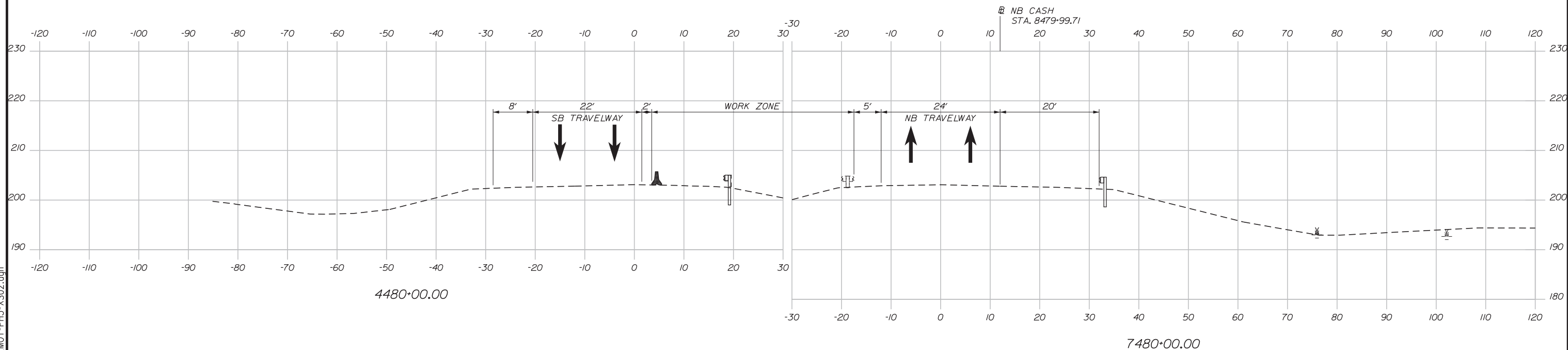
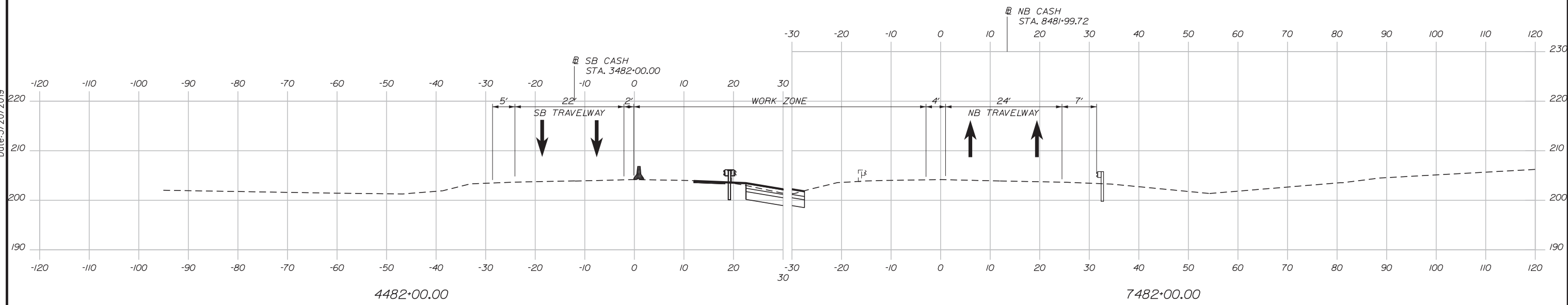
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81 OF 503

CONTRACT: 2019.04

Filename: ... \HIGHWAY\MSTA\...MOT-Ph5-XS01.dgn

Date: 3/20/2019

Filename: ... \HIGHWAY\MSTA\MOT-Ph5-XS02.dgn



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Designed	PLP	3\20\19	Checked	LEM	3\20\19
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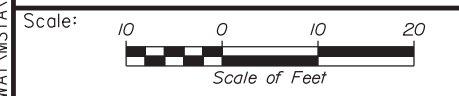
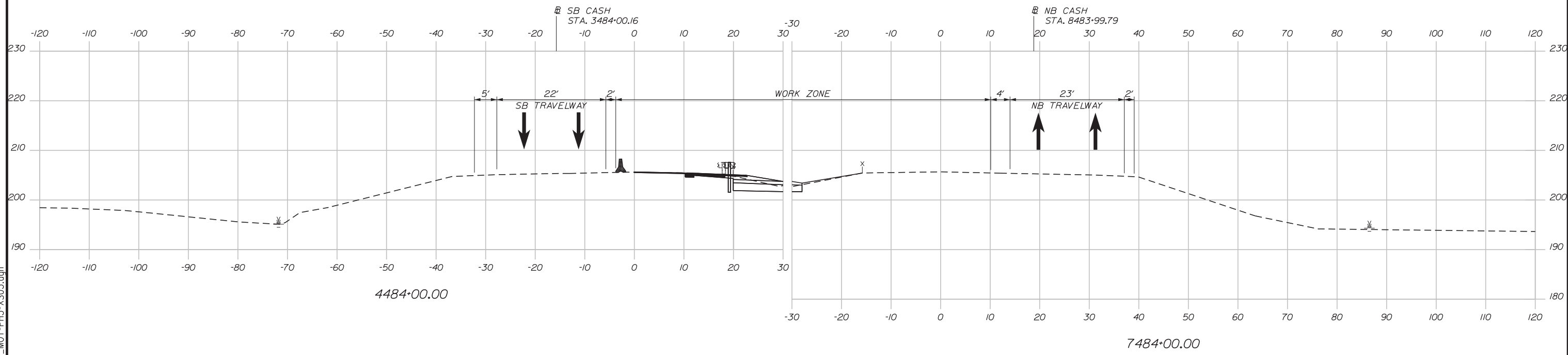
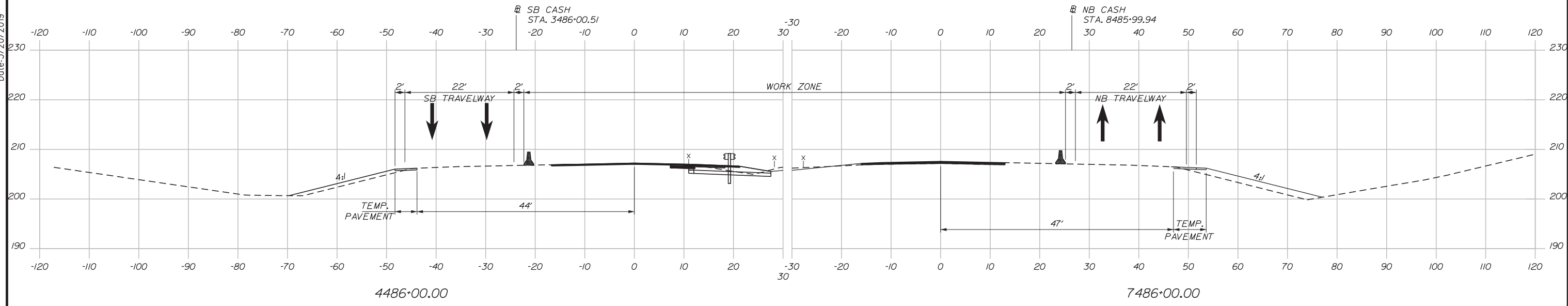
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 5
MAINTENANCE OF TRAFFIC SECTION 2

SHEET NUMBER: MOTXS-43
CONTRACT: 2019.04
82 OF 503

Date: 3/20/2019



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Designed	PLP	3\20\19	Checked	LEM	3\20\19
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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 5
 MAINTENANCE OF TRAFFIC SECTION 3

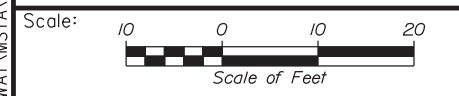
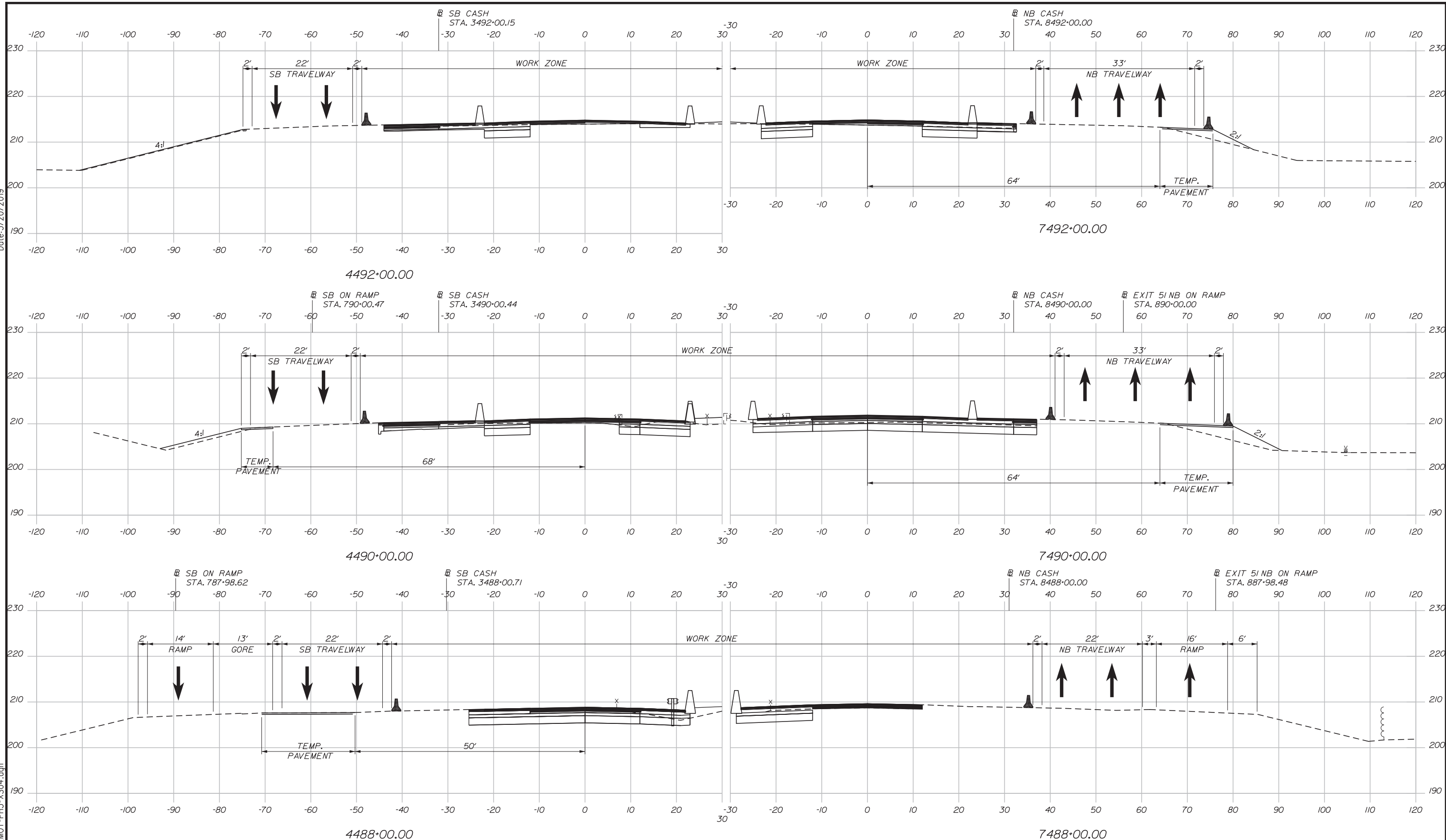
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 83 OF 503

CONTRACT: 2019.04

Filename: ... \HIGHWAY\MSTA\...MOT-PH5-XS03.dgn

Date: 3/20/2019

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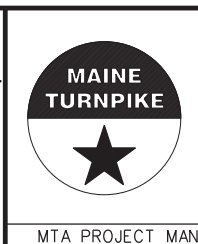
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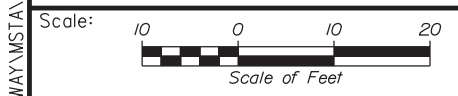
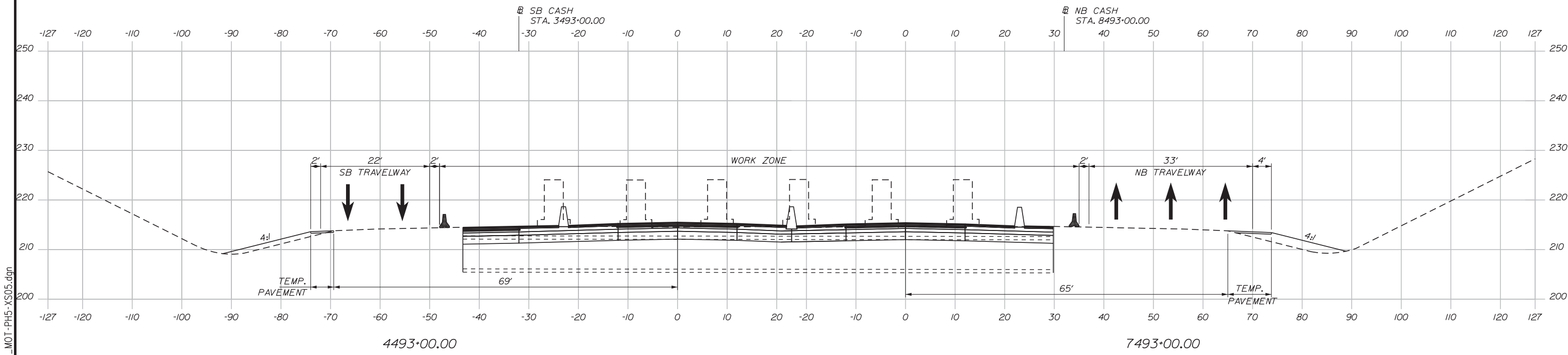
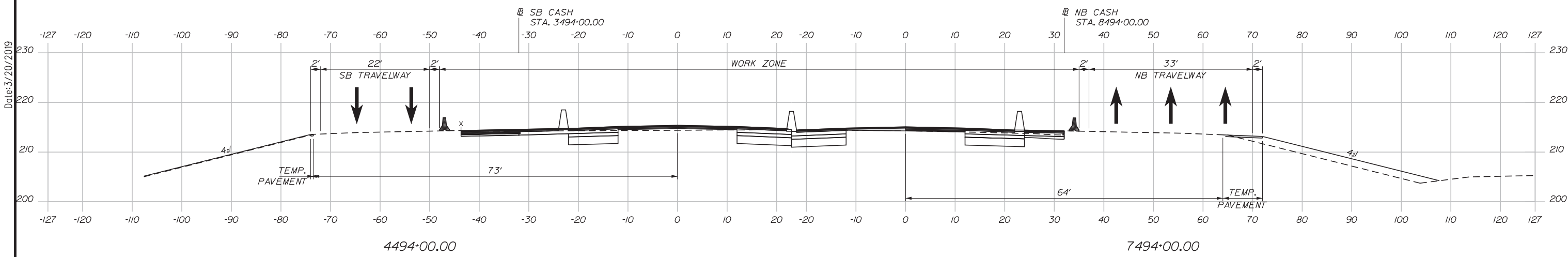
INTERCHANGE 103
ORT CONVERSION
PHASE 5
MAINTENANCE OF TRAFFIC SECTION 4

SHEET NUMBER: MOTXS-45
84 OF 503

CONTRACT: 2019.04

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

Date: 3/20/2019



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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

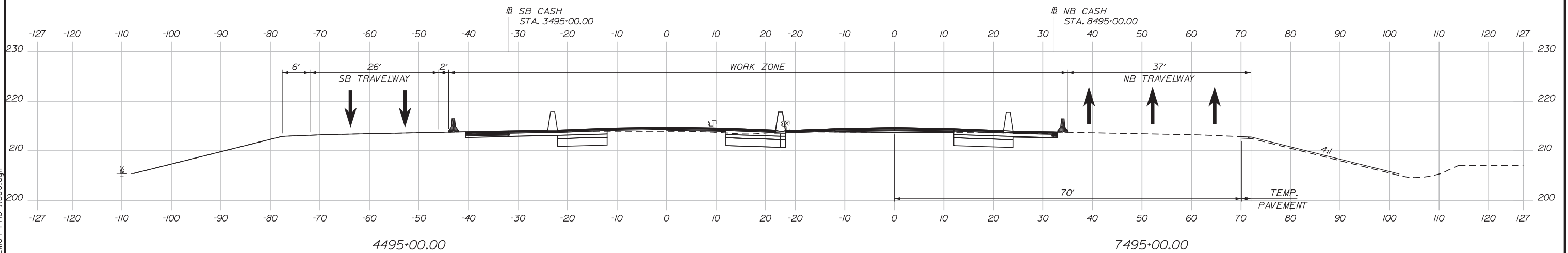
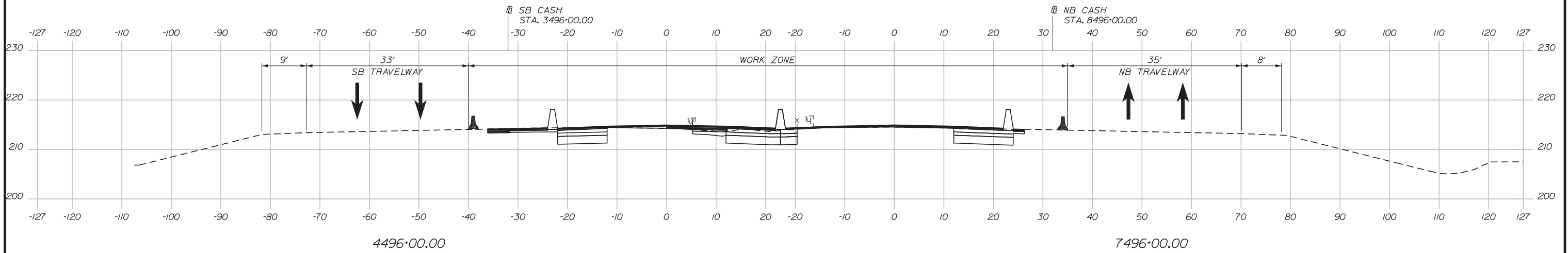
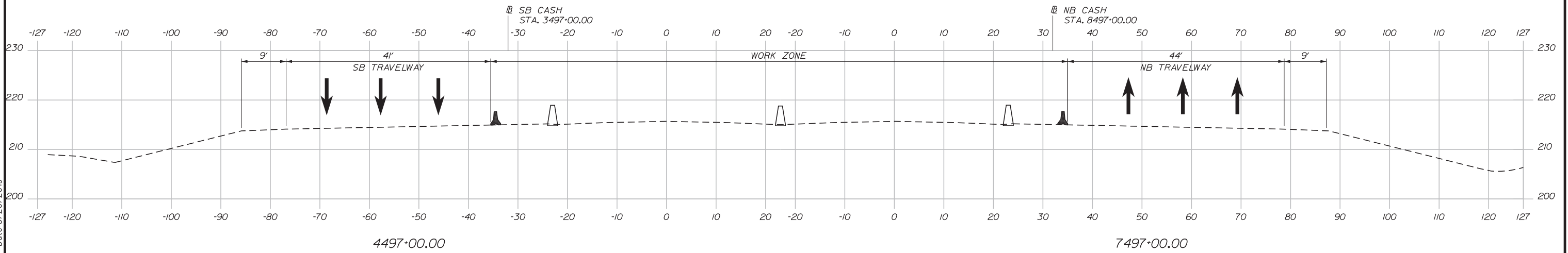
INTERCHANGE 103
 ORT CONVERSION
 PHASE 5
 MAINTENANCE OF TRAFFIC SECTION 5

SHEET NUMBER: MOTXS-46
 85 OF 503

CONTRACT: 2019.04

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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 5
 MAINTENANCE OF TRAFFIC SECTION 6

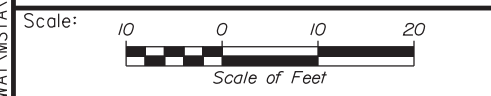
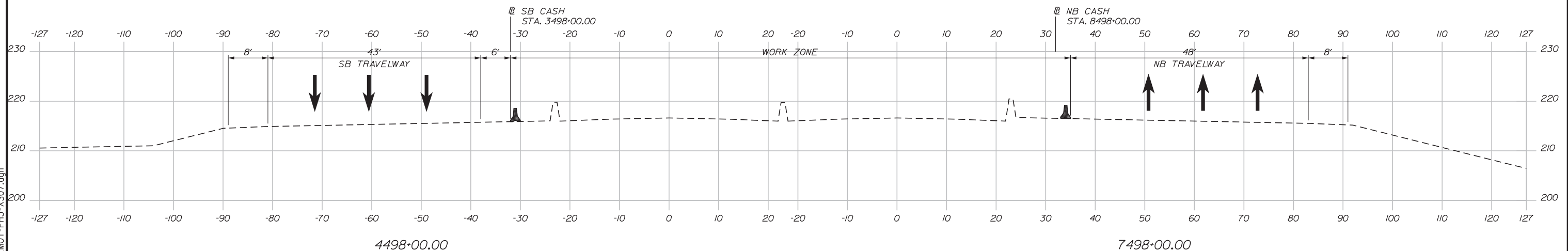
SHEET NUMBER: MOTXS-47
 86 OF 503

CONTRACT: 2019.04

Filename: ... \HIGHWAY\MSTA\...MOT-PH5-XS06.dgn

Date: 3/20/2019

Filename: ... \HIGHWAY\MSTA\...MOT-PH5-XS07.dgn



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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
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Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
MEMORIAL HIGHWAY**

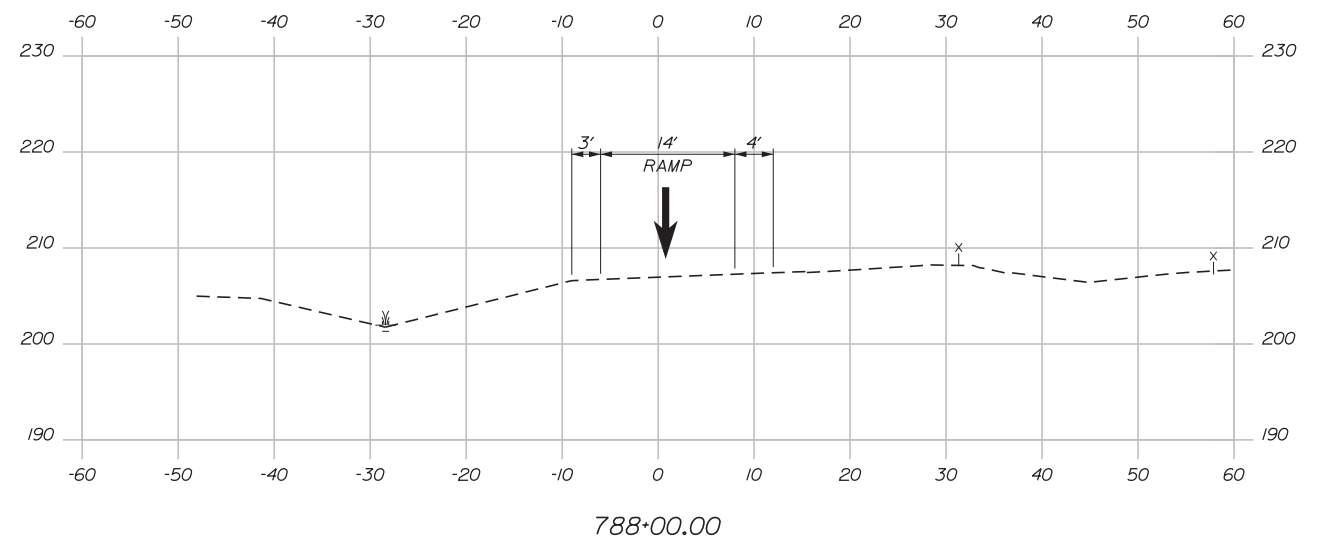
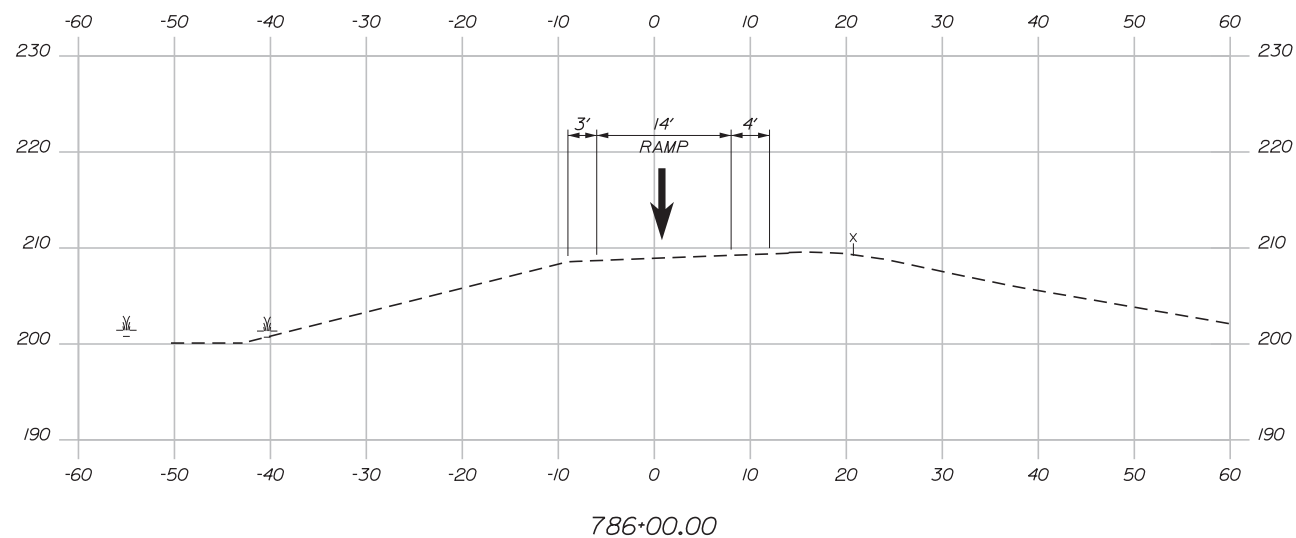
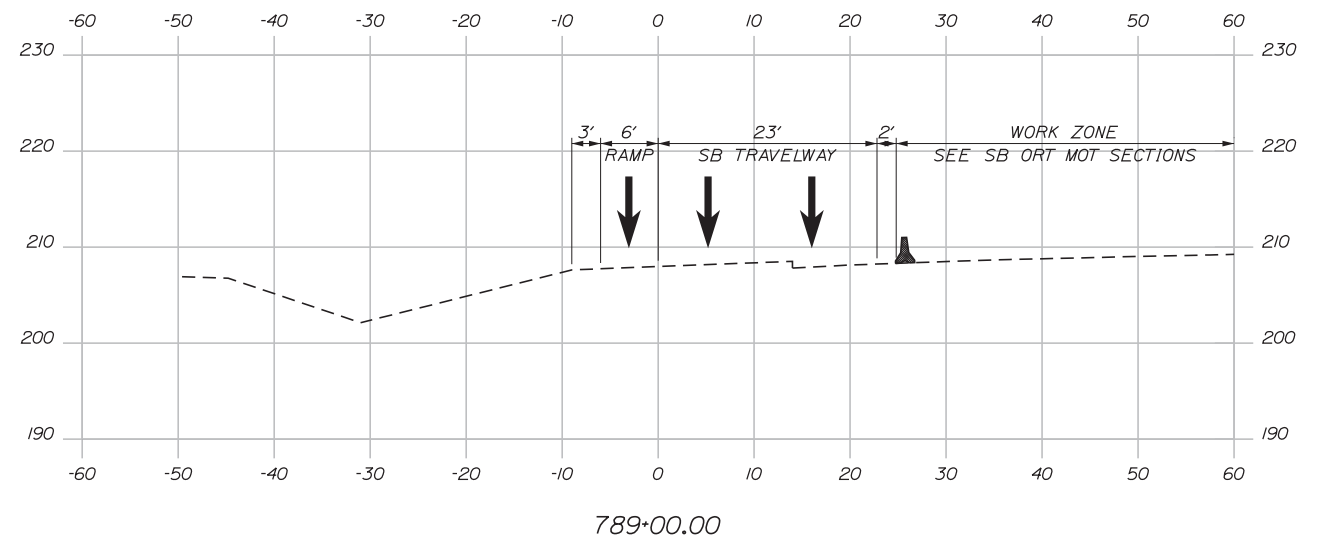
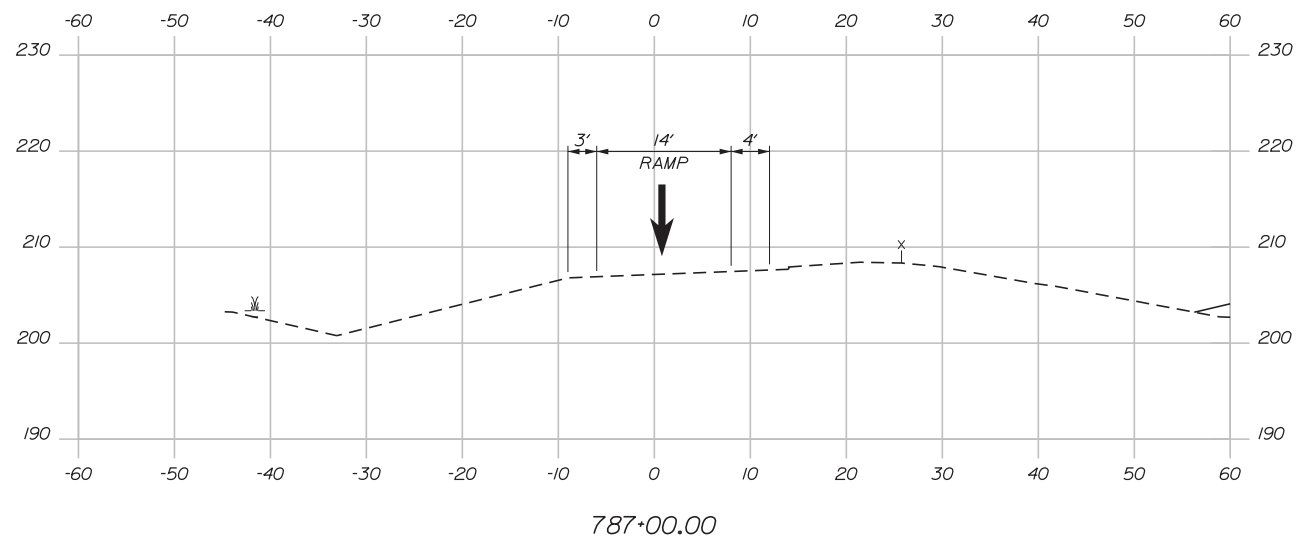
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INTERCHANGE 103
ORT CONVERSION
PHASE 5
MAINTENANCE OF TRAFFIC SECTION 7

SHEET NUMBER: MOTXS-48
CONTRACT: 2019.04
87 OF 503

Date: 3/20/2019

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INTERCHANGE 103
 ORT CONVERSION
 PHASE 5
 MAINTENANCE OF TRAFFIC SECTION 8

No.	Revision	By	Date

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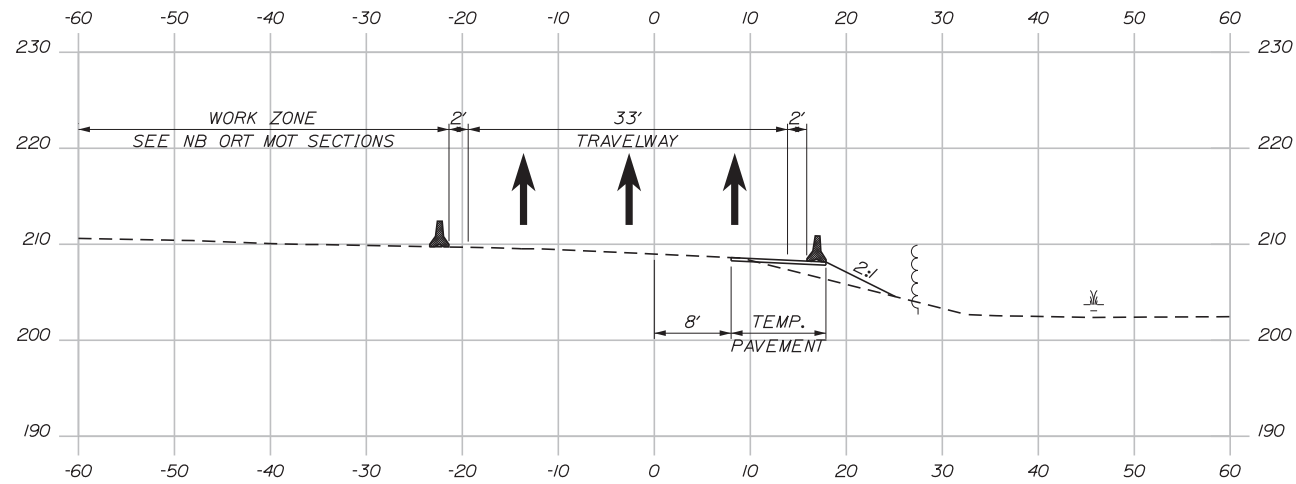
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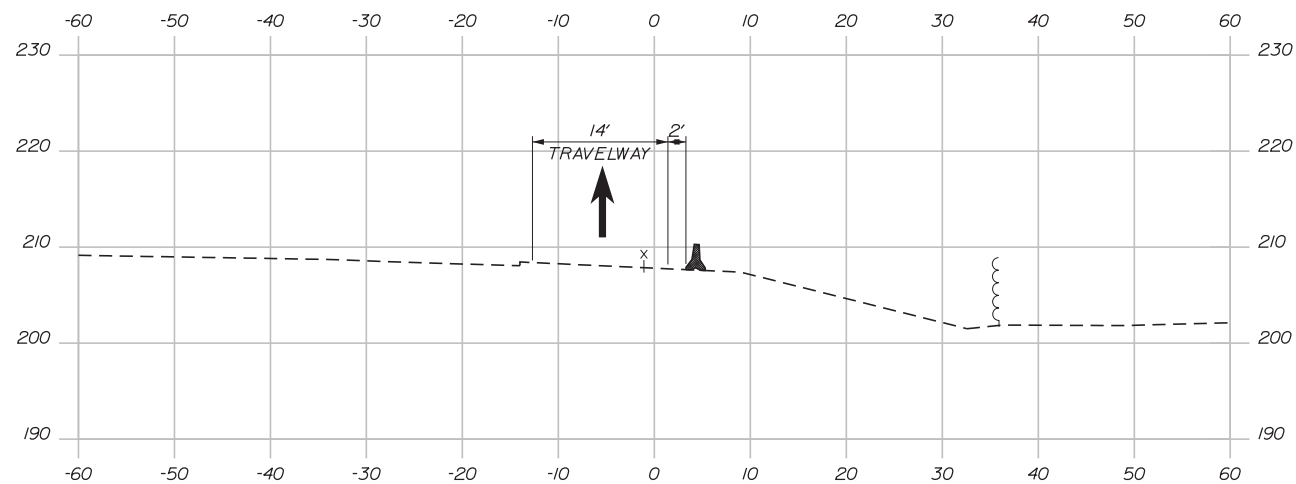
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Date: 3/20/2019

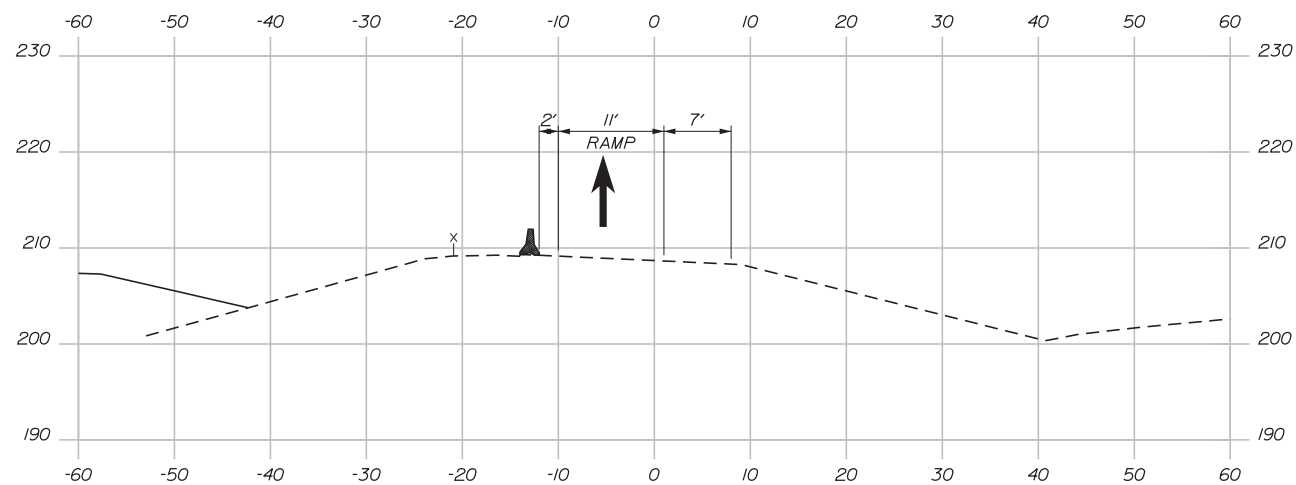
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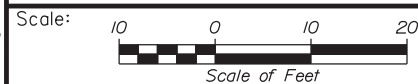
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THE GOLD STAR
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INTERCHANGE 103
ORT CONVERSION
PHASE 5
MAINTENANCE OF TRAFFIC SECTION 9

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CONTRACT: 2019.04

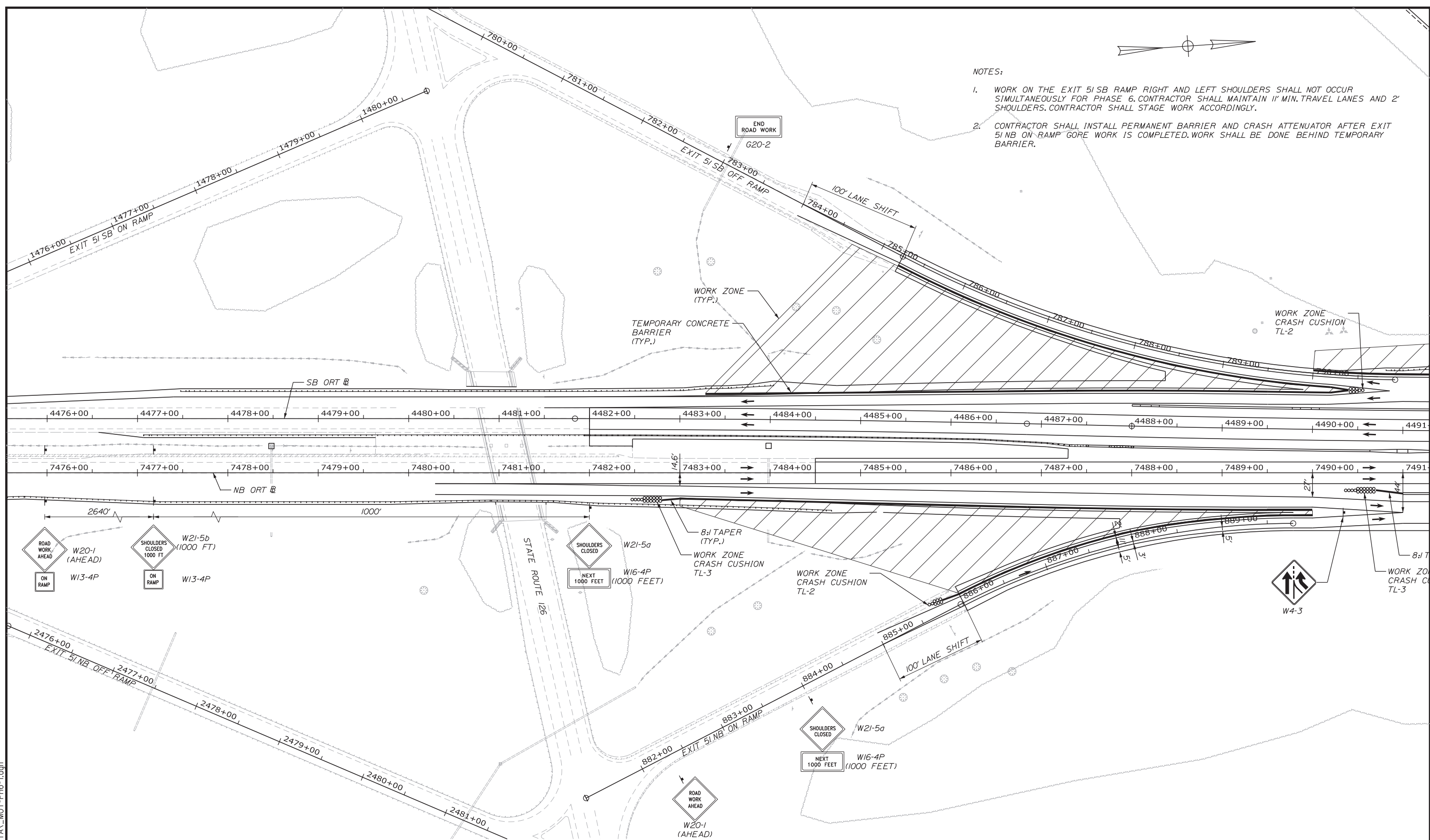
SHEET NUMBER: MOTXS-50

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NOTES:

1. WORK ON THE EXIT 5I SB RAMP RIGHT AND LEFT SHOULDERS SHALL NOT OCCUR SIMULTANEOUSLY FOR PHASE 6. CONTRACTOR SHALL MAINTAIN 11' MIN. TRAVEL LANES AND 2' SHOULDERS. CONTRACTOR SHALL STAGE WORK ACCORDINGLY.
2. CONTRACTOR SHALL INSTALL PERMANENT BARRIER AND CRASH ATTENUATOR AFTER EXIT 5I NB ON-RAMP GORE WORK IS COMPLETED. WORK SHALL BE DONE BEHIND TEMPORARY BARRIER.



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Scale: 50 0 50 100
Scale of Feet

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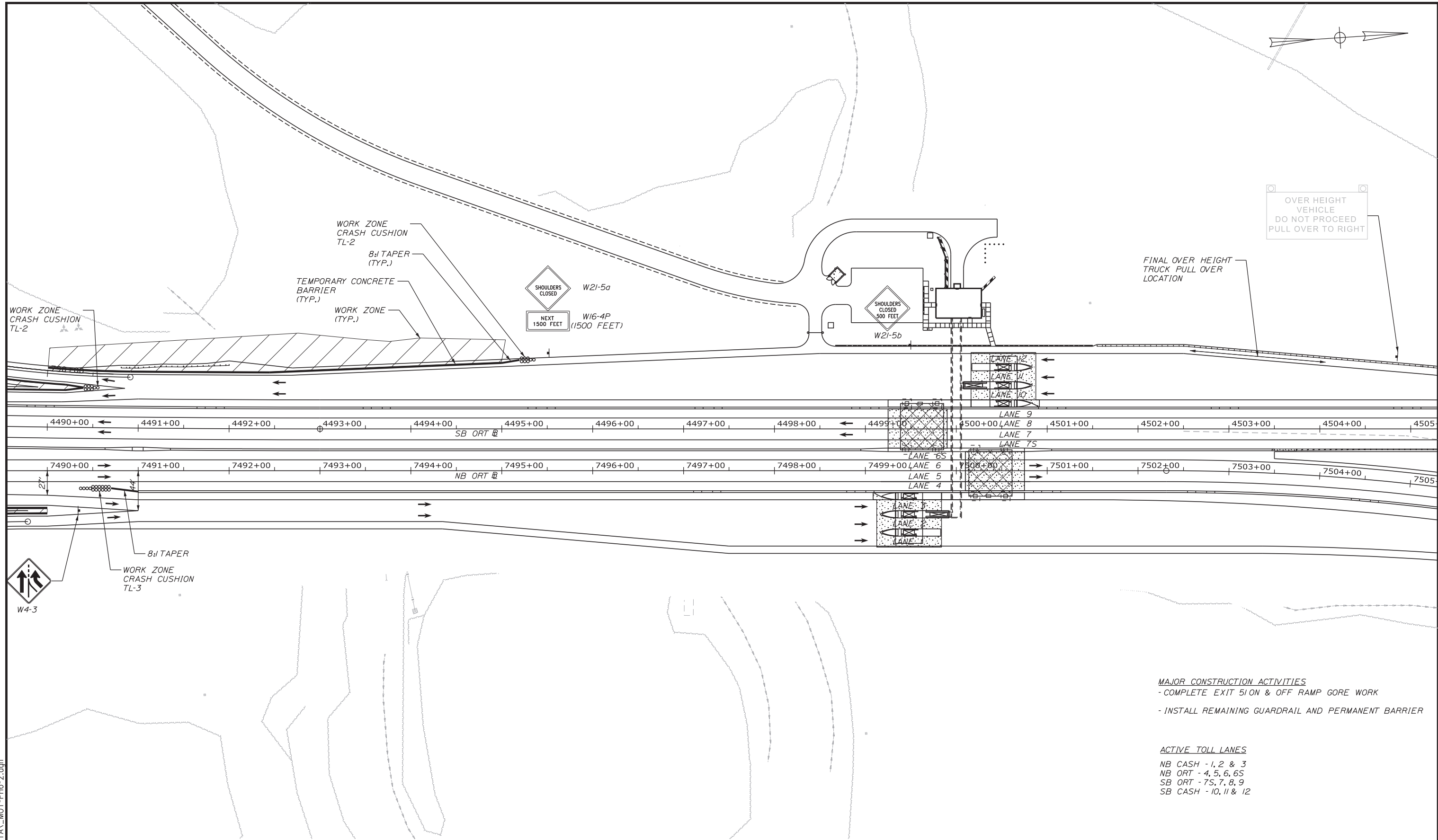
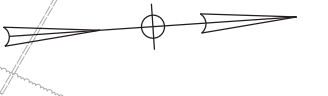
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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PHASE 6
MAINTENANCE OF TRAFFIC PLAN 1

SHEET NUMBER: MOT6-01
CONTRACT: 2019.04
90 OF 503



OVER HEIGHT VEHICLE
DO NOT PROCEED
PULL OVER TO RIGHT

FINAL OVER HEIGHT TRUCK PULL OVER LOCATION

MAJOR CONSTRUCTION ACTIVITIES
 - COMPLETE EXIT 51 ON & OFF RAMP GORE WORK
 - INSTALL REMAINING GUARDRAIL AND PERMANENT BARRIER

ACTIVE TOLL LANES
 NB CASH - 1, 2 & 3
 NB ORT - 4, 5, 6, 6S
 SB ORT - 7S, 7, 8, 9
 SB CASH - 10, 11 & 12

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Scale: 50 0 50 100
Scale of Feet

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 PHASE 6
 MAINTENANCE OF TRAFFIC PLAN 2

SHEET NUMBER: MOT6-01
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 91 OF 503

Date: 3/20/2019

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WIDE LOADS PROHIBITED CS-10
 LOGO S MAINE TURNPIKE 95 MI-2
 SOUTHBOUND CS-12
 AT EXIT 103 CS-13

SIGNS TO BE INSTALLED NEAR INTERSTATE 95 MILE MARKER 110.7 SOUTHBOUND.

WIDE LOADS PROHIBITED CS-10
 LOGO S MAINE TURNPIKE 95 MI-2
 NORTHBOUND CS-11

SIGNS TO BE INSTALLED ON ROUTE 9/126 EASTBOUND AT LEAST 200' PRIOR TO EXIT 51 NORTHBOUND ON RAMP.

WIDE LOADS PROHIBITED CS-10
 LOGO S MAINE TURNPIKE 95 MI-2
 NORTHBOUND CS-11

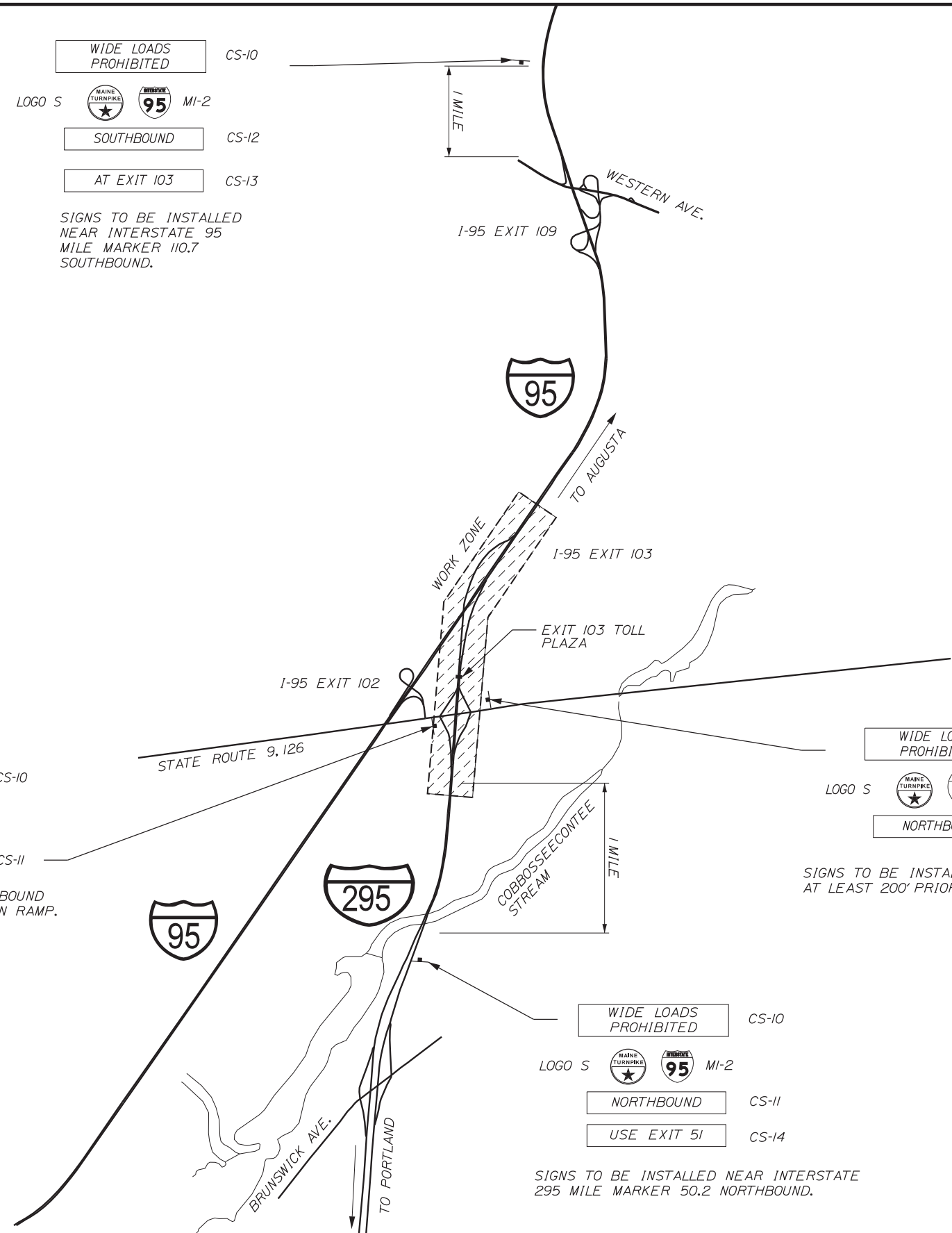
SIGNS TO BE INSTALLED ON ROUTE 9/126 WESTBOUND AT LEAST 200' PRIOR TO EXIT 51 NORTHBOUND ON RAMP.

WIDE LOADS PROHIBITED CS-10
 LOGO S MAINE TURNPIKE 95 MI-2
 NORTHBOUND CS-11
 USE EXIT 51 CS-14

SIGNS TO BE INSTALLED NEAR INTERSTATE 295 MILE MARKER 50.2 NORTHBOUND.

WIDE LOAD ROUTE NOTES:

1. ALL SIGNS TO BE MOUNTED ON MASH COMPLIANT POSTS UNLESS BEHIND GUARDRAIL.



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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 WIDE LOAD PLAN

SHEET NUMBER: MOT-XX
 CONTRACT: 2019.04
 92 OF 503

Date: 3/20/2019

Filename: ... \HIGHWAY\MSTA\MOT_Details1.dgn

GENERAL MAINTENANCE OF TRAFFIC NOTES:


1. ALL WORK TO CONFORM TO MAINE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGES, EXCEPT AS MODIFIED BY THE MAINE TURNPIKE AUTHORITY'S SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.
2. ALL PAVEMENT STRIPING & SIGNING SHALL BE IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", U.S.D.O.T., F.H.W.A., LATEST EDITION. NOTE THERE IS A 2009 EDITION OF MUTCD.
3. THESE PLANS SHOW THE GENERAL CONDITION FOR TURNPIKE MAINLINE TRAFFIC CONTROL. DURING CONSTRUCTION, SLIGHT MODIFICATIONS IN CONSTRUCTION PROCEDURE MAY OCCUR AND MAY REQUIRE SOME MINOR ADJUSTMENTS TO BE MADE IN THE FIELD. ALL PROCEDURES MUST BE APPROVED BY THE RESIDENT.
4. THE CONTRACTOR SHALL REMOVE ALL PAVEMENT MARKINGS THAT CONFLICT WITH PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH THE SPECIFICATIONS AND MUTCD. PAYMENT SHALL BE MADE UNDER ITEM 627.77 - REMOVING PAVEMENT MARKINGS.
5. TEMPORARY PAVEMENT MARKINGS SHALL BE PAINTED, UNLESS OTHERWISE NOTED. PAYMENT FOR MARKINGS SHALL BE UNDER ITEM 627.681 - TEMPORARY 6 INCH PAINTED PAVEMENT MARKING LINES, YELLOW OR WHITE.
6. TEMPORARY PAINTED PAVEMENT MARKINGS SHALL NOT BE INSTALLED ON NEW SURFACE PAVEMENT, TEMPORARY PAVEMENT MARKING TAPE SHALL BE USED. NEW SURFACE PAVEMENT INCLUDES PAVEMENT FROM ADJACENT BRIDGE PROJECT 2018.05.
7. EXPOSED BARRIER ENDS SHALL BE PROTECTED BY A WORK ZONE CRASH CUSHION. PAYMENT WILL BE UNDER ITEM 527.342 - WORK ZONE CRASH CUSHION - TL-2 OR ITEM 527.343 - WORK ZONE CRASH CUSHION - TL-3.
8. ALL TEMPORARY BARRIER SHALL BE SET WITH A MINIMUM 8:1 TAPER. THE 8:1 TAPER BARRIER LENGTH IS DEPENDENT ON THE LOCATION OF THE BARRIER RELATIVE TO THE MAINE TURNPIKE SHOULDERS OR LANES.
9. REMOVAL OF TEMPORARY PAVEMENT MARKING TAPE ON FINAL PAVEMENT SURFACE SHALL BE DONE IN SUCH A MANNER TO REMOVE AS LITTLE PAVEMENT AS POSSIBLE.
10. TWO COMPLETE LANE CLOSURE EASEL MOUNTED SIGN PACKAGES (LEFT OR RIGHT LANE - TURNPIKE), INCLUDING WORK ZONE SPEED SIGNS, HAVE BEEN INCLUDED IN THE CONTRACT TO PERMIT SETUP AND REMOVAL OF THE PERMANENT WORK ZONE MAINTENANCE OF TRAFFIC CONTROL DEVICES.
11. IF THE FINAL LIFT OF PAVEMENT IS NOT IN PLACE DURING THE TESTING OF THE ORT LANES, THE CONTRACTOR SHALL PROVIDE TEMPORARY RAMPS AT THE TRANSITION FROM THE ASPHALT BASE COURSE TO THE ORT CONCRETE SLABS AND FROM THE ASPHALT BASE COURSE TO THE EXISTING PAVEMENT. THESE SHALL BE 100 FEET IN LENGTH THE ENTIRE WIDTH OF PAVEMENT. TEMPORARY RAMPS SHALL BE CONSTRUCTED USING THE SAME MATERIAL AS BEING PLACED ON THAT DAY OR AS APPROVED BY THE RESIDENT. ALL TEMPORARY RAMPS ARE TO BE CONSTRUCTED ON A SAND JOINT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL REPAIRS AND MAINTENANCE REQUIRED FOR THE TEMPORARY RAMPS. ALL WORK ASSOCIATED WITH THE RAMPS SHALL BE INCIDENTAL TO THE PAVEMENT ITEMS.
12. THESE PLANS SHOW THE GENERAL LAYOUT OF CONSTRUCTION ZONE SIGNING; ALL LOCATIONS REQUIRING SIGNS MAY NOT BE SHOWN. FINAL CONTENT AND LAYOUT OF SIGNS MUST BE SUBMITTED TO AND APPROVED BY THE RESIDENT.

13. CANOPY LIGHT ABOVE CLOSED LANES SHALL BE RED.
14. CANOPY SIGNAGE ABOVE CLOSED LANES SHALL BE COVERED.
15. CONTRACTOR SHALL MAINTAIN A 5' WIDE PATHWAY FOR MTA EMPLOYEES TO ACCESS PLAZA BUILDING AND TOLL BOOTHS TO CROSS WORK ZONES SAFELY DURING ALL PHASES OF WORK. THIS MAY REQUIRE BREAKS IN BARRIER, USE OF BARRELS, SAFETY TAPE DELINEATING PATHS AND TEMPORARY SIGNAGE. THE PATH SHALL BE LIGHTED FROM DUSK TO DAWN AND FREE FROM TRIP HAZARDS. LIGHTING, SIGNAGE AND SAFETY TAPE SHALL BE INCIDENTAL TO ITEM 659.10 MOBILIZATION.
16. REFER TO APPENDIX G OF SPECIFICATIONS FOR PLAZA SAFETY REQUIREMENTS.
17. LANE CLOSURES BOTH NORTHBOUND AND SOUTHBOUND AND TEMPORARY CONCRETE BARRIER IN MEDIAN SHOULDERS ARE REQUIRED PRIOR TO REMOVING AND RESETTING EXISTING MEDIAN GUARDRAIL AND INSTALLING PROPOSED GUARDRAIL ITEMS AND SIGN BRIDGES.
18. NO CONSTRUCTION WORK ALLOWED IN PROPOSED TOLLING LANES DURING TOLL COMMISSIONING. COMMISSIONING REQUIRES ACCESS TO ALLOW TESTING VEHICLES AT HIGHWAY SPEEDS.
19. SPEED LIMIT, STOP, YIELD AND EXIT SIGNS SHALL BE A MINIMUM OF 5' ABOVE THE PAVEMENT.
20. FOR TRAFFIC CONTROL NOT COVERED IN THESE PLANS, REFERENCE MAINE TURNPIKE AUTHORITY TRAFFIC CONTROL PLANS DATED JANUARY 2018 ON MTA WEBSITE.
21. CONTRACTOR SHALL NOT USE EXISTING BRIDGE OVER TOLL PLAZA FOR VEHICULAR OR EQUIPMENT ACCESS.

ABBREVIATIONS FOR ALL M.O.T. PLANS

- BWLL = BROKEN WHITE LANE LINE
- SWEL = SOLID WHITE EDGE LINE
- SYEL = SOLID YELLOW EDGE LINE
- TBWL = TEMPORARY BROKEN WHITE LINE
- TSWL = TEMPORARY SOLID WHITE LINE
- TSYL = TEMPORARY SOLID YELLOW LINE
- DYCL = DOUBLE YELLOW CENTER LINE

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION

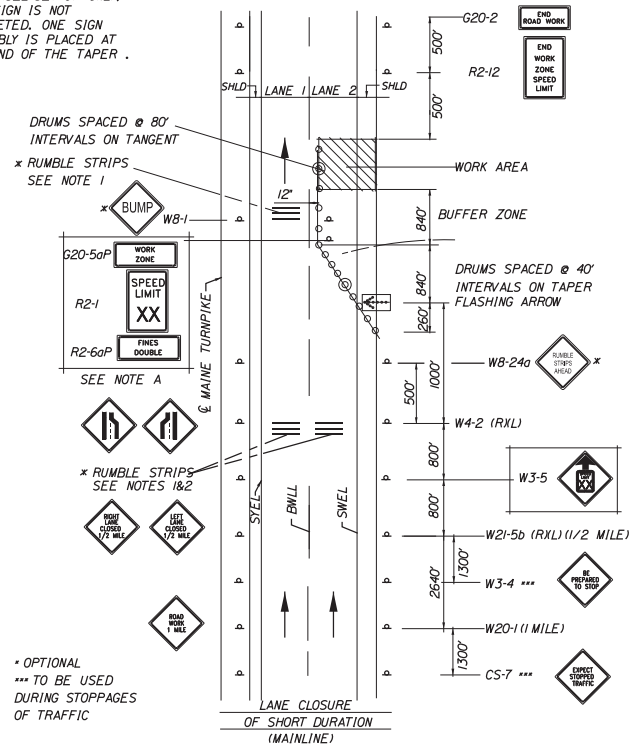
MOT DETAILS 1

SHEET NUMBER: DET-01
 93 OF 503

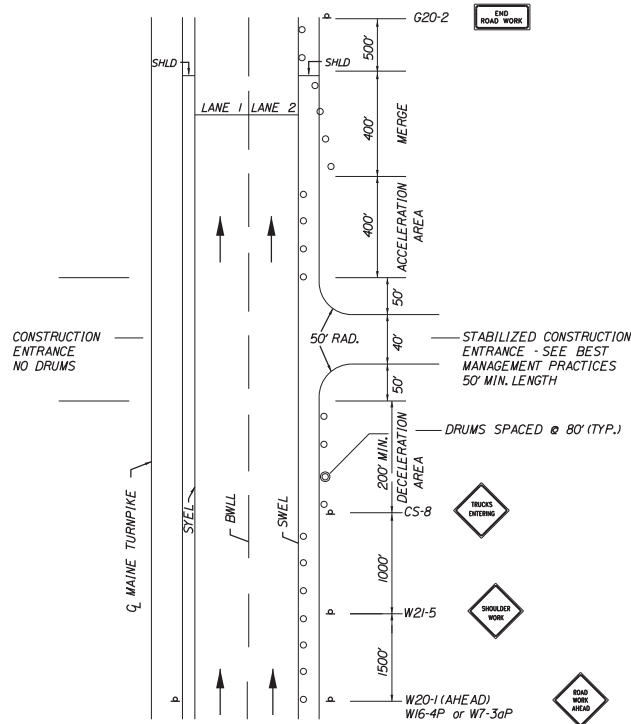
CONTRACT: 2019.04

Date: 3/20/2019

1. FOR EASEL SET UP ONLY. THIS SIGN IS NOT BRACKETED. ONE SIGN ASSEMBLY IS PLACED AT THE END OF THE TAPER.

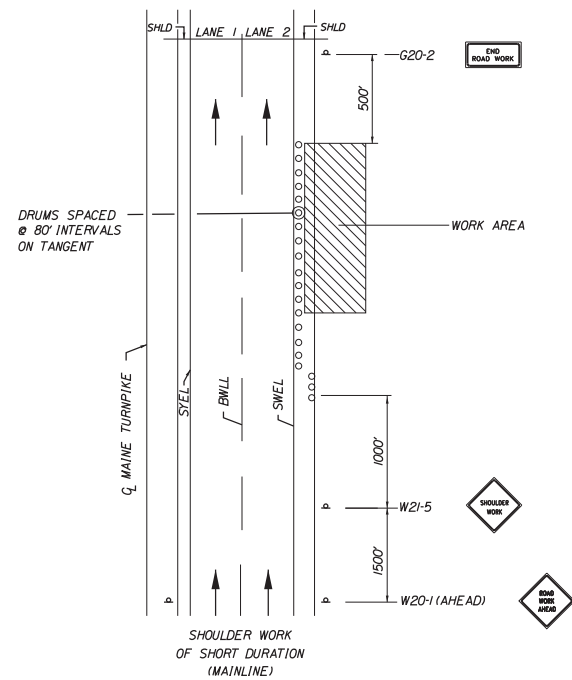


SINGLE LANE CLOSURE - SIGNS ON EASELS



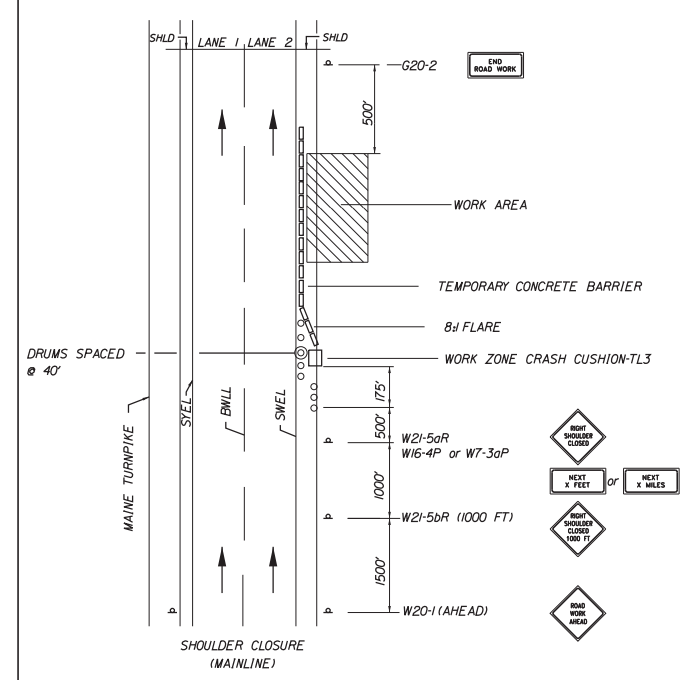
STABILIZED CONSTRUCTION ENTRANCE

1. SIGN G20-2 IS NOT REQUIRED IF ENTRANCE IS WITHIN THE WORK ZONE.



SHOULDER WORK

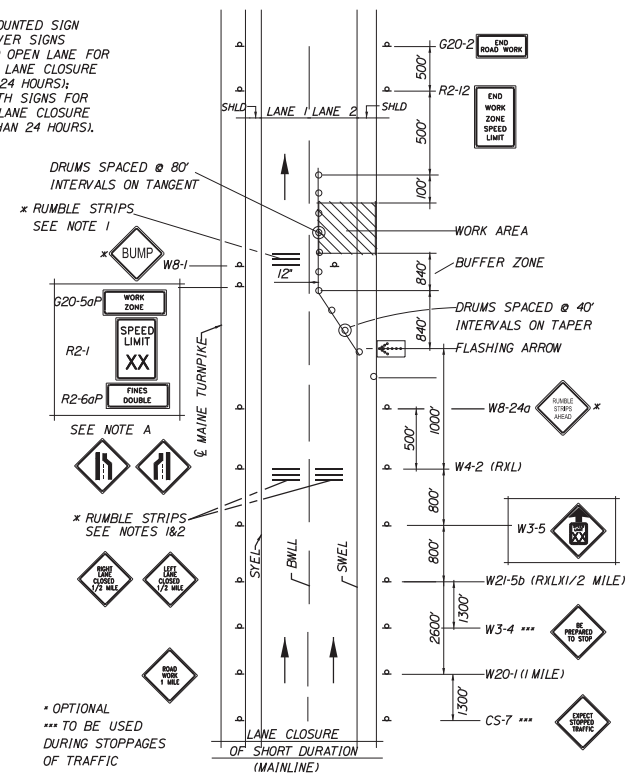
1. THE W20-1 AND G20-2 SIGNS ARE NOT REQUIRED IF THE SHOULDER CLOSURE IS WITHIN A PREVIOUSLY ESTABLISHED WORK ZONE.



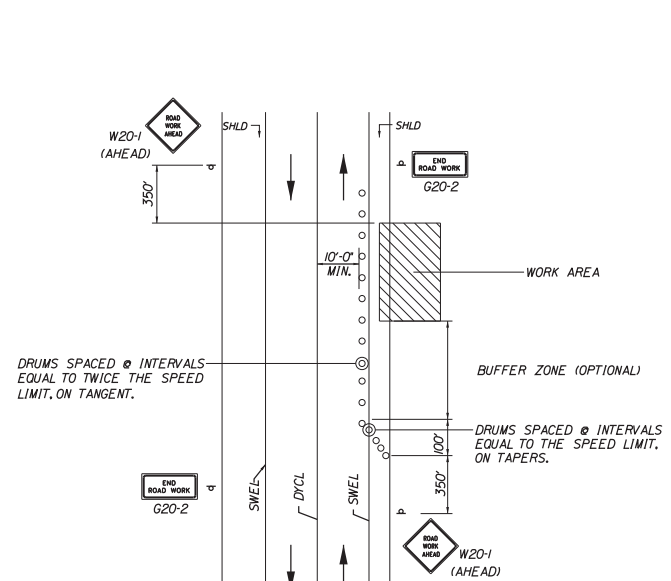
SHOULDER CLOSURE

1. THE W20-1 AND G20-2 SIGNS ARE NOT REQUIRED IF THE SHOULDER CLOSURE IS WITHIN A PREVIOUSLY ESTABLISHED WORK ZONE.
2. ONLY ONE SHOULDER CLOSURE PERMITTED PER BOUND AT ONE TIME UNLESS OTHERWISE NOTED.

1. FOR POST MOUNTED SIGN SETUP UNCOVER SIGNS ADJACENT TO OPEN LANE FOR SHORT TERM LANE CLOSURE (LESS THAN 24 HOURS). UNCOVER BOTH SIGNS FOR PERMANENT LANE CLOSURE (GREATER THAN 24 HOURS).

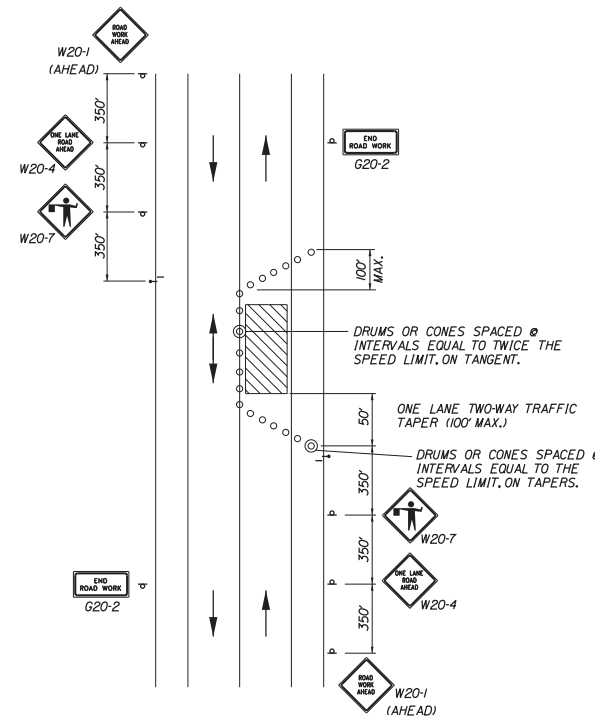


SINGLE LANE CLOSURE - POST MOUNTED SIGN SETUP



TEMP. LOCAL ROAD SHOULDER CLOSURE

1. FOR SHORT DURATION WORK (<1 HOUR), THE TAPER AND CHANNELIZATION DEVICES MAY BE OMITTED IF SHADOW VEHICLE IS PRESENT WITH ACTIVATED HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING OR STROBE LIGHTS.



LOCAL ROAD SINGLE LANE CLOSURE

1. ROAD WORK AHEAD & END ROAD WORK SIGNS MAY BE OMITTED FOR SHORT DURATION OPERATIONS (LESS THAN 1 HOUR).
2. FLAGGERS SHALL BE LOCATED SO THEY ARE CLEARLY VISIBLE TO APPROACHING DRIVERS.
3. SIGN SPACING ASSUMES LOCAL ROAD SPEED IS 40 MPH OR LESS.

NOTES:

- FOR SIGN DETAILS, SEE MAINTENANCE OF TRAFFIC SIGN SUMMARY SHEETS.
- "TRUCKS ENTERING" SIGN SHALL ALSO BE USED AT LOCATIONS WHERE TRUCKS ENTER THE WORK ZONE FROM THE TRAVEL LANE.
- SIGNS DESIGNATED *** SHALL BE USED DURING STOPPAGES OF TRAFFIC.
- TRUCK MOUNTED ATTENUATOR SHALL NOT BE LOCATED WITHIN THE BUFFER ZONE.
- SPEED LIMIT, STOP, YIELD AND EXIT SIGNS SHALL BE A MINIMUM OF 5' ABOVE THE PAVEMENT.

TEMPORARY RUMBLE STRIP NOTES:

- IF RUMBLE STRIPS ARE USED THEY SHALL BE PLACED IN ONE OF THE FOLLOWING CONFIGURATIONS:
• ADJACENT TO THE WORK ZONE (1 UNIT)
• UPSTREAM FROM THE TAPER FOR THE WORKZONE (2 UNITS)
• BOTH ADJACENT TO THE WORKZONE AND PRIOR TO THE TAPER (3 UNITS)
W8-1 SIGNS SHALL BE PLACED ADJACENT TO THE FIRST RUMBLE STRIP AT ANY LOCATION. ONLY ONE SET OF W8-24a SIGNS ARE REQUIRED FOR ANY OF THE ABOVE CONFIGURATIONS.
- RUMBLE STRIPS MAY BE PLACED UPSTREAM OF THE TAPER BETWEEN THE W3-5 SIGNS AND THE W4-2 SIGNS. IF RUMBLE STRIPS ARE INSTALLED PRIOR TO TAPER, W8-1 SIGNS SHALL BE PLACED ADJACENT TO THE FIRST STRIP AND THE W8-24a SIGNS SHALL BE MOVED TO 400' AFTER THE W20-5 SIGNS.

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	EJB	3\20\19	GAE

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

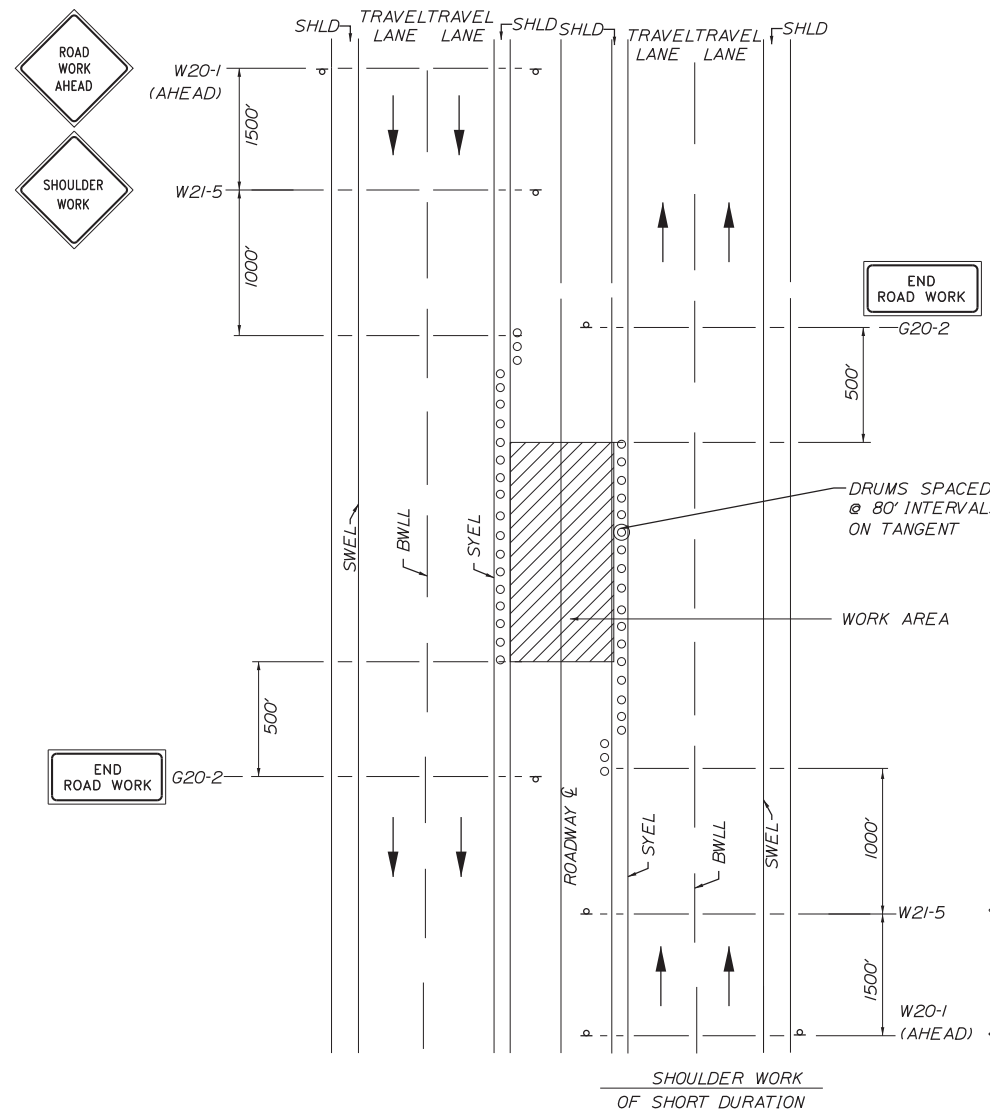
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ORT CONVERSION

MOT DETAILS 2

SHEET NUMBER: DET-02
CONTRACT: 2019.04
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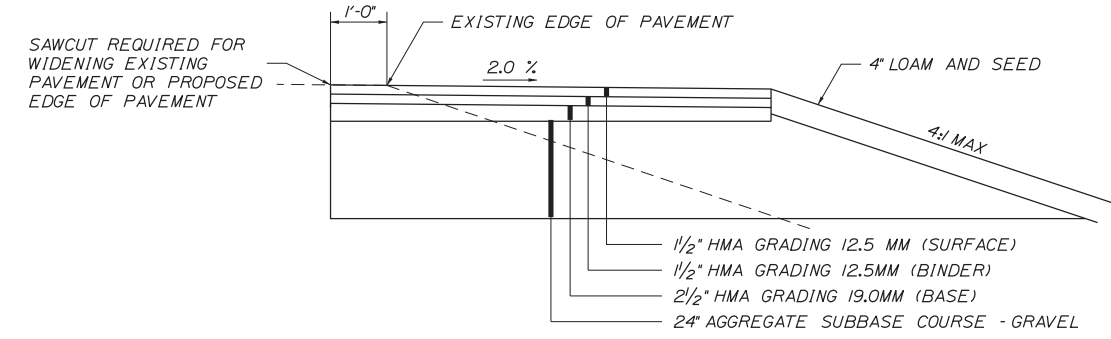
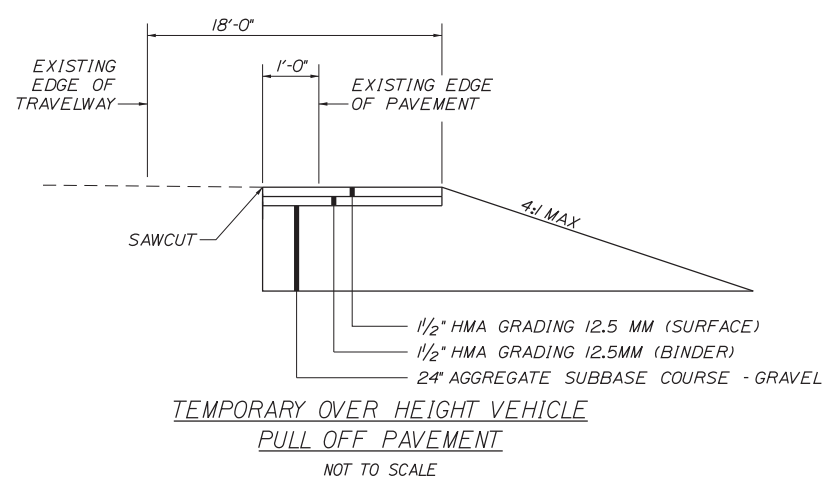
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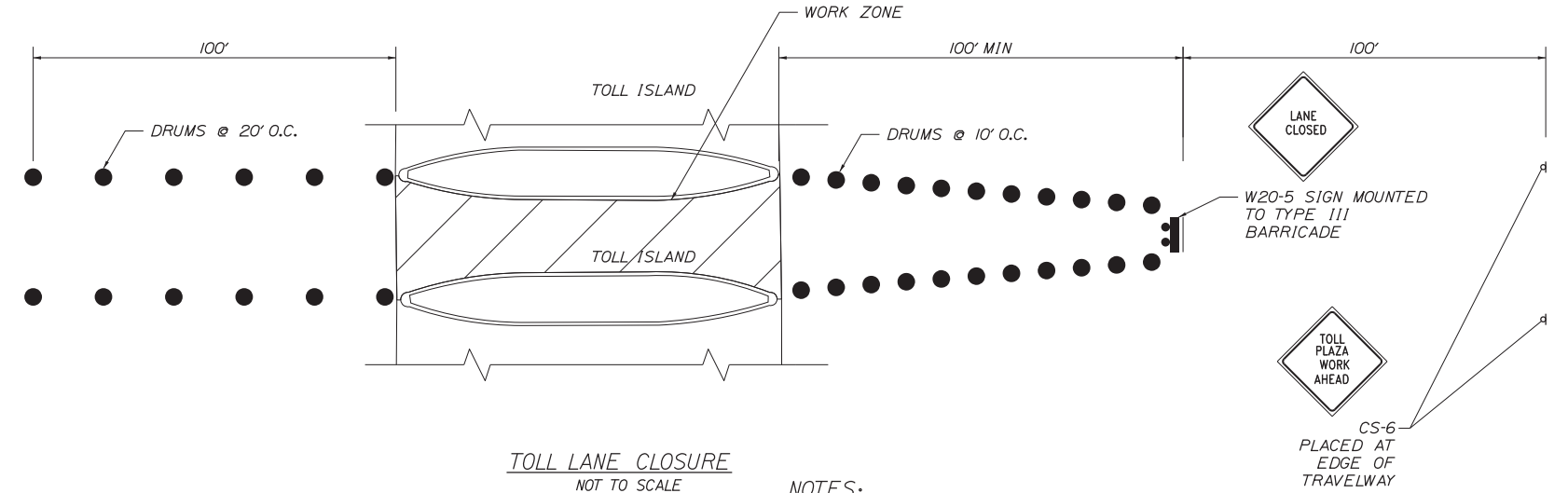
MEDIAN SHOULDER WORK
NOT TO SCALE

1. DETAIL SHALL APPLY FOR I-295 SHOULDER WORK CONSTRUCTION.
2. THE W20-1 AND G20-2 SIGNS ARE NOT REQUIRED IF THE SHOULDER CLOSURE IS WITHIN A PREVIOUSLY ESTABLISHED WORK ZONE.



NOTES:

1. TEMPORARY PAVEMENT SHALL BE PLACED ON A MINIMUM OF 24" AGGREGATE SUBBASE COURSE-GRAVEL. 24" OF AGGREGATE SUBBASE COURSE-GRAVEL HAS BEEN ESTIMATED UNDER THE TEMPORARY PAVEMENT; HOWEVER, IN FULL DEPTH CONSTRUCTION AREAS, THE CONTRACTOR CAN BUILD FINAL SUBGRADE AND EXTEND GRAVEL UNDER TEMPORARY PAVEMENT.
2. REMOVAL OF THE TEMPORARY PAVEMENT AND GRAVEL WILL BE PAID FOR AS COMMON EXCAVATION.
3. SIDESLOPES AT TEMPORARY WIDENED PAVEMENT SHALL BE AS NOTED ON THE PHASING CROSS SECTIONS. SIDESLOPES SHALL HAVE 4" LOAM AND SEED WHICH SHALL BE PAID FOR UNDER THE APPROPRIATE ITEMS.



NOTES:

1. CONTRACTOR SHALL MAINTAIN ACCESS AT ALL ACTIVE TOLL BOOTHS FOR MTA EMPLOYEES AND PLACE DRUMS TO BLOCK MTA EMPLOYEES FROM ACCESSING THE WORK ZONE.
2. CANOPY LIGHT ABOVE CLOSED LANE SHALL BE RED.

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

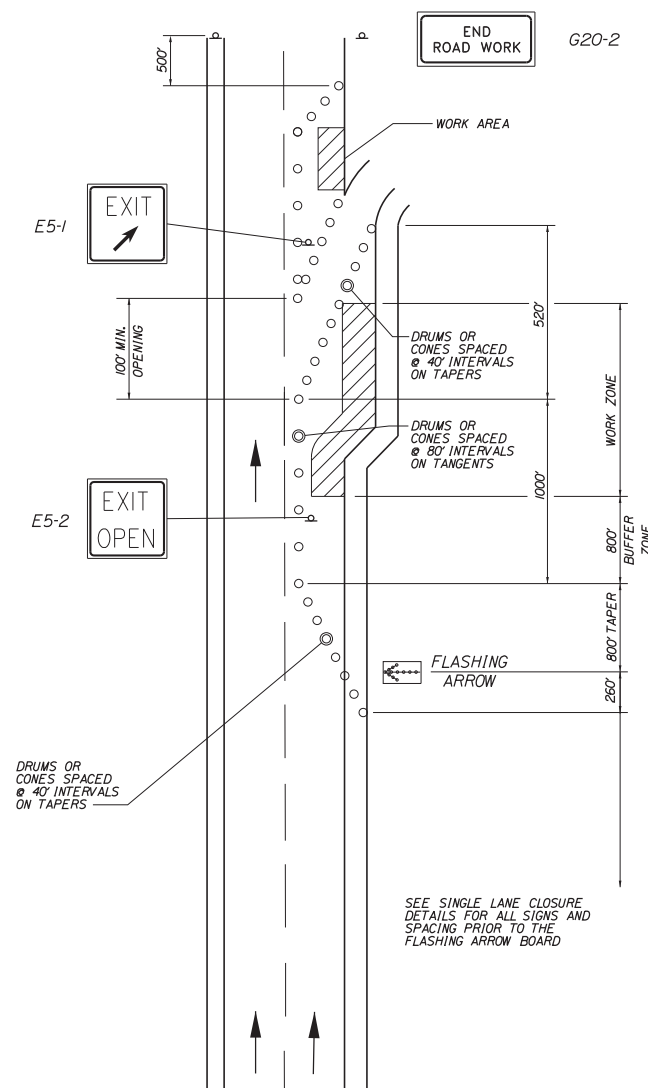
MOT DETAILS 3

SHEET NUMBER: DET-03
95 OF 503

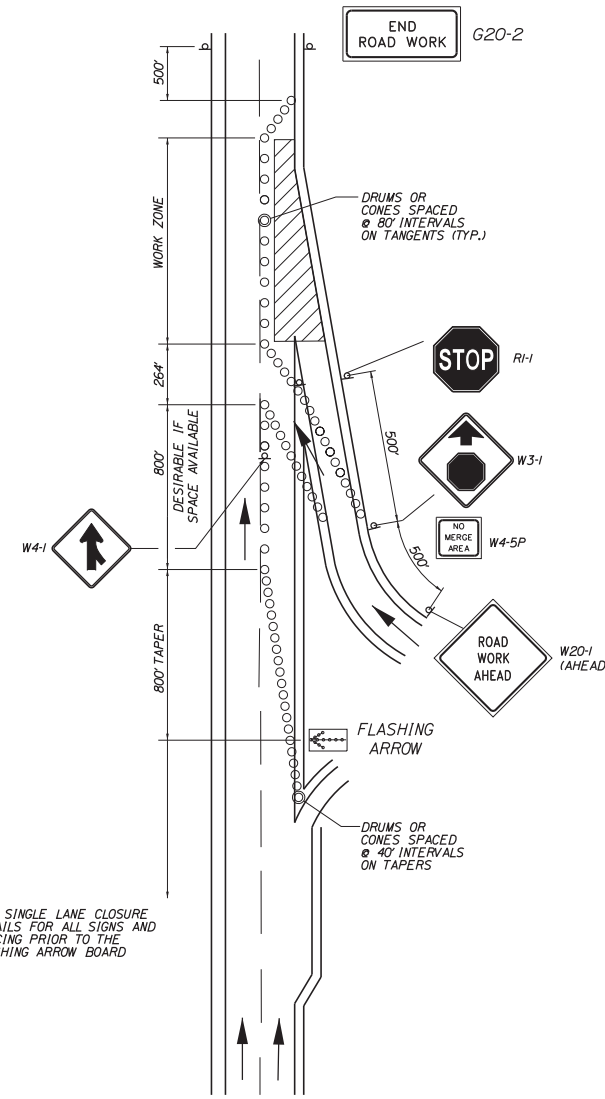
CONTRACT: 2019.04

Date: 3/20/2019

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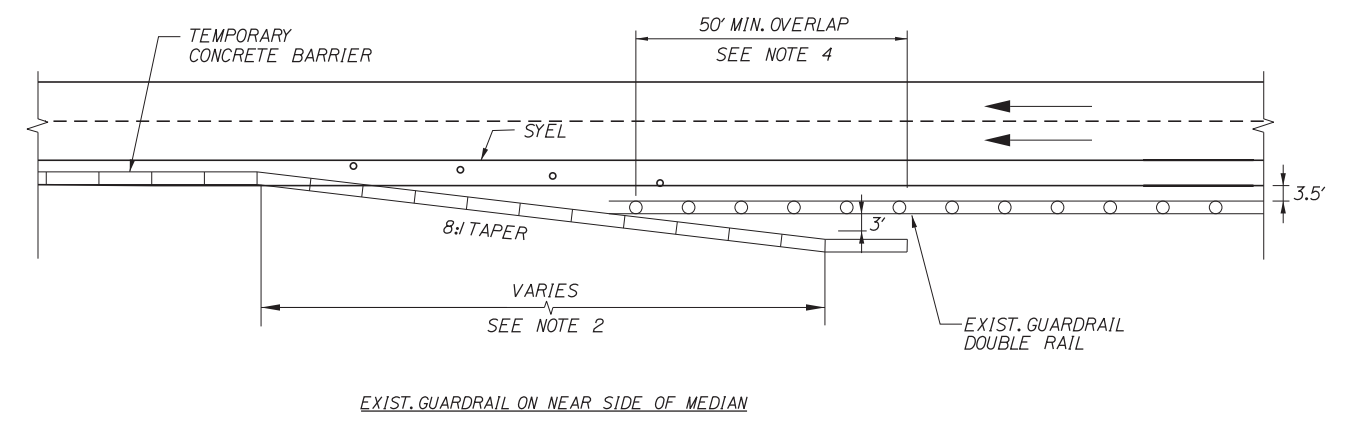
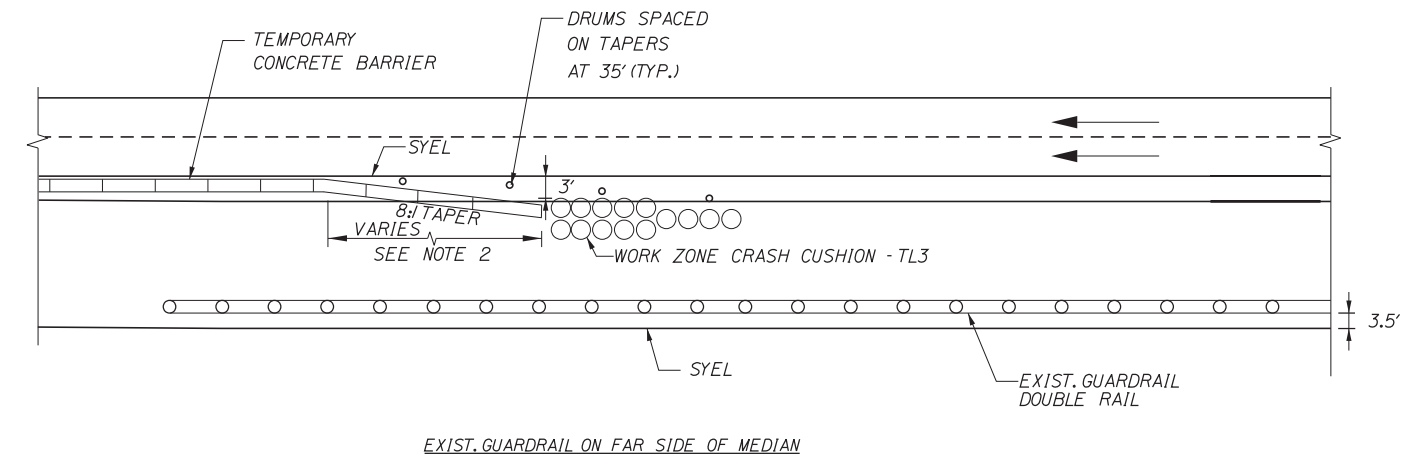


TRAVEL LANE CLOSURE AT AN EXIT RAMP



TRAVEL LANE CLOSURE AT AN ENTRANCE RAMP

TEMPORARY LANE CLOSURES AT RAMPS (MAINLINE)
NOT TO SCALE



CONCRETE BARRIER / GUARDRAIL OVERLAP DETAIL
NOT TO SCALE

NOTES:

1. BARRIER ENDS SHALL BE PROTECTED BY A TEMPORARY WORK ZONE CRASH CUSHION OR LAPPED BEHIND GUARDRAIL.
2. 8:1 TAPERED BARRIER LENGTH IS DEPENDANT ON THE LOCATION OF THE BARRIER RELATIVE TO SHOULDERS OR LANES.
3. IF A TEMPORARY WORK ZONE CRASH CUSHION IS USED, THE WORK ZONE CRASH CUSHION SYSTEM MUST BE FOUNDED ON A LEVEL SURFACE. ANY WORK NECESSARY TO PROVIDE A LEVEL SURFACE WILL BE INCIDENTAL TO THE WORK ZONE CRASH CUSHION ITEM.
4. IF THE 50' MIN. LENGTH OF OVERLAP CAN NOT BE MET THEN THE EXISTING GUARDRAIL END MUST BE ANCHORED IN ACCORDANCE WITH DRAWING SEW020 IN THE AASHTO-AGC-ARBTA JOINT COMMITTEE TASK FORCE 13 REPORT, DRAFTED MAY 1995.

Scale:			
No.	Revision	By	Date

Designed by:			
CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
	By	Date	
	PLP	3\20\19	
	Checked	LEM	3\20\19
	By	Date	
	Drawn	EJB	3\20\19
	In Charge of	GAE	3\20\19

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**THE GOLD STAR
MEMORIAL HIGHWAY**

INTERCHANGE 103
ORT CONVERSION

MOT DETAILS 4

SHEET NUMBER: DET-04
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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

MAINLINE CONSTRUCTION SIGN SUMMARY

MAINLINE CONSTRUCTION SIGN SUMMARY

ID NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER	
CS-1	108"	96"	1295 SB USE LEFT LANES EXIT 51 USE RIGHT LANE	10" D	-	-	2	ORANGE	BLACK	72.00 (144.00)
CS-2	48"	72"	RIGHT LANE EXIT ONLY	10" D	-	-	1	ORANGE	BLACK	24.00 (24.00)
CS-4	72"	24"	EXIT 51 WEST GARDINER SERVICE PLAZA	8" C	-	-	1	BLUE	WHITE	12.00 (12.00)
	96"	96"		10" D	-	-	1	BLUE	WHITE	64.00 (64.00)
CS-5	72"	24"	EXIT 51 9 126 GARDINER LITCHFIELD	8" C	-	-	1	GREEN	WHITE	12.00 (12.00)
	96"	96"		10" D	-	-	1	GREEN	WHITE	64.00 (64.00)
CS-6	48"	48"	TOLL PLAZA WORK AHEAD	8" D	6"	-	4	ORANGE	BLACK	16.00 (64.00)
				8" D						
CS-7	48"	48"	EXPECT STOPPED TRAFFIC	8" D	6"	-	6	ORANGE	BLACK	16.00 (96.00)
				8" D						
CS-8	48"	48"	TRUCKS ENTERING	8" D	6"	-	2	ORANGE	BLACK	16.00 (32.00)
				8" D						
CS-10	84"	36"	WIDE LOADS PROHIBITED	8" D	6"	-	4	ORANGE	BLACK	21.00 (84.00)
				8" D						
CS-11	84"	18"	NORTHBOUND	8" D	-	-	3	ORANGE	BLACK	10.50 (31.50)

ID NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER	
CS-12	84"	18"	SOUTHBOUND	8" D	-	-	1	ORANGE	BLACK	10.50 (10.50)
CS-13	84"	18"	AT EXIT 103	8" D	-	-	1	ORANGE	BLACK	10.50 (10.50)
CS-14	84"	18"	USE EXIT 51	8" D	-	-	1	ORANGE	BLACK	10.50 (10.50)
E5-1	48"	36"	EXIT	TEXT DIMENSIONS SHALL CONFORM TO "MUTCD" - 2009 EDITION - "STANDARD HIGHWAY SIGNS" - 2012 SUPPLEMENT			1	ORANGE	BLACK	12.00 (12.00)
E5-2	48"	36"	EXIT OPEN				1	ORANGE	BLACK	12.00 (12.00)
G20-2	48"	24"	END ROAD WORK				25	ORANGE	BLACK	8.00 (200.00)
G20-5aP	36"	24"	WORK ZONE				9	ORANGE	BLACK	6.00 (54.00)
LOGO S	36"	-	MAINE TURNPIKE				4	-	-	7.07 (28.28)
MI-2	36"	36"	INTERSTATE 95	TEXT DIMENSIONS SHALL CONFORM TO "MUTCD" - 2009 EDITION - "STANDARD HIGHWAY SIGNS" - 2012 SUPPLEMENT			4	-	-	9.00 (36.00)


Date: 3/20/2019

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By	Date	By	Date
Designed	TFD 3\20\19	Checked	LEM 3\20\19
Drawn	PLP 3\20\19	In Charge of	GAE 3\20\19

MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
MOT SIGN SUMMARY -1

SHEET NUMBER: S-18

CONTRACT: 2019.04

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MAINLINE CONSTRUCTION SIGN SUMMARY

ID NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER	
R1-1	36"	36"		TEXT DIMENSIONS SHALL CONFORM TO "MUTCD" - 2009 EDITION- "STANDARD HIGHWAY SIGNS" - 2012 SUPPLEMENT			1	RED	WHITE	9.00 (9.00)
R2-1 (25) (45) (50) (60)	48"	60"					2 2 2	WHITE	BLACK	20.00 (40.00) (40.00) (100.00) (40.00)
R2-6aP	36"	24"					9	WHITE	BLACK	6.00 (54.00)
R2-12	36"	54"					10	WHITE	BLACK	13.50 (135.00)
W1-4L	48"	48"					2	ORANGE	BLACK	16.00 (32.00)
W1-4R	48"	48"					1	ORANGE	BLACK	16.00 (16.00)
W1-4bL	48"	48"					4	ORANGE	BLACK	16.00 (64.00)
W1-4bR	48"	48"					6	ORANGE	BLACK	16.00 (96.00)
W3-1	48"	48"					1	ORANGE	BLACK	16.00 (16.00)

MAINLINE CONSTRUCTION SIGN SUMMARY

ID NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER	
W3-4	48"	48"		TEXT DIMENSIONS SHALL CONFORM TO "MUTCD" - 2009 EDITION- "STANDARD HIGHWAY SIGNS" - 2012 SUPPLEMENT			6	ORANGE	BLACK	16.00 (96.00)
W3-5 (25) (45)	48"	48"					2 8	ORANGE	BLACK	16.00 (32.00) (128.00)
W4-1	48"	48"					1	ORANGE	BLACK	16.00 (16.00)
W4-2L	48"	48"					6	ORANGE	BLACK	16.00 (96.00)
W4-2R	48"	48"					8	ORANGE	BLACK	16.00 (128.00)
W4-3	48"	48"					1	ORANGE	BLACK	16.00 (16.00)
W4-5P	24"	30"					1	ORANGE	BLACK	5.00 (5.00)
W7-3aP (1/2) (1) (2)	36"	30"					4 8 2	ORANGE	BLACK	7.50 (30.00) (60.00) (15.00)
W8-1	48"	48"					6	ORANGE	BLACK	16.00 (96.00)

Date: 3/20/2019

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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
By	Date	By	Date
Designed	TFD 3\20\19	Checked	LEM 3\20\19
Drawn	PLP 3\20\19	In Charge of	GAE 3\20\19

MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
MOT SIGN SUMMARY-2

SHEET NUMBER: S-18

CONTRACT: 2019.04

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Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\MOT_SS.dgn

ID NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER	
W8-24a	48"	48"		TEXT DIMENSIONS SHALL CONFORM TO "MUTCD" - 2009 EDITION - "STANDARD HIGHWAY SIGNS" - 2012 SUPPLEMENT			6	ORANGE	BLACK	16.00 (96.00)
W13-4P	36"	36"					4	ORANGE	BLACK	9.00 (36.00)
W16-4P (500) (700) (1000) (1500) (2000)	36"	30"					1 SIGN	ORANGE	BLACK	7.50 (7.50) (15.00) (22.50) (15.00) (15.00)
W20-1 (AHEAD) (1 MILE)	48"	48"					30 6	ORANGE	BLACK	16.00 (480.00) (128.00)
W20-4	48"	48"					1	ORANGE	BLACK	16.00 (16.00)
W20-5	48"	48"					2	ORANGE	BLACK	16.00 (32.00)
W20-5R (1/2 MILE)	48"	48"					8	ORANGE	BLACK	16.00 (128.00)
W20-5L (1/2 MILE)	48"	48"					6	ORANGE	BLACK	16.00 (96.00)
W20-7	48"	48"					2	ORANGE	BLACK	16.00 (32.00)
W21-5	48"	48"					8	ORANGE	BLACK	16.00 (128.00)

ID NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER	
W21-5a	48"	48"		TEXT DIMENSIONS SHALL CONFORM TO "MUTCD" - 2009 EDITION - "STANDARD HIGHWAY SIGNS" - 2012 SUPPLEMENT			5	ORANGE	BLACK	16.00 (80.00)
W21-5aR	48"	48"					6	ORANGE	BLACK	16.00 (96.00)
W21-5aL	48"	48"					4	ORANGE	BLACK	16.00 (64.00)
W21-5b (500) (1000)	48"	48"					2 4	ORANGE	BLACK	16.00 (32.00) (64.00)
W21-5bR (1000 FT) (1/2 MILE)	48"	48"					3 2	ORANGE	BLACK	16.00 (48.00) (32.00)
W21-5bL (1000 FT)	48"	48"					2	ORANGE	BLACK	16.00 (32.00)
W24-1R	48"	48"					1	ORANGE	BLACK	16.00 (16.00)

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No.	Revision	By	Date

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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

By	Date	By	Date
Designed TFD	3\20\19	Checked LEM	3\20\19
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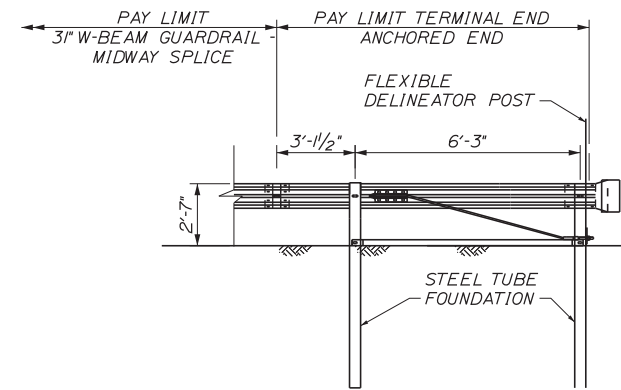
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 MOT SIGN SUMMARY - 3

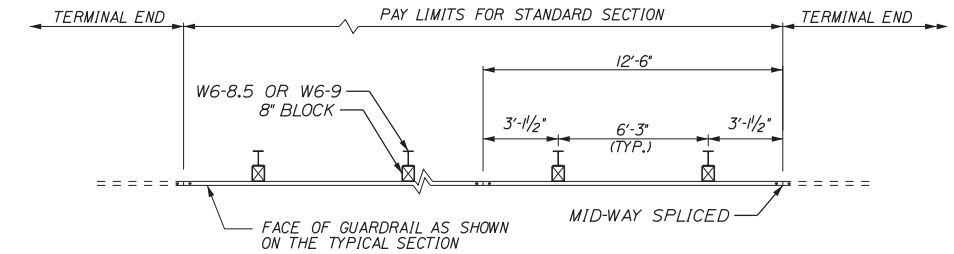
SHEET NUMBER: S-18
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 99 OF 503

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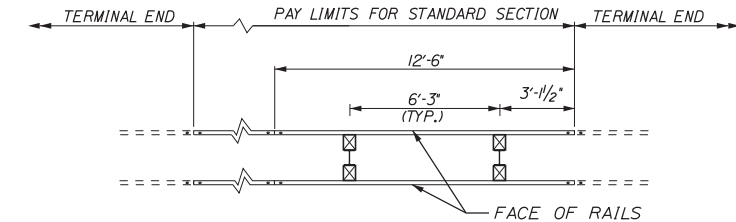


TERMINAL END - ANCHORED END - 3" W-BEAM GUARDRAIL
(ITEM 606.1351)
NOT TO SCALE

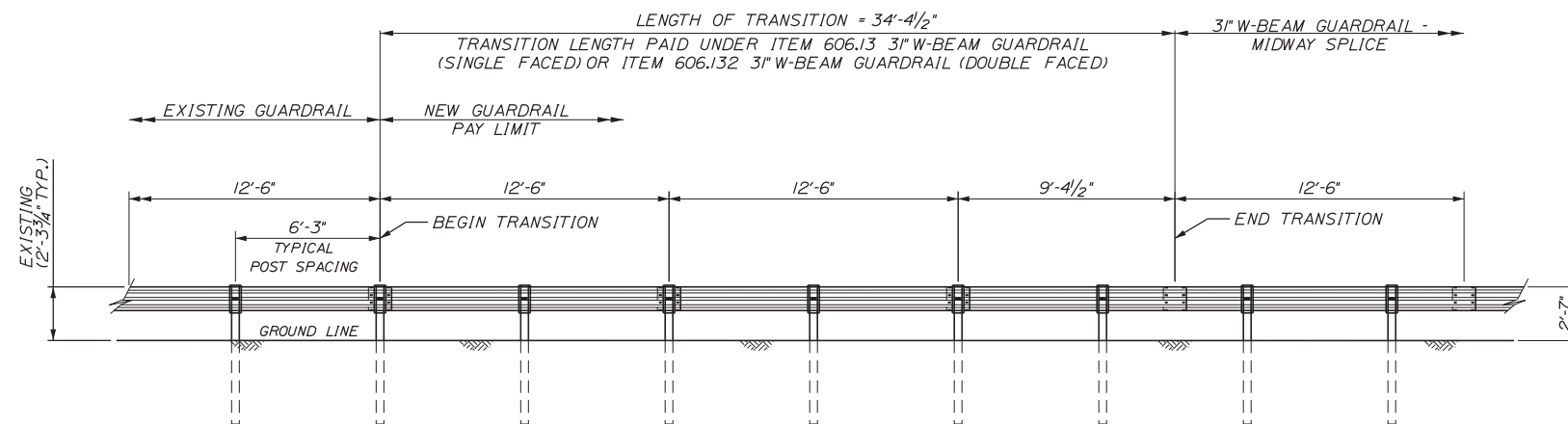
NOTES:
1. THIS DETAIL MODIFIES THE SEW31 DRAWING SUCH THAT W-BEAM DOES NOT EXTEND BEYOND THE LAST GUARDRAIL POST. THE RWM40 W-BEAM PANEL SHALL HAVE A LENGTH OF 9'-4 1/2" MEASURED FROM THE CENTER OF THE MIDWAY SPLICE TO THE CENTER OF THE LAST GUARDRAIL POST.



PLAN - SINGLE FACED



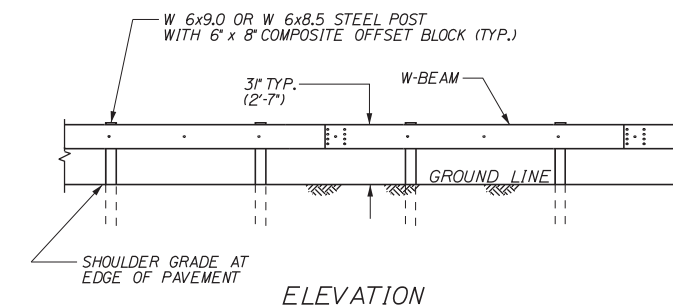
PLAN - DOUBLE FACED



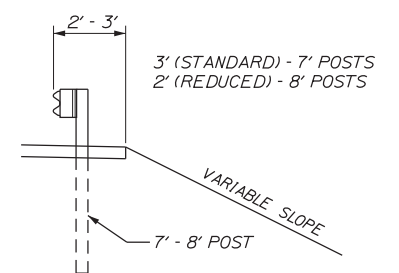
TRANSITION FROM EXISTING GUARDRAIL TO 3" MID-WAY SPLICED GUARDRAIL
NOT TO SCALE

NOTES:

1. MAINTAIN STANDARD 1" CLEARANCE OF POST ABOVE PANEL THROUGHOUT THE ENTIRE LENGTH OF TRANSITION.
2. A MINIMUM OF ONE (1) 12'-6" PANEL SHALL BE PLACED BETWEEN THIS TRANSITION AND THE START OF ANY END TREATMENT OR ANCHORAGE.
3. ALL NEW POST SHALL BE 84" IN LENGTH UNLESS OTHERWISE SPECIFIED BY THE ENGINEER.
4. ALL NEW POSTS AND OFFSET BLOCK MATERIALS SHALL MATCH EXISTING UNLESS OTHERWISE SPECIFIED BY THE ENGINEER.



ELEVATION



CROSS-SECTION

3" W-BEAM GUARDRAIL - MID-WAY SPLICE (8" OFFSET BLOCKS)
(ITEMS 606.13, 606.132)
NOT TO SCALE

GENERAL GUARDRAIL INSTALLATION NOTE:

1. THE CONTRACTOR SHALL FOLLOW ALL MANUFACTURER'S INSTALLATION INSTRUCTIONS FOR THE INSTALLATION OF ALL GUARDRAIL COMPONENTS TO BE INSTALLED. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER OF ANY CONFLICTS OR IRREGULARITIES BETWEEN THE MANUFACTURER'S INSTALLATION INSTRUCTIONS AND THE CONTRACT DOCUMENTS.
2. THE CONTRACTOR IS REQUIRED TO HAVE AN APPROVED CRASH END TREATMENT ON ALL GUARDRAIL ENDS PRIOR TO THE REMOVAL OF ANY MOT FEATURES.
3. GUARDRAIL HEIGHT SHALL BE 3'-1/2". GUARDRAIL HEIGHT SHALL BE MEASURED FROM AT GROUND ELEVATION ADJACENT TO EACH POST.
4. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL CUT BOLTS, DISCARD PARTS, TRASH, PAVEMENT DEBRIS, ETC., AFTER COMPLETING THE GUARDRAIL INSTALLATION.
5. CONNECTION FOR PROPOSED RAIL TO EXISTING RAIL SHALL BE INCIDENTAL TO THE PROPOSED GUARDRAIL ITEMS.

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Scale:			
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			GAE

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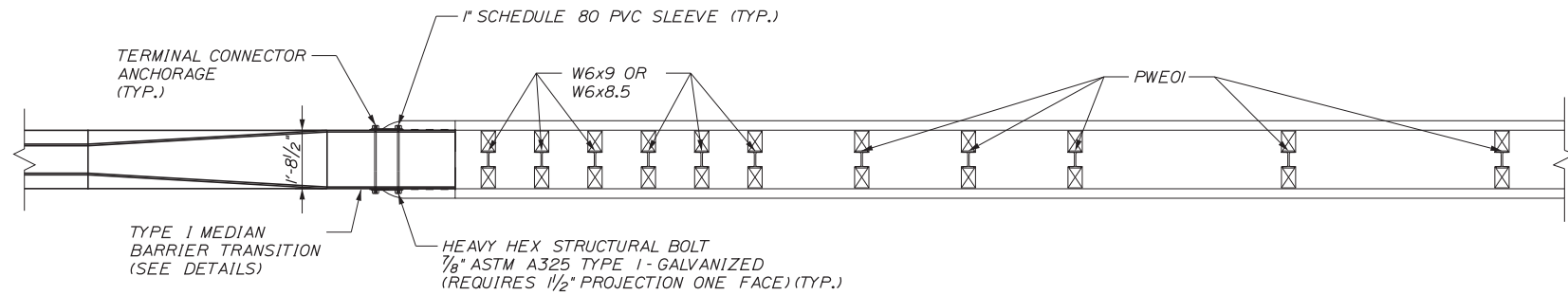
**THE GOLD STAR
MEMORIAL HIGHWAY**

INTERCHANGE 103
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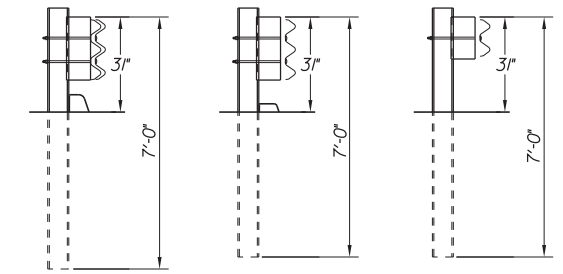
GUARDRAIL DETAILS 2

SHEET NUMBER: DET-02
CONTRACT: 2019.04
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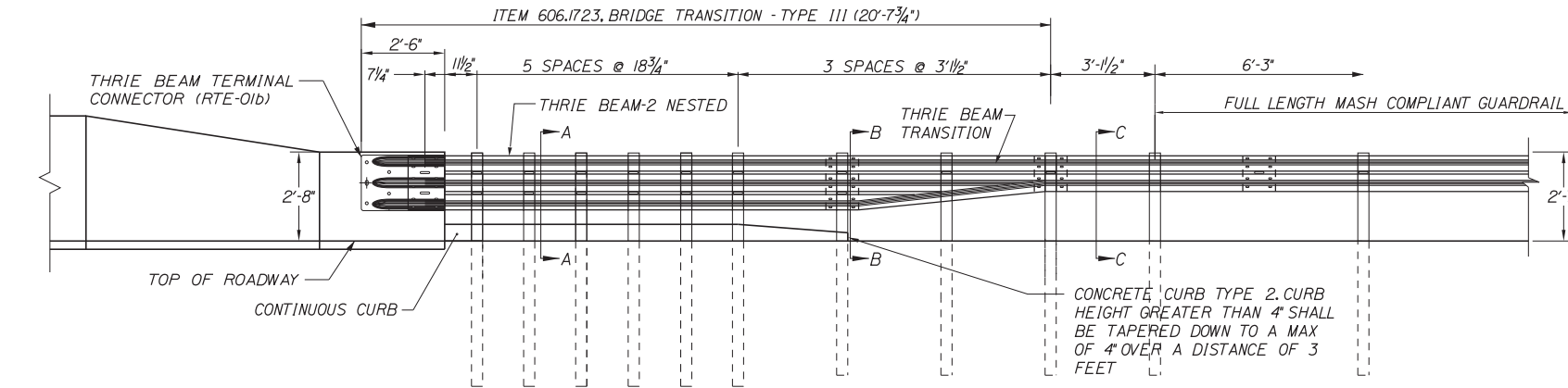
Date: 3/20/2019



PLAN



SECTION A-A SECTION B-B SECTION C-C



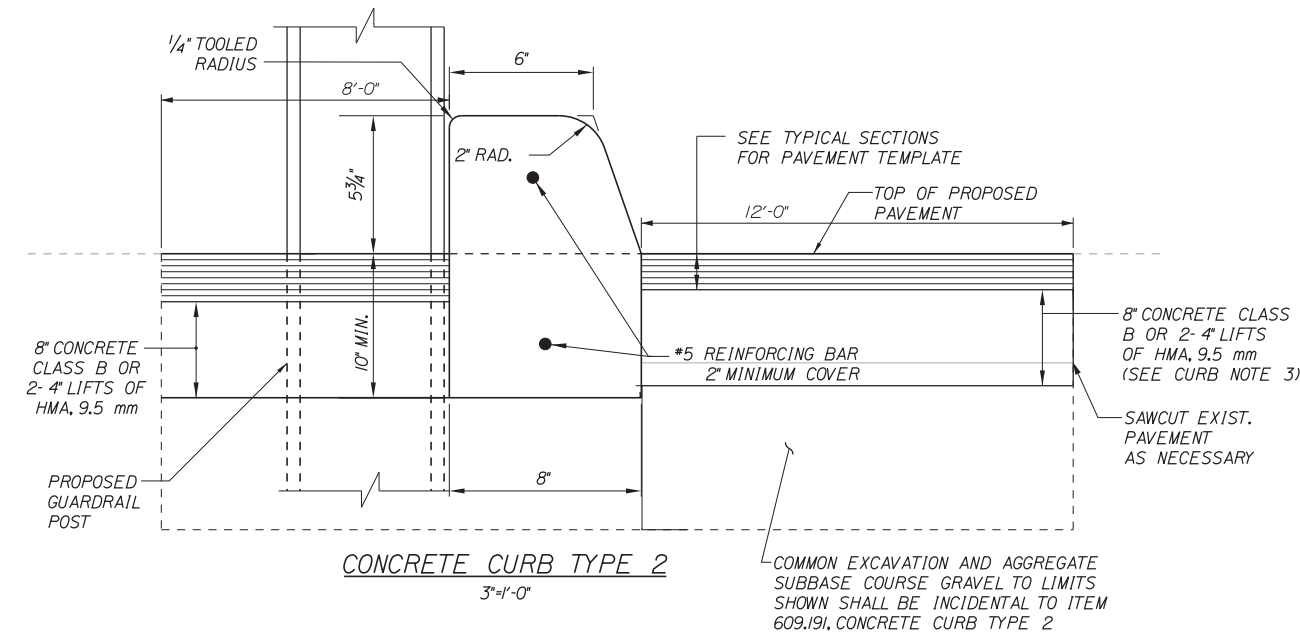
ELEVATION

BRIDGE TRANSITION - TYPE III (ITEM 606.1723)

3/8" = 1'-0"

GENERAL NOTES:

- ADDITIONAL HOLES MAY BE MADE IN THE THRIE-BEAM PANELS BY DRILLING, PUNCHING, OR OTHER MEANS THAT PRODUCE A NEAT, CLEAN HOLE. BURNING HOLES WILL NOT BE ALLOWED.
- THRIE BEAM SHALL BE PLACED WITH THE COMPOSITE BLOCKOUT FACE IN FRONT OF OR DIRECTLY ABOVE THE CURB FACE.
- RAIL ELEMENT SHALL MEET ALL REQUIREMENTS OF AASHTO M-180 EXCEPT AS MODIFIED ON THE PLANS. THE THRIE BEAM TRANSITION TO W-BEAM SHALL BE OF THE SAME MATERIAL, BUT SHALL NOT BE LESS THAN 10 GAUGE.
- AFTER INSTALLATION IS COMPLETE, UPSET THE THREAD ON THE ANCHOR BOLTS IN THREE PLACES AROUND EACH BOLT, AT THE JUNCTION OF THE NUT AND THE EXPOSED THREAD, WITH A CENTER PUNCH OR SIMILAR TOOL.
- STANDARD BARRIER HARDWARE HAS BEEN USED TO DEVELOP THESE GUARDRAIL ATTACHMENTS. DESIGNATIONS PROVIDED IN PARENTHESIS RELATE TO STANDARD ELEMENTS DETAILED IN "A GUIDE TO STANDARDIZED BARRIER RAIL HARDWARE," 1979. AASHTO-AGC-ARTBA JOINT COOPERATE COMMITTEE.
- 1" HOLE IN CONCRETE SHALL BE FORMED BY A 1" I.D. PVC SLEEVE AS APPROVED BY THE ENGINEER.
- GUARDRAIL HEIGHT SHALL BE ADJUSTED UNIFORMLY BETWEEN SECTION CALLOUTS.



CONCRETE CURB TYPE 2

3" = 1'-0"

COMMON EXCAVATION AND AGGREGATE SUBBASE COURSE GRAVEL TO LIMITS SHOWN SHALL BE INCIDENTAL TO ITEM 609.191, CONCRETE CURB TYPE 2

CURB NOTES:

- CURBING SHALL BE EITHER PRECAST CONCRETE, CAST-IN-PLACE CONCRETE OR GRANITE TO MEET DIMENSIONS SHOWN ON THE PLANS.
- CONCRETE CURBS USED IN CONJUNCTION WITH THRIE-BEAM BRIDGE ATTACHMENT SHALL BE TYPE 2. SEE DETAILS THIS SHEET. CONCRETE CURBS SHALL BE CONTINUOUS WITHOUT DRAINAGE CUTS.
- CURB TRANSITION SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCIDENTAL TO THE BRIDGE TRANSITION PAY ITEM.
- SAWCUTTING EXISTING PAVEMENT, CONCRETE FILL, AND PAVEMENT TO LIMITS SHOWN SHALL BE INCIDENTAL TO THE BRIDGE TRANSITION PAY ITEM.

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Scale:			
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	By	Date	
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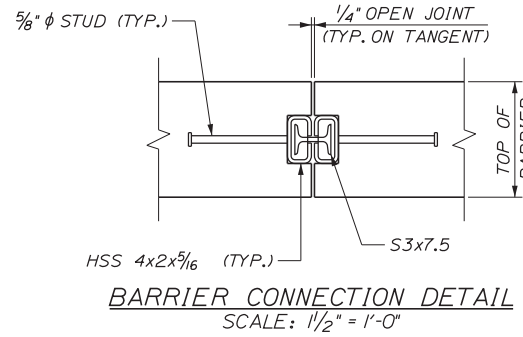
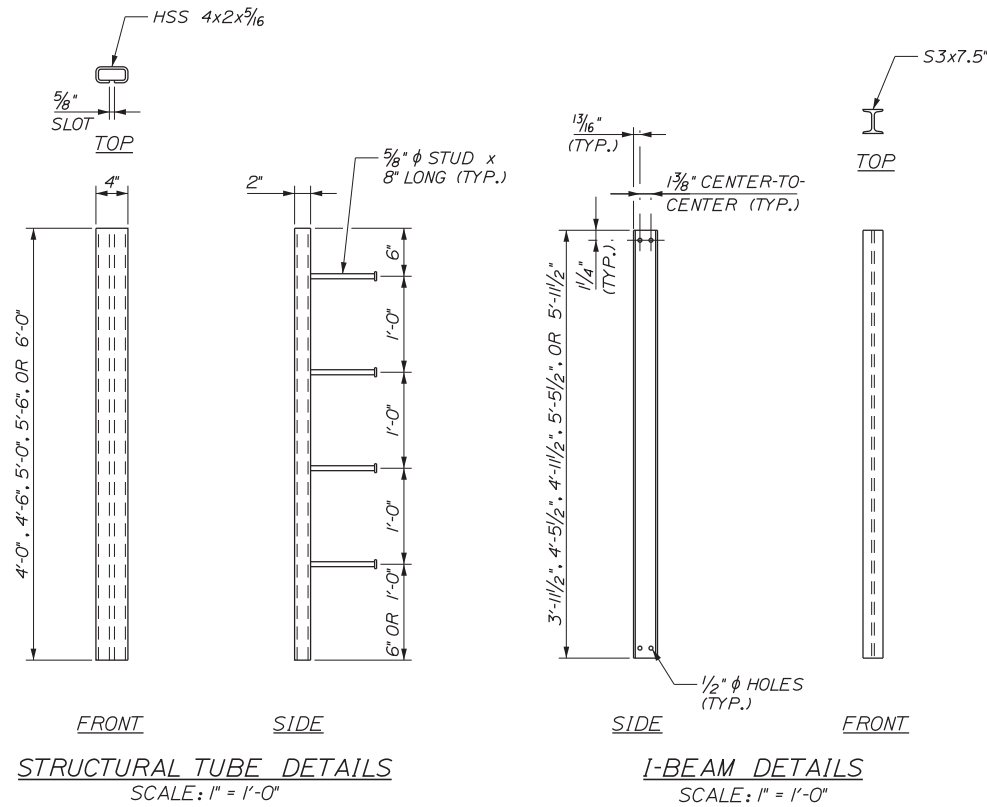
THE GOLD STAR MEMORIAL HIGHWAY

INTERCHANGE 103
ORT CONVERSION

GUARDRAIL DETAILS 3

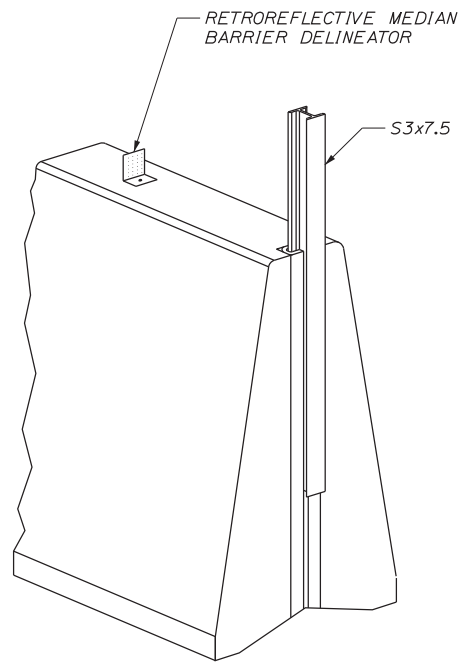
SHEET NUMBER: DET-03
CONTRACT: 2019.04
102 OF 503

Date: 3/20/2019



BARRIER NOTES

1. THE CONCRETE BARRIER DETAILS, AS SHOWN, ARE IN COMPLIANCE WITH THE REQUIREMENTS OF NCHRP REPORT 350, TL-4.
2. I-BEAMS AND STRUCTURAL TUBES SHALL BE GALVANIZED AFTER FABRICATION.
3. SHOP DRAWINGS SHALL INCLUDE REINFORCING SCHEDULE. CONTRACTOR SHALL PROVIDE SHOP DRAWINGS FOR ANGLE POINTS IN BARRIER.
4. MEDIAN BARRIER SHALL BE PAID FOR UNDER APPROPRIATE 526 ITEMS.
5. BARRIERS SHALL BE LIGHT COLORED CLASS 'P' CONCRETE HAVING A MINIMUM 28 DAY COMPRESSION STRENGTH OF 4,500 PSI. BARRIERS SHALL HAVE A SMOOTH UNIFORM SURFACE FREE OF DEFECTS AND IRREGULARITIES. CASTING DATE SHALL BE SHOWN ON THE BARRIER. ALL EXPOSED EDGES OF CONCRETE SHALL BE CHAMFERED UNLESS OTHERWISE NOTED.
6. ALL REINFORCING STEEL SHALL BE ASSHTO M31 (ASTM-A615) GRADE 60, EPOXY COATED. REINFORCEMENT SHOWN IS THE MINIMUM REQUIRED.
7. CLEAR PROTECTIVE COATING FOR CONCRETE SURFACES SHALL BE APPLIED ON-SITE AND APPROVED BY THE RESIDENT. COATING SHALL BE INCIDENTAL TO BARRIER.



526.351 MEDIAN BARRIER TYPE I - PRECAST (LF)

STA. 4496+00, RT.	TO	STA. 4498+50, RT.	250 LF
STA. 7492+54, RT.	TO	STA. 7494+00, RT.	146 LF
STA. 4492+50, LT.	TO	STA. 4493+00, LT.	50 LF

526.3515 MEDIAN BARRIER TYPE ID - PRECAST (LF)

STA. 4501+74, LT.	TO	STA. 4502+30, LT.	56 LF
STA. 4503+11, LT.	TO	STA. 4503+62, LT.	51 LF

526.3511 MEDIAN BARRIER TYPE IA - PRECAST (LF)

STA. 4487+70, RT.	TO	STA. 4487+84, RT.	14 LF
STA. 4490+08, RT.	TO	STA. 4490+53, RT.	45 LF
STA. 4491+47, RT.	TO	STA. 4496+00, RT.	453 LF
STA. 4498+50, RT.	TO	STA. 4498+95, RT.	45 LF
STA. 4501+05, RT.	TO	STA. 4502+22, RT.	117 LF
STA. 7488+00, RT.	TO	STA. 7492+54, RT.	454 LF
STA. 7494+00, RT.	TO	STA. 7497+11, RT.	311 LF
STA. 7497+41, RT.	TO	STA. 7497+50, RT.	9 LF
STA. 7502+32, RT.	TO	STA. 7502+55, RT.	23 LF
STA. 7502+85, RT.	TO	STA. 7503+80, RT.	95 LF
STA. 7506+48, RT.	TO	STA. 7508+12, RT.	164 LF
STA. 4488+30, LT.	TO	STA. 4489+77, LT.	147 LF
STA. 4490+07, LT.	TO	STA. 4491+65, LT.	158 LF
STA. 4491+95, LT.	TO	STA. 4492+50, LT.	55 LF
STA. 4493+00, LT.	TO	STA. 4493+48, LT.	48 LF
STA. 4493+78, LT.	TO	STA. 4495+29, LT.	151 LF
STA. 4495+59, LT.	TO	STA. 4497+13, LT.	154 LF
STA. 4497+43, LT.	TO	STA. 4498+85, LT.	142 LF
STA. 4499+15, LT.	TO	STA. 4500+92, LT.	177 LF
STA. 4504+56, LT.	TO	STA. 4504+81, LT.	25 LF
STA. 4505+11, LT.	TO	STA. 4505+30, LT.	19 LF

526.3516 MEDIAN BARRIER TYPE IE - PRECAST (LF)

STA. 4502+30, LT.	TO	STA. 4502+81, LT.	51 LF
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526.352 MEDIAN BARRIER TYPE II (LF)

STA. 4499+25, RT.	TO	STA. 4499+44, RT.	19 LF
STA. 4499+48, RT.	TO	STA. 4499+77, RT.	29 LF
STA. 4499+81, RT.	TO	STA. 4500+19, RT.	38 LF
STA. 4500+23, RT.	TO	STA. 4500+52, RT.	29 LF
STA. 4500+56, RT.	TO	STA. 4500+75, RT.	19 LF

526.3611 MEDIAN BARRIER TRANSITION TYPE IA - PRECAST (EA)

STA. 4487+50, RT.	TO	STA. 4487+70, RT.	1 EA
STA. 7487+80, RT.	TO	STA. 7488+00, RT.	1 EA
STA. 4505+30, LT.	TO	STA. 4505+50, LT.	1 EA

526.3612 MEDIAN BARRIER TRANSITION TYPE IB - PRECAST (EA)

STA. 4503+30, RT.	TO	STA. 4503+50, RT.	1 EA
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526.362 MEDIAN BARRIER TRANSITION TYPE II - PRECAST (EA)

STA. 4490+53, RT.	TO	STA. 4490+83, RT.	1 EA
STA. 4491+17, RT.	TO	STA. 4491+47, RT.	1 EA
STA. 4498+95, RT.	TO	STA. 4499+25, RT.	1 EA
STA. 4500+75, RT.	TO	STA. 4501+05, RT.	1 EA

526.3513 MEDIAN BARRIER TYPE IB - PRECAST (LF)

STA. 4487+84, RT.	TO	STA. 4490+08, RT.	224 LF
STA. 4502+22, RT.	TO	STA. 4503+30, RT.	108 LF
STA. 7497+50, RT.	TO	STA. 7498+79, RT.	129 LF
STA. 7499+09, RT.	TO	STA. 7500+42, RT.	133 LF
STA. 7501+58, RT.	TO	STA. 7502+32, RT.	74 LF
STA. 7503+80, RT.	TO	STA. 7504+37, RT.	57 LF
STA. 7504+67, RT.	TO	STA. 7506+18, RT.	151 LF
STA. 4501+22, LT.	TO	STA. 4501+36, LT.	14 LF
STA. 4504+08, LT.	TO	STA. 4504+56, LT.	48 LF

526.371 MEDIAN BARRIER WITH MOUNTED LIGHT POLE TYPE I (EA)

STA. 7497+11, RT.	TO	STA. 7497+41, RT.	1 EA
STA. 7498+79, RT.	TO	STA. 7499+09, RT.	1 EA
STA. 7500+85, RT.	TO	STA. 7501+15, RT.	1 EA
STA. 7502+55, RT.	TO	STA. 7502+85, RT.	1 EA
STA. 7504+37, RT.	TO	STA. 7504+67, RT.	1 EA
STA. 7506+18, RT.	TO	STA. 7506+48, RT.	1 EA
STA. 7508+12, RT.	TO	STA. 7508+42, RT.	1 EA
STA. 4488+00, LT.	TO	STA. 4488+30, LT.	1 EA
STA. 4489+77, LT.	TO	STA. 4490+07, LT.	1 EA
STA. 4491+65, LT.	TO	STA. 4491+95, LT.	1 EA
STA. 4493+48, LT.	TO	STA. 4493+78, LT.	1 EA
STA. 4495+29, LT.	TO	STA. 4495+59, LT.	1 EA
STA. 4497+13, LT.	TO	STA. 4497+43, LT.	1 EA
STA. 4498+85, LT.	TO	STA. 4499+15, LT.	1 EA
STA. 4500+92, LT.	TO	STA. 4501+22, LT.	1 EA
STA. 4502+81, LT.	TO	STA. 4503+11, LT.	1 EA
STA. 4504+81, LT.	TO	STA. 4505+11, LT.	1 EA

526.3514 MEDIAN BARRIER TYPE IC - PRECAST (LF)

STA. 7500+42, RT.	TO	STA. 7500+85, RT.	43 LF
STA. 7501+15, RT.	TO	STA. 7501+58, RT.	43 LF
STA. 4501+36, LT.	TO	STA. 4501+74, LT.	38 LF
STA. 4503+62, LT.	TO	STA. 4504+08, LT.	46 LF

Filename: ...MSTA...Detail-Barrier_1.dgn

Scale:

No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

By	Date	By	Date
Designed	PLP 3\20\19	Checked	LEM 3\20\19
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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

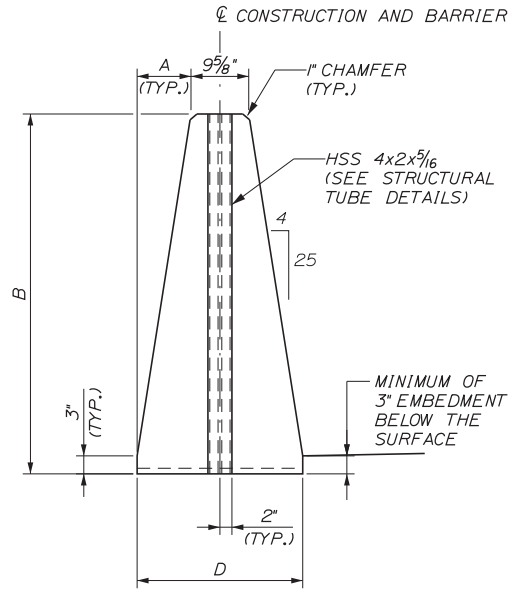
INTERCHANGE 103
ORT CONVERSION

BARRIER DETAILS 1

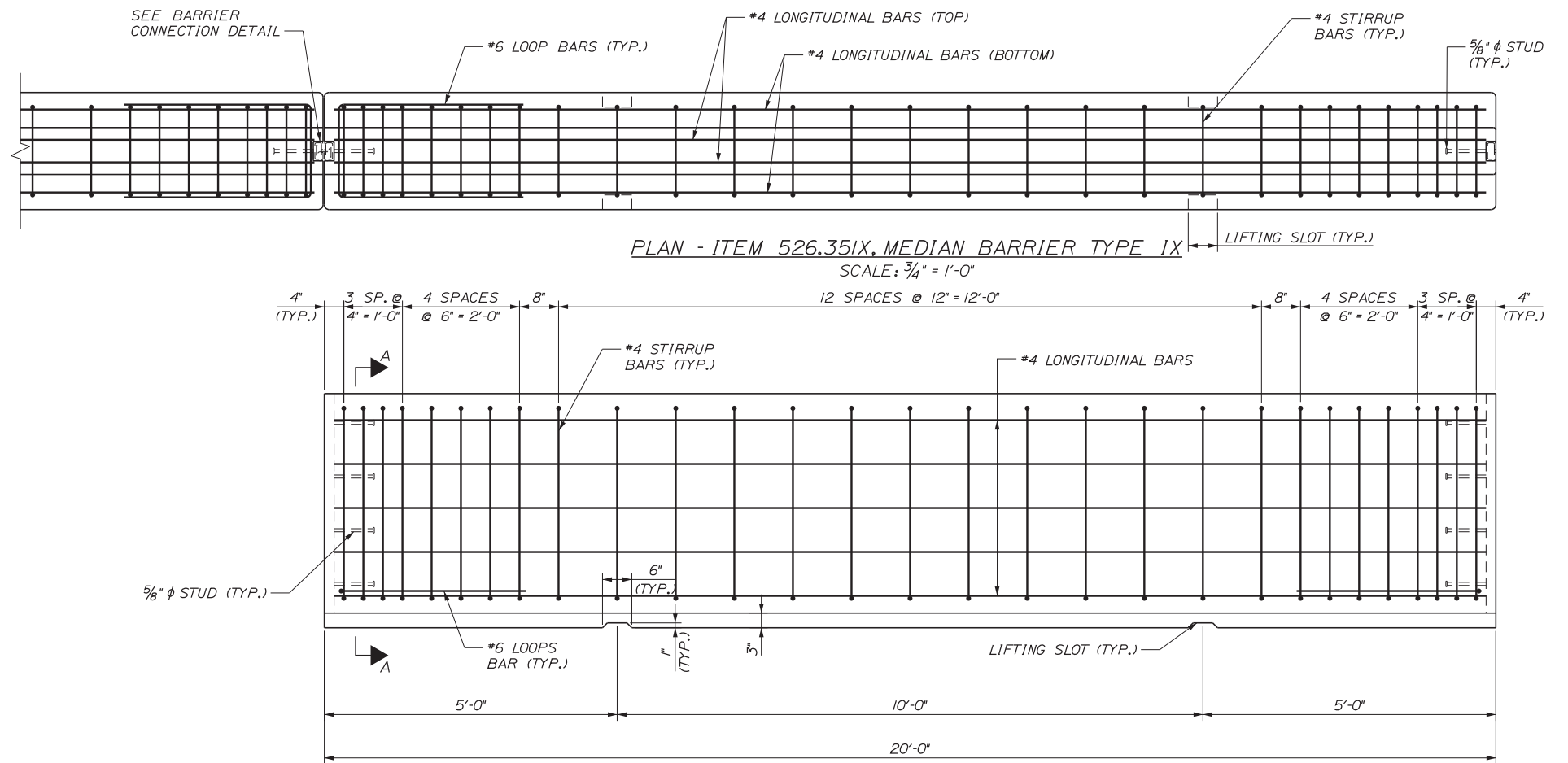
SHEET NUMBER: DET-04
103 OF 503

CONTRACT: 2019.04

Date: 3/20/2019



END VIEW (MASONRY)
SCALE: 3/4" = 1'-0"

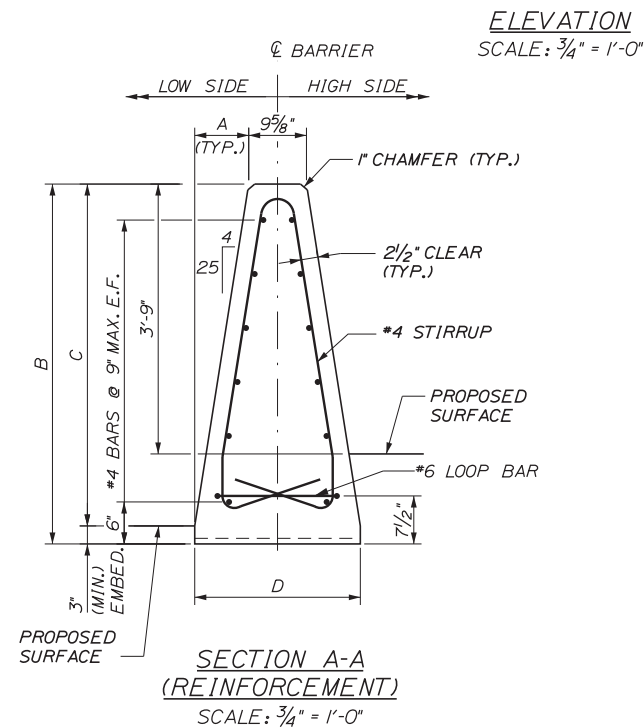


PLAN - ITEM 526.351X, MEDIAN BARRIER TYPE IX

SCALE: 3/4" = 1'-0"

ITEM NO.	DESCRIPTION	A	B	C	D
526.351	MEDIAN BARRIER TYPE I	7 1/4"	4'-0"	3'-9"	2'-0 1/8"
526.351I	MEDIAN BARRIER TYPE IA	8 1/8"	4'-6"	4'-3"	2'-1 1/8"
526.3513	MEDIAN BARRIER TYPE IB	9 1/8"	5'-0"	4'-9"	2'-3 1/8"
526.3514	MEDIAN BARRIER TYPE IC	10"	5'-6"	5'-3"	2'-5 5/8"
526.3515	MEDIAN BARRIER TYPE ID	11"	6'-0"	5'-9"	2'-7 5/8"
526.3516	MEDIAN BARRIER TYPE IE	12"	6'-6"	6'-3"	2'-9 5/8"

VARIABLE HEIGHT MEDIAN BARRIER
SCALE: 3/4" = 1'-0"



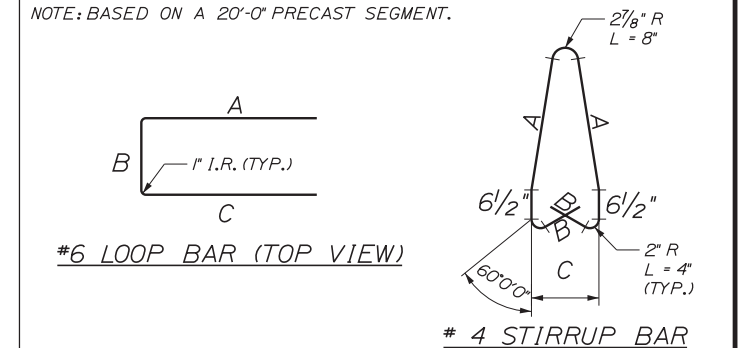
SECTION A-A (REINFORCEMENT)

SCALE: 3/4" = 1'-0"

CONCRETE BARRIER REINFORCING SCHEDULE

DESCRIPTION	BARRIER HEIGHT	SIZE	NO.	UNBENT LENGTH	TYPE	A	B	C
LONGITUDINAL (EACH FACE)	N/A	#4	VARIES	19'-8"				
STIRRUP	4'-0"	#4	29	8'-11"		2'-6"	9"	1'-2 3/4"
STIRRUP	4'-6"	#4	29	10'-1"		3'-0"	10"	1'-4 1/2"
STIRRUP	5'-0"	#4	29	11'-3"		3'-6"	11"	1'-6 1/2"
STIRRUP	5'-6"	#4	29	12'-5"		4'-0"	1'-0"	1'-8 1/4"
STIRRUP	6'-0"	#4	29	13'-7"		4'-6"	1'-1"	1'-10 1/4"
STIRRUP	6'-6"	#4	29	14'-9"		5'-0"	1'-2"	2'-0 1/4"
LOOP BAR	N/A	#6	2	7'-11"		3'-3"	1'-5"	3'-3"

NOTE: BASED ON A 20'-0" PRECAST SEGMENT.



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No.	Revision	By	Date

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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

By	Date	By	Date
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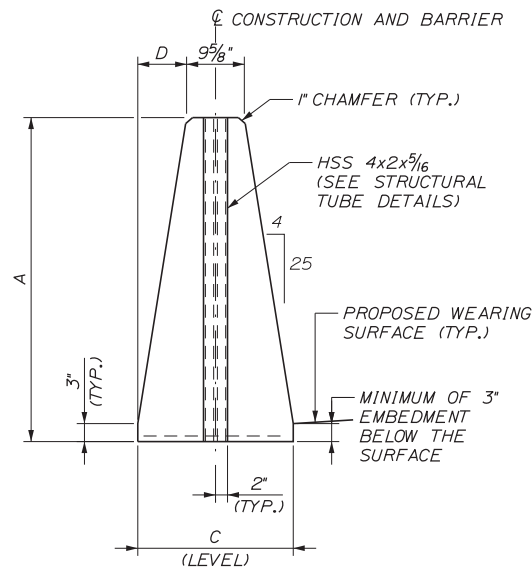
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
BARRIER DETAILS 2

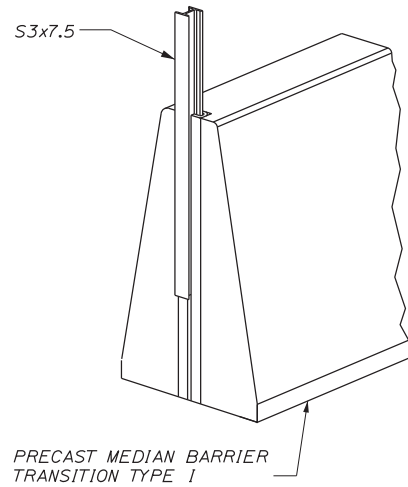
SHEET NUMBER: DET-05
104 OF 503

CONTRACT: 2019.04

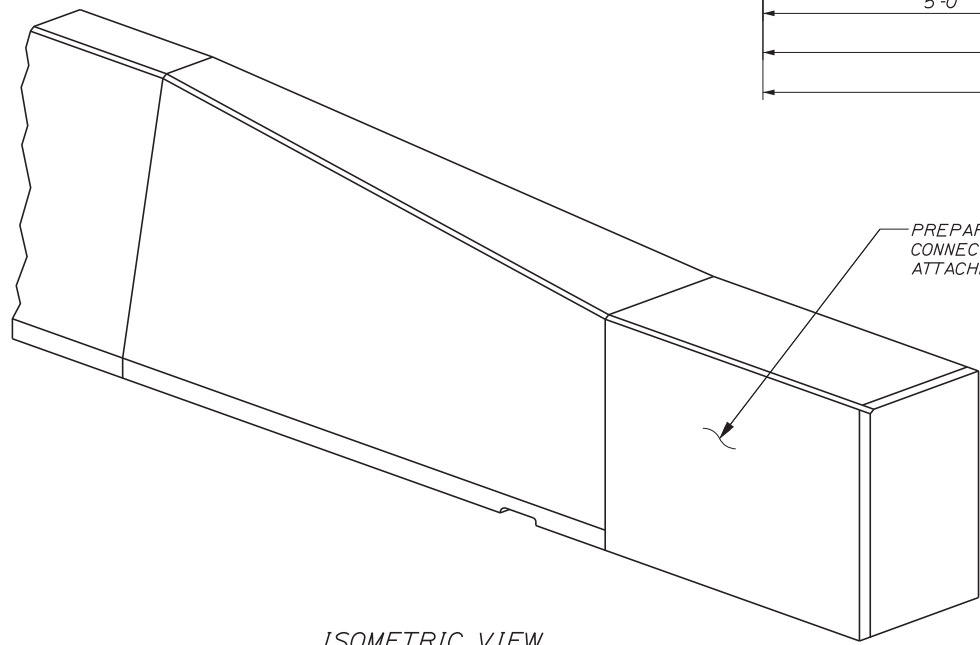
ITEM NUMBER	LOCATION	A	B	C	D
526.3611	STA. 4487+50 TO STA. 4487+70, RT	4'-6"	3'-9"	2'-11/8"	8/8"
526.3612	STA. 4503+30 TO STA. 4503+50, RT	5'-0"	4'-3"	2'-3/8"	9/8"
526.3611	STA. 4505+45 TO STA. 4505+65, LT	4'-6"	3'-9"	2'-11/8"	8/8"
526.3611	STA. 7487+80 TO STA. 7488+00, RT	4'-6"	3'-9"	2'-11/8"	8/8"



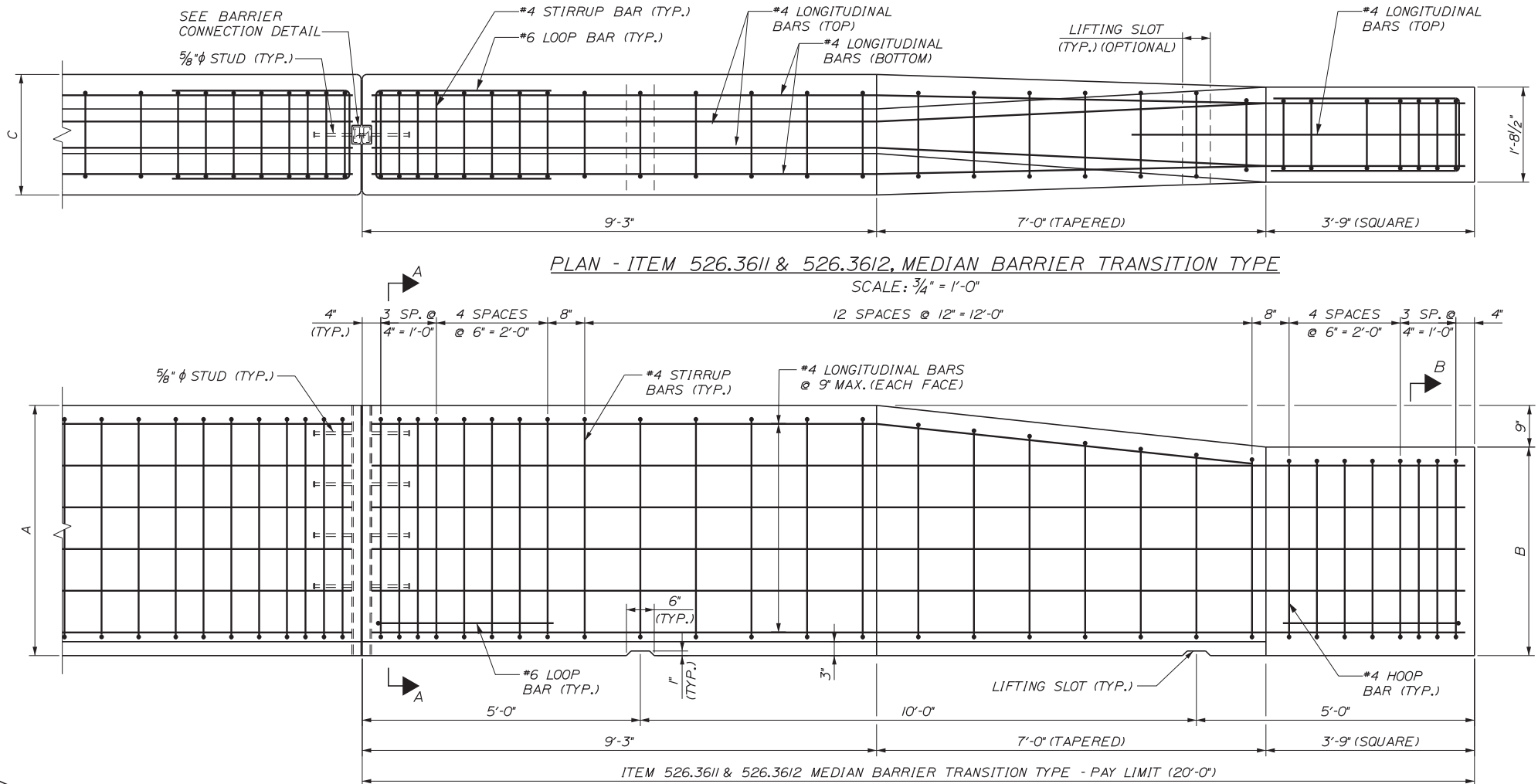
END VIEW (MASONRY)
SCALE: 3/4" = 1'-0"



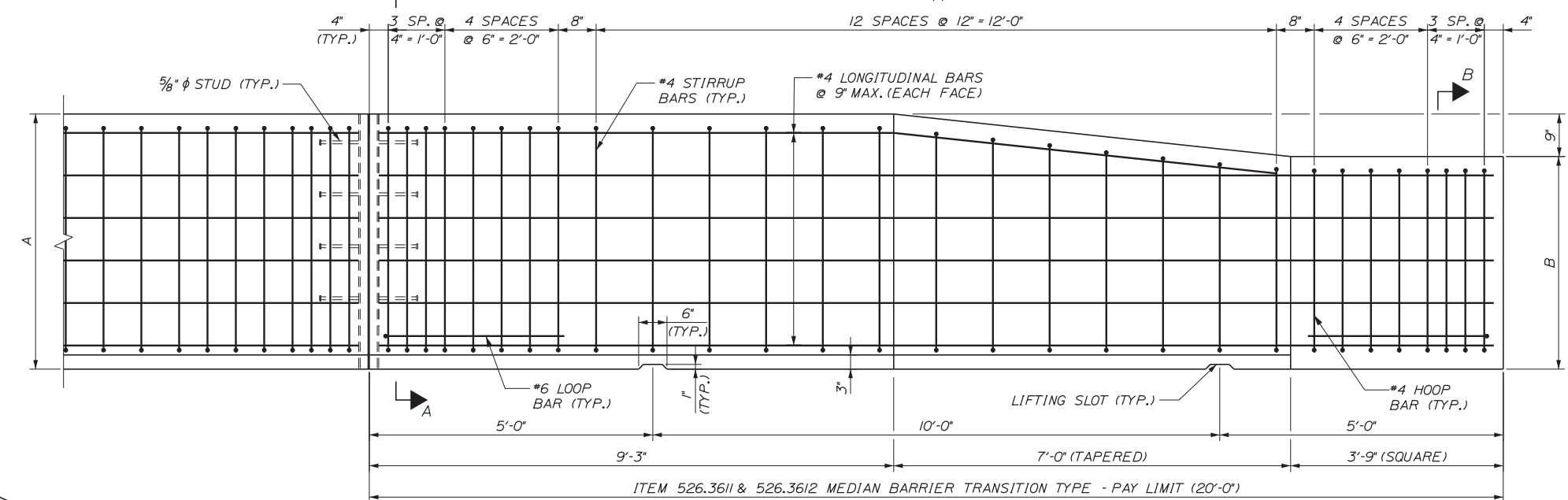
ISOMETRIC VIEW
UNTAPERED END
NOT TO SCALE



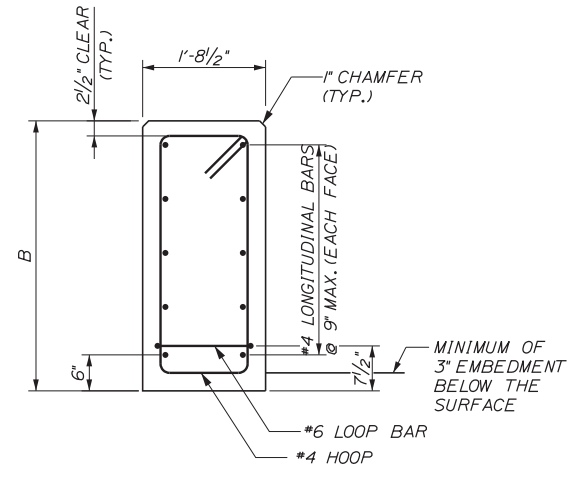
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TAPERED END
NOT TO SCALE



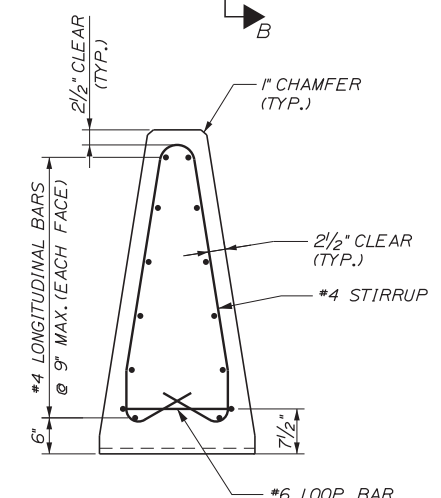
PLAN - ITEM 526.3611 & 526.3612, MEDIAN BARRIER TRANSITION TYPE
SCALE: 3/4" = 1'-0"



ELEVATION
SCALE: 3/4" = 1'-0"



SECTION B-B
(MASONRY AND REINFORCEMENT)
SCALE: 3/4" = 1'-0"



SECTION A-A
(REINFORCEMENT)
SCALE: 3/4" = 1'-0"

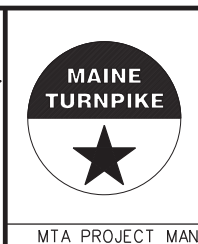
Date: 3/20/2019

Filename: ...MSTA...Detail-Barrier_3.dgn

No.	Revision	By	Date

Designed by:			
CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
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	PLP	3\20\19	LEM
	EJB	3\20\19	GAE

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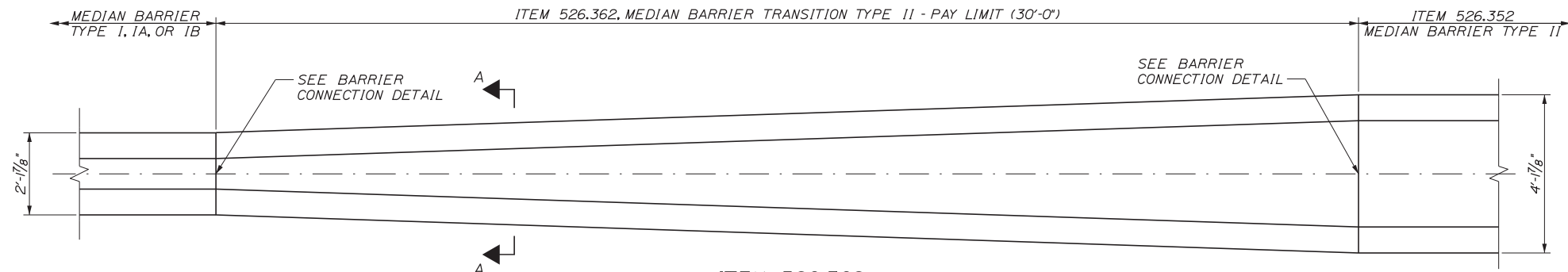


THE GOLD STAR
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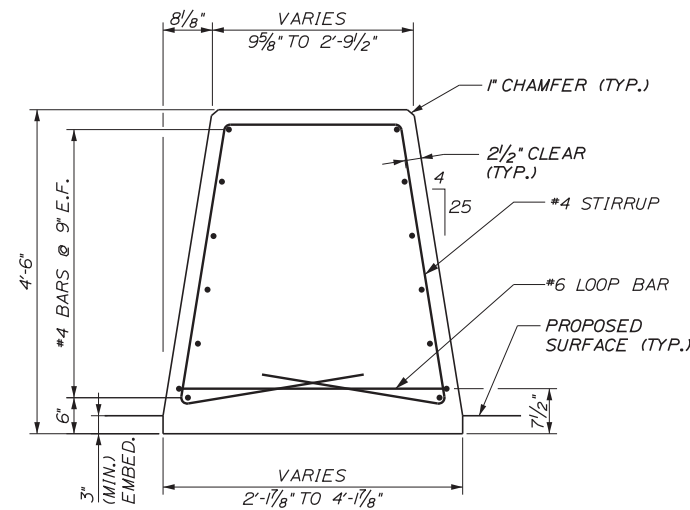
INTERCHANGE 103
ORT CONVERSION
BARRIER DETAILS 3

SHEET NUMBER: DET-06
CONTRACT: 2019.04
105 OF 503

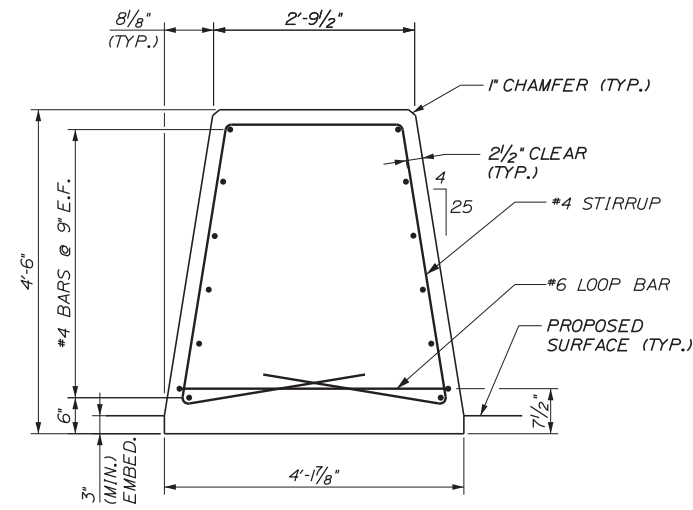
Date: 3/20/2019



ITEM 526.362
 MEDIAN BARRIER TRANSITION TYPE II
 SCALE: 1/2" = 1'-0"



SECTION A-A
 SCALE: 3/4" = 1'-0"



ITEM 526.352
 MEDIAN BARRIER TYPE II
 SCALE: 3/4" = 1'-0"

NOTES

- BARRIER FROM STA 4490+83.00, RT. TO STA. 4491+17.00, RT. SHALL BE INCIDENTAL TO ITEM 645.125. THE BARRIER SHALL HAVE THE SHAPE OF MEDIAN BARRIER TYPE II.
- BARRIER FROM STA 4499+43.75 RT. TO STA. 4499+47.75 RT., STA 4499+77.00 RT. TO STA. 4499+81.00 RT., STA 4500+19.00 RT. TO STA. 4500+23.00 RT. AND STA 4500+52.25 RT. TO STA. 4500+56.25 RT. IS PART OF THE ORT SPACE FRAME PEDESTAL AND SHALL BE PAID FOR UNDER ITEM 502.231. THE BARRIER SHALL HAVE THE SHAPE OF MEDIAN BARRIER TYPE II.

Filename: ...MSTA\...Detail-Barrier_4.dgn

Scale:			
No.	Revision	By	Date

Designed by:					
CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE					
	By	Date		By	Date
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Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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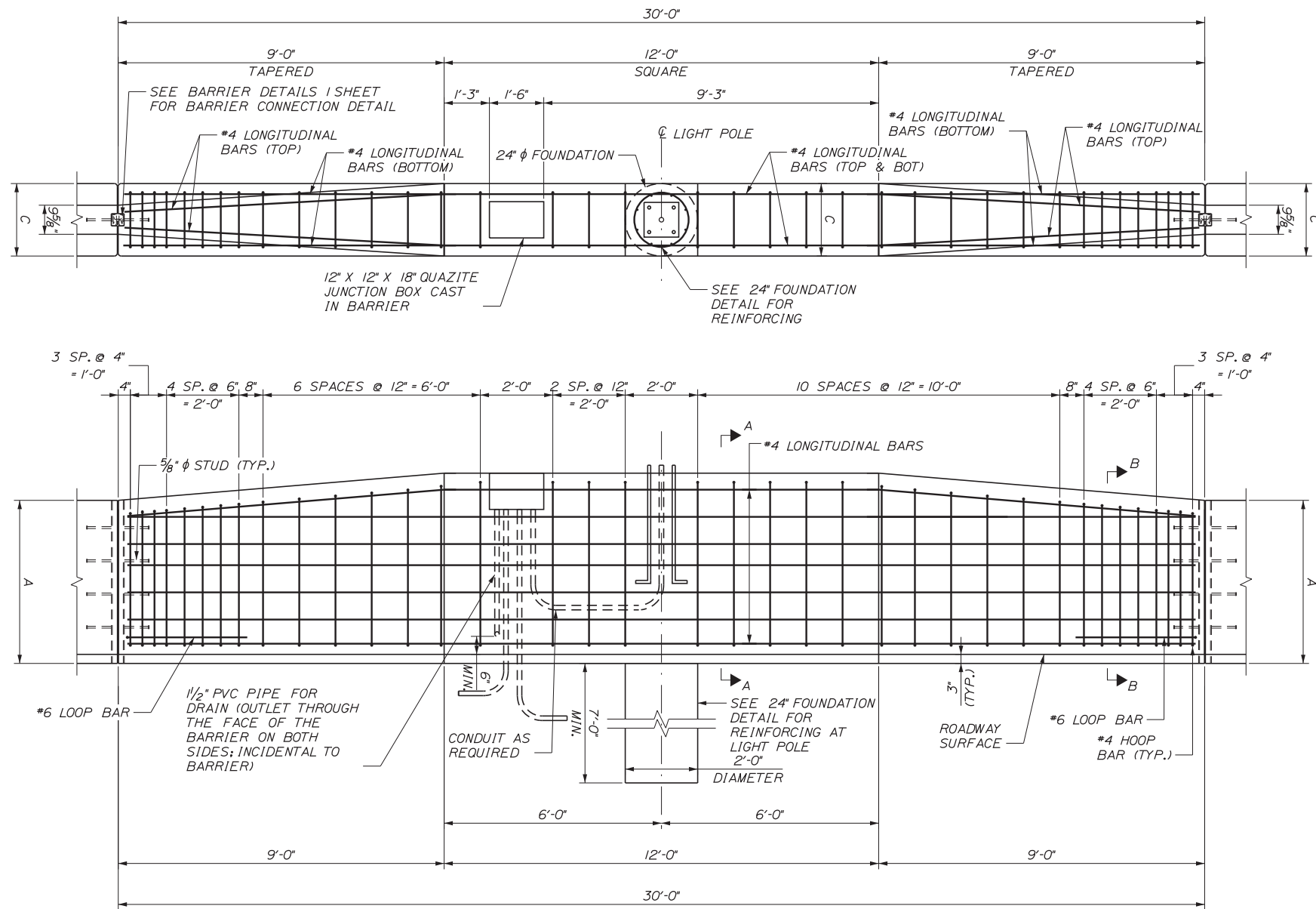
**THE GOLD STAR
 MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 BARRIER DETAILS 4

SHEET NUMBER: DET-07
 CONTRACT: 2019.04
 106 OF 503

Date: 3/20/2019

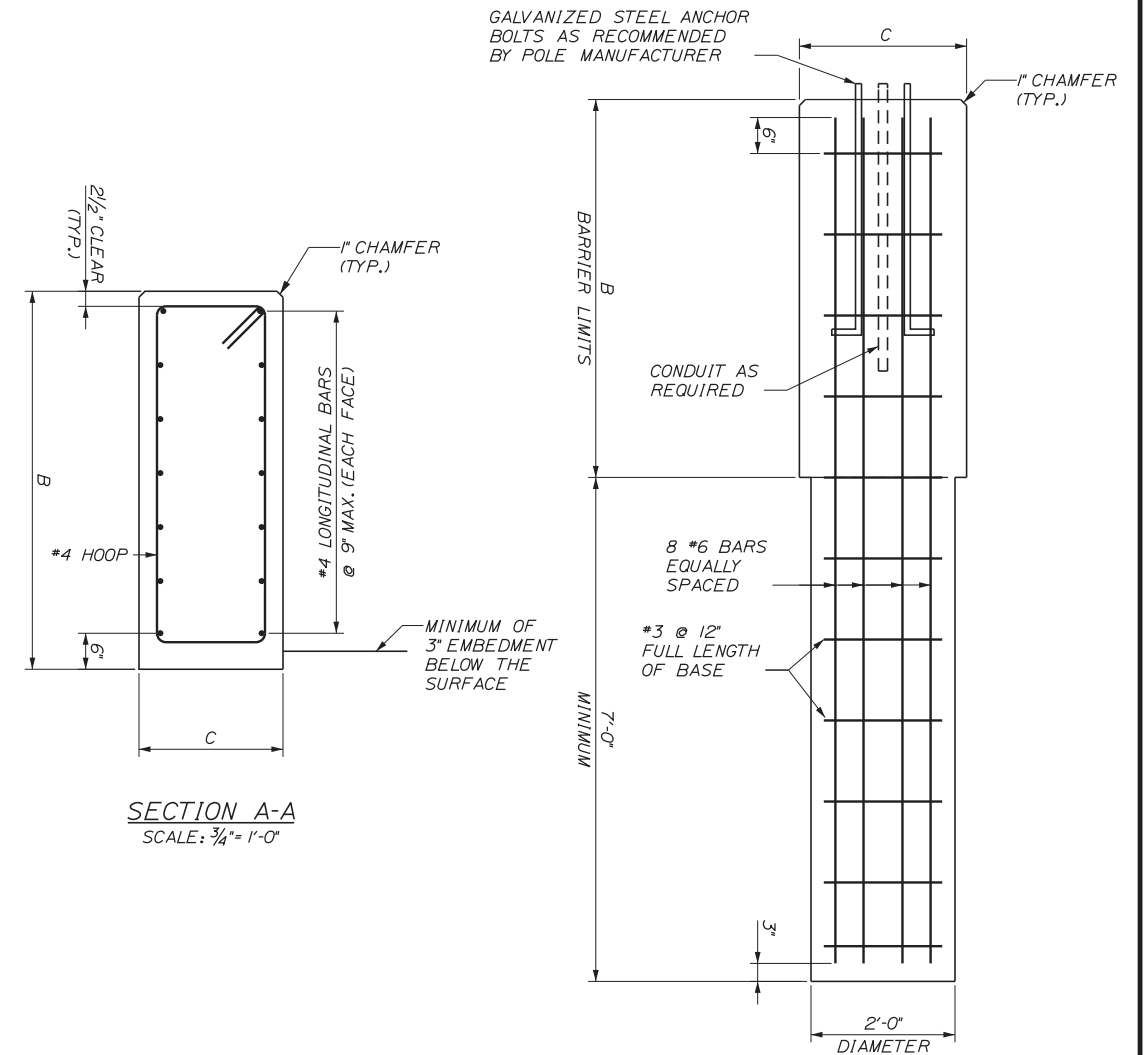


526.371 MEDIAN BARRIER WITH MOUNTED LIGHT POLE TYPE I
SCALE: 1/2" = 1'-0"

LOCATION	A	B	C
4488+00.00, LT. TO 4488+30.00, LT.	4'-6"	5'-3"	2'-11/8"
4489+77.29, LT. TO 4490+07.29, LT.	4'-6"	5'-3"	2'-11/8"
4491+64.59, LT. TO 4491+94.59, LT.	4'-6"	5'-3"	2'-11/8"
4493+47.50, LT. TO 4493+77.50, LT.	4'-6"	5'-3"	2'-11/8"
4495+28.84, LT. TO 4495+58.84, LT.	4'-6"	5'-3"	2'-11/8"
4497+12.50, LT. TO 4497+42.50, LT.	4'-6"	5'-3"	2'-11/8"
4498+85.00, LT. TO 449+15.00, LT.	4'-6"	5'-3"	2'-11/8"
4500+91.50, LT. TO 4501+21.50, LT.	4'-6"	5'-3"	2'-11/8"
4502+80.50, LT. TO 4503+10.50, LT.	6'-6"	7'-3"	2'-9 5/8"
4504+81.25, LT. TO 4505+11.25, LT.	5'-0"	5'-9"	2'-3 3/8"
7497+10.98, RT. TO 7497+40.98, RT.	4'-6"	5'-3"	2'-11/8"
7498+78.50, RT. TO 7499+08.50, RT.	5'-0"	5'-9"	2'-3 3/8"
7500+85.00, RT. TO 7501+15.00, RT.	5'-6"	6'-3"	2'-5 7/8"
7502+54.65, RT. TO 7502+84.65, RT.	4'-6"	5'-3"	2'-11/8"
7504+36.78, RT. TO 7504+66.78, RT.	5'-0"	5'-9"	2'-3 3/8"
7506+18.07, RT. TO 7506+48.07, RT.	5'-0"	5'-9"	2'-3 3/8"
7508+11.72, RT. TO 7508+41.72, RT.	4'-6"	5'-3"	2'-11/8"

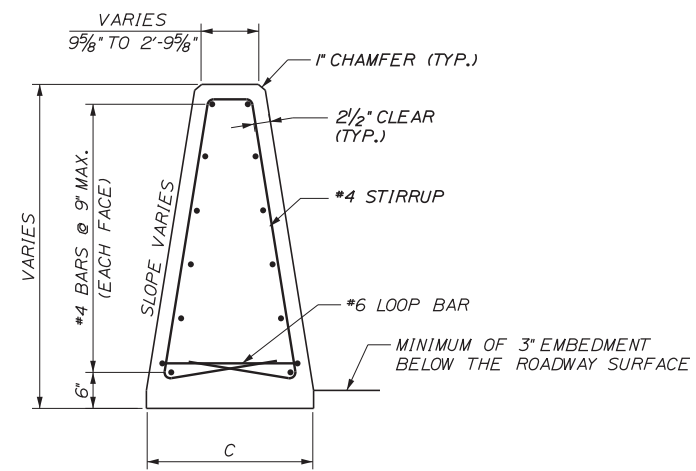
BARRIER NOTES

- EACH MEDIAN BARRIER WITH MOUNTED LIGHT POLE TYPE I UNIT SHALL INCLUDE TWO S3x7.5 AS SHOWN ON THE BARRIER DETAILS I SHEET.
- JUNCTION BOXES SHALL BE PLACED IN A BED OF NON-SHRINK GROUT. COST OF FURNISHING AND PLACING MORTAR SHALL BE INCIDENTAL TO BARRIER.
- BARRIER REINFORCEMENT AT JUNCTION BOXES SHALL BE TRIMMED TO FIT, AS DIRECTED BY THE RESIDENT.
- THE 24" FOUNDATION, JUNCTION BOX AND CONDUIT UNLESS NOTED OTHERWISE SHALL BE PAID FOR UNDER THEIR RESPECTIVE ITEMS.



SECTION A-A
SCALE: 3/4" = 1'-0"

24" FOUNDATION
SCALE: 3/4" = 1'-0"



SECTION B-B
SCALE: 3/4" = 1'-0"

No.	Revision	By	Date

Designed by:			
CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
Designed	PLP	3\20\19	Checked
Drawn	EJB	3\20\19	In Charge of
			By
			Date
			By
			Date

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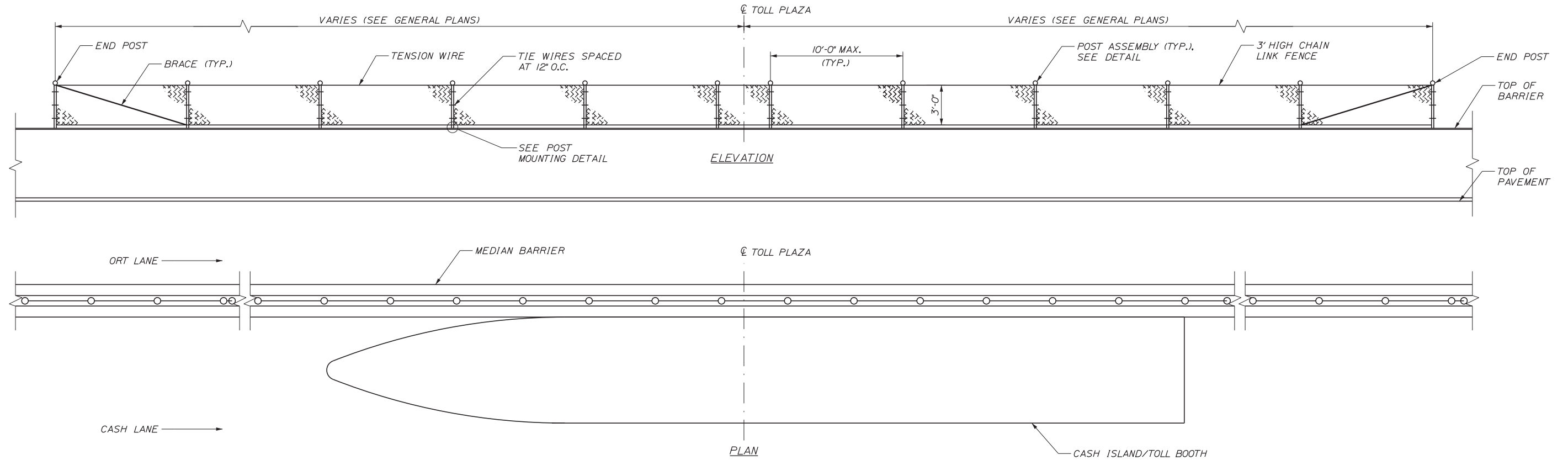
**THE GOLD STAR
MEMORIAL HIGHWAY**

INTERCHANGE 103
ORT CONVERSION
BARRIER DETAILS 5

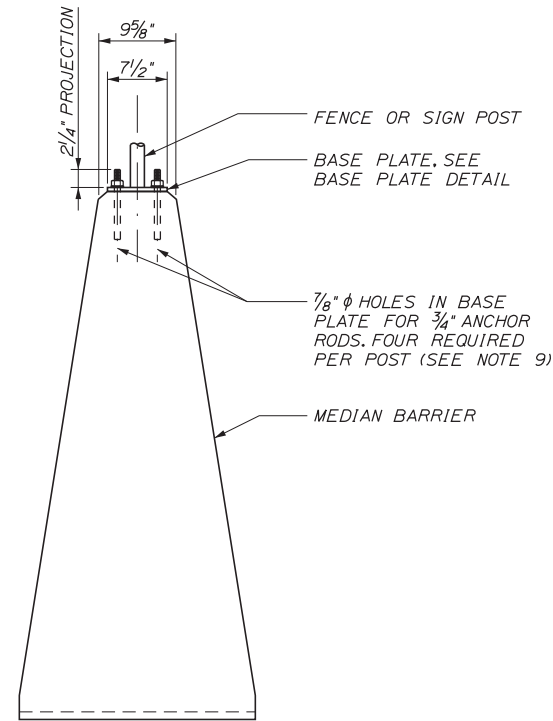
SHEET NUMBER: DET-08
CONTRACT: 2019.04
107 OF 503

Filename: ...MSTA...Detail-Barrier_5.dgn

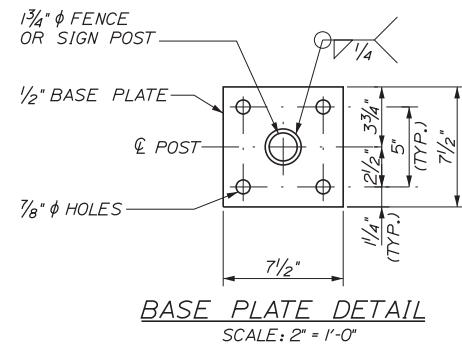
Date: 3/20/2019



BARRIER MOUNTED CHAIN LINK FENCE
NOT TO SCALE



POST MOUNTING DETAIL
SCALE: 1" = 1'-0"



BASE PLATE DETAIL
SCALE: 2" = 1'-0"

NOTES:

1. BREAKAWAY POSTS SHALL NOT BE USED FOR BARRIER MOUNTED SIGNS OR FENCE.
2. DRILLING AND GROUTING OF BARRIER, FURNISHING AND INSTALLING OF ANCHOR RODS, AND BASE PLATES SHALL BE SUBSIDIARY TO ITEM 607.41, POST ASSEMBLIES FOR SIGN OR CHAIN LINK FENCE.
3. ANCHOR RODS SHALL BE GROUTED TO BARRIER WITH A MATERIAL LISTED ON THE MAINE DEPARTMENT OF TRANSPORTATION PREQUALIFIED LIST OF CHEMICAL ANCHORING MATERIALS. ANCHOR RODS SHALL MEET THE REQUIREMENTS OF ASTM F1554, GRADE 55 AND BE SWEDGED OR THREADED ON THE EMBEDDED PORTION. THE DEPTH OF EMBEDMENT SHALL BE SUFFICIENT TO DEVELOP THE FULL TENSILE STRENGTH OF THE ANCHOR ROD.
4. BRACING AT ENDS OF FENCE SHALL BE INCIDENTAL TO THE FENCE. TENSION WIRE ALONG THE TOP OF THE FENCE SHALL BE INCIDENTAL TO THE FENCE.
5. SIGNS SHALL BE ANGLED TOWARD TRAFFIC SO THAT NO PORTION OF THE SIGN PROTRUDES BEYOND THE LIMIT OF THE TOP WIDTH OF THE BARRIER.
6. BOTTOM OF SIGN SHALL BE MOUNTED 5'-0" ABOVE THE EDGE OF PAVEMENT ON THE HIGH SIDE.
7. ATTACHMENT OF SIGNS TO POSTS MUST PREVENT ROTATION. HARDWARE SHALL BE GALVANIZED OR STAINLESS STEEL AND WILL BE INCIDENTAL TO THE SIGN. METHOD SHALL BE APPROVED BY RESIDENT.
8. ALL EXPOSED STEEL SHALL BE HOT DIPPED GALVANIZED UNLESS NOTED OTHERWISE.
9. THE POST ANCHOR RODS SHALL BE LOCATED TO AVOID CONFLICTS WITH THE BARRIER REINFORCING STEEL AT THE CONTRACTOR'S OPTION. ANCHORAGES MAY BE PRECAST IN TO THE BARRIER. DETAILS FOR ANCHORAGES TO BE PRECAST INTO THE BARRIER SHALL BE SUBMITTED TO THE RESIDENT FOR APPROVAL.
10. CHAIN LINK SHALL BE STANDARD 2 INCH MESH NO. 9 BARBED FINISH. POSTS AND CHAIN LINK SHALL BE HOT DIP GALVANIZED.

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Scale:			
No.	Revision	By	Date

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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

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THE GOLD STAR MEMORIAL HIGHWAY

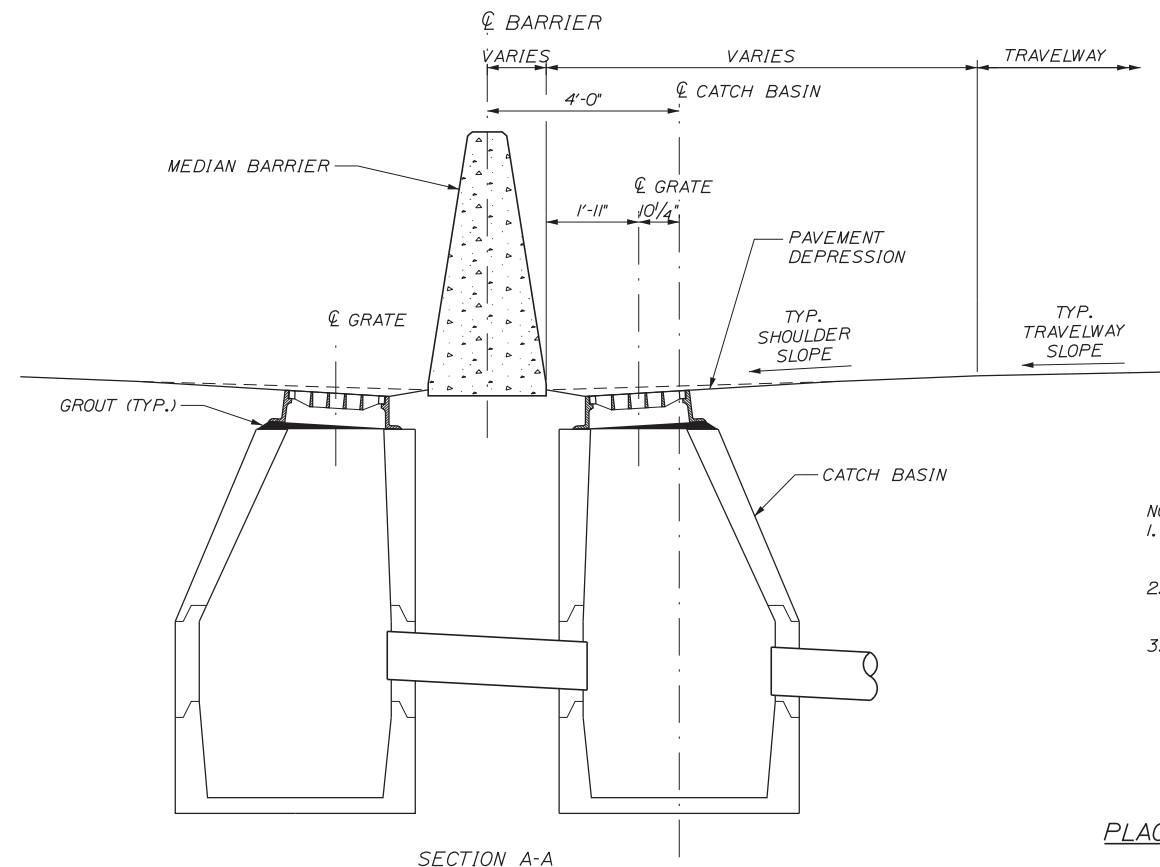
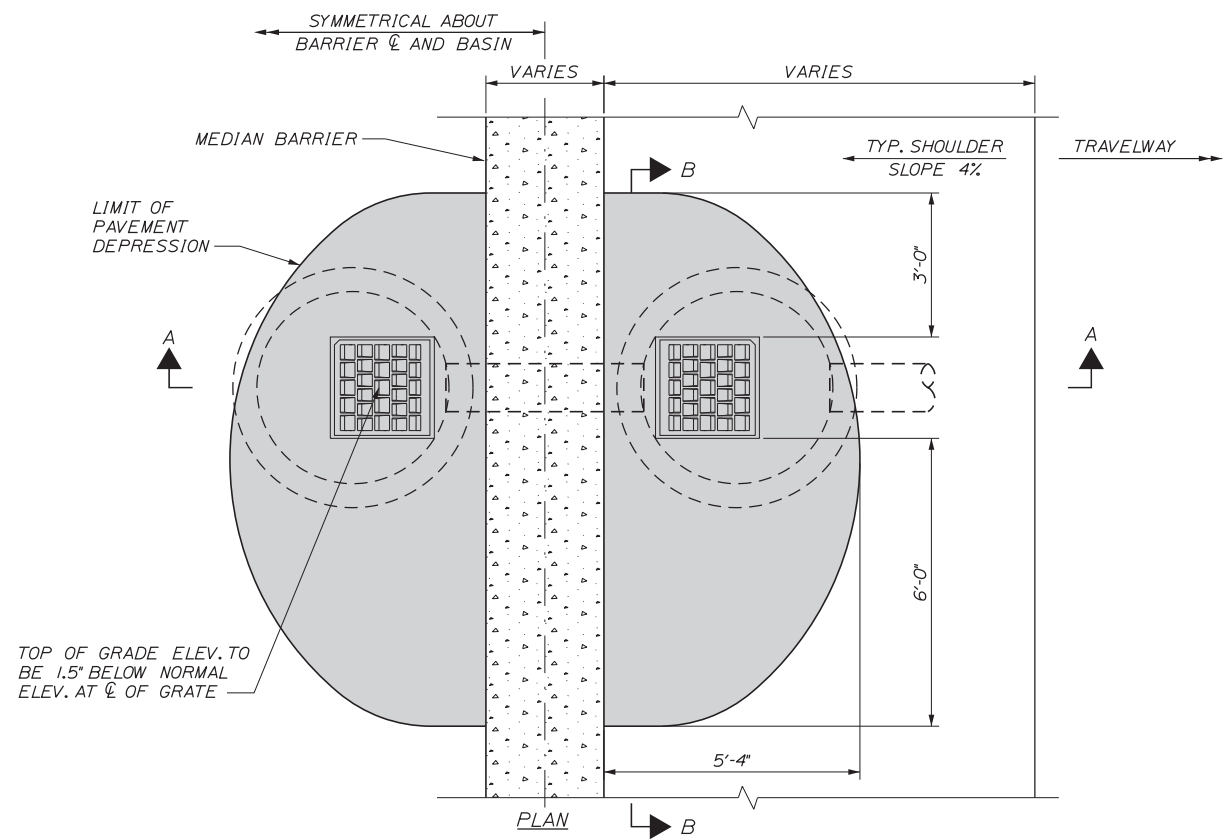
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
BARRIER DETAILS 6

SHEET NUMBER: DET-09
CONTRACT: 2019.04
108 OF 503

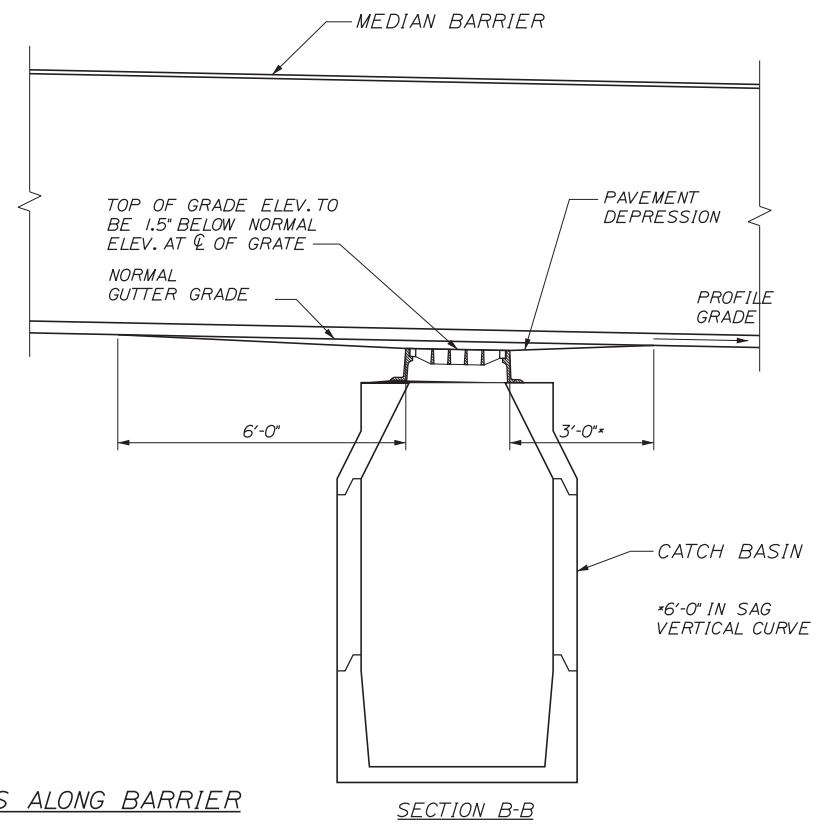
Date: 3/20/2019

Filename: ...MSTA\...Detail-Drain_01.dgn



- NOTES:
1. SHADED AREA REPRESENTS APPROXIMATE LIMITS OF PAVEMENT DEPRESSION.
 2. GRATE SHALL BE PLACED 1.5' BELOW AND PARALLEL TO THE NORMAL PAVEMENT GRADE.
 3. FRESHLY PLACED PAVEMENT SHALL BE REMOVED BEFORE COMPACTING DEPRESSED AREA.

PLACEMENT OF DRAINAGE STRUCTURES ALONG BARRIER
NOT TO SCALE



Scale:

No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

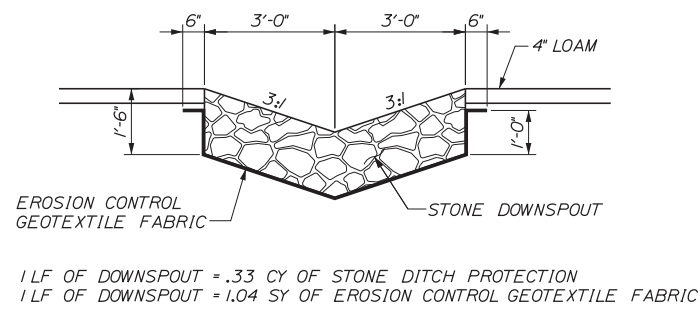
INTERCHANGE 103
ORT CONVERSION

DRAINAGE DETAILS 1

SHEET NUMBER: DET-10
109 OF 503

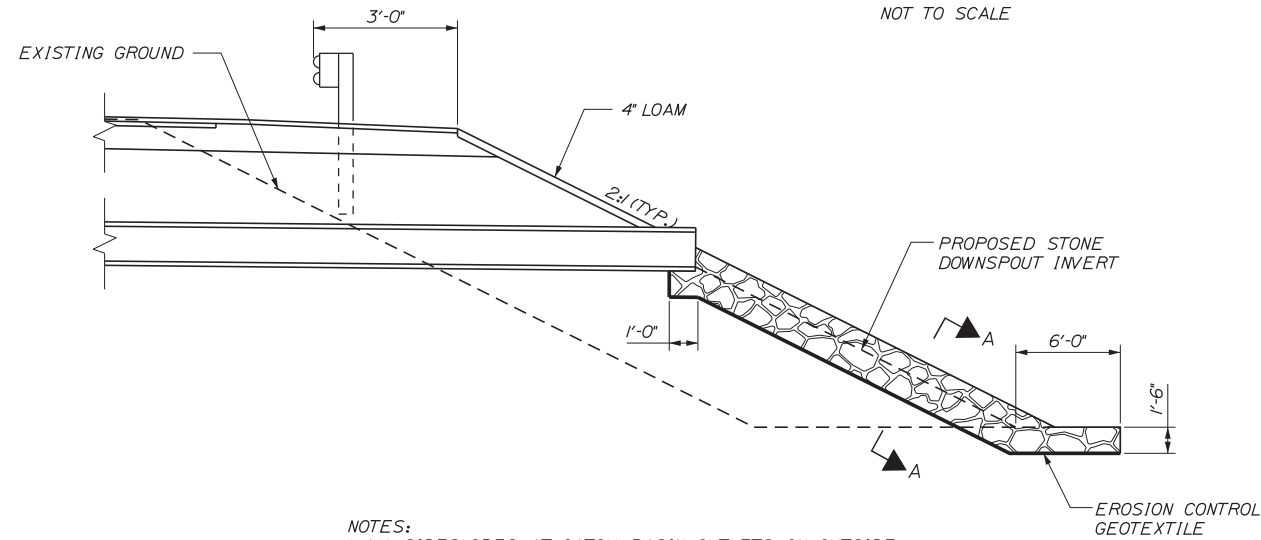
CONTRACT: 2019.04

Date: 3/20/2019



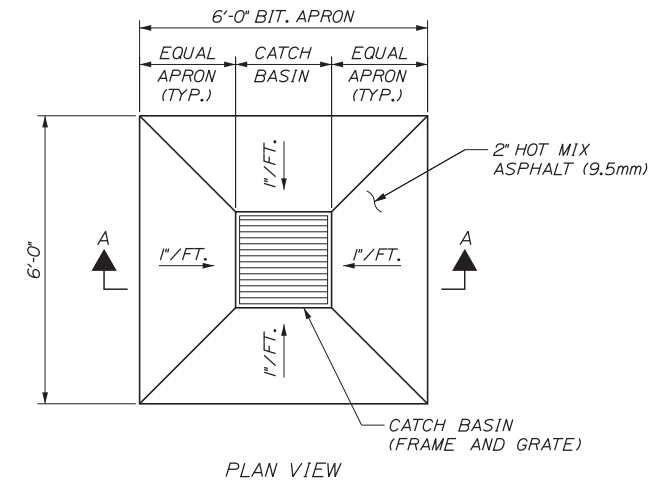
1 LF OF DOWNSPOUT = .33 CY OF STONE DITCH PROTECTION
 1 LF OF DOWNSPOUT = 1.04 SY OF EROSION CONTROL GEOTEXTILE FABRIC

SECTION A-A
NOT TO SCALE

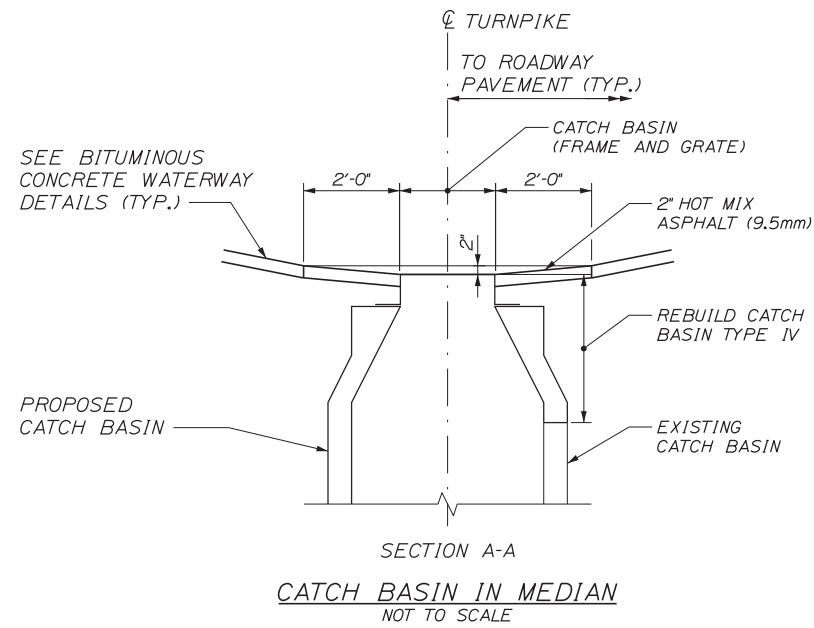


NOTES:
 1. ALL SIDESLOPES AT CATCH BASIN OUTLETS ON OUTSIDE EDGE OF SHOULDER SHALL HAVE STONE DOWNSPOUTS.

CATCH BASIN OUTLET ON OUTSIDE EDGE OF SHOULDER
NOT TO SCALE

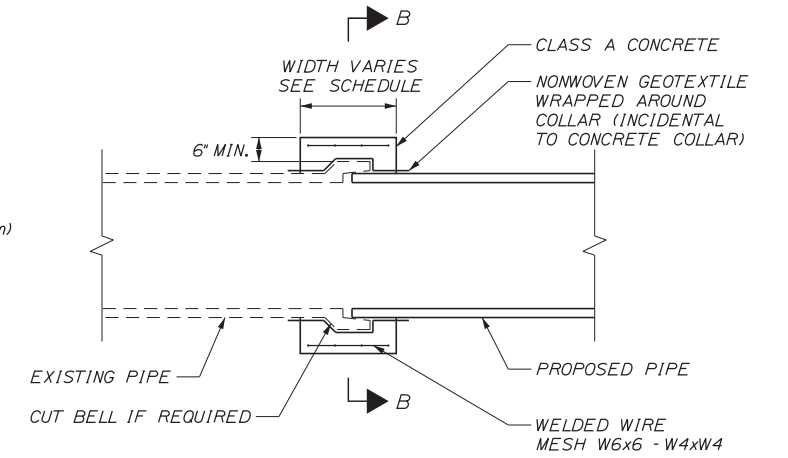


PLAN VIEW



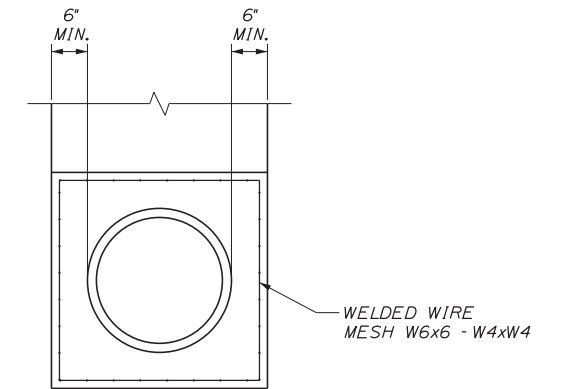
SECTION A-A

CATCH BASIN IN MEDIAN
NOT TO SCALE



NOTES:

1. CONNECTION AND PIPE TO BE BACKFILLED PER ASTM D2321 (CRUSHED STONE).
2. CONCRETE SHALL BE CLASS A FIBER REINFORCED.
3. SEE CONCRETE COLLAR WIDTH SCHEDULE FOR DIMENSIONS.



SECTION B-B

CAST IN PLACE CONCRETE COLLAR FOR DRAINAGE PIPES

Filename: ...MSTA...Detail-Drain_02.dgn

Scale:			
No.	Revision	By	Date

Designed by:			
CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
	By	Date	
	By	Date	
	By	Date	

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THE GOLD STAR MEMORIAL HIGHWAY

INTERCHANGE 103
 ORT CONVERSION

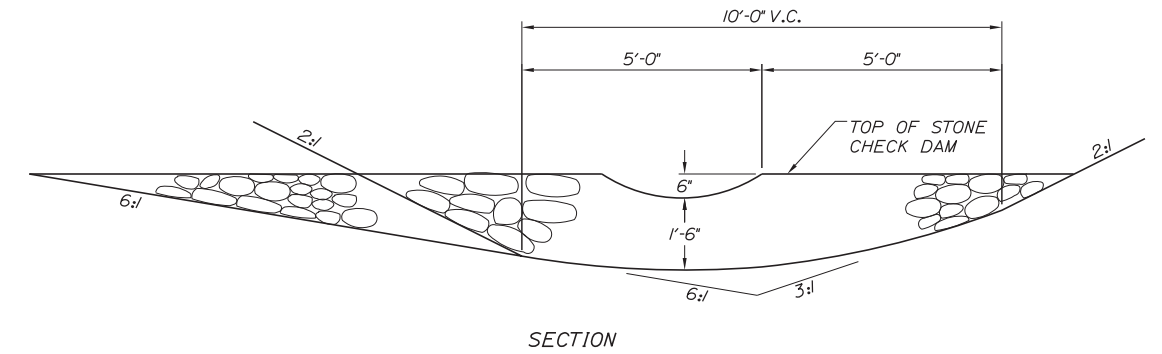
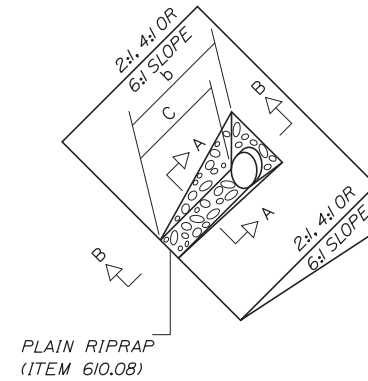
DRAINAGE DETAILS 2

SHEET NUMBER: DET-11
 CONTRACT: 2019.04
 110 OF 503

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

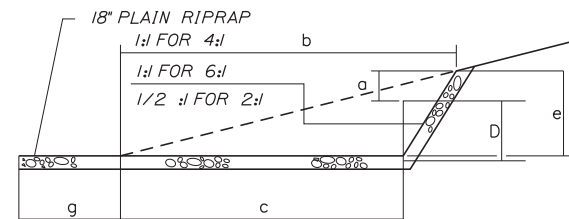
DIMENSIONS FOR SLOPE OF 2:1

D	a (FT)	b (FT)	c (FT)	e (FT)	f (FT)	g (FT)	STONE DEPTH (FT)	STONE (CY)
12"	1.00	4.00	3.00	2.00	6.00	1.00	1.50	1.30
15"	1.00	4.50	3.37	2.25	6.75	1.63	1.50	1.70
18"	1.00	5.00	3.75	2.50	7.50	2.25	1.50	2.09
21"	1.00	5.50	4.13	2.75	8.25	2.88	1.50	2.58
24"	1.00	6.00	4.50	3.00	9.00	3.50	1.50	3.12
30"	1.00	7.00	5.25	3.50	10.50	4.75	1.50	4.33
36"	1.00	8.00	6.00	4.00	12.00	6.00	1.50	5.75
42"	1.00	9.00	6.75	4.50	13.50	7.25	1.50	7.37
48"	1.00	10.00	7.50	5.00	15.00	8.50	1.50	9.18
54"	1.00	11.00	8.25	5.50	16.50	9.75	1.50	11.19
60"	1.00	12.00	9.00	6.00	18.00	11.00	1.50	13.40
66"	1.00	13.00	9.75	6.50	19.50	12.25	1.50	15.81
72"	1.00	14.00	10.50	7.00	21.00	13.50	1.50	18.41
84"	1.00	16.00	12.00	8.00	24.00	16.00	1.50	24.22

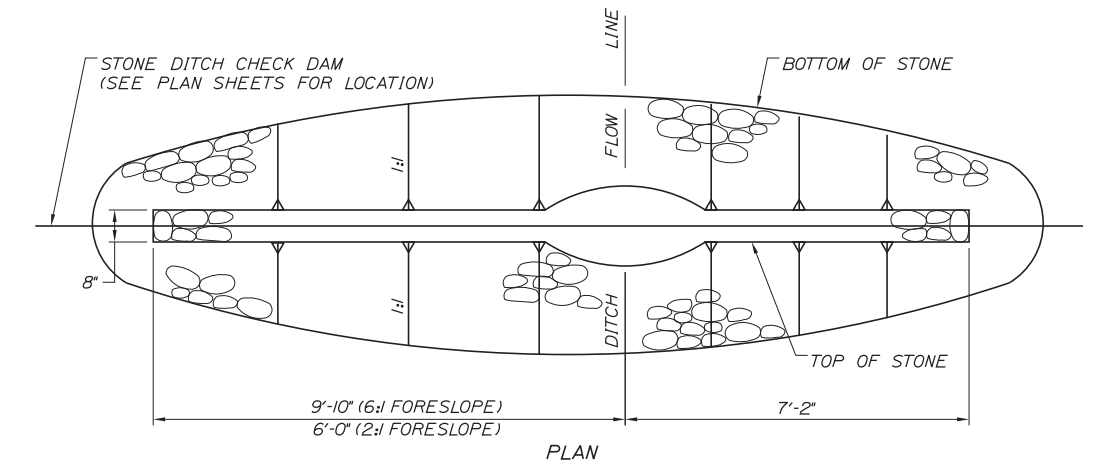


DIMENSIONS FOR SLOPE OF 4:1

D	a (FT)	b (FT)	c (FT)	e (FT)	f (FT)	g (FT)	STONE DEPTH (FT)	STONE (CY)
12"	1.00	8.00	6.00	2.00	6.00	0.00	1.50	2.20
15"	1.00	9.00	6.75	2.25	6.75	0.00	1.50	2.80
18"	1.00	10.00	7.50	2.50	7.50	0.00	1.50	3.40
21"	1.00	11.00	8.25	2.75	8.25	0.00	1.50	4.10
24"	1.00	12.00	9.00	3.00	9.00	0.00	1.50	4.86
30"	1.00	14.00	10.50	3.50	10.50	0.00	1.50	6.58
36"	1.00	16.00	12.00	4.00	12.00	0.00	1.50	8.56
42"	1.00	18.00	13.50	4.50	13.50	0.50	1.50	10.92
48"	1.00	20.00	15.00	5.00	15.00	1.00	1.50	13.57
54"	1.00	22.00	16.50	5.50	16.50	1.50	1.50	16.50
60"	1.00	24.00	18.00	6.00	18.00	2.00	1.50	19.72
66"	1.00	26.00	19.50	6.50	19.50	2.50	1.50	23.22
72"	1.00	28.00	21.00	7.00	21.00	3.00	1.50	27.01
84"	1.00	32.00	24.00	8.00	24.00	4.00	1.50	35.45



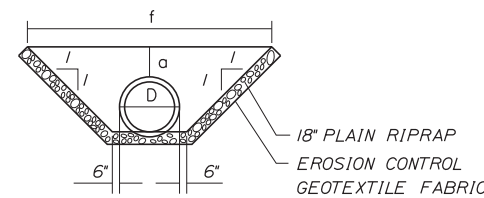
SECTION B-B



PLAN

DIMENSIONS FOR SLOPE OF 6:1

D	a (FT)	b (FT)	c (FT)	e (FT)	f (FT)	g (FT)	STONE DEPTH (FT)	STONE (CY)
12"	0.50	9.00	7.50	1.50	4.50	0.00	1.50	2.30
15"	0.50	10.50	8.75	1.75	5.50	0.00	1.50	2.93
18"	0.50	12.00	10.00	2.00	6.50	0.00	1.50	3.57
21"	0.50	13.50	11.25	2.25	7.25	0.00	1.50	4.46
24"	0.50	15.00	12.50	2.50	8.00	0.00	1.50	5.44
30"	0.50	18.00	15.00	3.00	9.50	0.00	1.50	7.71
36"	0.50	21.00	17.50	3.50	11.00	0.00	1.50	10.37
42"	0.50	24.00	20.00	4.00	12.50	0.00	1.50	13.42
48"	0.50	27.00	22.50	4.50	14.00	0.00	1.50	16.87
54"	0.50	30.00	25.00	5.00	15.50	0.00	1.50	20.70
60"	0.50	33.00	27.50	5.50	17.00	0.00	1.50	24.93
66"	0.50	36.00	30.00	6.00	18.50	0.00	1.50	29.55
72"	0.50	39.00	32.50	6.50	20.00	0.00	1.50	34.56
84"	0.50	45.00	37.50	7.50	23.00	0.00	1.50	45.76



SECTION A-A

ROADWAY CULVERT END SLOPE TREATMENT

NOT TO SCALE

- NOTES:
1. THE DIMENSIONS SHOWN ARE APPROXIMATE AND MAY BE MODIFIED BY THE RESIDENT.
 2. STONE QUANTITIES ARE FOR ONE END OF THE PIPE.

FORESLOPE	BACKSLOPE	QUANTITY C.Y. STONE
6:1	3:1	2.5
2:1	3:1	2.0

- NOTES:
1. STONE FOR TEMPORARY AND PERMANENT STONE CHECK DAMS SHALL MEET THE REQUIREMENTS OF MDT SPECIFICATION 703.29, STONE DITCH PROTECTION.
 2. TEMPORARY STONE CHECK DAMS WILL BE PAID FOR UNDER ITEM 610.181.

STONE CHECK DAM

NOT TO SCALE

Date: 3/20/2019

Filename: ...MSTA...Detail-Erosion_01.dgn

No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

By	Date	By	Date
Designed	PLP 3\20\19	Checked	LEM 3\20\19
Drawn	EJB 3\20\19	In Charge of	GAE 3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

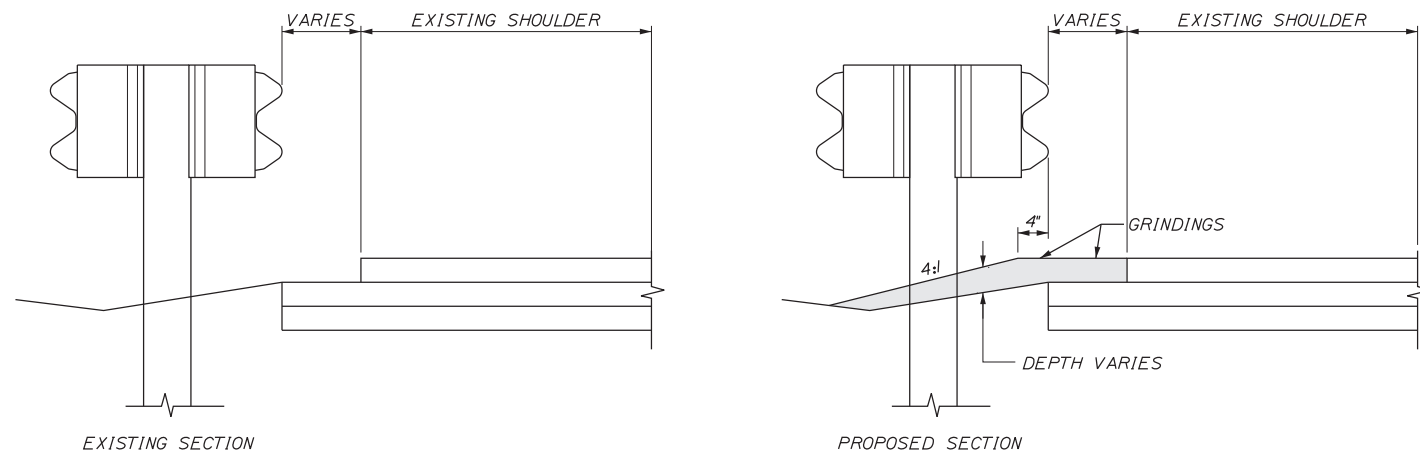
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 EROSION DETAILS 1

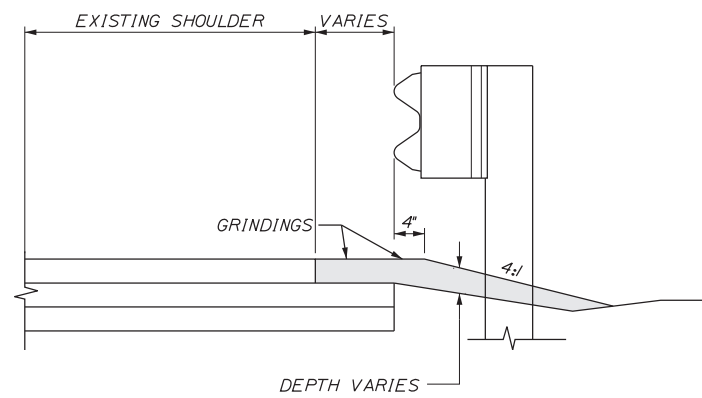
SHEET NUMBER: DET-12
 111 OF 503

CONTRACT: 2019.04

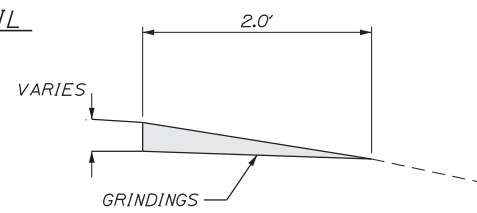
Date: 3/20/2019



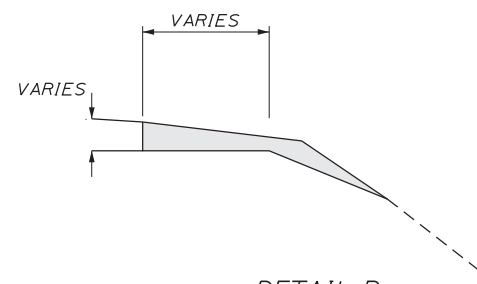
**BERM DROPOFF CORRECTION WITH GUARDRAIL
MEDIAN GUARDRAIL**
N.T.S.



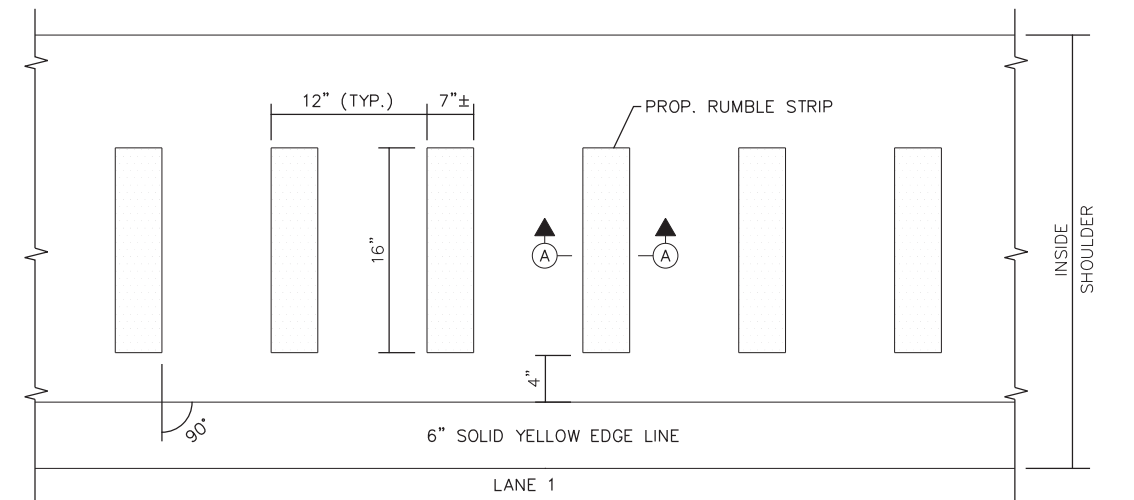
**BERM DROPOFF CORRECTION WITH GUARDRAIL
RIGHT OUTSIDE OF SHOULDER**
N.T.S.



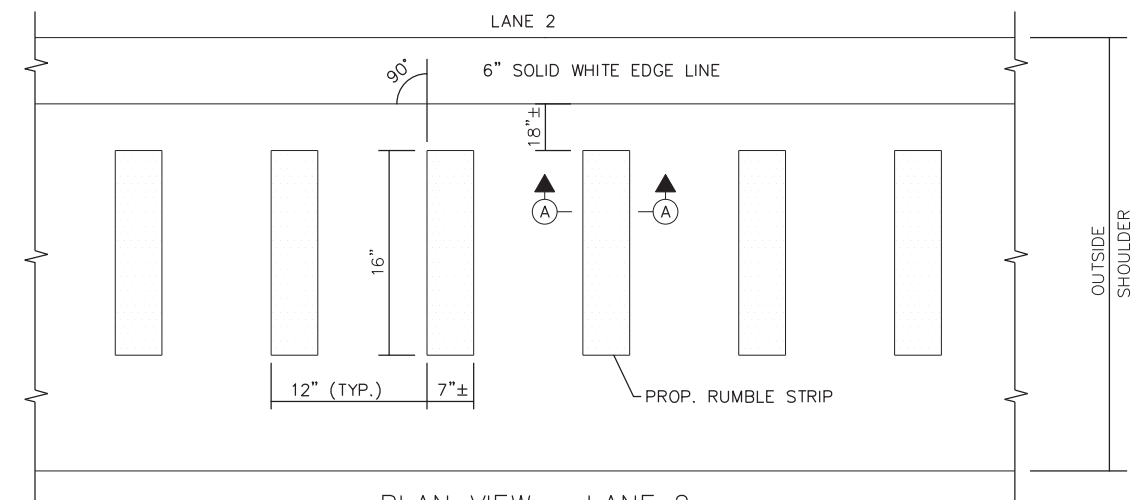
**DETAIL A
(WITHOUT GUARDRAIL)**



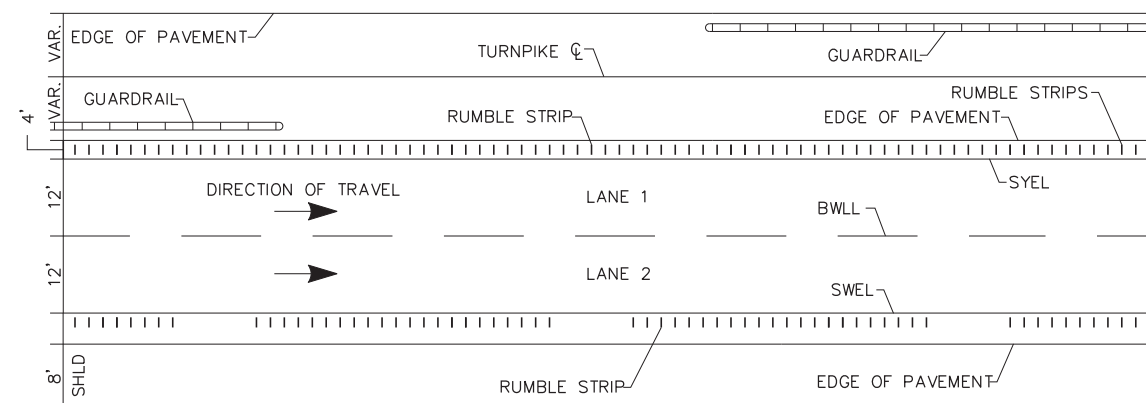
**DETAIL B
(WITHOUT GUARDRAIL)**



PLAN VIEW - LANE 1
N.T.S.



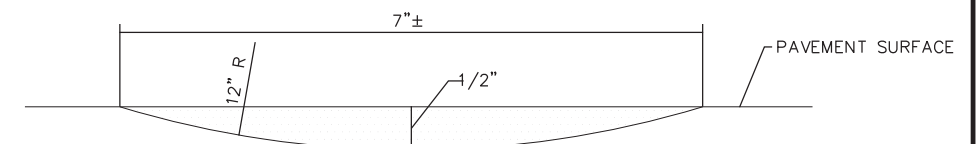
PLAN VIEW - LANE 2
N.T.S.



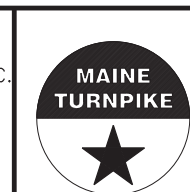
LOCATION OF RUMBLE STRIPS
N.T.S.

NOTES:

1. INSTALL LANE 2 RUMBLE STRIP WITH A 20' GAP EVERY 100'. (I.E. 80' OF RUMBLE STRIP AND 20' OF NO RUMBLE STRIP; REPEAT...)
2. INSTALL LANE 1 RUMBLE STRIP WITH NO GAPS. A BREAK IN THE LANE 1 RUMBLE STRIP IS REQUIRED FOR A MEDIAN CROSSOVER. SEE RUMBLE STRIP PLACEMENT DETAIL FOR LAYOUT REQUIREMENTS.



SECTION A-A
N.T.S.



**THE GOLD STAR
MEMORIAL HIGHWAY**

**INTERCHANGE 103
ORT CONVERSION
MISCELLANEOUS DETAILS
BERM DROPOFF & RUMBLE STRIP**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

CONTRACT: 2019.04

SHEET NUMBER: DET-14

113 OF 503

Filename: ... \HIGHWAY\MSTA\... \Detail-Misc.dgn

Scale:			
No.	Revision	By	Date

Designed by:			
CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
	By	Date	
Designed	PLP	3\20\19	Checked LEM 3\20\19
Drawn	EJB	3\20\19	In Charge of GAE 3\20\19

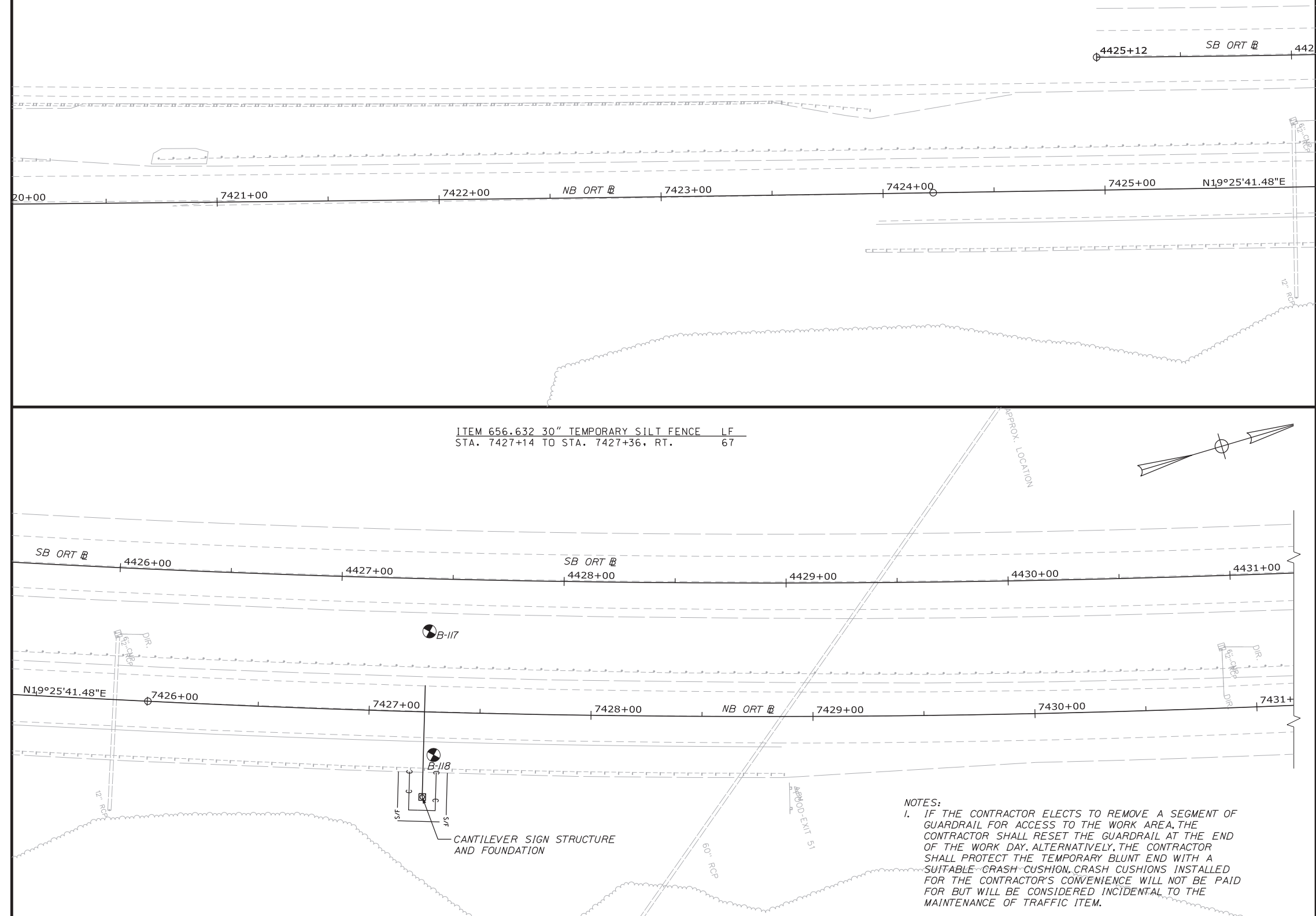
STANTEC CONSULTING SERVICES INC.
482 PAYNE ROAD
SCARBOROUGH, ME 04074
TEL (207) 887-3448
FAX (207) 883-3376

Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\GP_01.dgn

PLAN LEGEND

	EXISTING	PROPOSED
R/W LINES	---	---
EDGE OF GRAVEL	---	---
EDGE OF PAVEMENT	---	---
TRAVELWAY	---	---
BORING	⊗ B-6	
SAWCUT	---	---
CULVERT	---	---
GUARDRAIL (BEAM)	---	---
GUARDRAIL (CABLE)	---	---
WATERWAY	---	---
TREE LINE	---	---
CATCH BASINS	---	---
DITCH	---	---
RIPRAP	---	---
TEMPORARY STONE CHECK DAM	---	---
SLOPE CUT LINE	---	---
SLOPE FILL LINE	---	---
CLEARING LIMIT LINE	---	---
SILT FENCE	---	---
SIGNS (SINGLE POST)	---	---
SIGNS (DOUBLE POST)	---	---
LIGHT POLES	---	---
UTILITY POLES	---	---
ELECTRICAL LINE	---	---
WATER LINE	---	---
COMMUNICATIONS LINE	---	---
QUAZITE JUNCTION BOX	---	---



NOTES:
 1. IF THE CONTRACTOR ELECTS TO REMOVE A SEGMENT OF GUARDRAIL FOR ACCESS TO THE WORK AREA, THE CONTRACTOR SHALL RESET THE GUARDRAIL AT THE END OF THE WORK DAY. ALTERNATIVELY, THE CONTRACTOR SHALL PROTECT THE TEMPORARY BLUNT END WITH A SUITABLE CRASH CUSHION. CRASH CUSHIONS INSTALLED FOR THE CONTRACTOR'S CONVENIENCE WILL NOT BE PAID FOR BUT WILL BE CONSIDERED INCIDENTAL TO THE MAINTENANCE OF TRAFFIC ITEM.



Designed by:

Stantec

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

By	Date	By	Date
PLP	3\20\19	LEM	3\20\19
EJB	3\20\19	GAE	3\20\19

Designed: PLP 3\20\19 Checked: LEM 3\20\19
 Drawn: EJB 3\20\19 In Charge of: GAE 3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

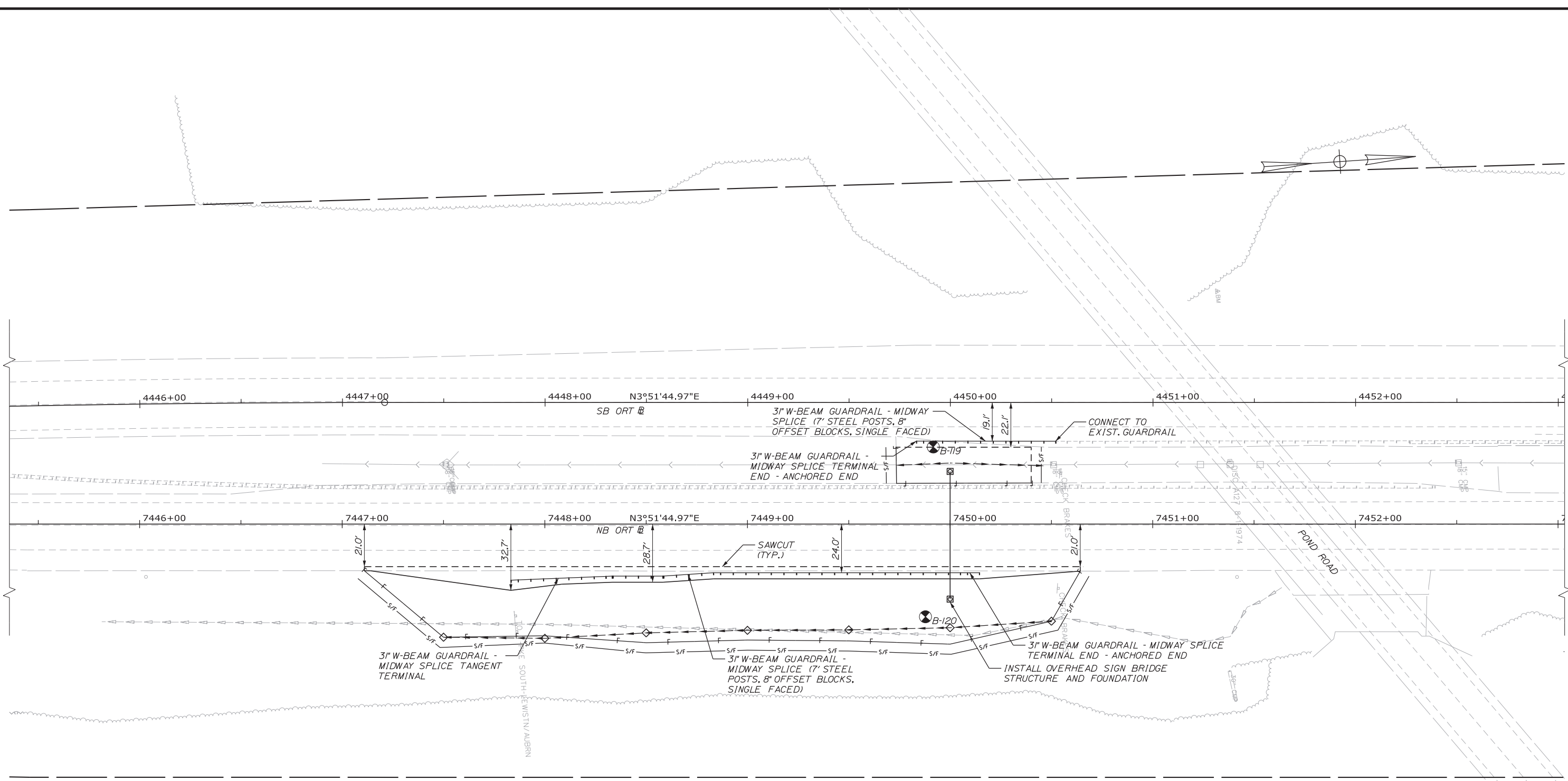
INTERCHANGE 103
 ORT CONVERSION
 GENERAL PLAN 1

SHEET NUMBER: GP-01
 114 OF 503

CONTRACT: 2019.04

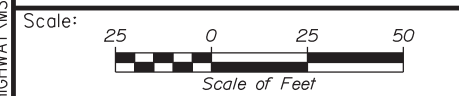
Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\GP_02.dgn



ITEM 606.13	31" W-BEAM GUARDRAIL - MIDWAY SPLICE (7' STEEL POSTS, 8" OFFSET BLOCKS, SINGLE FACED)	LF
	STA. 4450+02.56 TO STA. 4450+61.94, RT.	59.375
	STA. 7448+30.06 TO STA. 7450+15.00, RT.	175.00
ITEM 606.1306	31" W-BEAM GUARDRAIL - MIDWAY SPLICE TANGENT TERMINAL	EA
	STA. 7448+30.06, RT.	1
ITEM 606.1351	31" W-BEAM GUARDRAIL - MIDWAY SPLICE TERMINAL END - ANCHORED END	EA
	STA. 4450+02.56, 19.08' RT.	1
	STA. 7450+15.00, 24.00' RT.	1

ITEM 656.632	30" TEMPORARY SILT FENCE	LF
	STA. 7447+08 TO STA. 7450+68, RT.	390
	STA. 7449+68 TO STA. 7449+68, LT.	18
	STA. 7450+45 TO STA. 7450+45, LT.	18



Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE					
	By	Date	By	Date	
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

**INTERCHANGE 103
ORT CONVERSION
GENERAL PLAN 2**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

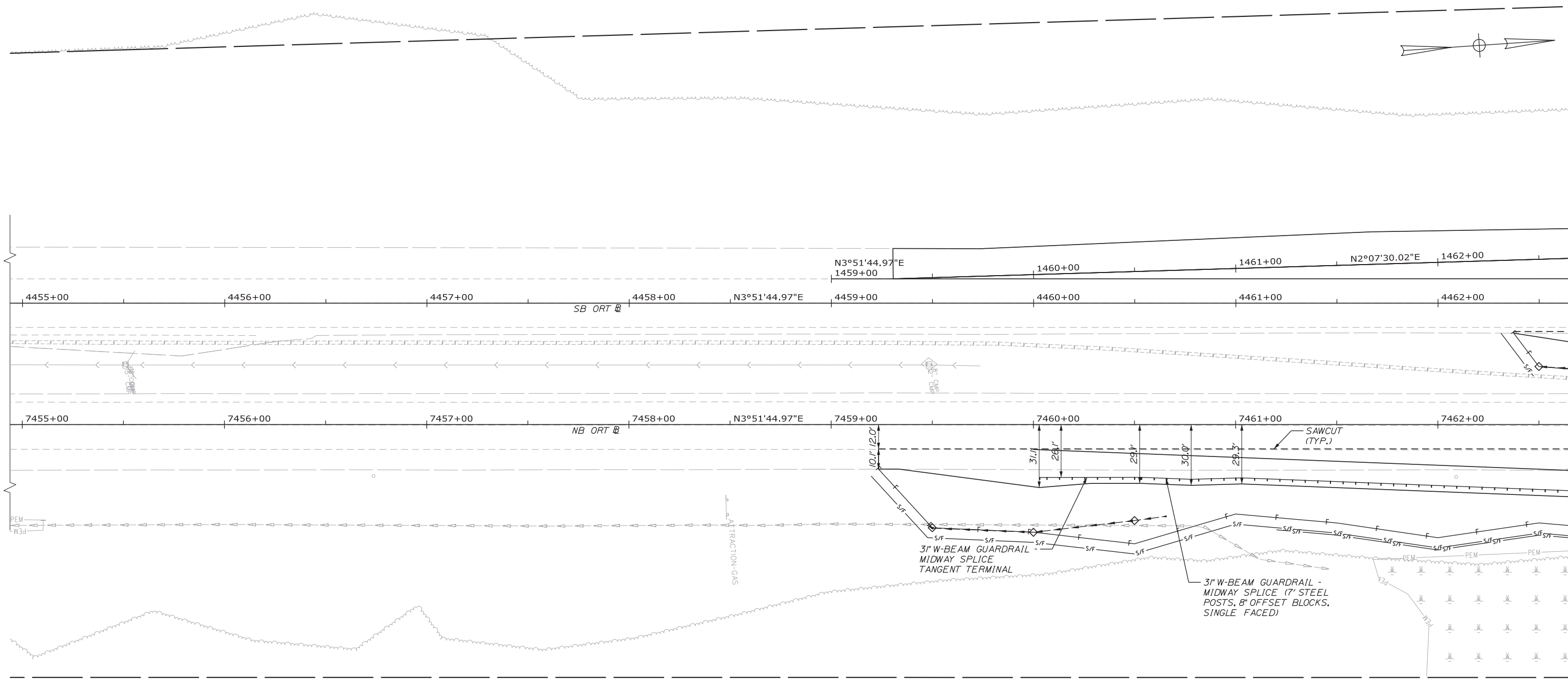
CONTRACT: 2019.04

SHEET NUMBER: GP-02

115 OF 503

Date: 3/20/2019

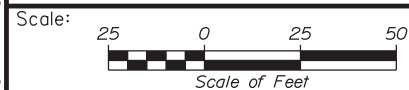
Filename: ...00\HIGHWAY\MSTA\GP_03.dgn



ITEM 606.13 31" W-BEAM GUARDRAIL - MIDWAY SPLICE
 (7' STEEL POSTS, 8" OFFSET BLOCKS, SINGLE FACED) LF
 STA. 7460+49.93 TO STA. 7463+49.81, RT. 300.00

ITEM 606.1306 31" W-BEAM GUARDRAIL
 - MIDWAY SPLICE TANGENT TERMINAL EA
 STA. 7460+49.93, RT. 1

ITEM 656.632 30" TEMPORARY SILT FENCE LF
 STA. 7459+20 TO STA. 7467+70, RT. 901
 STA. 7461+20 TO STA. 7467+70, RT. 685
 STA. 7462+32 TO STA. 7462+48, LT. 27



Designed by:



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THE GOLD STAR
 MEMORIAL HIGHWAY

INTERCHANGE 103
 ORT CONVERSION

GENERAL PLAN 3

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
Designed	By	Date	Checked
	PLP	3\20\19	LEM
Drawn	By	Date	In Charge of
	EJB	3\20\19	GAE

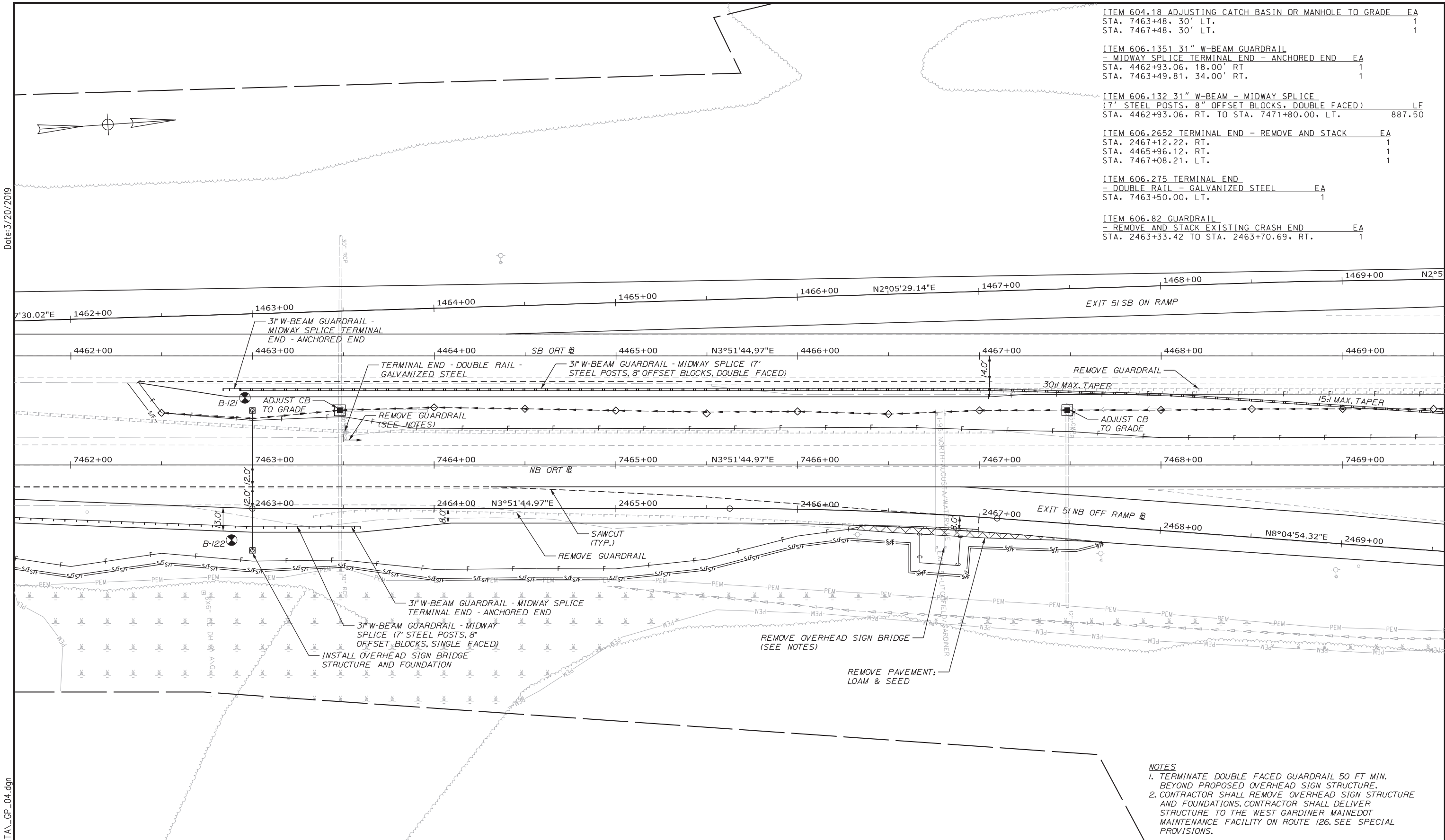
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

CONTRACT: 2019.04

SHEET NUMBER: GP-03

116 OF 503

ITEM 604.18 ADJUSTING CATCH BASIN OR MANHOLE TO GRADE	EA	1
STA. 7463+48, 30' LT.		
STA. 7467+48, 30' LT.		1
ITEM 606.1351 31" W-BEAM GUARDRAIL - MIDWAY SPLICE TERMINAL END - ANCHORED END	EA	1
STA. 4462+93.06, 18.00' RT.		
STA. 7463+49.81, 34.00' RT.		1
ITEM 606.132 31" W-BEAM - MIDWAY SPLICE (7' STEEL POSTS, 8" OFFSET BLOCKS, DOUBLE FACED)	LF	887.50
STA. 4462+93.06, RT. TO STA. 7471+80.00, LT.		
ITEM 606.2652 TERMINAL END - REMOVE AND STACK	EA	1
STA. 2467+12.22, RT.		
STA. 4465+96.12, RT.		1
STA. 7467+08.21, LT.		1
ITEM 606.275 TERMINAL END - DOUBLE RAIL - GALVANIZED STEEL	EA	1
STA. 7463+50.00, LT.		
ITEM 606.82 GUARDRAIL - REMOVE AND STACK EXISTING CRASH END	EA	1
STA. 2463+33.42 TO STA. 2463+70.69, RT.		



NOTES
 1. TERMINATE DOUBLE FACED GUARDRAIL 50 FT MIN. BEYOND PROPOSED OVERHEAD SIGN STRUCTURE.
 2. CONTRACTOR SHALL REMOVE OVERHEAD SIGN STRUCTURE AND FOUNDATIONS. CONTRACTOR SHALL DELIVER STRUCTURE TO THE WEST GARDINER MAINE DOT MAINTENANCE FACILITY ON ROUTE 126. SEE SPECIAL PROVISIONS.

Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\GP_04.dgn

Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

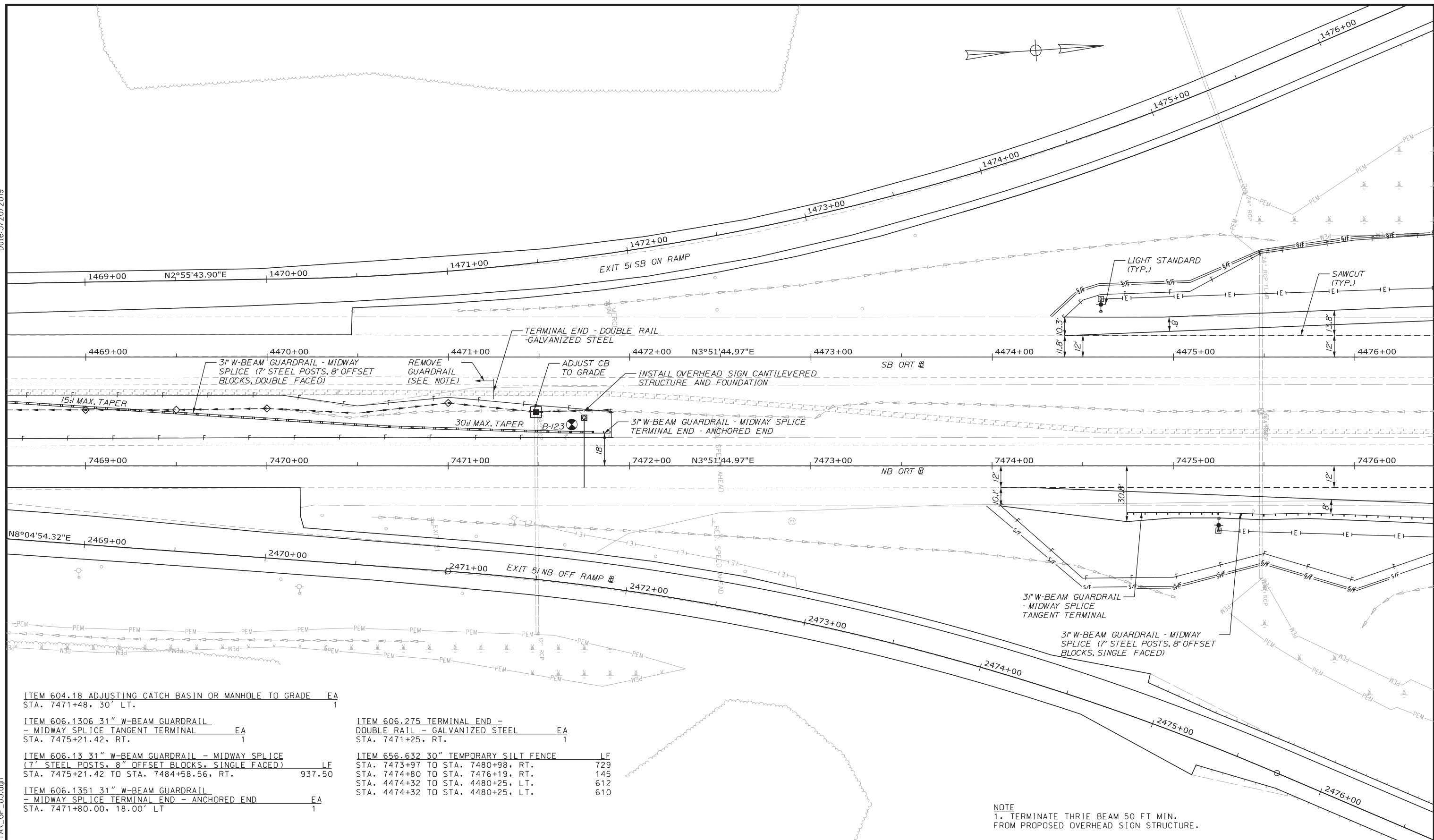
INTERCHANGE 103
 ORT CONVERSION
 GENERAL PLAN 4

SHEET NUMBER: GP-04
 117 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\GP_05.dgn



- ITEM 604.18 ADJUSTING CATCH BASIN OR MANHOLE TO GRADE EA
STA. 7471+48, 30' LT. 1
- ITEM 606.1306 31" W-BEAM GUARDRAIL - MIDWAY SPLICE TANGENT TERMINAL EA
STA. 7475+21.42, RT. 1
- ITEM 606.13 31" W-BEAM GUARDRAIL - MIDWAY SPLICE (7' STEEL POSTS, 8" OFFSET BLOCKS, SINGLE FACED) LF
STA. 7475+21.42 TO STA. 7484+58.56, RT. 937.50
- ITEM 606.1351 31" W-BEAM GUARDRAIL - MIDWAY SPLICE TERMINAL END - ANCHORED END EA
STA. 7471+80.00, 18.00' LT. 1

- ITEM 606.275 TERMINAL END - DOUBLE RAIL - GALVANIZED STEEL EA
STA. 7471+25, RT. 1
- ITEM 656.632 30" TEMPORARY SILT FENCE LF
STA. 7473+97 TO STA. 7480+98, RT. 729
STA. 7474+80 TO STA. 7476+19, RT. 145
STA. 4474+32 TO STA. 4480+25, LT. 612
STA. 4474+32 TO STA. 4480+25, LT. 610

NOTE
1. TERMINATE THRIE BEAM 50 FT MIN. FROM PROPOSED OVERHEAD SIGN STRUCTURE.

Scale: Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date	By	Date	
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

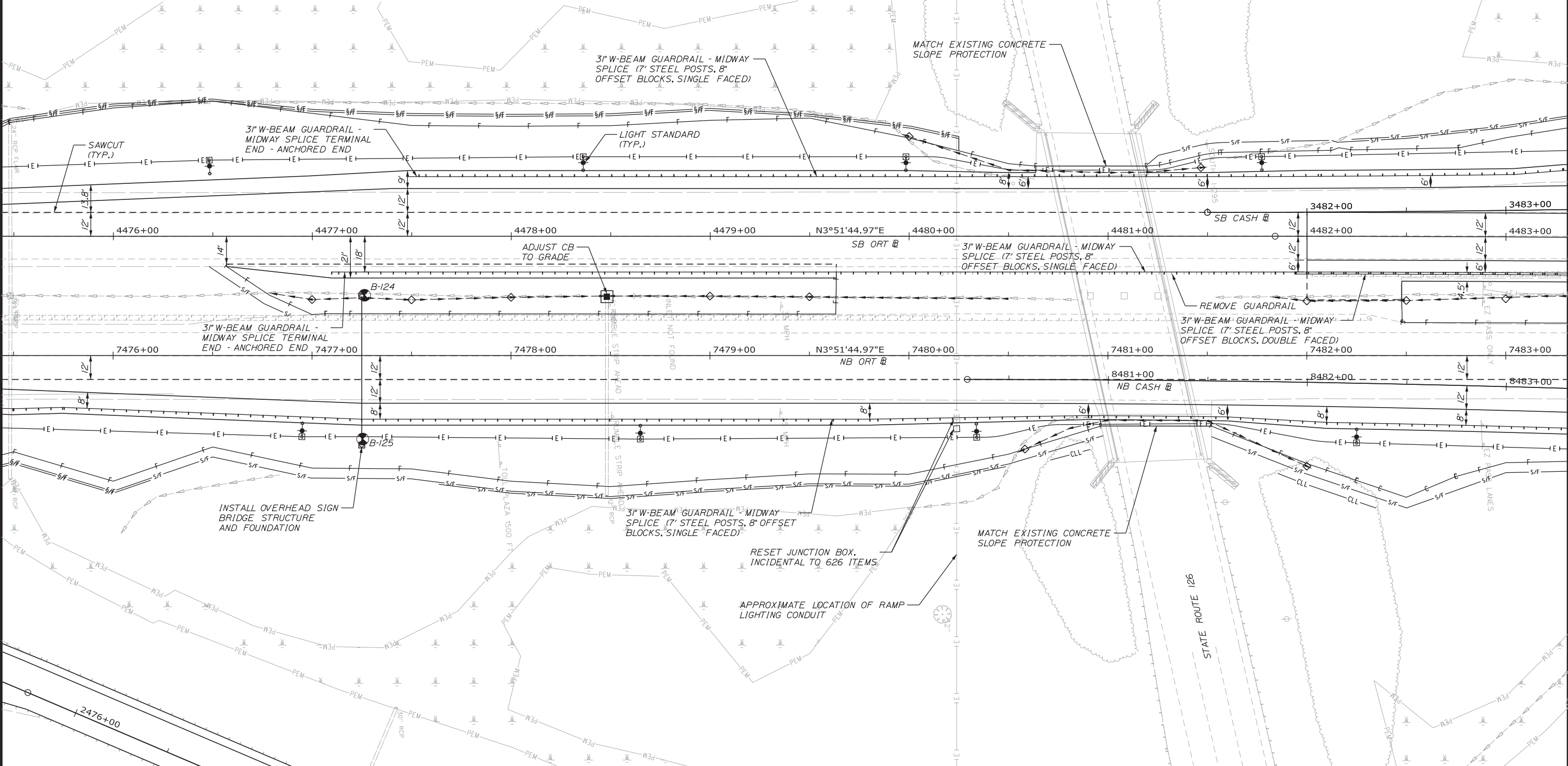
INTERCHANGE 103
ORT CONVERSION
GENERAL PLAN 5

SHEET NUMBER: GP-05
CONTRACT: 2019.04
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ITEM 201.11 CLEARING
 STA. 7480+71, 53' RT. TO STA. 7480+91, 47' RT.
 STA. 7481+85, 59' RT. TO STA. 7482+35, 77' RT.
 ITEM 604.18 ADJUSTING CATCH BASIN OR MANHOLE TO GRADE EA
 STA. 4478+48, 30' RT. 1
 ITEM 606.13 31" W-BEAM GUARDRAIL - MIDWAY SPLICE
 (7' STEEL POSTS, 8" OFFSET BLOCKS, SINGLE FACED) LF
 STA. 4477+19.03 TO STA. 4481+94.04, RT. 475.00
 STA. 4477+57.25 TO STA. 3483+57.52, LT. 600.00
 ITEM 606.132 31" W-BEAM GUARDRAIL - MIDWAY SPLICE
 (7' STEEL POSTS, 8" OFFSET BLOCKS, DOUBLE FACED) LF
 STA. 4481+94.04 TO STA. 4487+31.85, RT. 537.50

ITEM 606.1351 31" W-BEAM GUARDRAIL
 - MIDWAY SPLICE TERMINAL END - ANCHORED END EA
 STA. 4477+19.03, 18.00' RT. 1
 STA. 4477+57.25, 30.00' LT. 1
 ITEM 656.632 30" TEMPORARY SILT FENCE LF
 STA. 7476+48 TO STA. 7476+89, LT. 48
 STA. 7477+64 TO STA. 7480+48, RT. 287
 STA. 7481+52 TO STA. 885+78, RT. - LT. 657
 STA. 4481+20 TO STA. 785+24, LT. - RT. 670
 STA. 4482+22 TO STA. 4485+19, LT. 300

Date: 3/20/2019



Filename: ...100.HIGHWAY.MSTA_GP_06.dgn

Scale: 25 0 25 50
 Scale of Feet

No.	Revision	By	Date

Designed by: **Stantec**

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date	By	Date	
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Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 GENERAL PLAN 6

SHEET NUMBER: GP-06
 119 OF 503

CONTRACT: 2019.04

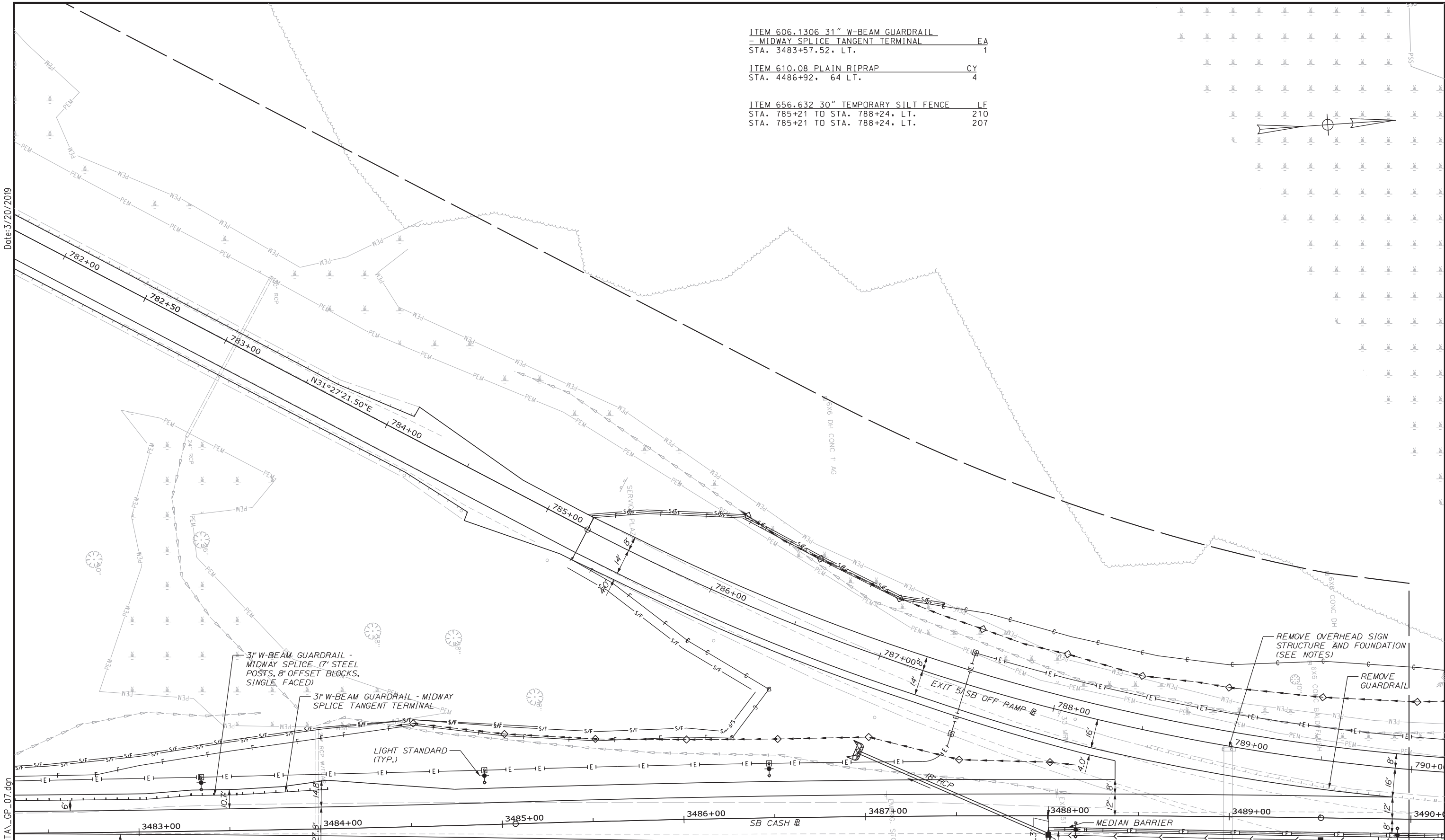
ITEM 606.1306 31" W-BEAM GUARDRAIL
 - MIDWAY SPLICE TANGENT TERMINAL EA
 STA. 3483+57.52, LT. 1

ITEM 610.08 PLAIN RIPRAP CY
 STA. 4486+92, 64 LT. 4

ITEM 656.632 30" TEMPORARY SILT FENCE LF
 STA. 785+21 TO STA. 788+24, LT. 210
 STA. 785+21 TO STA. 788+24, LT. 207



Date: 3/20/2019



Scale: 25 0 25 50
 Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

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**THE GOLD STAR
 MEMORIAL HIGHWAY**

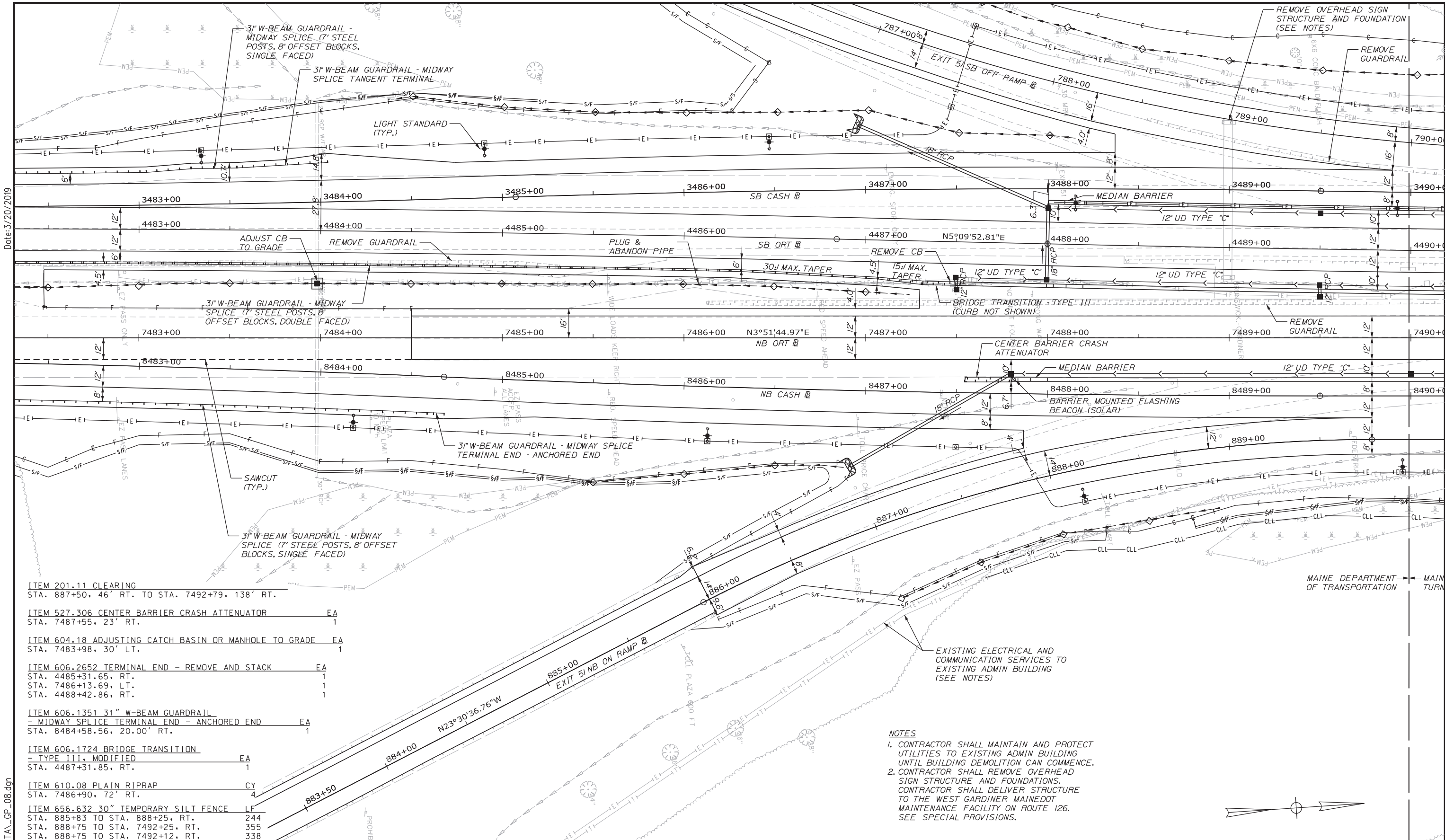
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 GENERAL PLAN 7

SHEET NUMBER: GP-07
 120 OF 503

CONTRACT: 2019.04

Filename: ...00.HIGHWAY.MSTA_GP_07.dgn



- ITEM 201.11 CLEARING
STA. 887+50, 46' RT. TO STA. 7492+79, 138' RT.
- ITEM 527.306 CENTER BARRIER CRASH ATTENUATOR EA
STA. 7487+55, 23' RT. 1
- ITEM 604.18 ADJUSTING CATCH BASIN OR MANHOLE TO GRADE EA
STA. 7483+98, 30' LT. 1
- ITEM 606.2652 TERMINAL END - REMOVE AND STACK EA
STA. 4485+31.65, RT. 1
STA. 7486+13.69, LT. 1
STA. 4488+42.86, RT. 1
- ITEM 606.1351 31" W-BEAM GUARDRAIL
- MIDWAY SPLICE TERMINAL END - ANCHORED END EA
STA. 8484+58.56, 20.00' RT. 1
- ITEM 606.1724 BRIDGE TRANSITION
- TYPE III, MODIFIED EA
STA. 4487+31.85, RT. 1
- ITEM 610.08 PLAIN RIPRAP CY
STA. 7486+90, 72' RT. 4
- ITEM 656.632 30" TEMPORARY SILT FENCE LF
STA. 885+83 TO STA. 888+25, RT. 244
STA. 888+75 TO STA. 7492+25, RT. 355
STA. 888+75 TO STA. 7492+12, RT. 338

EXISTING ELECTRICAL AND COMMUNICATION SERVICES TO EXISTING ADMIN BUILDING (SEE NOTES)

- NOTES**
1. CONTRACTOR SHALL MAINTAIN AND PROTECT UTILITIES TO EXISTING ADMIN BUILDING UNTIL BUILDING DEMOLITION CAN COMMENCE.
 2. CONTRACTOR SHALL REMOVE OVERHEAD SIGN STRUCTURE AND FOUNDATIONS. CONTRACTOR SHALL DELIVER STRUCTURE TO THE WEST GARDINER MAINE DOT MAINTENANCE FACILITY ON ROUTE 126. SEE SPECIAL PROVISIONS.



Date: 3/20/2019
Filename: ...00\HIGHWAY\MSTA..._GP_08.dgn

Scale: Scale of Feet

No.	Revision	By	Date

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Designed	PLP	3\20\19	Checked	LEM	3\20\19
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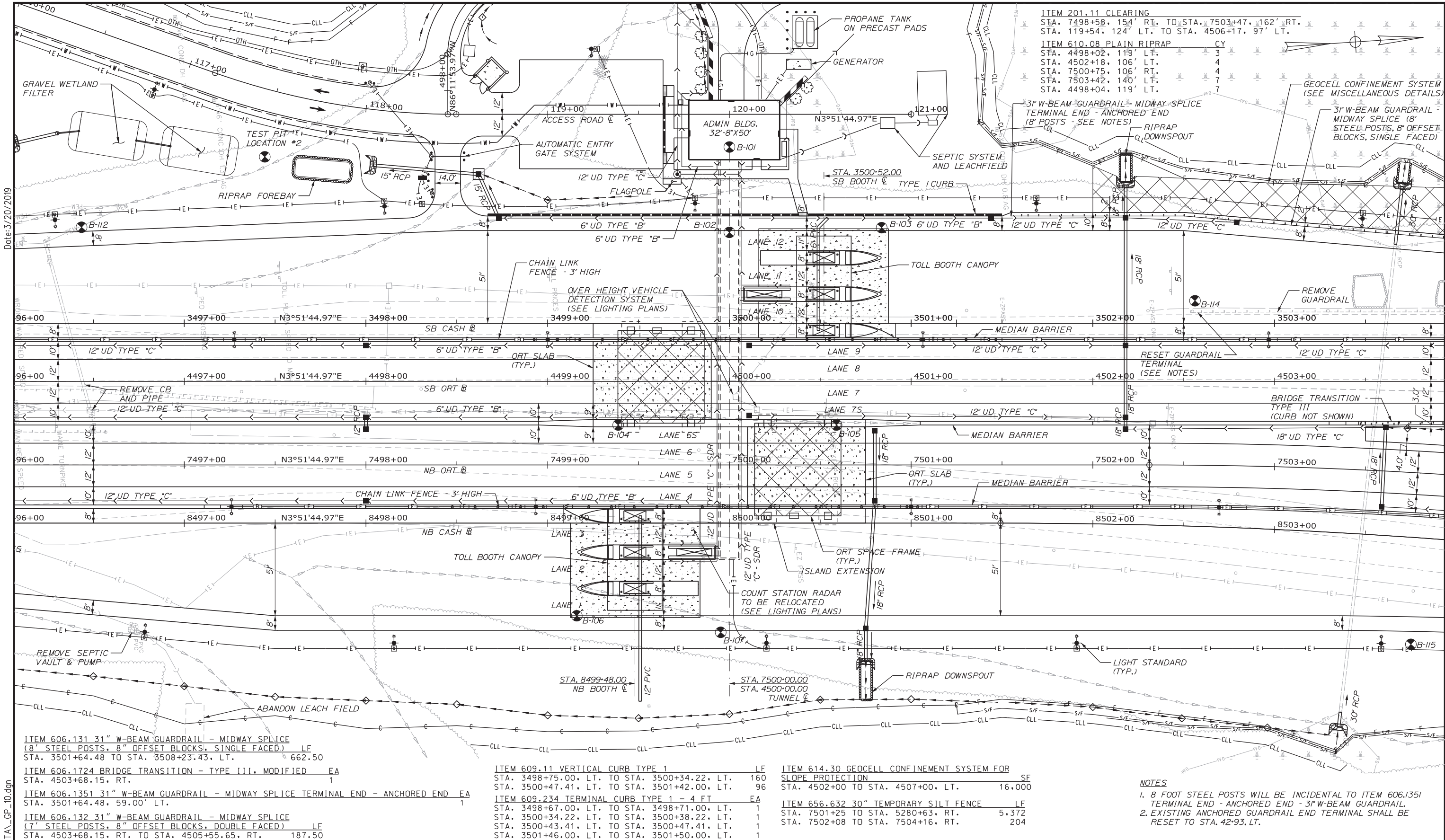
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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
GENERAL PLAN 8

SHEET NUMBER: GP-08
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121 OF 503



Date: 3/20/2019
 File name: ...00.HIGHWAY\MSTA..._GP_10.dgn

NOTES
 1. 8 FOOT STEEL POSTS WILL BE INCIDENTAL TO ITEM 606.1351 TERMINAL END - ANCHORED END - 31" W-BEAM GUARDRAIL.
 2. EXISTING ANCHORED GUARDRAIL END TERMINAL SHALL BE RESET TO STA. 42+93, LT.

Scale: 25 0 25 50
 Scale of Feet

No.	Revision	By	Date

Designed by: **Stantec**

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
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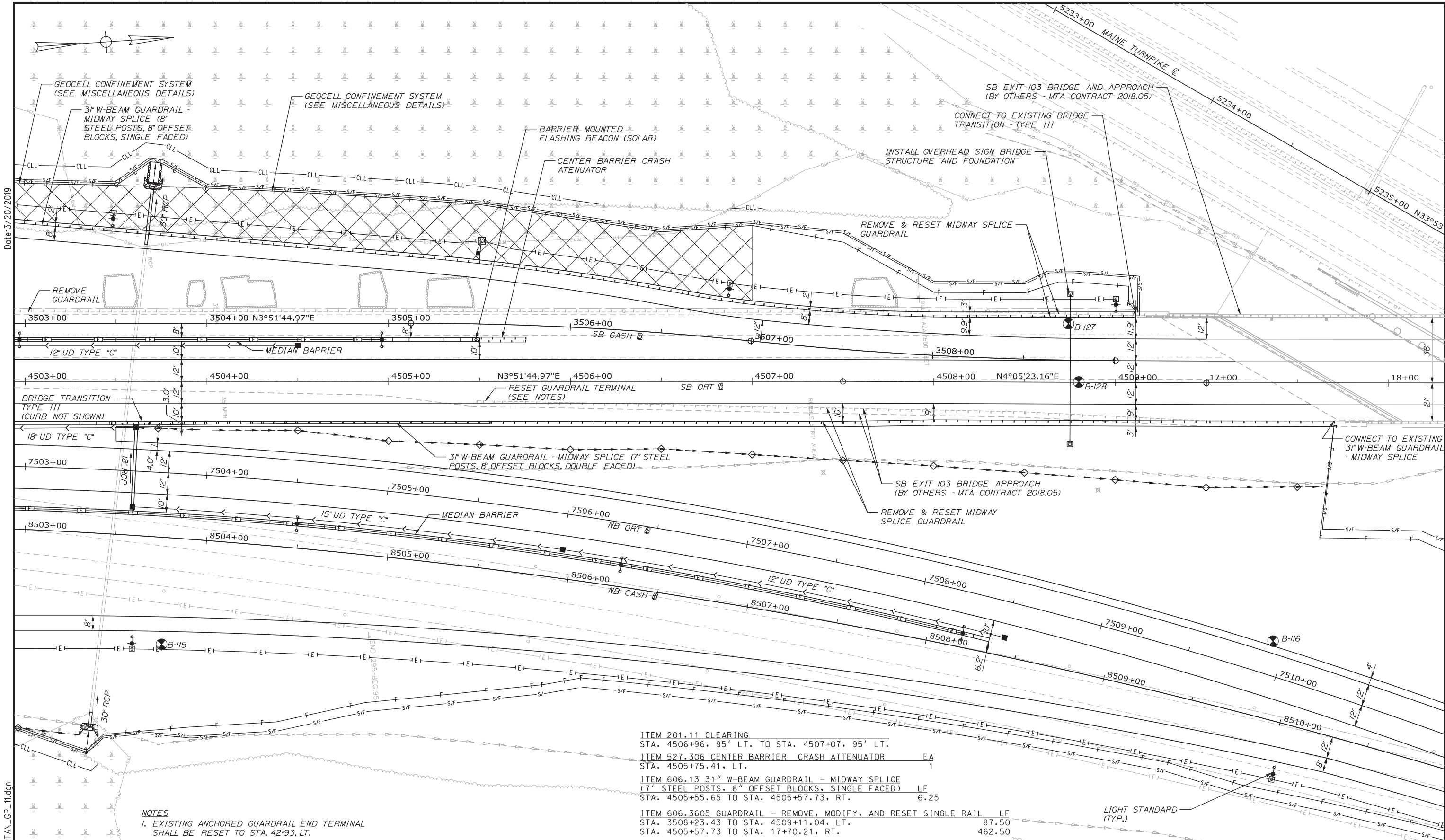
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 GENERAL PLAN 10

SHEET NUMBER: GP-10
 123 OF 503

CONTRACT: 2019.04

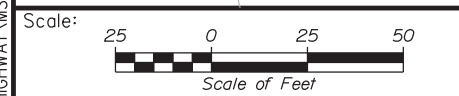


- ITEM 201.11 CLEARING
STA. 4506+96.95' LT. TO STA. 4507+07.95' LT.
- ITEM 527.306 CENTER BARRIER CRASH ATTENUATOR EA
STA. 4505+75.41, LT. 1
- ITEM 606.13 31" W-BEAM GUARDRAIL - MIDWAY SPLICE (7' STEEL POSTS, 8" OFFSET BLOCKS, SINGLE FACED) LF
STA. 4505+55.65 TO STA. 4505+57.73, RT. 6.25
- ITEM 606.3605 GUARDRAIL - REMOVE, MODIFY, AND RESET SINGLE RAIL LF
STA. 3508+23.43 TO STA. 4509+11.04, LT. 87.50
STA. 4505+57.73 TO STA. 17+70.21, RT. 462.50

NOTES
 1. EXISTING ANCHORED GUARDRAIL END TERMINAL SHALL BE RESET TO STA. 42+93, LT.


Date: 3/20/2019

Filename: ...100.HIGHWAY.MSTA_GP_11.dgn



No.	Revision	By	Date

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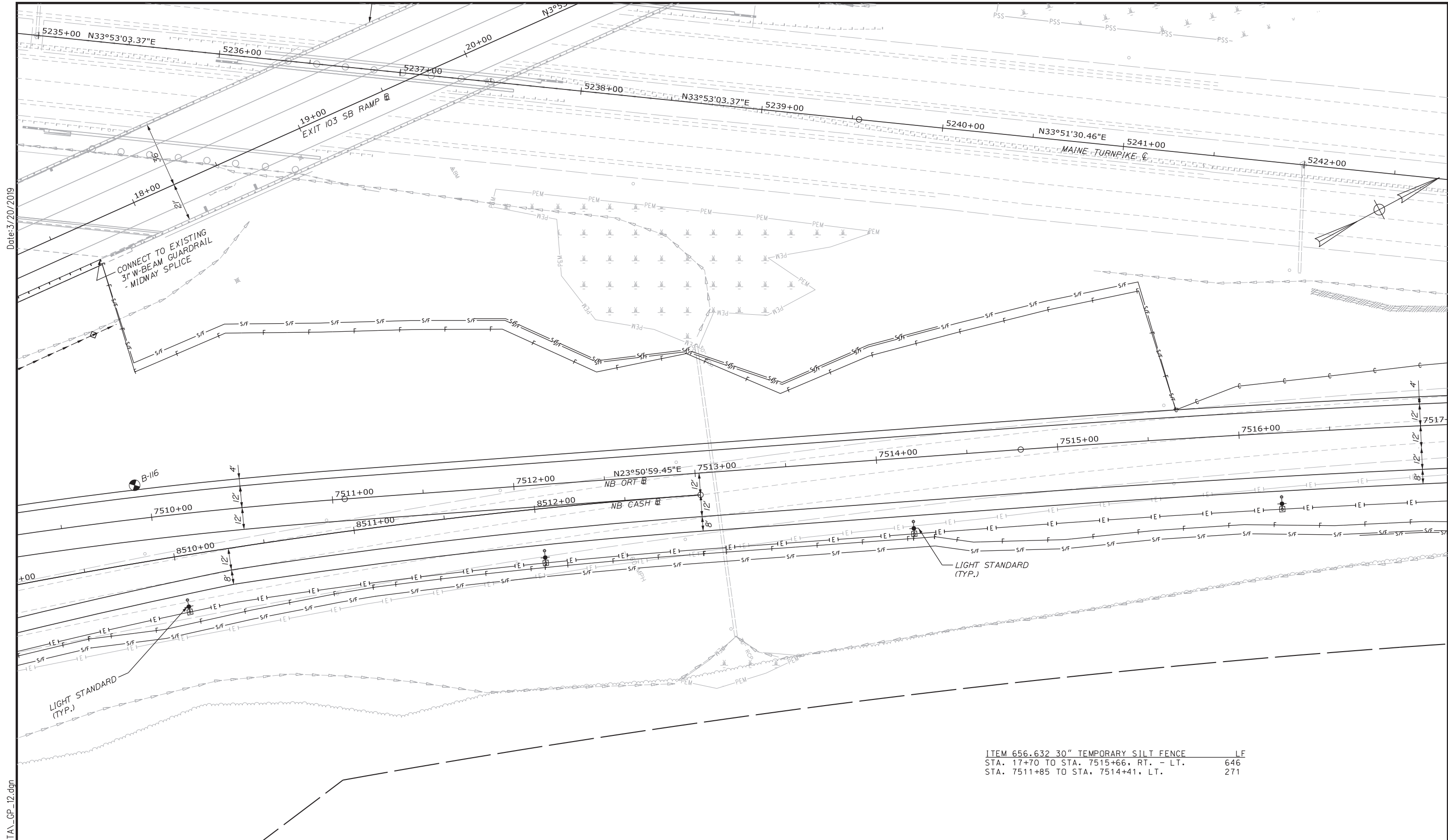
**THE GOLD STAR
 MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 GENERAL PLAN 11

SHEET NUMBER: GP-11
 124 OF 503

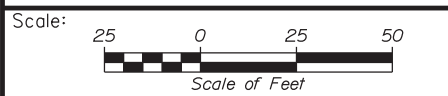
CONTRACT: 2019.04



Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\GP_12.dgn

ITEM 656.632 30" TEMPORARY SILT FENCE LF
 STA. 17+70 TO STA. 7515+66, RT. - LT. 646
 STA. 7511+85 TO STA. 7514+41, LT. 271



Designed by:

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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

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Designed	PLP	3\20\19	Checked	LEM	3\20\19
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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

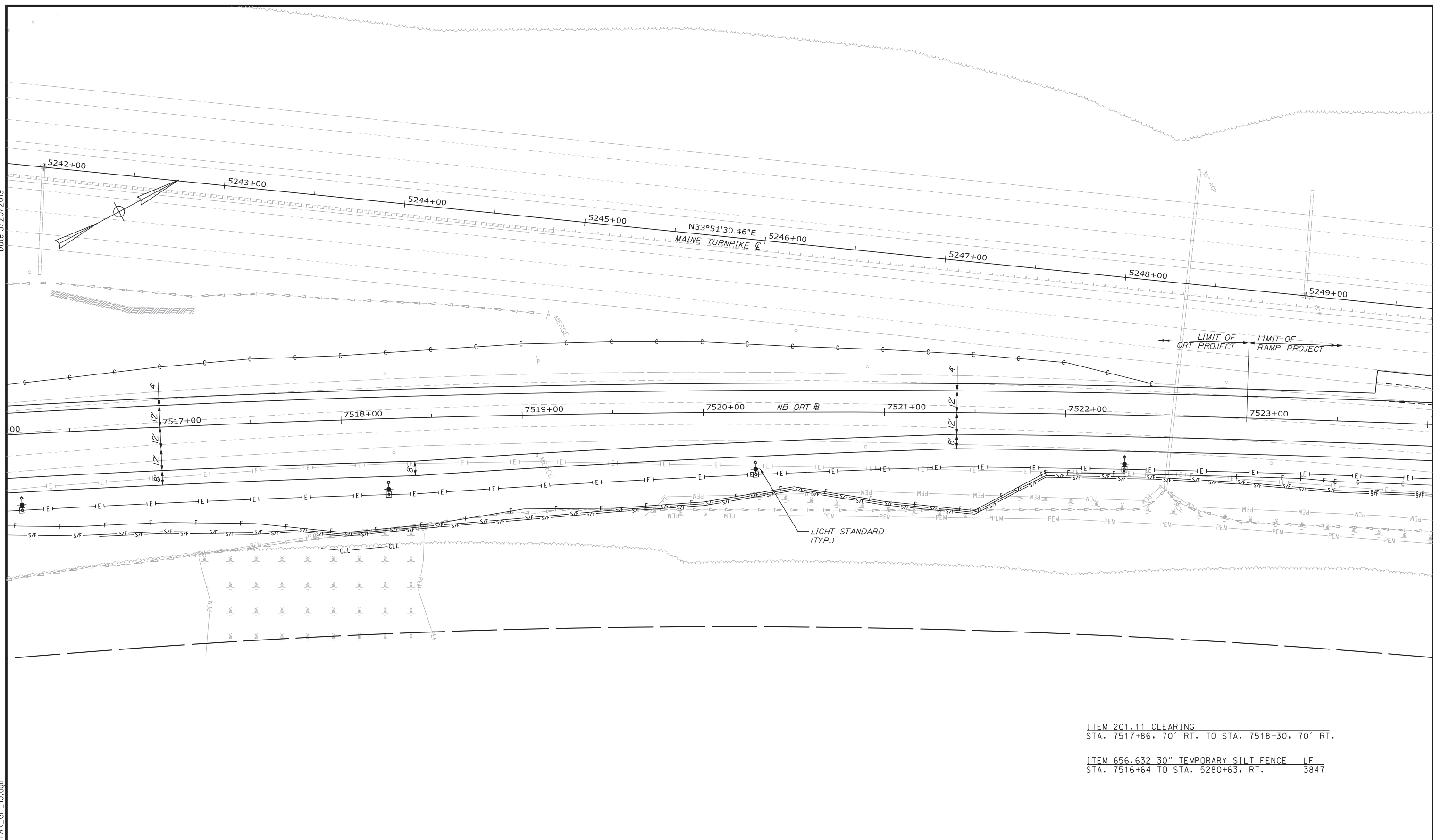
INTERCHANGE 103
 ORT CONVERSION
 GENERAL PLAN 12

SHEET NUMBER: GP-12
 125 OF 503

CONTRACT: 2019.04

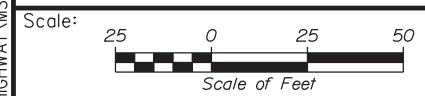
Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\GP_13.dgn



ITEM 201.11 CLEARING
 STA. 7517+86, 70' RT. TO STA. 7518+30, 70' RT.

ITEM 656.632 30" TEMPORARY SILT FENCE LF
 STA. 7516+64 TO STA. 5280+63, RT. 3847



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No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
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THE GOLD STAR MEMORIAL HIGHWAY

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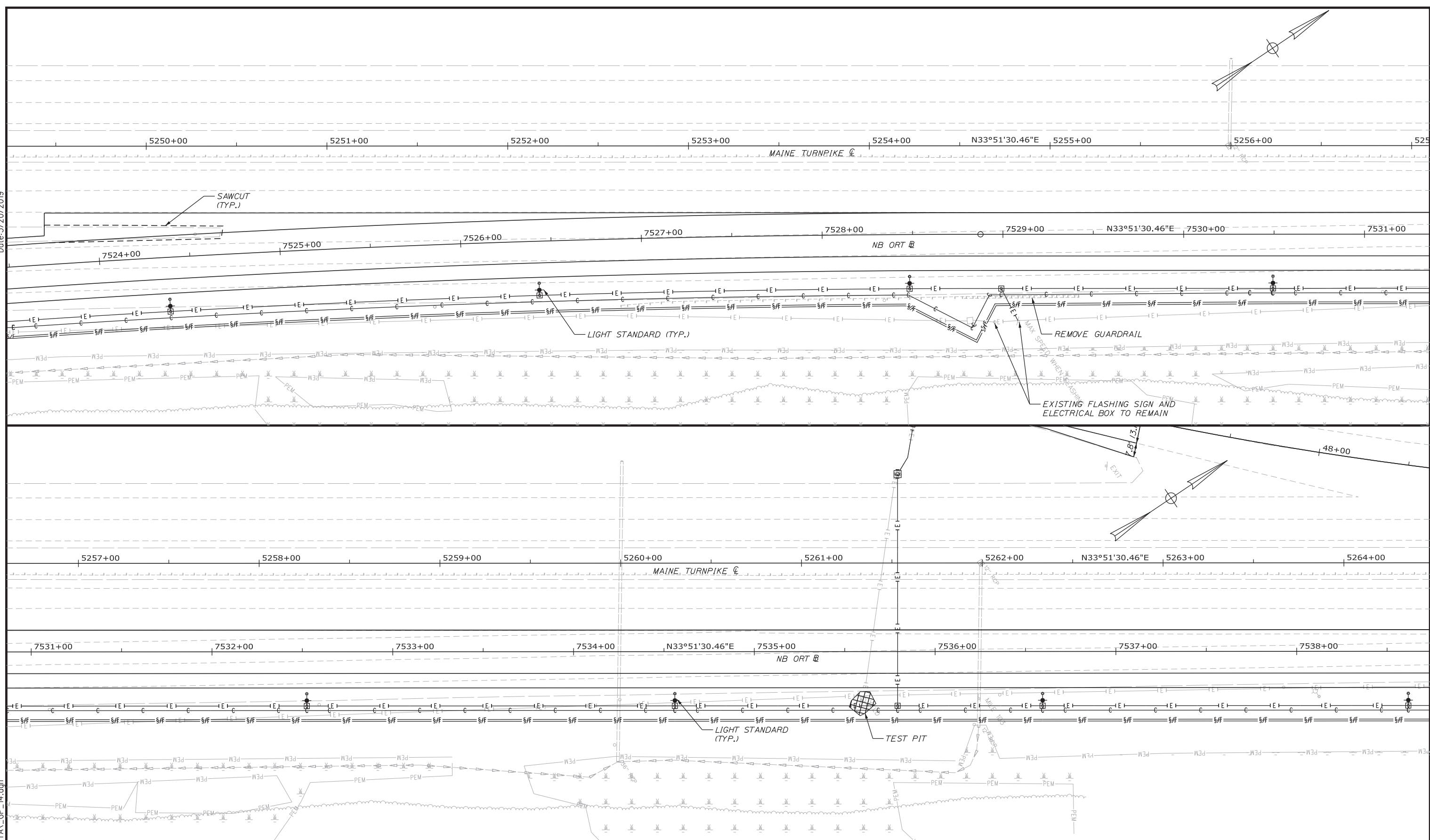
INTERCHANGE 103
 ORT CONVERSION
 GENERAL PLAN 13

SHEET NUMBER: GP-13
 126 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\GP_14.dgn



Scale: Scale of Feet

No.	Revision	By	Date

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**THE GOLD STAR
 MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

**INTERCHANGE 103
 ORT CONVERSION**

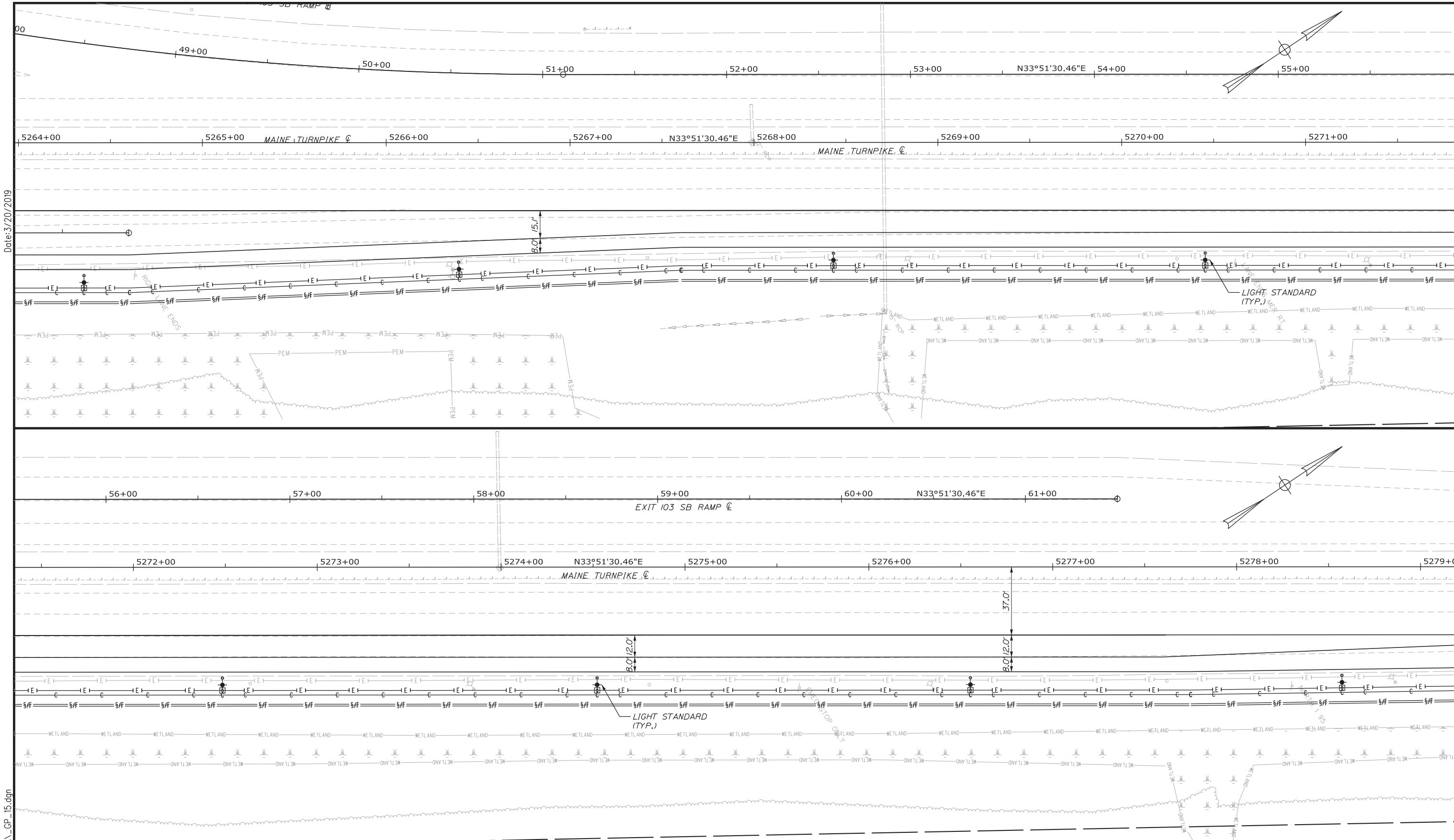
GENERAL PLAN 14

SHEET NUMBER: GP-14
 127 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\GP_15.dgn



Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

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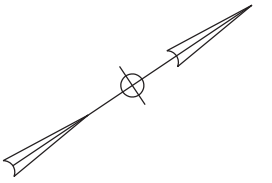
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

**INTERCHANGE 103
ORT CONVERSION**

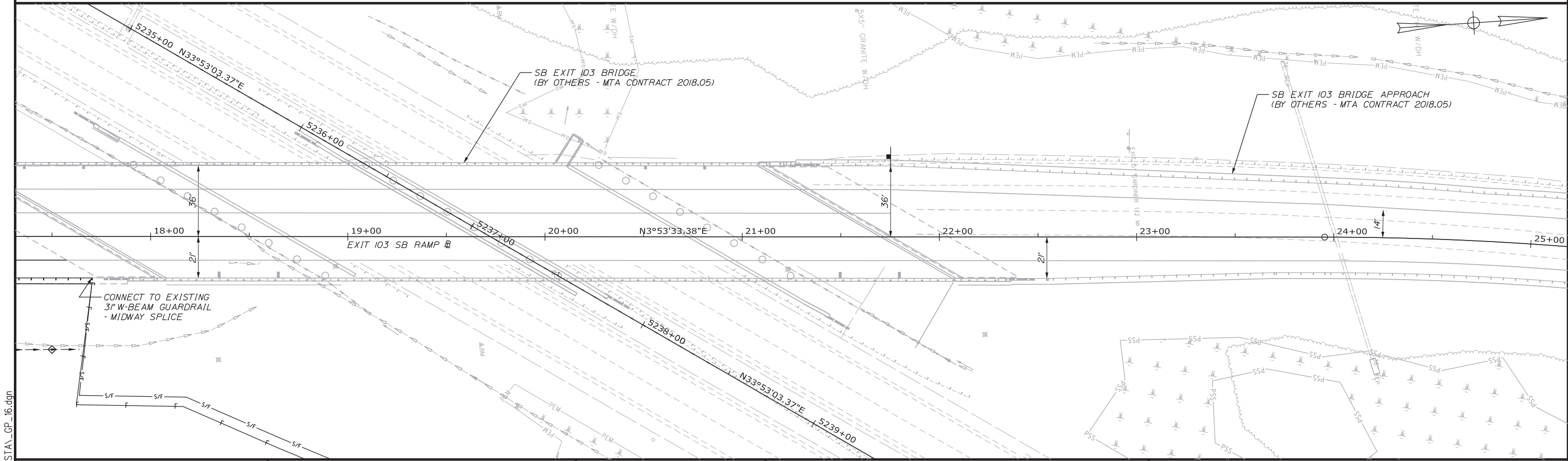
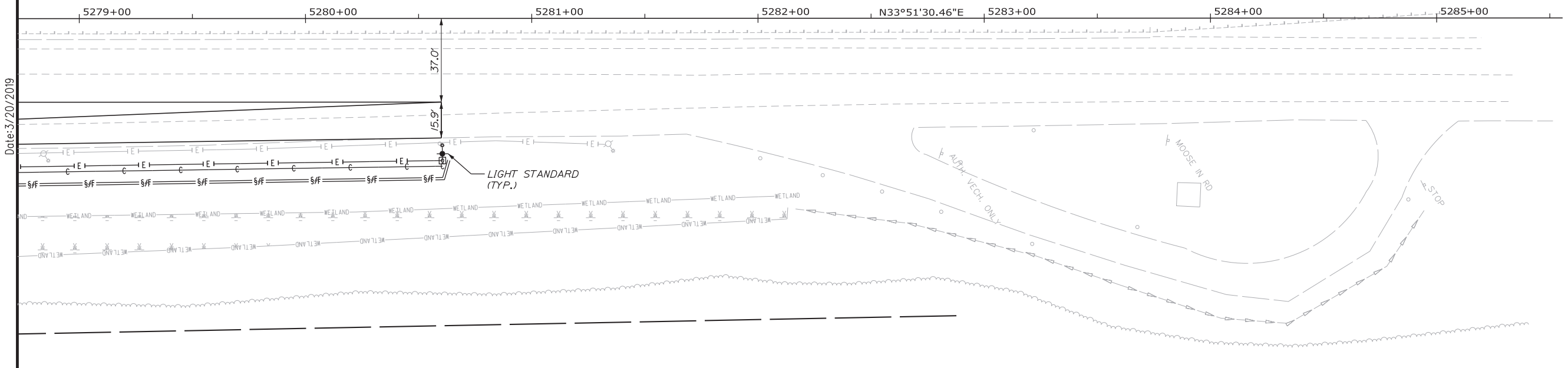
GENERAL PLAN 15

SHEET NUMBER: GP-15
128 OF 503

CONTRACT: 2019.04



Date: 3/20/2019



Scale: 1" = 25'

No.	Revision	By	Date

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**THE GOLD STAR
 MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

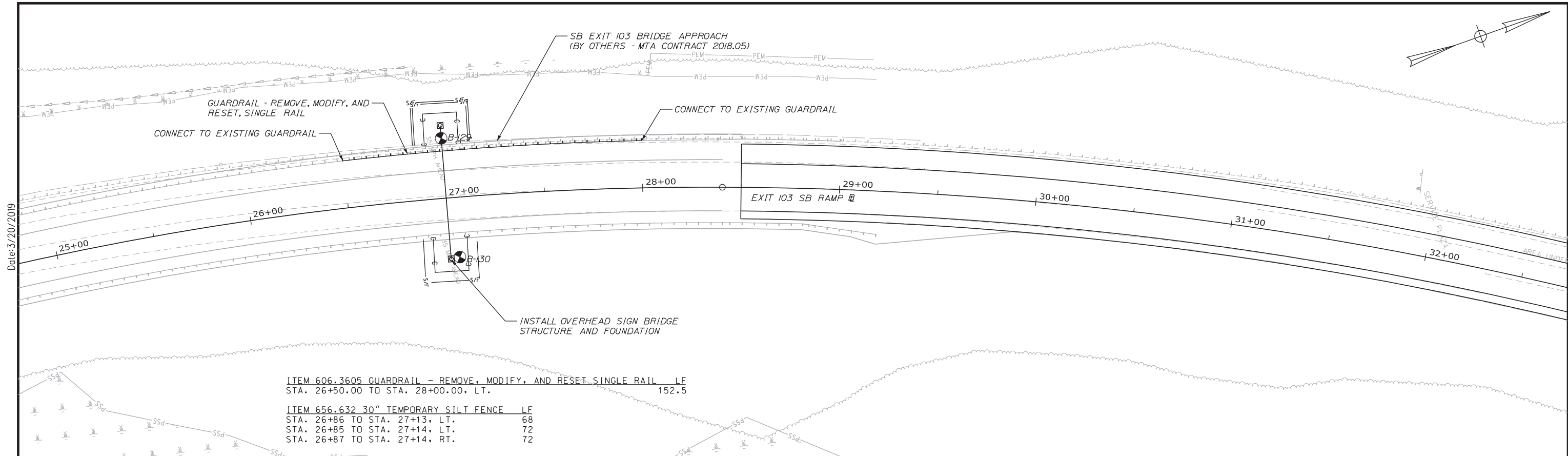
**INTERCHANGE 103
 ORT CONVERSION**

GENERAL PLAN 16

SHEET NUMBER: GP-16
 129 OF 503

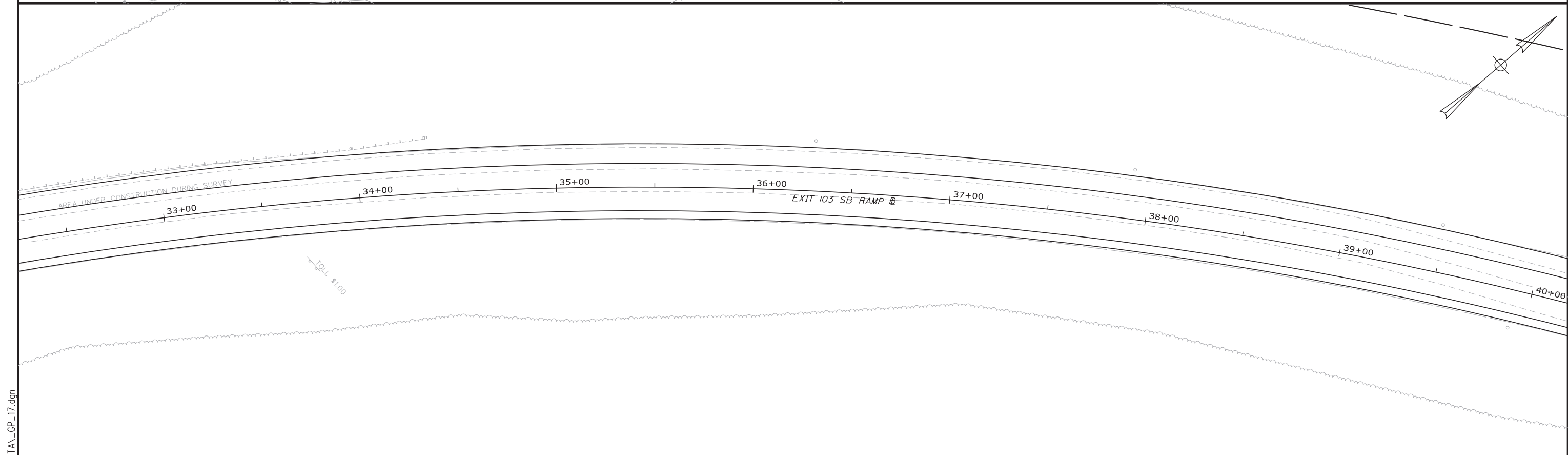
CONTRACT: 2019.04

Filename: ...00\HIGHWAY\MSTA\GP_16.dgn



ITEM 606.3605 GUARDRAIL - REMOVE, MODIFY, AND RESET SINGLE RAIL LF
 STA. 26+50.00 TO STA. 28+00.00, LT. 152.5

ITEM 656.632 30" TEMPORARY SILT FENCE LF
 STA. 26+86 TO STA. 27+13, LT. 68
 STA. 26+85 TO STA. 27+14, LT. 72
 STA. 26+87 TO STA. 27+14, RT. 72



Scale: 25 0 25 50
 Scale of Feet

No.	Revision	By	Date

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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 GENERAL PLAN 17

SHEET NUMBER: GP-17
 130 OF 503

CONTRACT: 2019.04

Filename: ...\\00\HIGHWAY\MSTA\GP_17.dgn

Date: 3/20/2019

Date: 3/20/2019

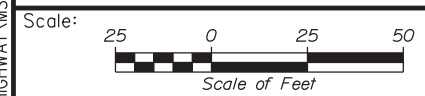
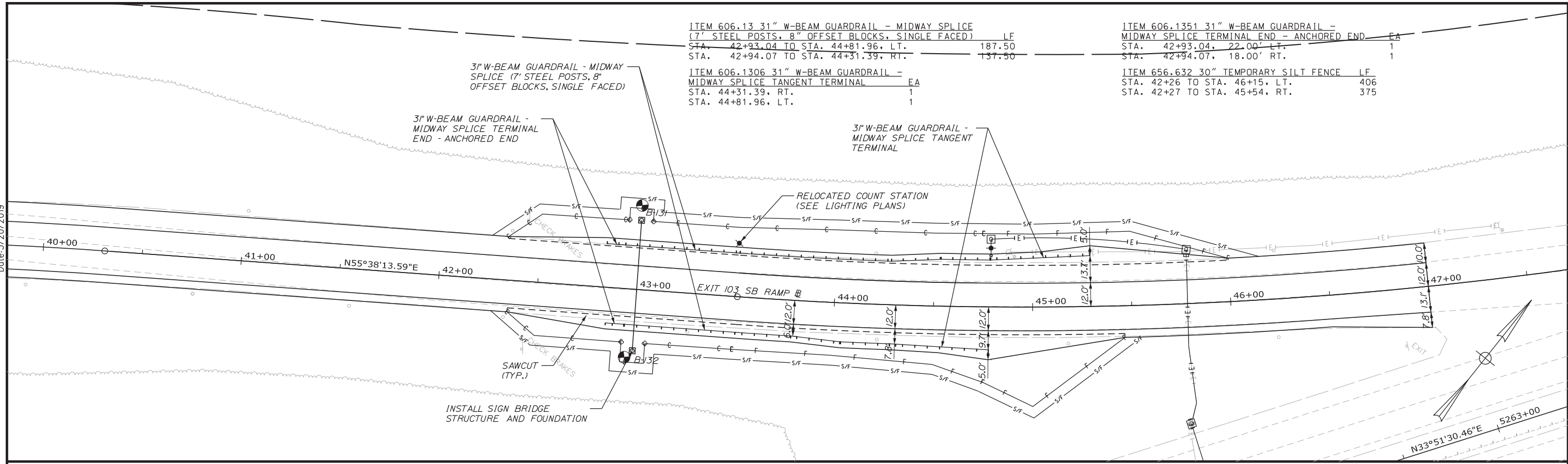
Filename: ...00\HIGHWAY\MSTA\GP_18.dgn

ITEM 606.13 31" W-BEAM GUARDRAIL - MIDWAY SPLICE
(7' STEEL POSTS, 8" OFFSET BLOCKS, SINGLE FACED) LF
STA. 42+93.04 TO STA. 44+81.96, LT. 187.50
STA. 42+94.07 TO STA. 44+31.39, RT. 137.50

ITEM 606.1306 31" W-BEAM GUARDRAIL -
MIDWAY SPLICE TANGENT TERMINAL EA
STA. 44+31.39, RT. 1
STA. 44+81.96, LT. 1

ITEM 606.1351 31" W-BEAM GUARDRAIL -
MIDWAY SPLICE TERMINAL END - ANCHORED END EA
STA. 42+93.04, 22.00' LT. 1
STA. 42+94.07, 18.00' RT. 1

ITEM 656.632 30" TEMPORARY SILT FENCE LF
STA. 42+26 TO STA. 46+15, LT. 406
STA. 42+27 TO STA. 45+54, RT. 375



No.	Revision	By	Date

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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

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MAINE TURNPIKE

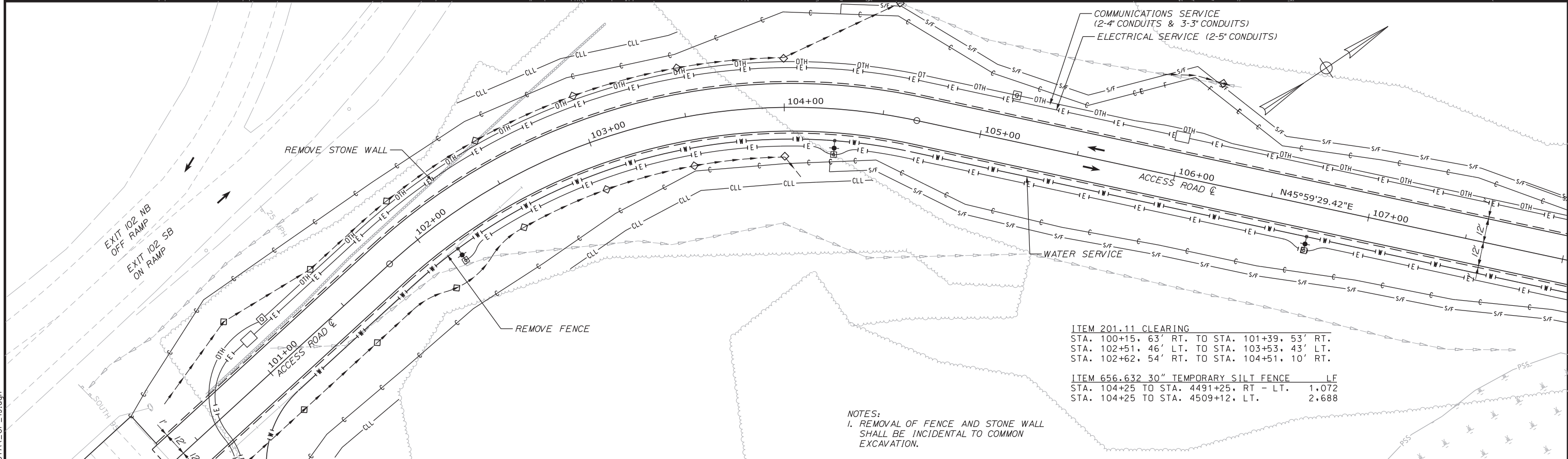
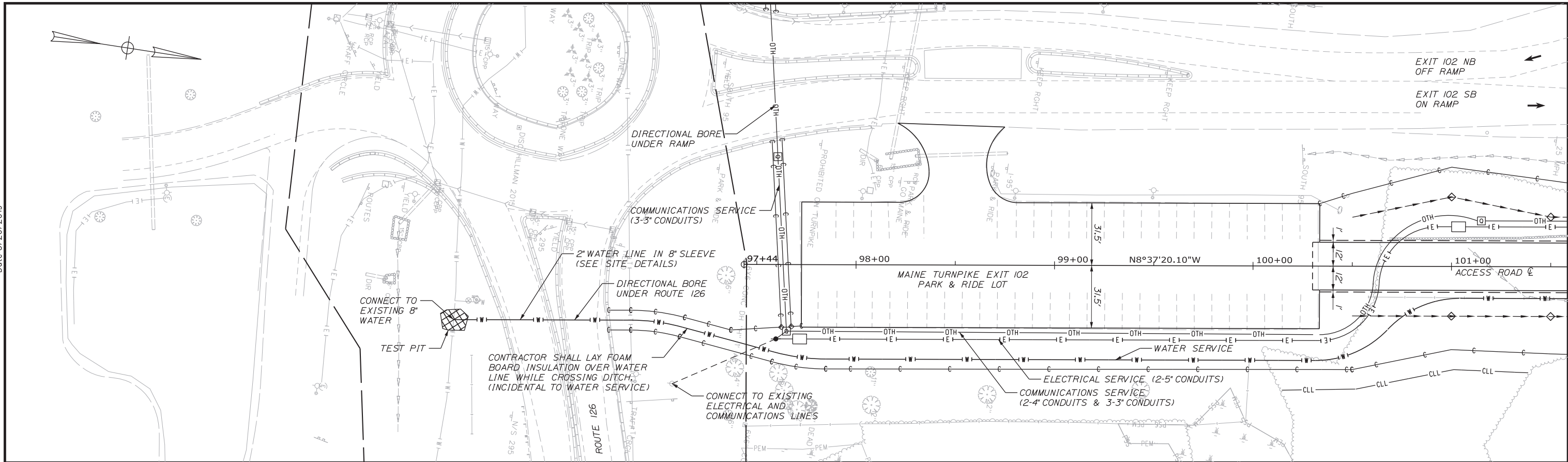
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
GENERAL PLAN 18

SHEET NUMBER: GP-18
CONTRACT: 2019.04
131 OF 503

Date: 3/20/2019



ITEM 201.11 CLEARING
 STA. 100+15, 63' RT. TO STA. 101+39, 53' RT.
 STA. 102+51, 46' LT. TO STA. 103+53, 43' LT.
 STA. 102+62, 54' RT. TO STA. 104+51, 10' RT.

ITEM 656.632 30" TEMPORARY SILT FENCE LF
 STA. 104+25 TO STA. 4491+25, RT - LT. 1.072
 STA. 104+25 TO STA. 4509+12, LT. 2.688

NOTES:
 1. REMOVAL OF FENCE AND STONE WALL SHALL BE INCIDENTAL TO COMMON EXCAVATION.

Scale: 25 0 25 50
 Scale of Feet

No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

STANTEC CONSULTING SERVICES INC.
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 FAX (207) 883-3376

MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

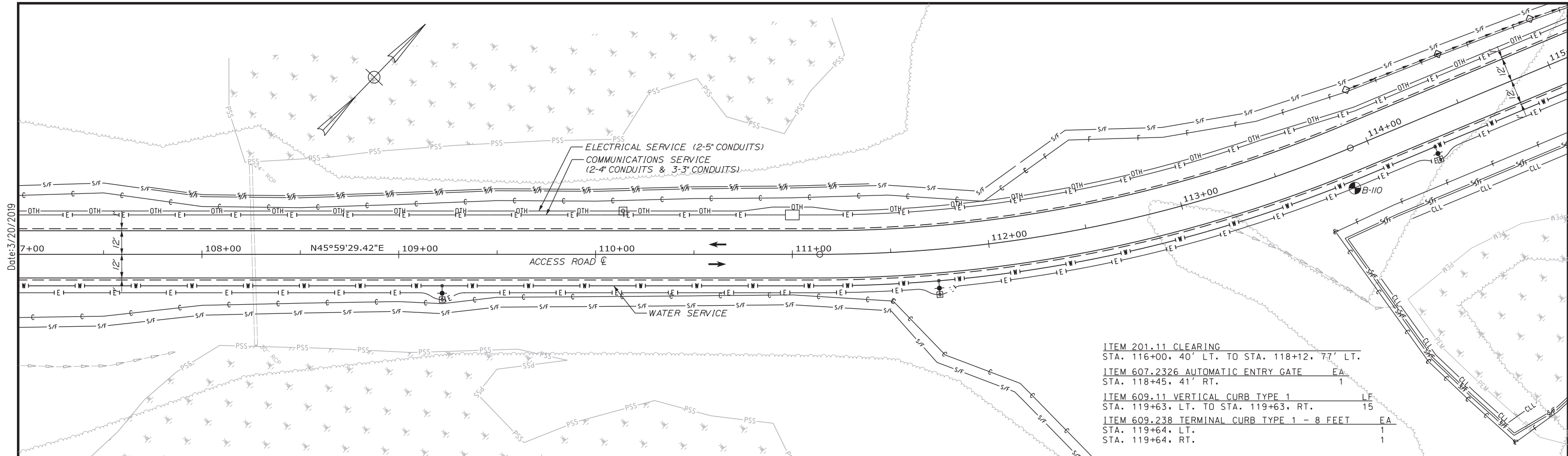
INTERCHANGE 103
 ORT CONVERSION

GENERAL PLAN 19

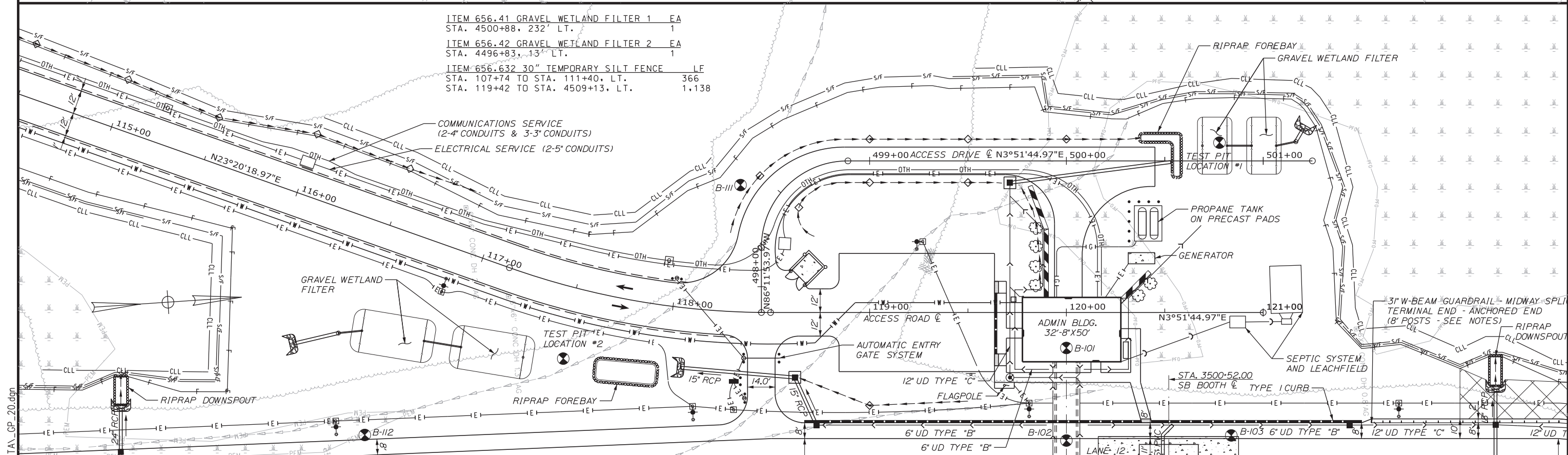
SHEET NUMBER: GP-19
 132 OF 503

CONTRACT: 2019.04

Filename: ...100\HIGHWAY\MSTA1_GP_19.dgn



- ITEM 201.11 CLEARING
STA. 116+00, 40' LT. TO STA. 118+12, 77' LT. EA
- ITEM 607.2326 AUTOMATIC ENTRY GATE EA 1
STA. 118+45, 41' RT.
- ITEM 609.11 VERTICAL CURB TYPE 1 LF 15
STA. 119+63, LT. TO STA. 119+63, RT.
- ITEM 609.238 TERMINAL CURB TYPE 1 - 8 FEET EA 1
STA. 119+64, LT. 1
STA. 119+64, RT. 1



- ITEM 656.41 GRAVEL WETLAND FILTER 1 EA 1
STA. 4500+88, 232' LT.
- ITEM 656.42 GRAVEL WETLAND FILTER 2 EA 1
STA. 4496+83, 13' LT.
- ITEM 656.632 30" TEMPORARY SILT FENCE LF 366
STA. 107+74 TO STA. 111+40, LT. 1,138
STA. 119+42 TO STA. 4509+13, LT.

Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE					
	By	Date	By	Date	
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

**INTERCHANGE 103
ORT CONVERSION**

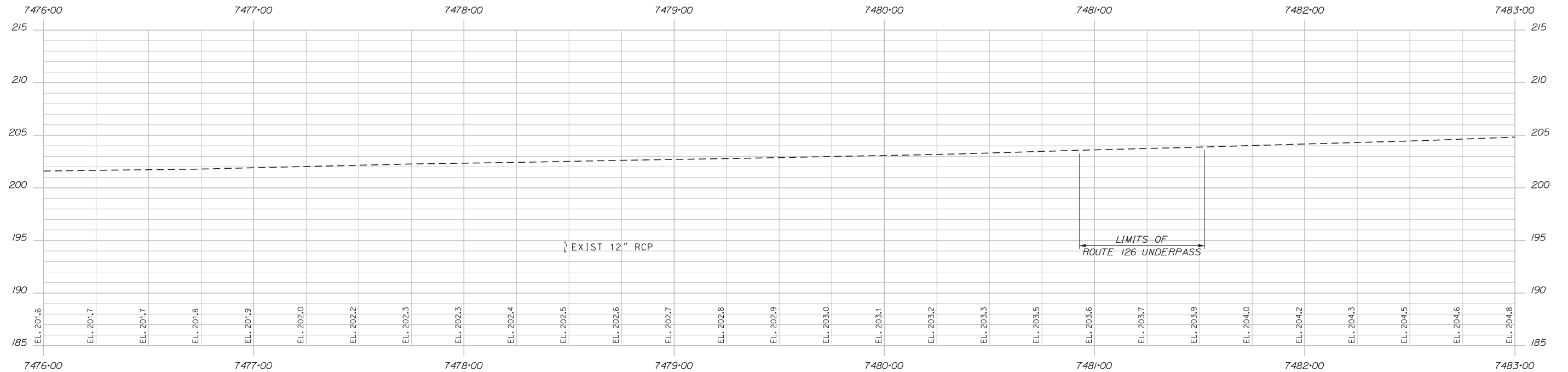
GENERAL PLAN 20

SHEET NUMBER: GP-20
133 OF 503

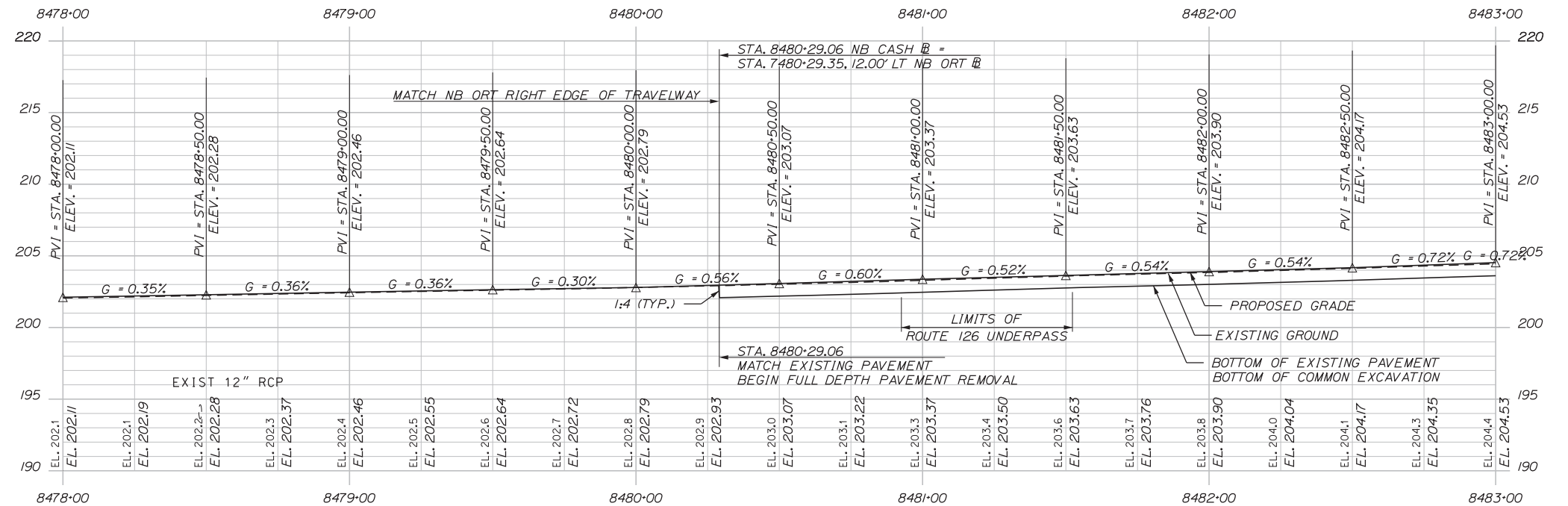
CONTRACT: 2019.04

Date: 3/20/2019
Filename: ...00.HIGHWAY.MSTA_GP_20.dgn

Date: 3/20/2019

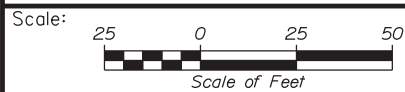


NB ORT PROFILE



NB CASH PROFILE

Filename: ... \HIGHWAY\MSTA\...Profile-NB01.dgn



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THE GOLD STAR
MEMORIAL HIGHWAY

INTERCHANGE 103
ORT CONVERSION
NORTHBOUND PROFILE 1

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
Designed	By	Date	Checked
Drawn	By	Date	In Charge of
	PLP	3\20\19	LEM
	EJB	3\20\19	GAE

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

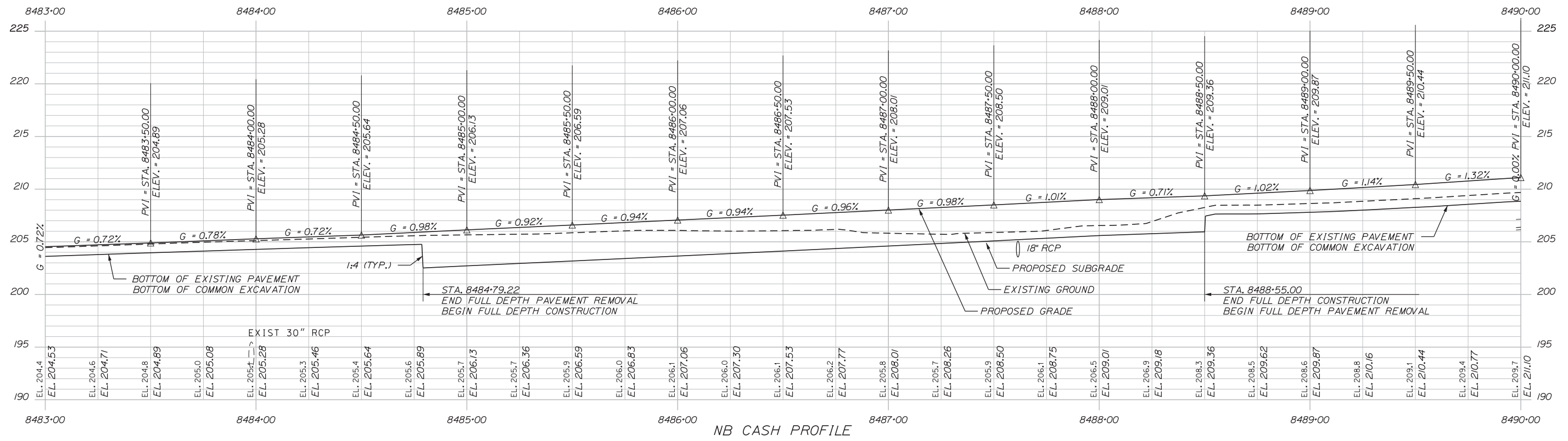
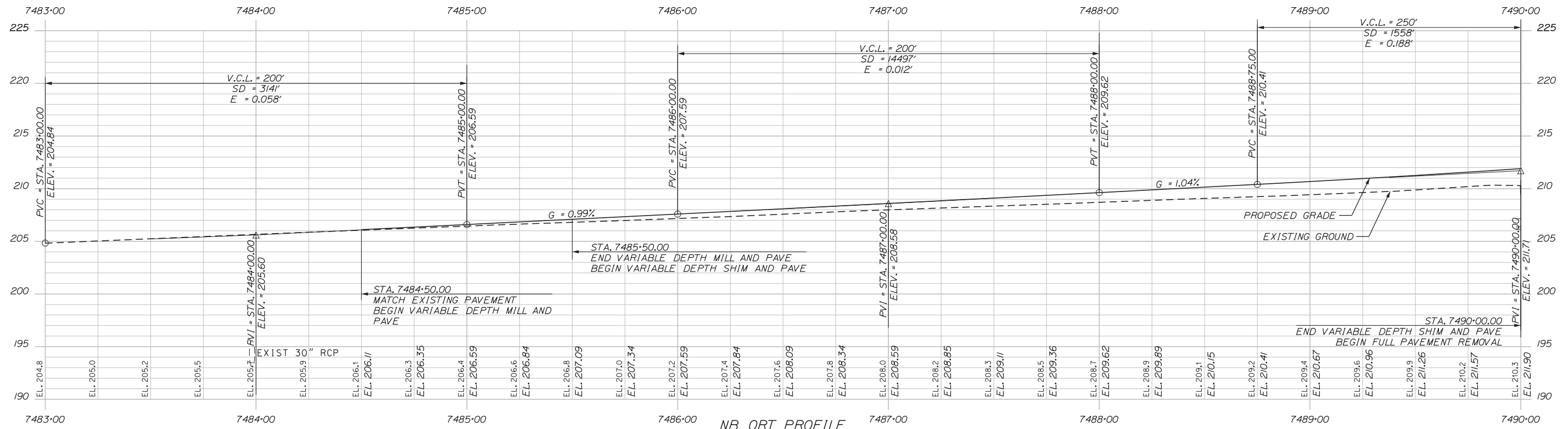
CONTRACT: 2019.04

SHEET NUMBER: PR-1

134 OF 503

Date: 3/20/2019

Filename: ... \HIGHWAY\MSTA\Profile-NB02.dgn



Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date	By	Date	
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

NORTHBOUND PROFILE 2

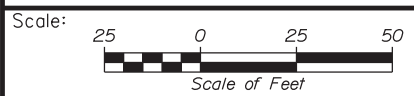
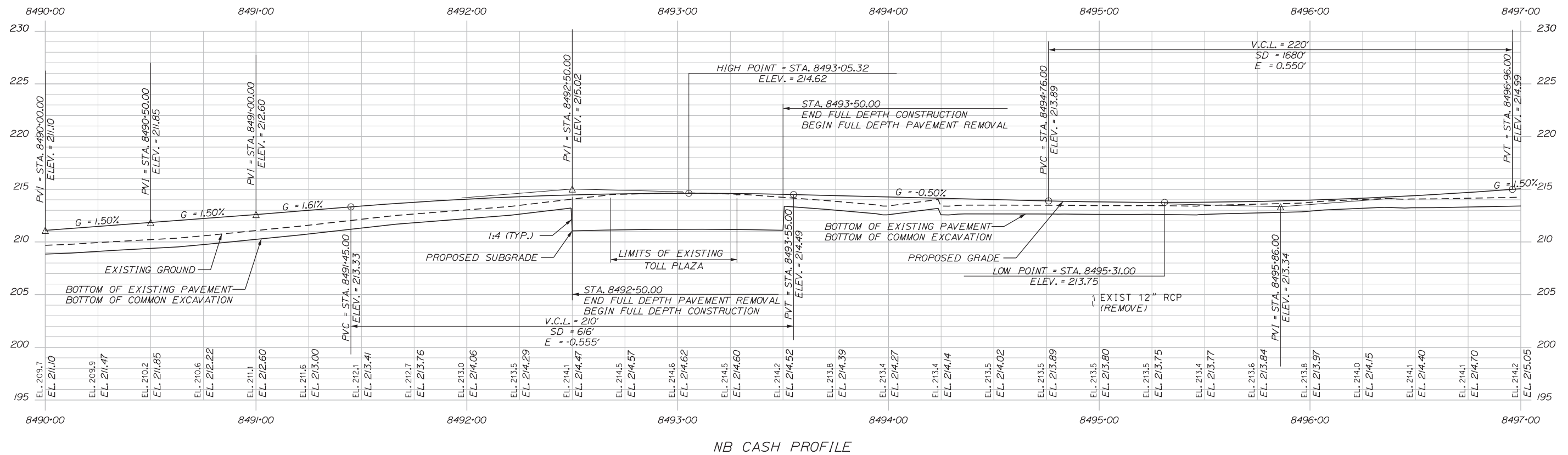
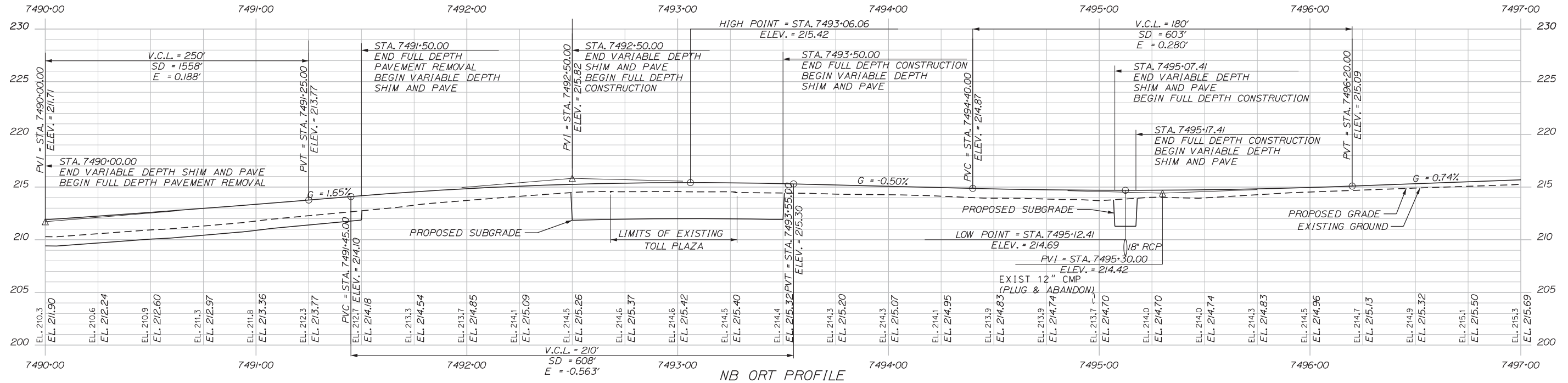
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CONTRACT: 2019.04

135 OF 503

Date: 3/20/2019

Filename: ...HIGHWAY\MSTA\Profile-NB03.dgn



No.	Revision	By	Date

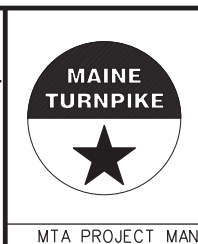
Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
 MEMORIAL HIGHWAY**

INTERCHANGE 103
 ORT CONVERSION

NORTHBOUND PROFILE 3

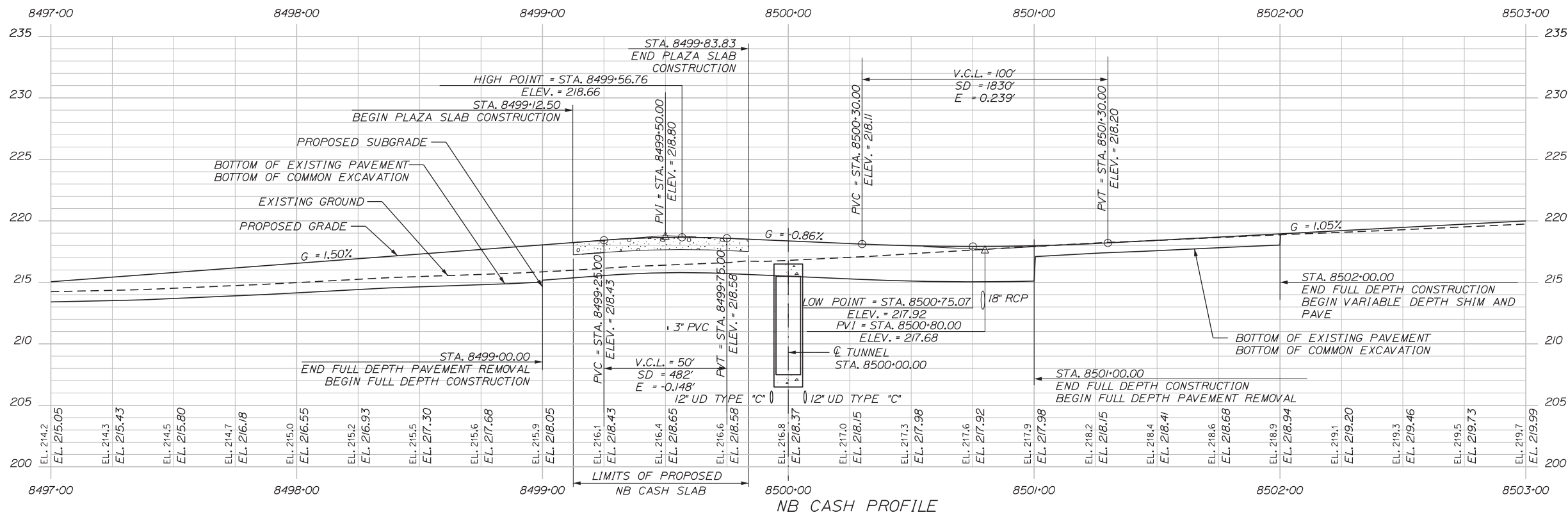
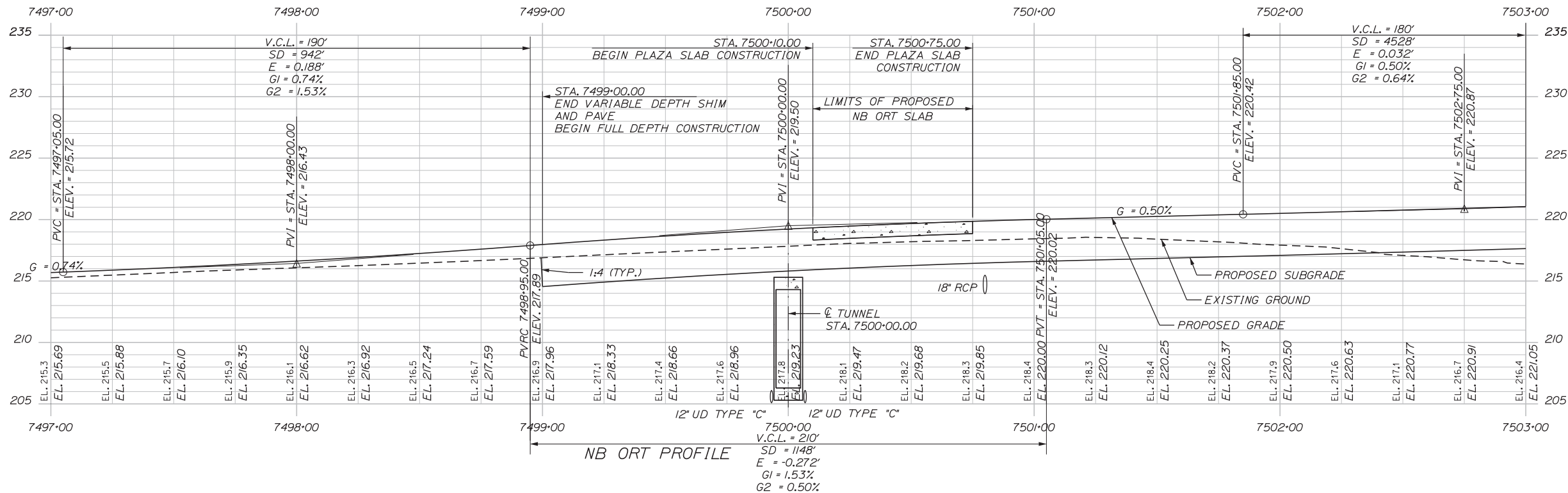
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CONTRACT: 2019.04

136 OF 503

Date: 3/20/2019

Filename: ...HIGHWAY\MSTA\Profile-NB04.dgn



Designed by:

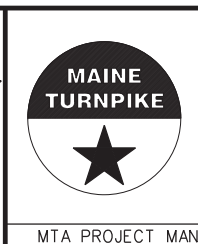
Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
 MEMORIAL HIGHWAY**

INTERCHANGE 103
 ORT CONVERSION

NORTHBOUND PROFILE 4

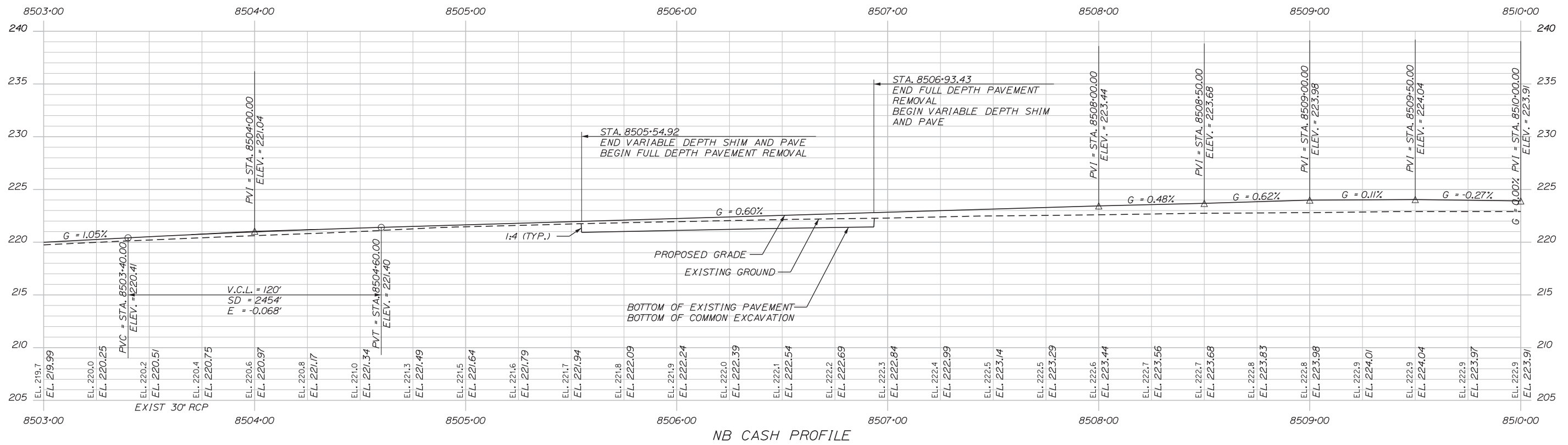
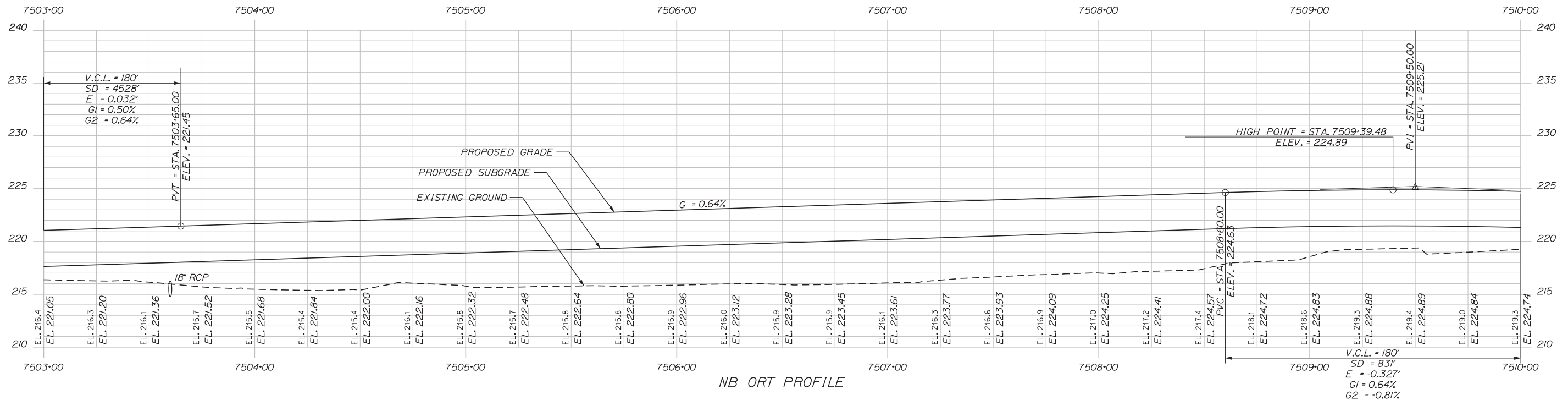
SHEET NUMBER: PR-04

CONTRACT: 2019.04

137 OF 503

Date: 3/20/2019

Filename: ...HIGHWAY\MSTA\Profile-NB05.dgn



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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

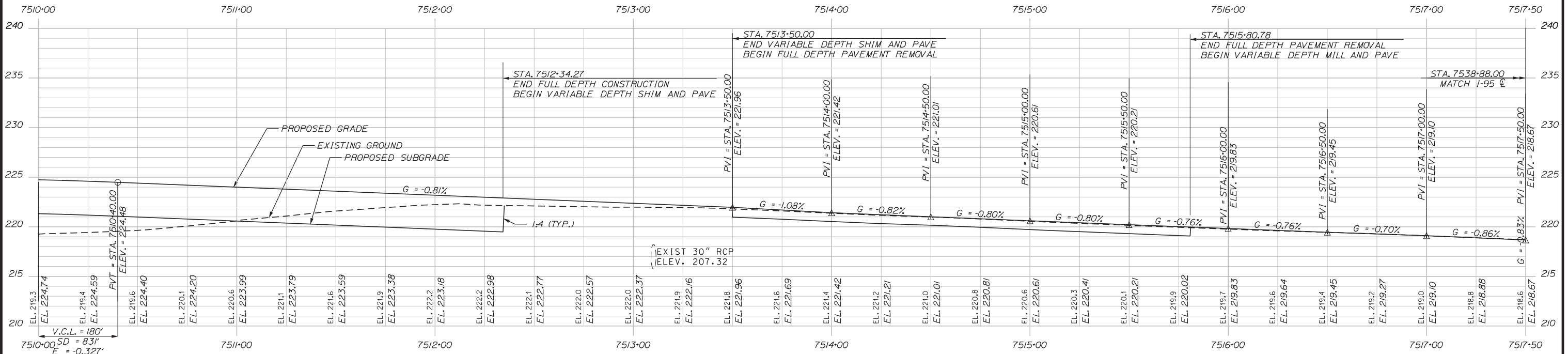
NORTHBOUND PROFILE 5

SHEET NUMBER: PR-05

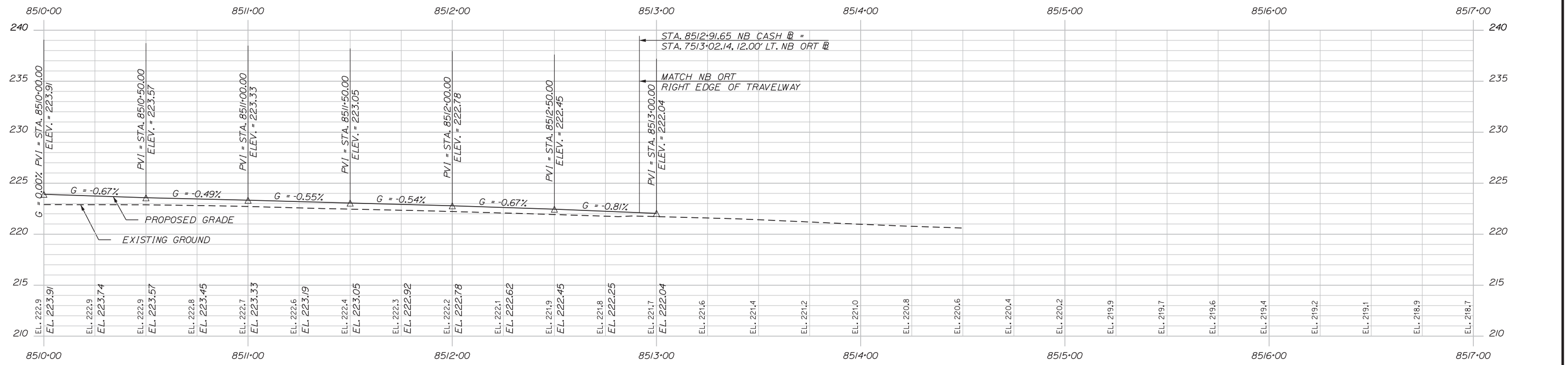
CONTRACT: 2019.04

138 OF 503

Date: 3/20/2019

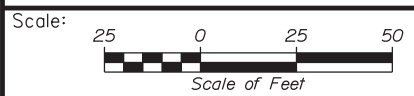


NB ORT PROFILE



NB CASH PROFILE

Filename: ...HIGHWAY\MSTA\Profile-NB06.dgn



No.	Revision	By	Date

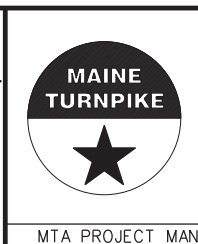
Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
 MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

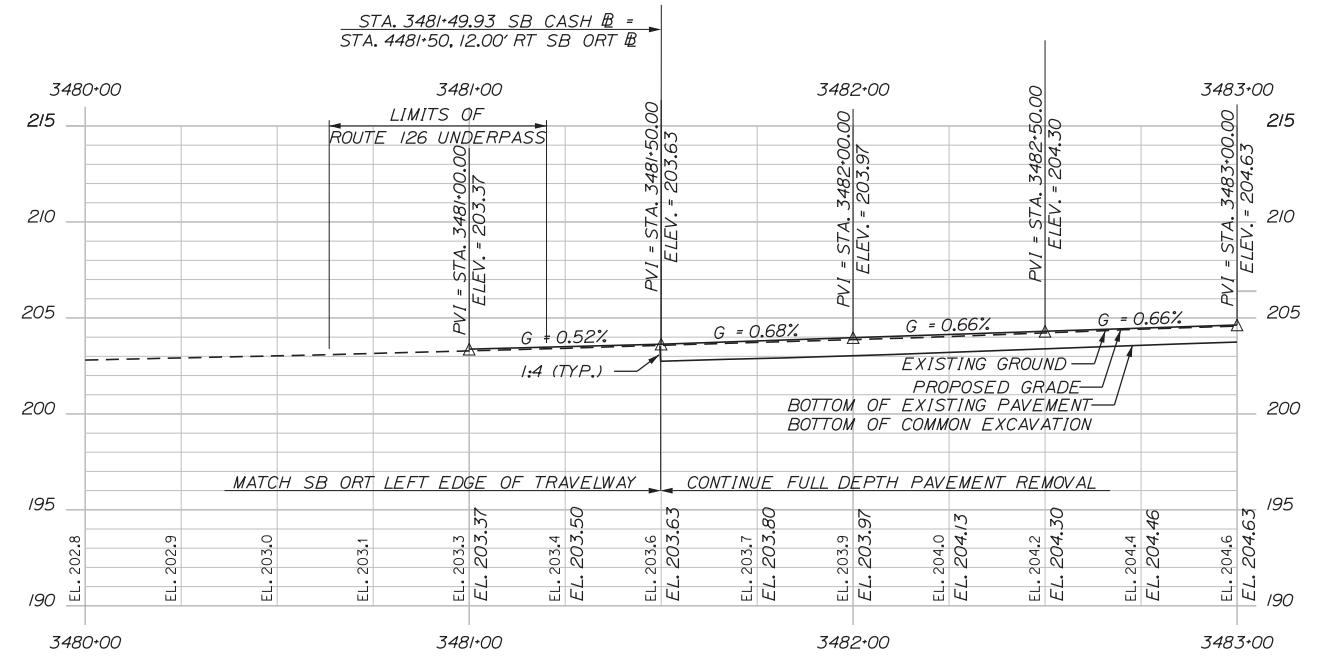
INTERCHANGE 103
 ORT CONVERSION

NORTHBOUND PROFILE 6

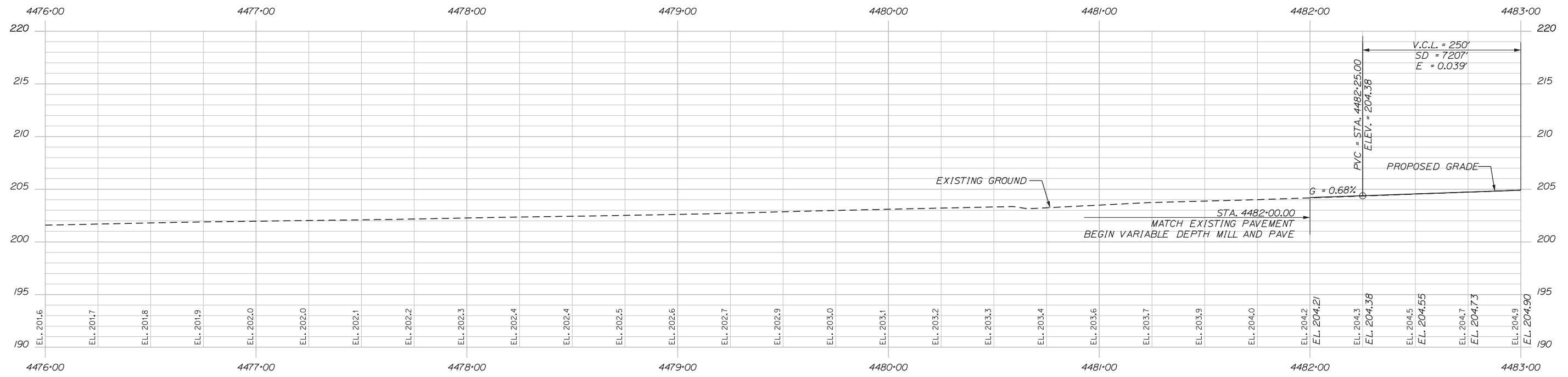
SHEET NUMBER: PR-06
 139 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

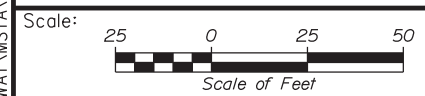


SB CASH PROFILE



SB ORT PROFILE

Filename: ...HIGHWAY\MSTA\Profile-SB01.dgn



Designed by:

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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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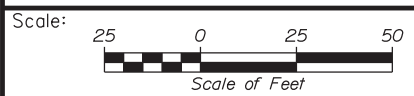
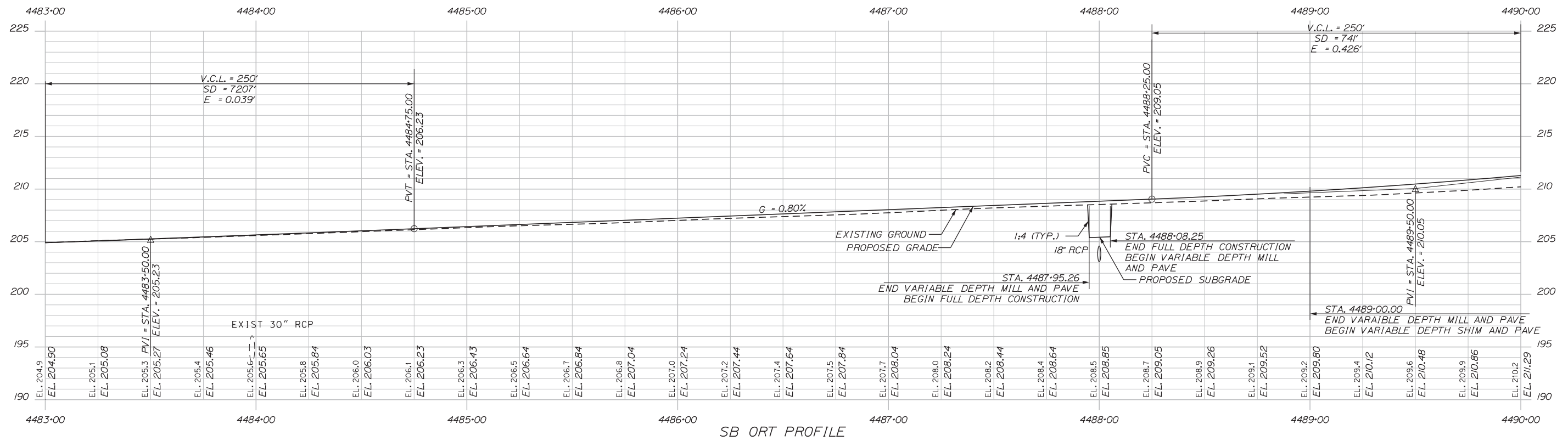
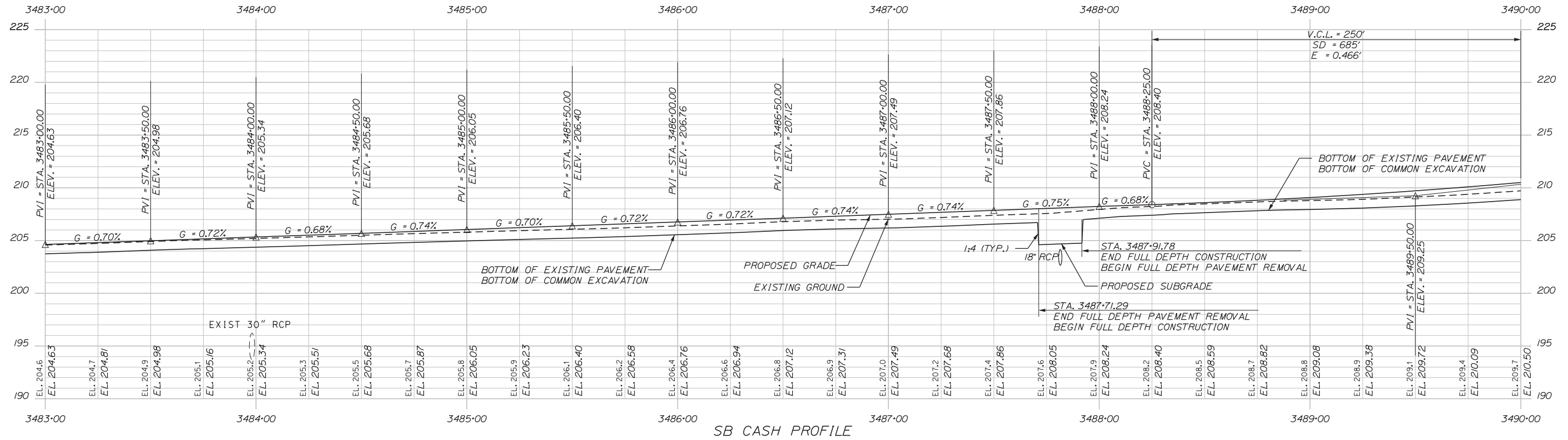
**THE GOLD STAR
MEMORIAL HIGHWAY**

INTERCHANGE 103
ORT CONVERSION
SOUTHBOUND PROFILE 1

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

Date: 3/20/2019

Filename: ...HIGHWAY\MSTA\Profile-SB02.dgn



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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION

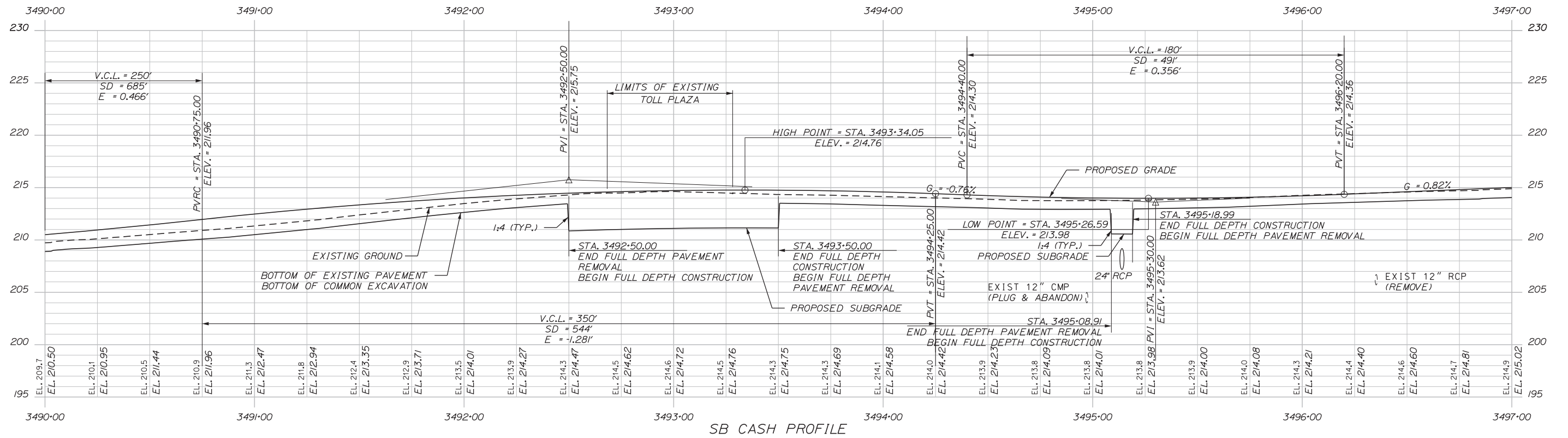
SOUTHBOUND PROFILE 2

SHEET NUMBER: PR-08
 141 OF 503

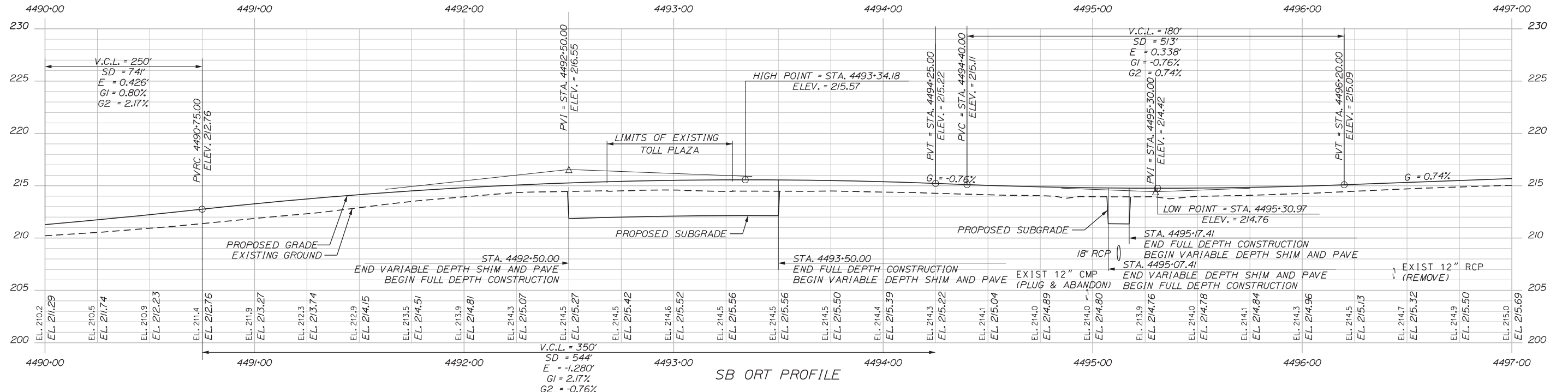
CONTRACT: 2019.04

Date: 3/20/2019

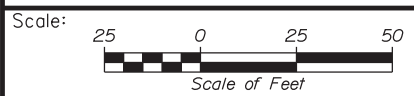
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SB CASH PROFILE



SB ORT PROFILE



No.	Revision	By	Date

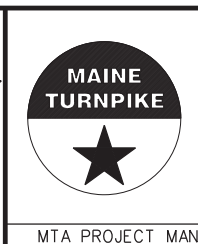
Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
 MEMORIAL HIGHWAY**

INTERCHANGE 103
 ORT CONVERSION

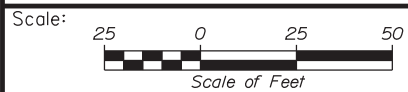
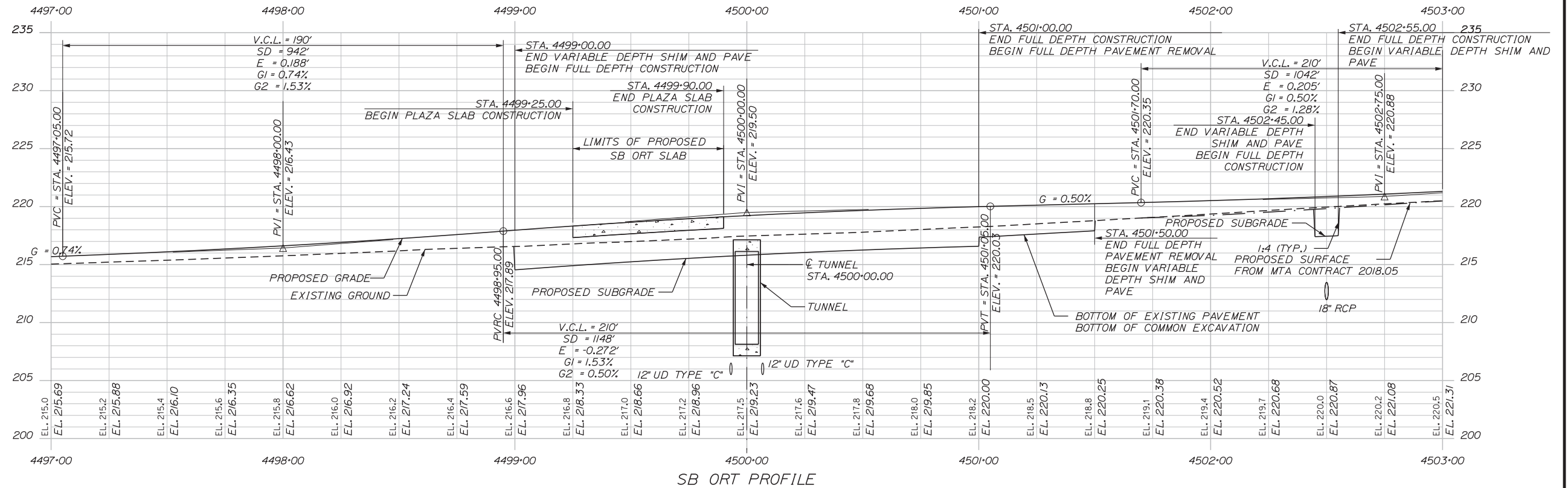
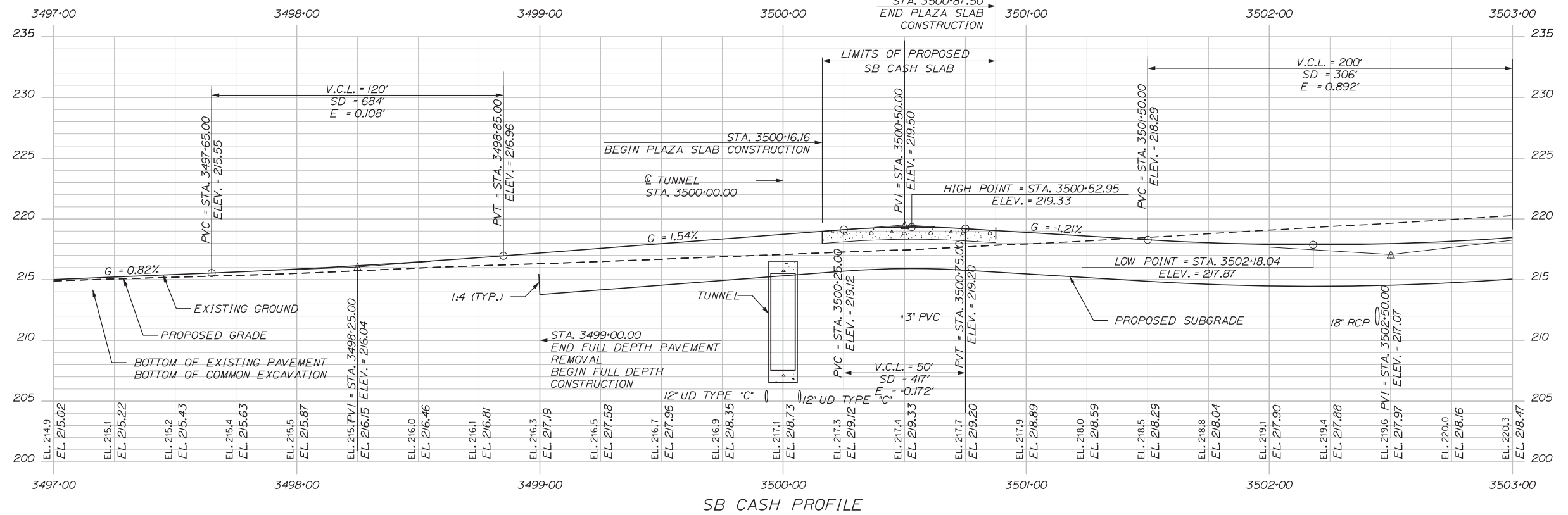
SOUTHBOUND PROFILE 3

SHEET NUMBER: PR-09
 142 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ... \HIGHWAY\MSTA\Profile-SB04.dgn



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THE GOLD STAR
MEMORIAL HIGHWAY

INTERCHANGE 103
ORT CONVERSION

SOUTHBOUND PROFILE 4

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
	By	Date	
Designed	PLP	3\20\19	Checked LEM 3\20\19
Drawn	EJB	3\20\19	In Charge of GAE 3\20\19

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

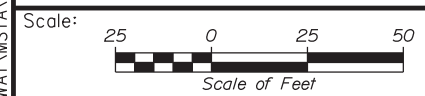
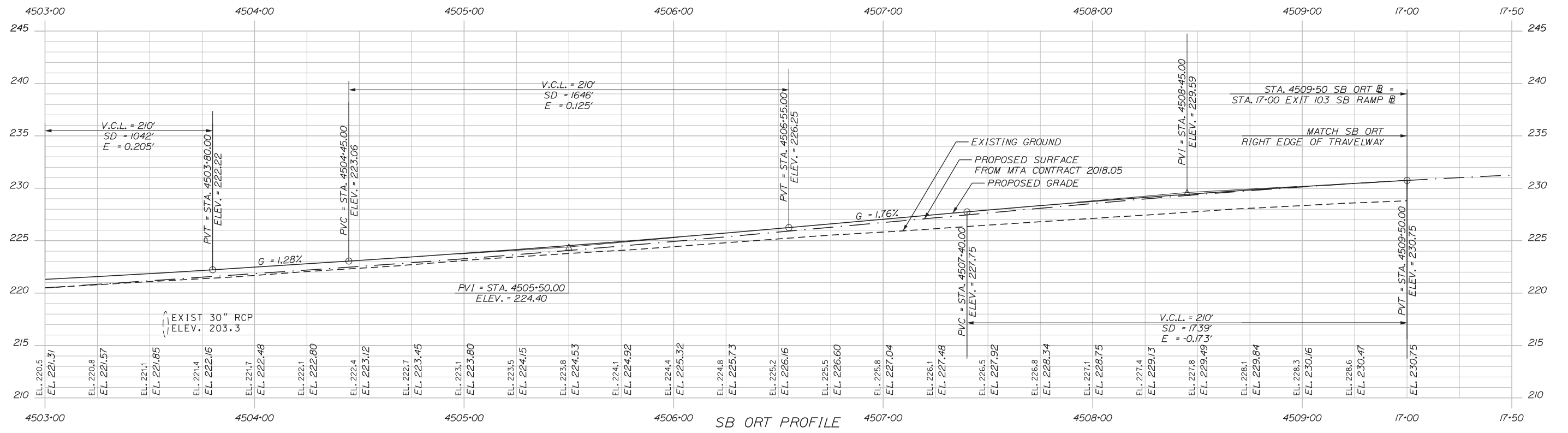
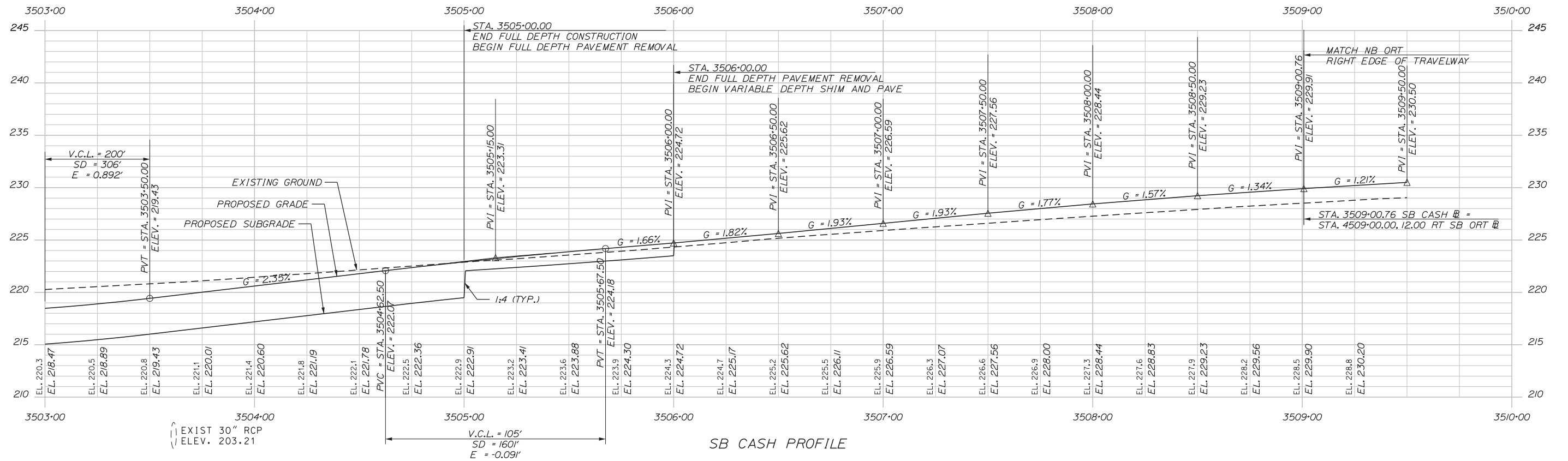
CONTRACT: 2019.04

SHEET NUMBER: PR-10

14.3 OF 50.3

Date: 3/20/2019

Filename: ...HIGHWAY\MSTA\Profile-SB05.dgn



Designed by:

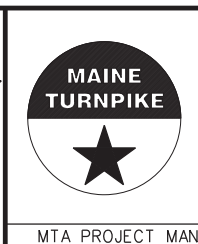
Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

STANTEC CONSULTING SERVICES INC.
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**THE GOLD STAR
MEMORIAL HIGHWAY**

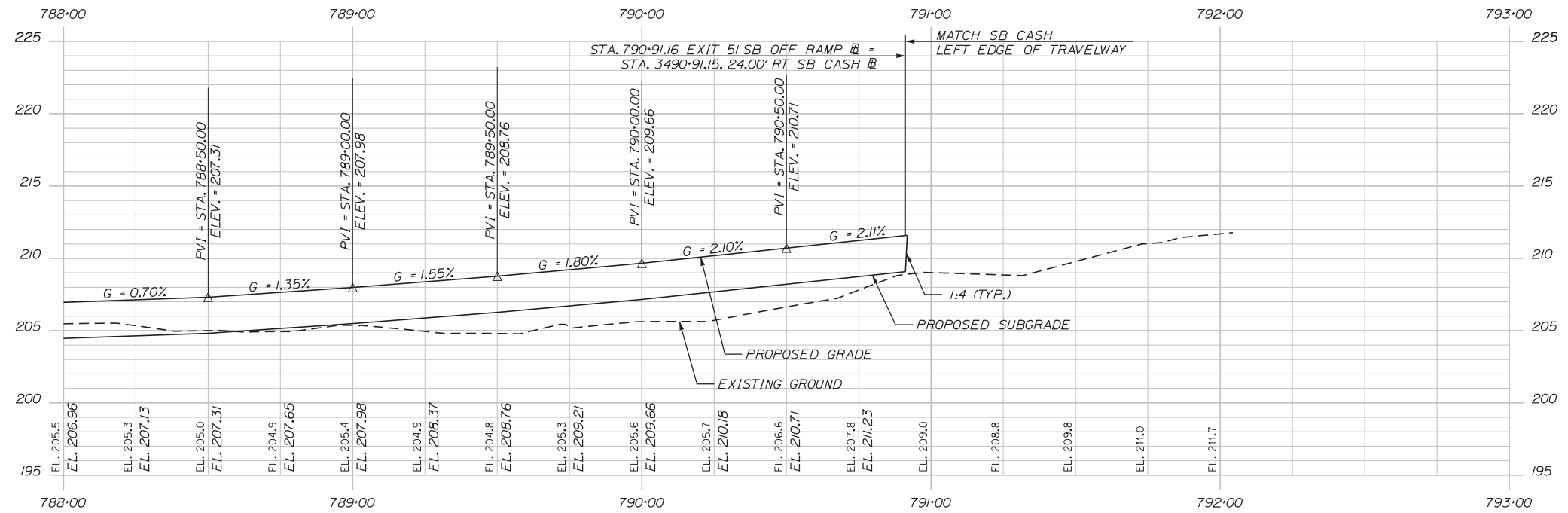
INTERCHANGE 103
ORT CONVERSION

SOUTHBOUND PROFILE 5

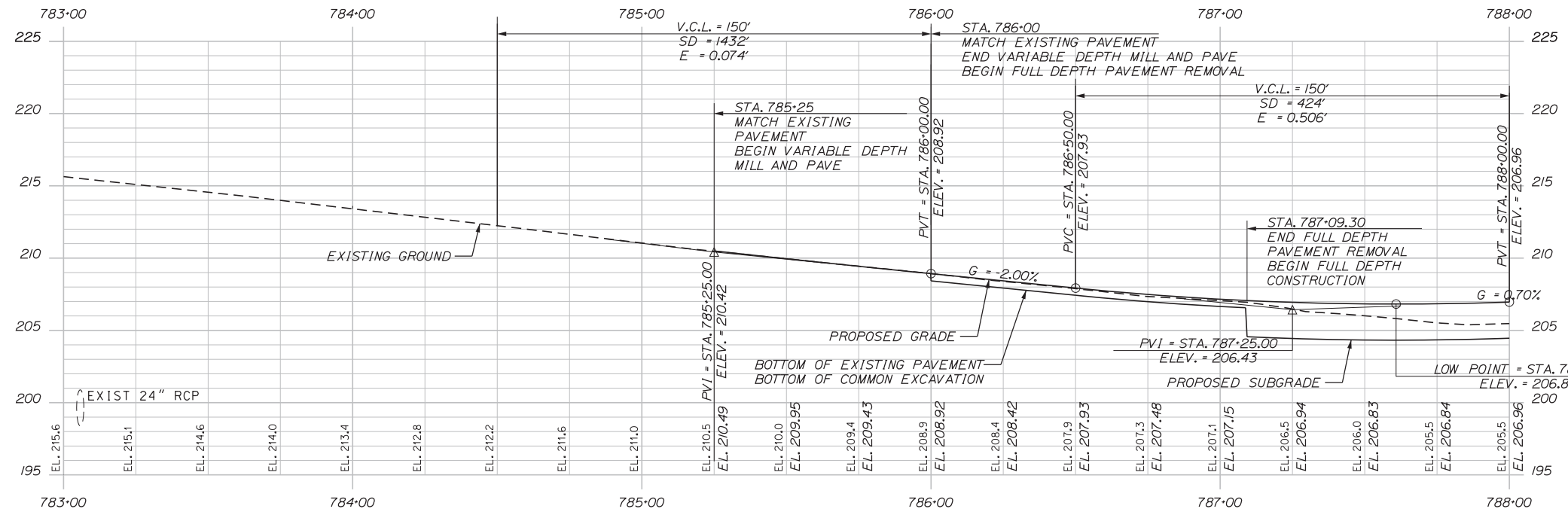
SHEET NUMBER: PR-11
144 OF 503

CONTRACT: 2019.04

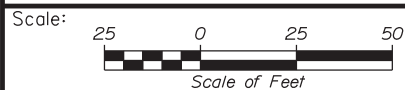
Date: 3/20/2019



EXIT 51 SB OFF RAMP PROFILE



EXIT 51 SB OFF RAMP PROFILE



No.	Revision	By	Date

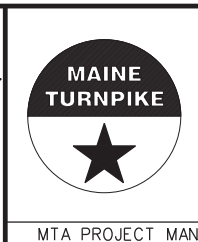
Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

STANTEC CONSULTING SERVICES INC.
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**THE GOLD STAR
 MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION

EXIT 51 SB OFF RAMP PROFILE

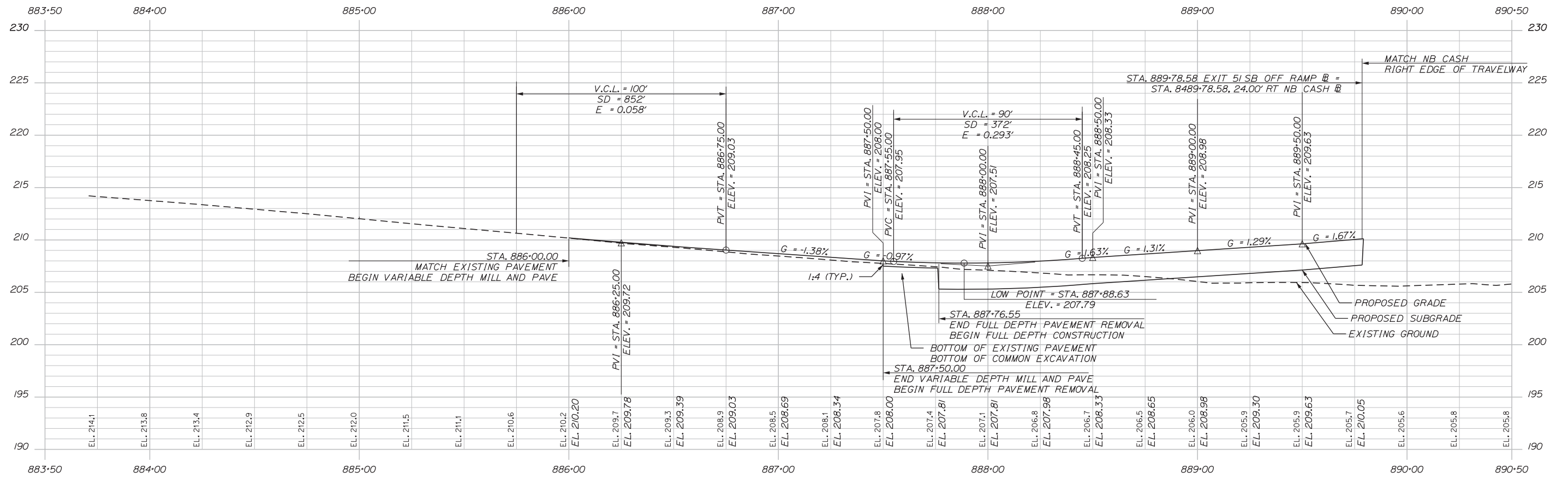
SHEET NUMBER: PR-12
 145 OF 503

CONTRACT: 2019.04

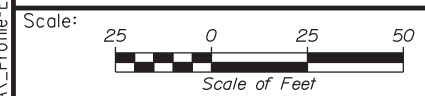
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Date: 3/20/2019

Filename: ...MSTA_Profile-Exit51-02.dgn



EXIT 51 NB ON RAMP PROFILE



No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
 MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

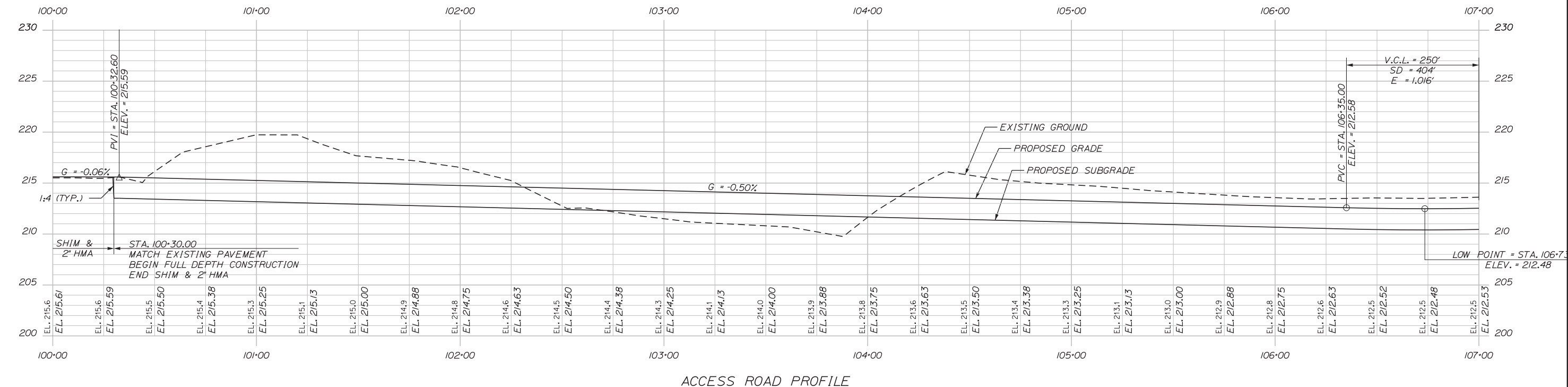
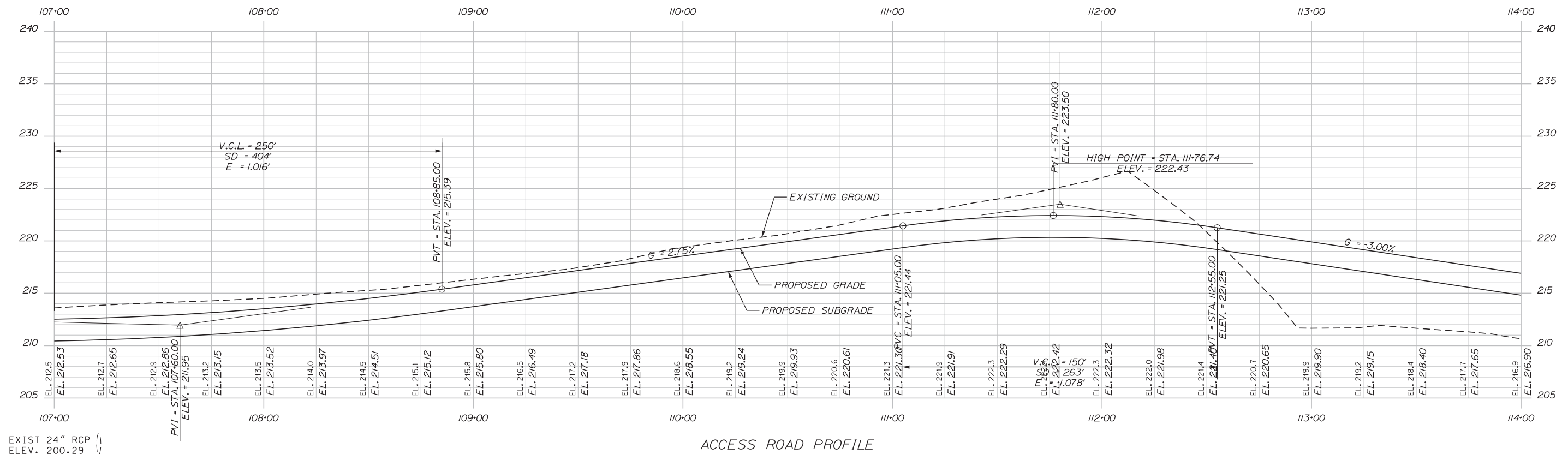
INTERCHANGE 103
 ORT CONVERSION
 EXIT 51 NB ON RAMP PROFILE

CONTRACT: 2019.04

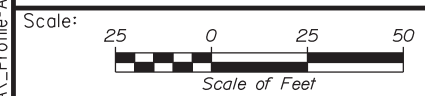
SHEET NUMBER: PR-13

146 OF 503

Date: 3/20/2019



Filename: ...MSTA_Profile-AccessRd01.dgn



Designed by:

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No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

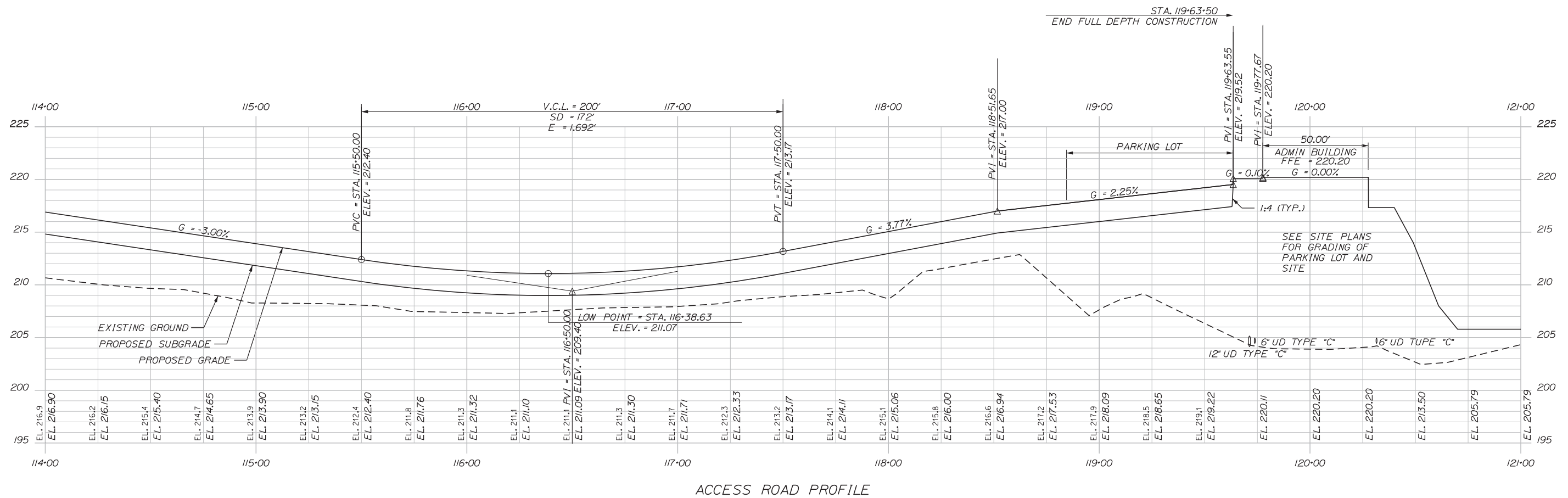
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

ACCESS ROAD PROFILE 1

SHEET NUMBER: PR-14
14.7 OF 50.3

CONTRACT: 2019.04



No.	Revision	By	Date

Designed by:

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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION

ACCESS ROAD PROFILE 2

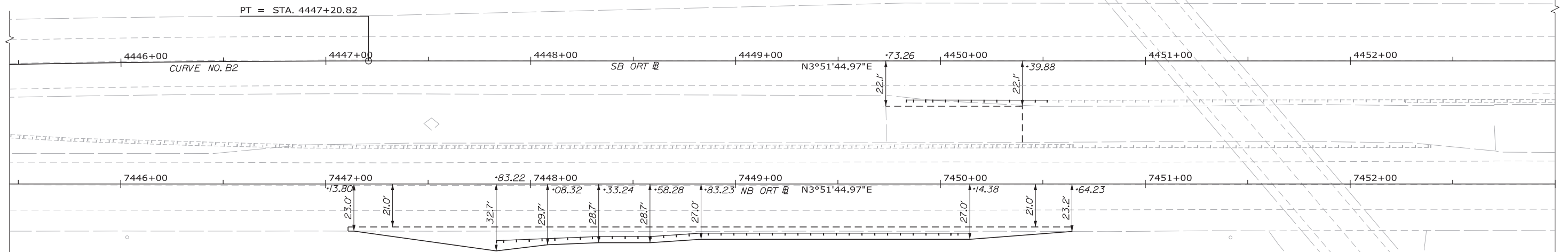
SHEET NUMBER: PR-15
 148 OF 503

CONTRACT: 2019.04

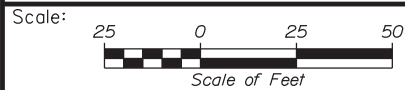
Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\...GM_02.dgn

SB ORT											
CURVE NO.	POINT	STATION	DELTA	Dc	R	T	L	EXT.	N	E	BEARING
B1	PC	4425+12.64							496204.33	3041784.06	
	PI	4434+89.83	17° 57' 17.8" LEFT	0° 55' 33.3"	6188.00	977.59	1939.15	76.74	497122.36	3042120.06	
	PRC	4444+51.39							498099.27	3042156.69	
B2	PI	4445+86.11	0° 42' 54.9" RIGHT	0° 38' 11.8"	9000.00	134.72	269.43	1.01	498233.90	3042161.73	
	PT	4447+20.82							498368.31	3042170.81	N 03° 51' 45.0" E
	PC	4481+83.97							501823.60	3042404.09	
B3	PI	4484+33.98	0° 18' 07.8" RIGHT	0° 15' 37.6"	22000.00	250.01	500.00	1.42	502073.04	3042420.94	
	PT	4486+83.97							502322.04	3042443.44	N 05° 09' 52.8" E
	PC	4488+00.00							502437.60	3042453.89	
B4	PI	4490+50.01	0° 18' 07.8" LEFT	0° 15' 37.6"	22000.00	250.01	500.00	1.42	502686.60	3042476.39	
	PT	4493+00.00							502936.04	3042493.23	N 03° 51' 45.0" E
	PJ	4507+50.00							504382.74	3042590.91	N 04° 05' 23.2" E
	POE	4509+50.00							504582.24	3042605.17	



NB ORT											
CURVE NO.	POINT	STATION	DELTA	Dc	R	T	L	EXT.	N	E	BEARING
C1	POB	7364+21.52							490359.98	3040243.74	N 03° 51' 45.0" E
	PC	7373+03.70							491240.16	3040303.16	
	PI	7381+11.68	15° 51' 40.8" RIGHT	0° 59' 16.3"	5800.00	807.98	1605.63	56.01	492046.31	3040357.59	
C2	PT	7389+09.33							492806.88	3040630.27	N 19° 43' 25.8" E
	PI	7418+45.54							495570.82	3041621.21	N 19° 53' 24.9" E
	PC	7419+54.86							495673.63	3041658.40	
C3	PI	7421+88.73	0° 27' 43.4" LEFT	0° 05' 55.6"	58000.00	233.87	467.73	0.47	495893.54	3041737.97	
	PT	7424+22.59							496114.09	3041815.75	N 19° 25' 41.5" E
	PC	7426+00.28							496281.66	3041874.86	
C4	PI	7432+60.78	13° 27' 12.6" LEFT	0° 01' 23.3"	5600.00	660.50	1314.92	38.82	496904.55	3042094.56	
	PT	7439+15.21							497561.47	3042163.31	N 05° 58' 28.9" E
	PC	7441+05.59							497750.81	3042183.13	
C5	PI	7442+71.50	02° 06' 43.9" LEFT	0° 38' 11.8"	9000.00	165.91	331.78	1.53	497915.82	3042200.40	
	PT	7444+37.37							498081.36	3042211.57	N 03° 51' 45.0" E
	PC	7502+31.11							503861.93	3042601.85	
C6	PI	7506+73.40	19° 59' 14.5" RIGHT	02° 16' 57.7"	2510.00	442.30	875.60	38.67	504303.22	3042631.64	
	PT	7511+06.71							504707.75	3042810.48	N 23° 50' 59.4" E
	PC	7514+79.66							505048.85	3042961.28	
C6	PI	7521+85.43	10° 00' 31.0" RIGHT	0° 42' 39.1"	8060.00	705.77	1407.95	30.84	505694.35	3043246.65	
	PT	7528+87.61							506280.44	3043639.87	N 33° 51' 30.5" E
	POE	7538+86.12							507109.62	3044196.18	



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THE GOLD STAR
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INTERCHANGE 103
ORT CONVERSION

GEOMETRIC PLAN 1

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
	By	Date	
Designed	PLP	3\20\19	Checked
Drawn	EJB	3\20\19	In Charge of

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

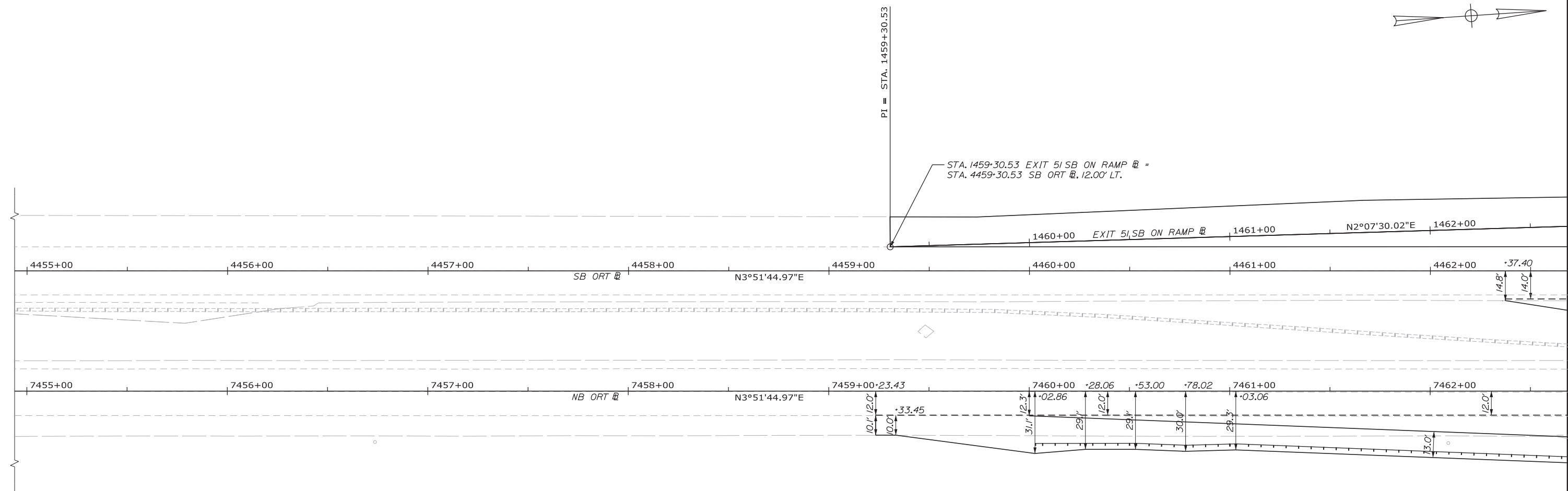
CONTRACT: 2019.04

SHEET NUMBER: GM-01

149 OF 503

Date: 3/20/2019

Filename: ...\\00\HIGHWAY\MSTA\GM_03.dgn



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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
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Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
 MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 GEOMETRIC PLAN 2

SHEET NUMBER: GM-02
 150 OF 503

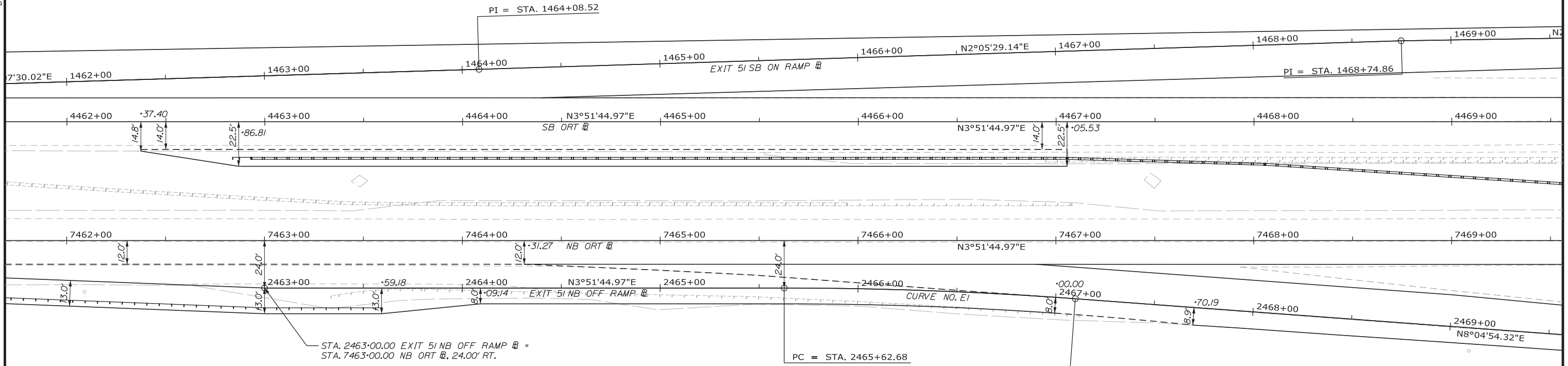
CONTRACT: 2019.04

Date: 3/20/2019

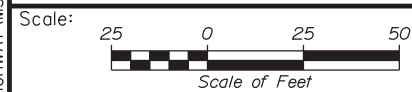
Filename: ... \00\HIGHWAY\MSTA\GM_04.dgn



EXIT 5I SB ON RAMP #											
CURVE NO.	POINT	STATION	DELTA	Dc	R	T	L	EXT.	N	E	BEARING
	POB	1459+00.00							499545.63	3042238.27	N 03° 51' 45.0" E
	PI	1459+30.53							499576.09	3042240.32	N 02° 07' 30.0" E
	PI	1464+08.52							500053.75	3042258.05	N 02° 05' 29.1" E
	PI	1468+74.86							500519.78	3042275.07	N 02° 55' 43.9" E
	PC	1470+12.52							500657.26	3042282.10	
KI	PI	1472+89.56	22° 13' 14.0" LEFT	04° 05' 33.0"	1400.00	277.04	547.02	27.15	500933.94	3042296.26	
	PT	1475+59.54							501195.16	3042203.97	N 19° 27' 30.0" W
	POE	1480+78.37							501684.36	3042031.13	



EXIT 5I NB OFF RAMP #											
CURVE NO.	POINT	STATION	DELTA	Dc	R	T	L	EXT.	N	E	BEARING
	POB	2463+00.00							499938.14	3042360.99	N 03° 51' 45.0" E
	PC	2465+62.68							500200.22	3042378.68	
E1	PI	2466+36.35	04° 13' 09.3" RIGHT	02° 51' 53.2"	2000.00	73.67	147.28	1.36	500273.72	3042383.64	
	PT	2467+09.96							500346.66	3042394.00	N 08° 04' 54.3" E
	PC	2471+01.01							500733.83	3042448.98	
E2	PI	2473+39.59	18° 55' 05.7" RIGHT	04° 00' 04.0"	1432.00	238.58	472.83	19.74	500970.05	3042482.52	
	PT	2475+73.83							501182.63	3042590.83	N 27° 00' 00.0" E
	POE	2482+91.55							501822.12	3042916.67	



Designed by:



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THE GOLD STAR
 MEMORIAL HIGHWAY

INTERCHANGE 103
 ORT CONVERSION

GEOMETRIC PLAN 3

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
	By	Date	
Designed	PLP	3\20\19	Checked
Drawn	EJB	3\20\19	In Charge of

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

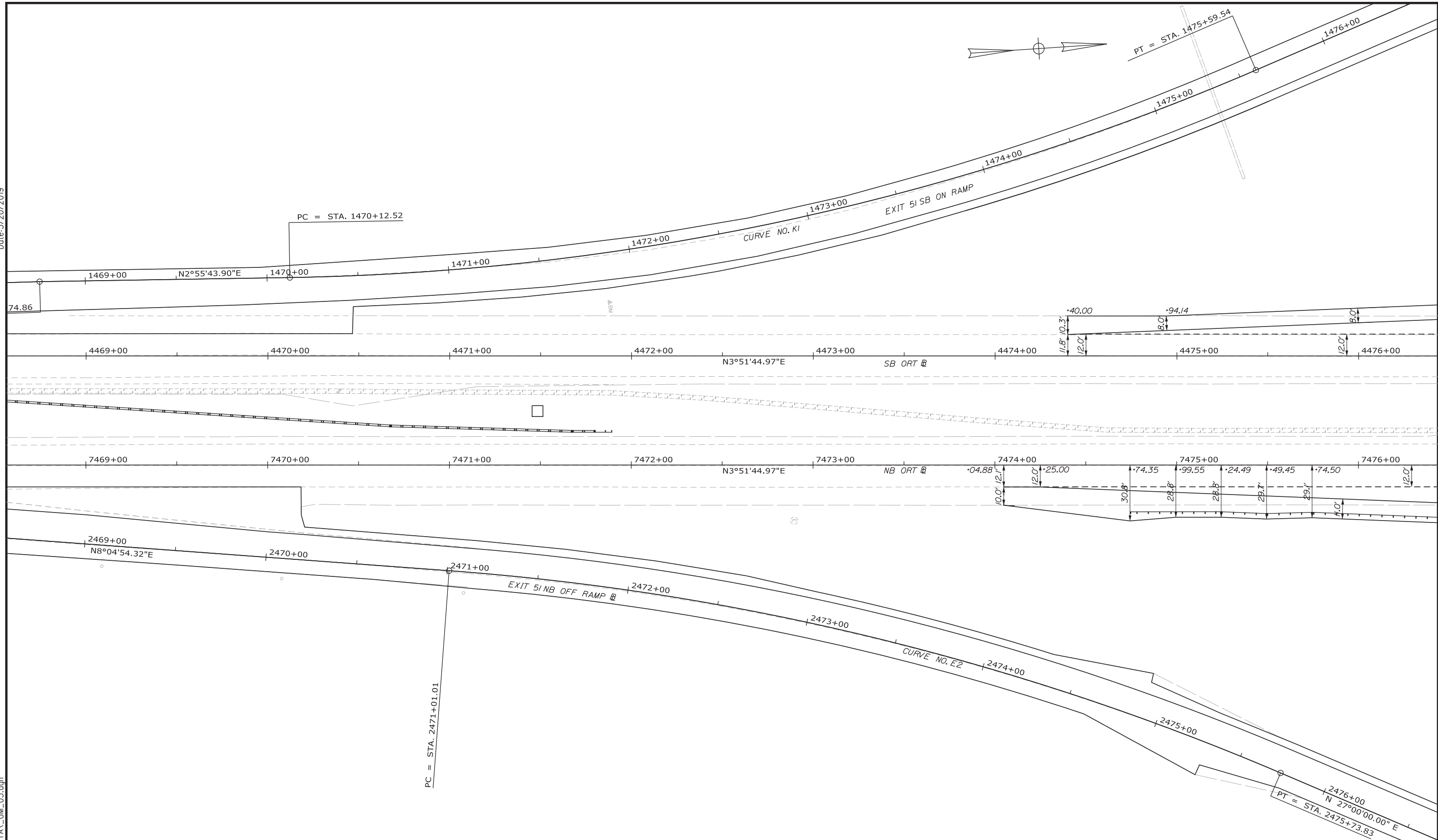
CONTRACT: 2019.04

SHEET NUMBER: GM-03

151 OF 503

Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\GM_05.dgn



Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

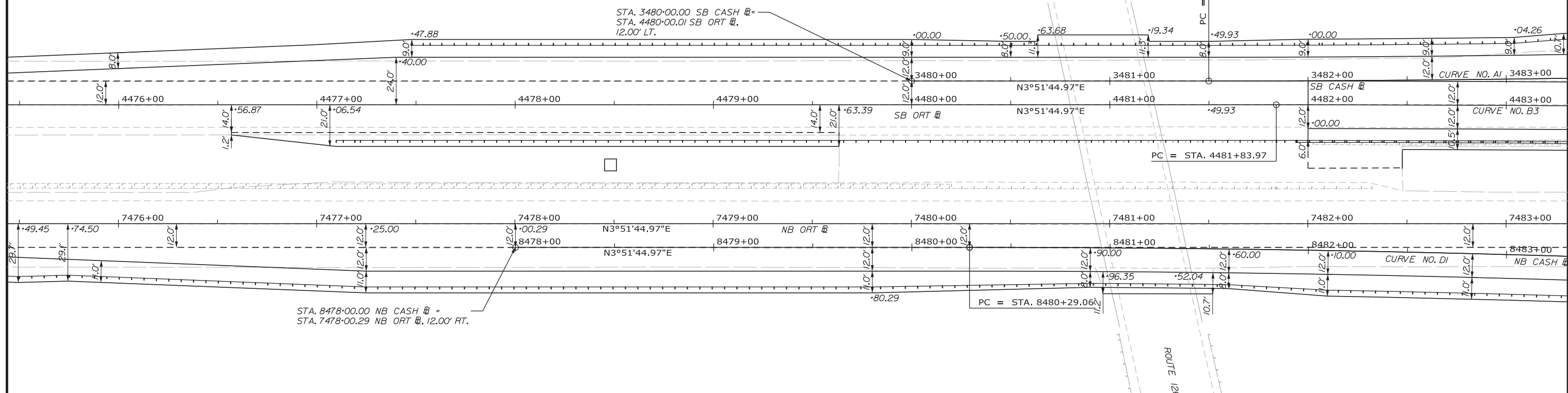
INTERCHANGE 103
ORT CONVERSION
GEOMETRIC PLAN 4

SHEET NUMBER: GM-04
152 OF 503

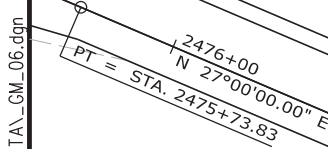
CONTRACT: 2019.04

Date: 3/20/2019

SB CASH											
CURVE NO.	POINT	STATION	DELTA	Dc	R	T	L	EXT.	N	E	BEARING
	POB	3480+00.00							501640.87	3042379.73	N 03° 51' 45.0" E
	PC	3481+49.93							501790.45	3042389.83	
A1	PI	3483+28.63	01° 45' 41.2" LEFT	00° 29' 34.3"	11625.00	178.71	357.39	1.37	501968.75	3042401.87	
	PRC	3485+07.31							502147.34	3042408.42	
A2	PI	3487+28.95	02° 40' 22.7" RIGHT	00° 36' 11.2"	9500.00	221.64	443.19	2.59	502368.83	3042416.54	
	PRC	3489+50.51							502589.70	3042434.99	
A3	PI	3491+25.26	0° 54' 41.5" LEFT	00° 15' 38.9"	21968.00	174.75	349.49	0.70	502763.84	3042449.53	
	PT	3493+00.00							502938.19	3042461.31	N 03° 51' 45.0" E
	PC	3505+12.25							504147.69	3042542.97	
A4	PI	3506+06.00	05° 57' 44.6" RIGHT	03° 10' 59.2"	1800.00	93.74	187.31	2.44	504241.22	3042549.28	
	PRC	3506+99.57							504333.59	3042565.28	
A5	PI	3508+00.25	05° 44' 06.4" LEFT	02° 51' 01.9"	2010.00	100.68	201.19	2.52	504432.79	3042582.46	
	PT	3509+00.76							504533.22	3042589.64	N 04° 05' 23.2" E
	POE	3509+50.00							504582.33	3042593.15	



NB CASH											
CURVE NO.	POINT	STATION	DELTA	Dc	R	T	L	EXT.	N	E	BEARING
	POB	8478+00.00							501435.83	3042450.08	N 03° 51' 45.0" E
	PC	8480+29.06							501664.37	3042465.51	
D1	PI	8482+48.47	02° 29' 20.3" RIGHT	00° 34' 02.2"	10100.00	219.41	438.75	2.38	501883.28	3042480.29	
	PRC	8484+67.81							502101.34	3042504.56	
D2	PI	8487+08.94	02° 29' 20.3" LEFT	00° 30' 58.2"	11100.00	241.13	482.19	2.62	502340.99	3042531.24	
	PT	8489+50.00							502581.58	3042547.48	N 03° 51' 45.0" E
	PC	8501+49.18							503778.03	3042628.26	
D3	PI	8507+26.28	19° 59' 14.5" RIGHT	01° 44' 58.2"	3275.00	577.10	1142.47	50.46	504353.82	3042667.13	
	PT	8512+91.65							504881.64	3042900.48	N 23° 50' 59.4" E



Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

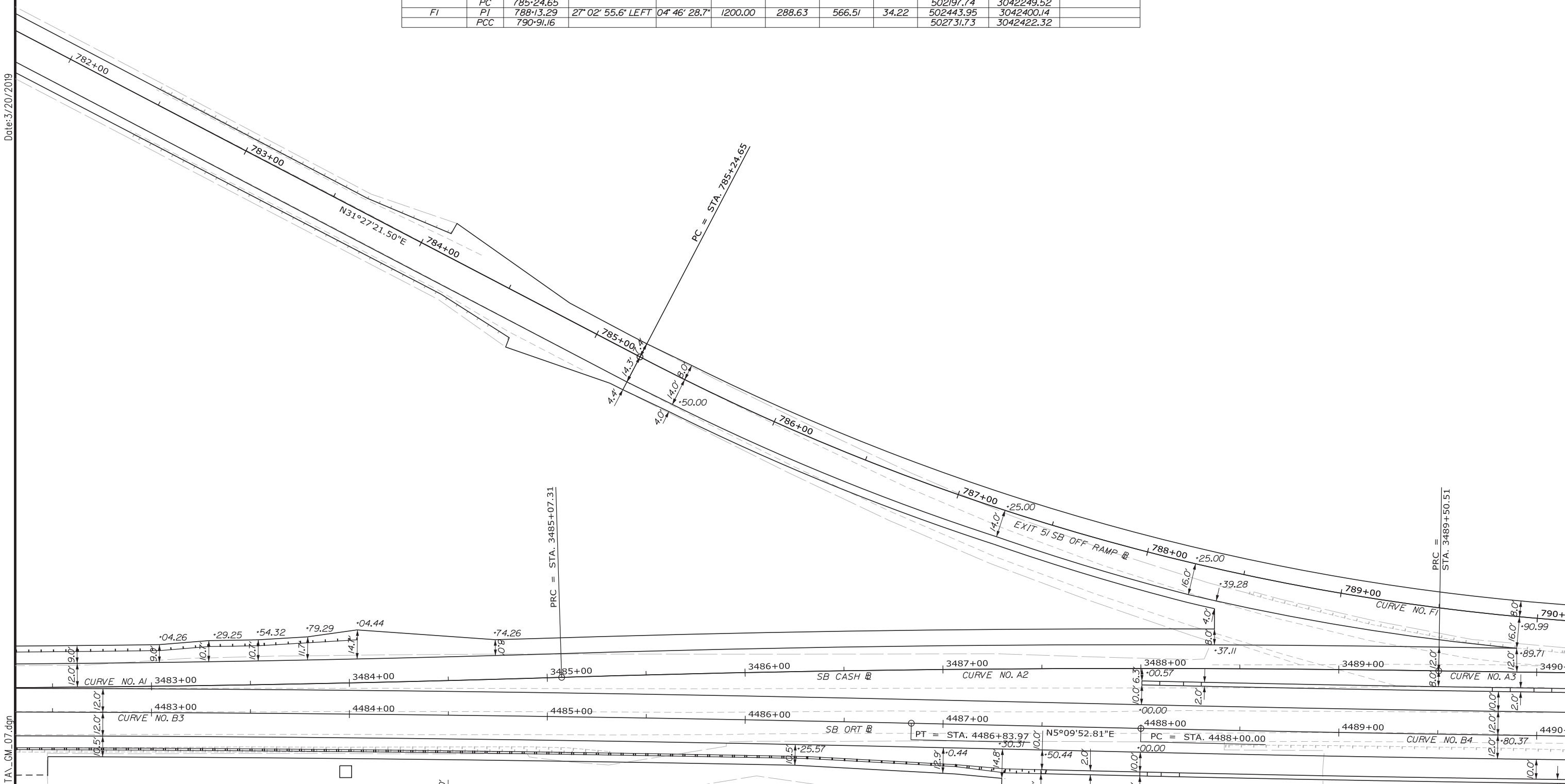
GEOMETRIC PLAN 5

SHEET NUMBER: GM-05
153 OF 503

CONTRACT: 2019.04



EXIT 51 SB OFF RAMP #											
CURVE NO.	POINT	STATION	DELTA	Dc	R	T	L	EXT.	N	E	BEARING
	POB	779+03.34							501667.73	3041925.29	N 31° 27' 21.5" E
	PC	785+24.65							502197.74	3042249.52	
FI	PI	788+13.29	27° 02' 55.6" LEFT	04° 46' 28.7"	1200.00	288.63	566.51	34.22	502443.95	3042400.14	
	PCC	790+91.16							502731.73	3042422.32	



Date: 3/20/2019

Filename: ...00.HIGHWAY\MSTA_GM_07.dgn

Scale: Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
Designed	By	Date	Checked
Drawn	By	Date	In Charge of
	PLP	3\20\19	LEM
	EJB	3\20\19	GAE

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

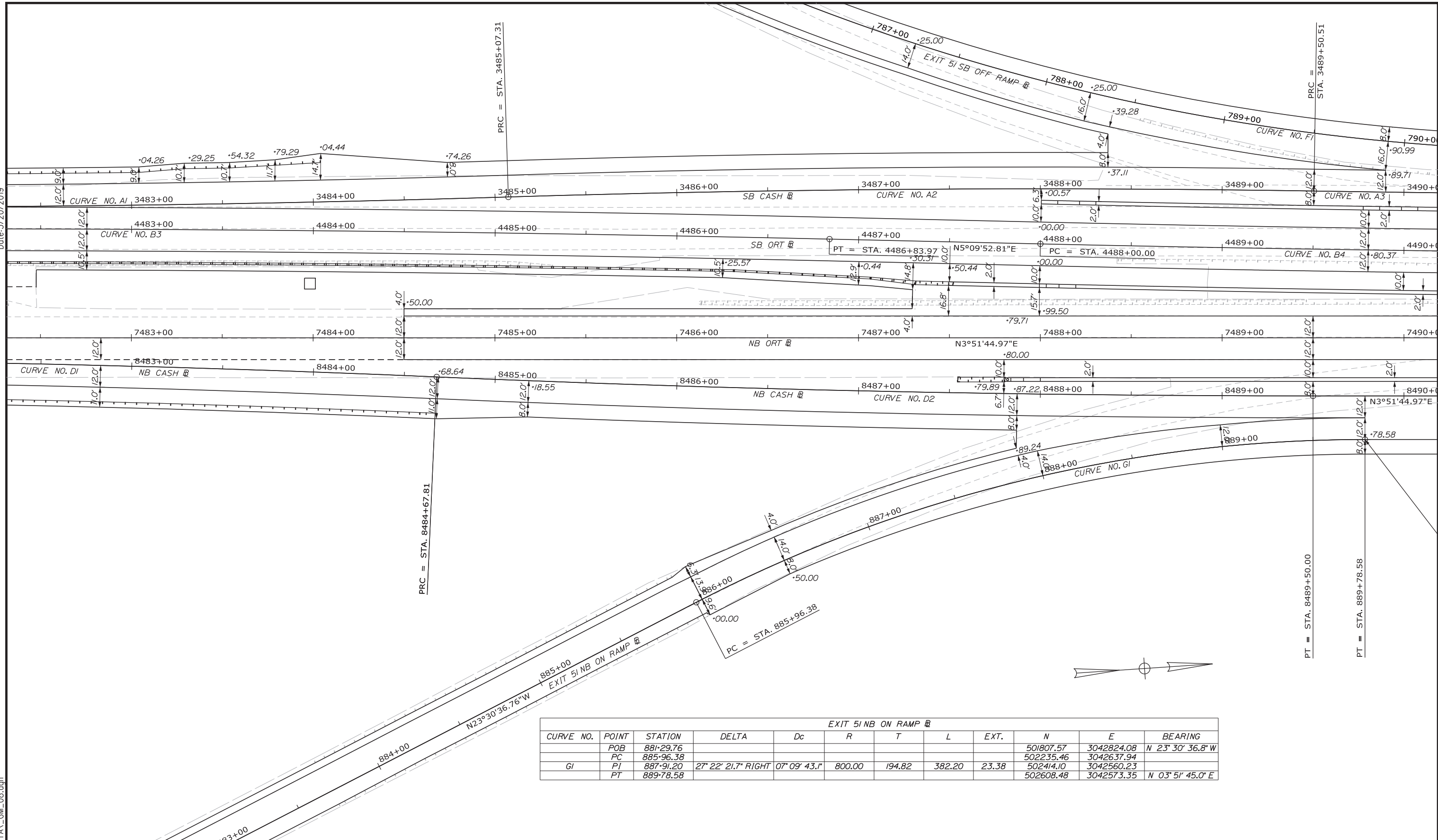
INTERCHANGE 103
 ORT CONVERSION
 GEOMETRIC PLAN 6

SHEET NUMBER: GM-06
 154 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...\\00\HIGHWAY\MSTA\GM_08.dgn



EXIT 5I NB ON RAMP											
CURVE NO.	POINT	STATION	DELTA	Dc	R	T	L	EXT.	N	E	BEARING
	POB	881+29.76							501807.57	3042824.08	N 23° 30' 36.8" W
	PC	885+96.38							502235.46	3042637.94	
G1	PJ	887+91.20	27° 22' 21.7" RIGHT	07° 09' 43.1"	800.00	194.82	382.20	23.38	502414.10	3042560.23	
	PT	889+78.58							502608.48	3042573.35	N 03° 51' 45.0" E

Scale: 1" = 25'

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

By	Date	By	Date
Designed	PLP 3\20\19	Checked	LEM 3\20\19
Drawn	EJB 3\20\19	In Charge of	GAE 3\20\19

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**THE GOLD STAR
 MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

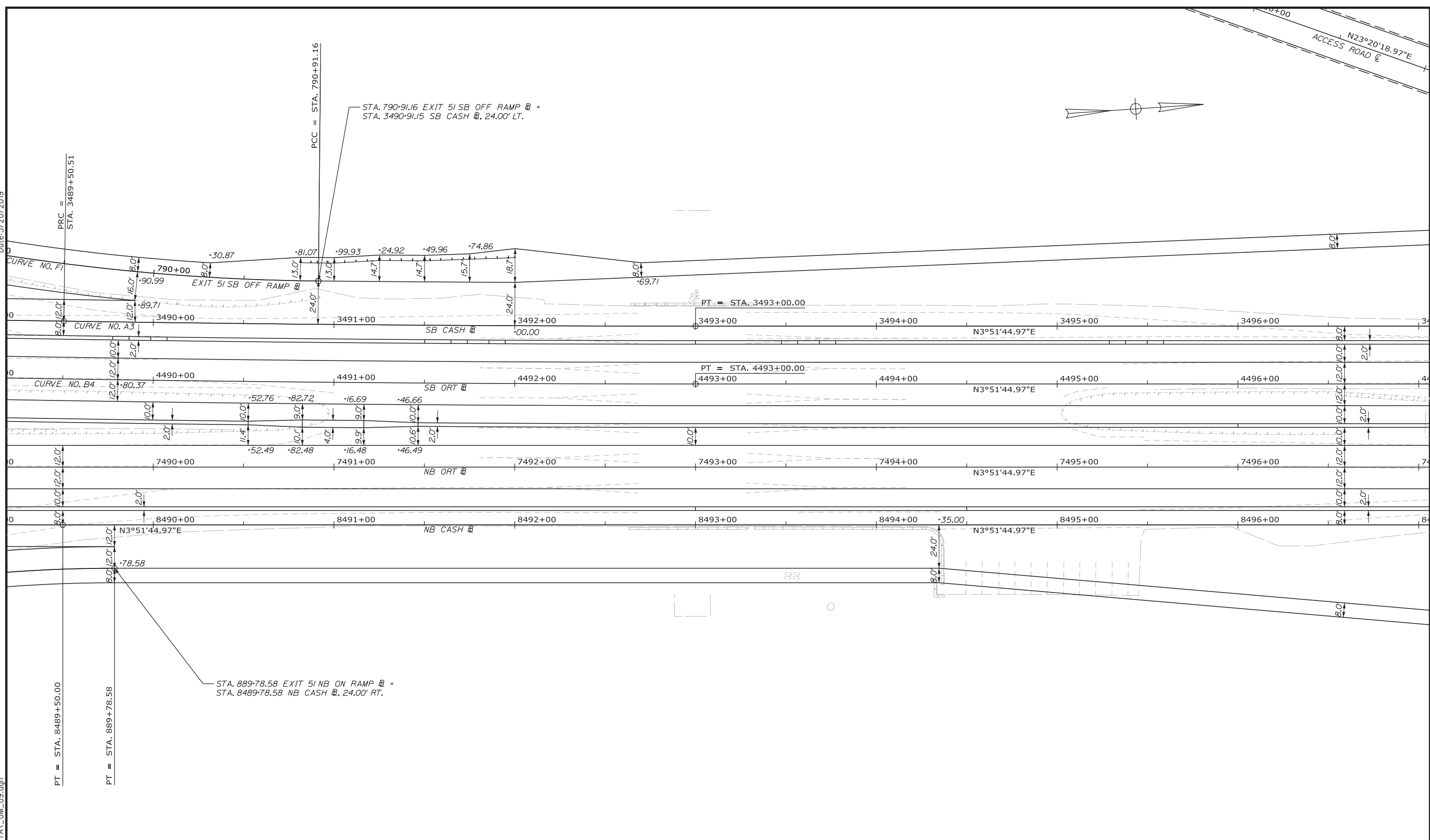
INTERCHANGE 103
 ORT CONVERSION
 GEOMETRIC PLAN 7

SHEET NUMBER: GM-07
 155 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\GM_09.dgn



Scale: 1" = 25'

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

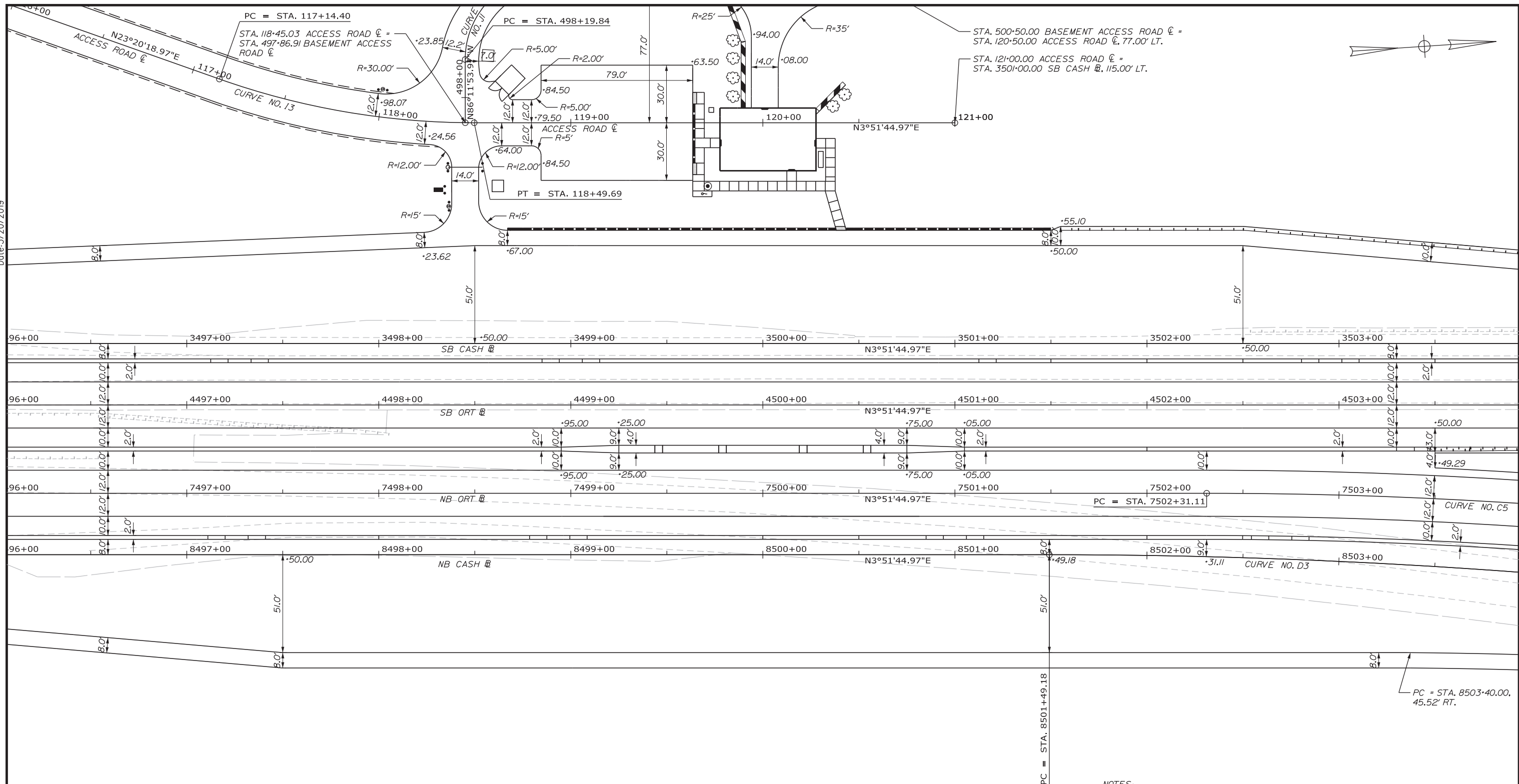
INTERCHANGE 103
 ORT CONVERSION
 GEOMETRIC PLAN 8

SHEET NUMBER: GM-08
 156 OF 503

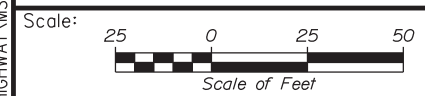
CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\GM_10.dgn



NOTES:
1. PLAZA AND TUNNEL NOT SHOWN.



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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MAINE TURNPIKE

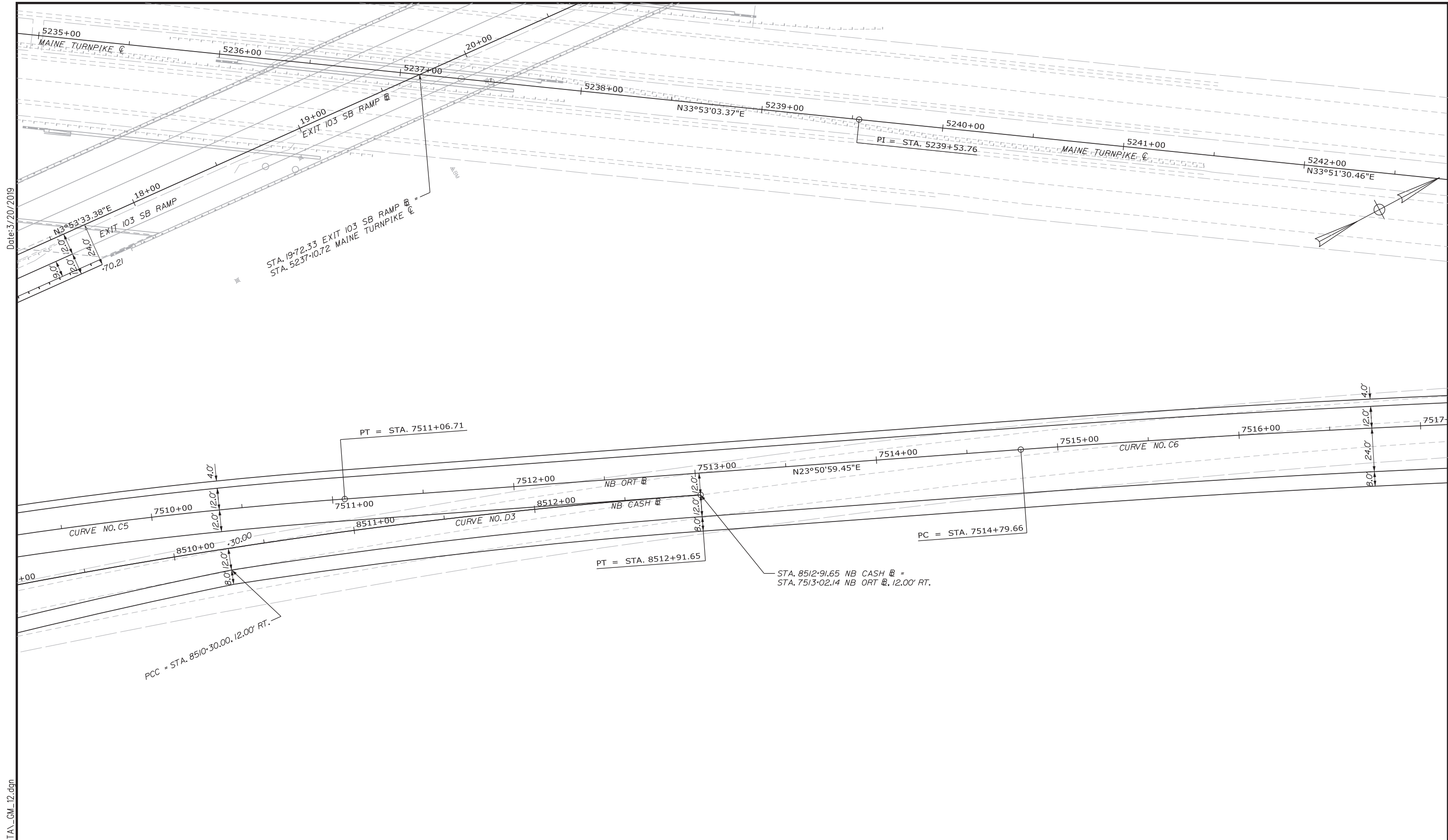
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
GEOMETRIC PLAN 9

SHEET NUMBER: GM-09
157 OF 503

CONTRACT: 2019.04



Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\GM_12.dgn

Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

STANTEC CONSULTING SERVICES INC.
482 PAYNE ROAD
SCARBOROUGH, ME 04074
TEL (207) 887-3448
FAX (207) 883-3376

MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

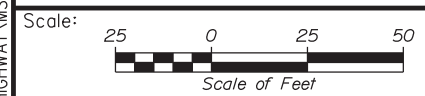
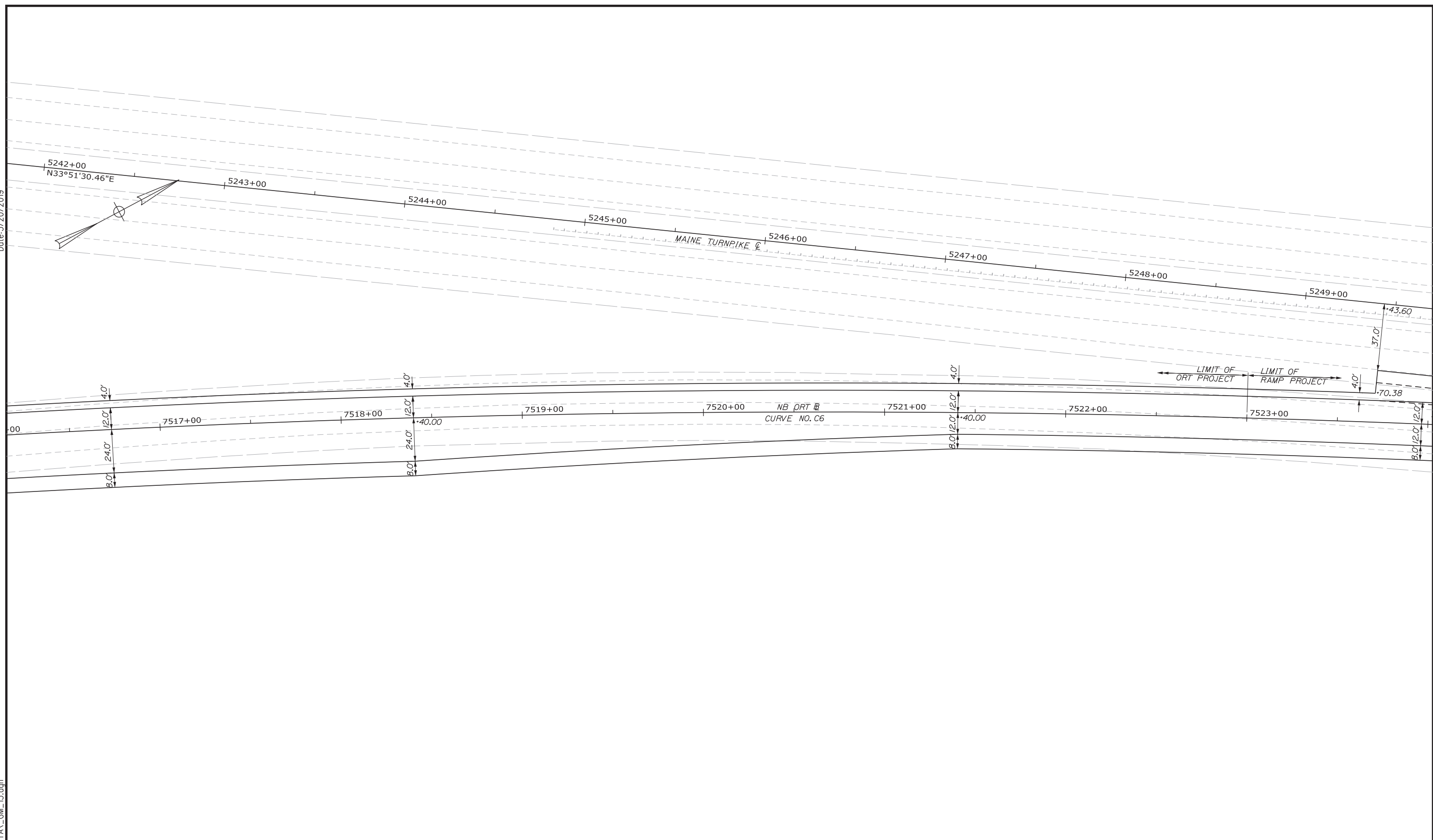
GEOMETRIC PLAN 11

SHEET NUMBER: GM-11
159 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\...GM_13.dgn



Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
MEMORIAL HIGHWAY**

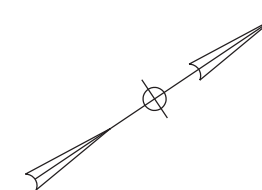
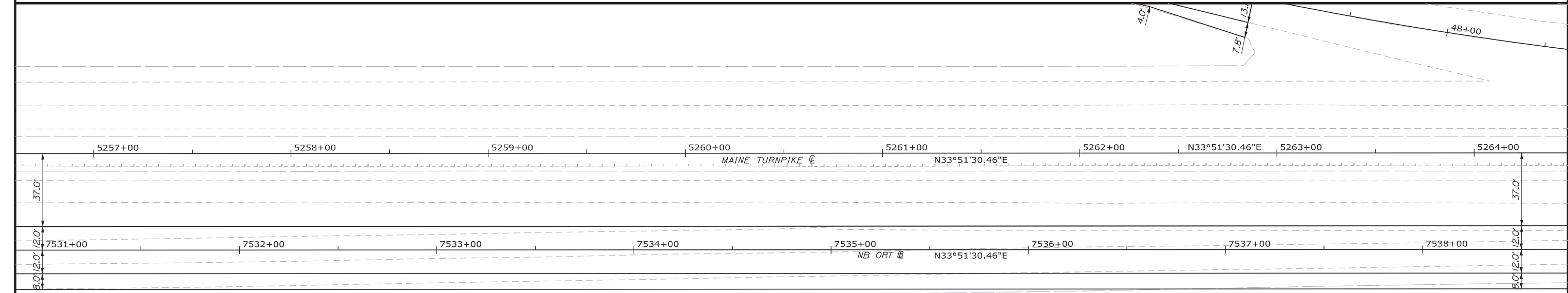
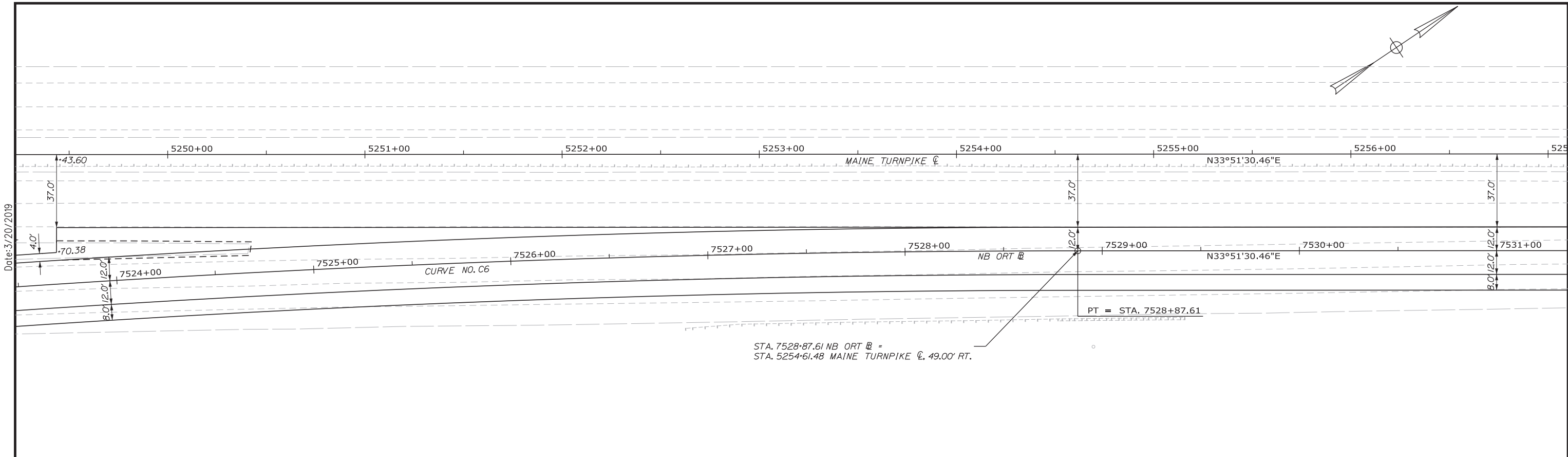
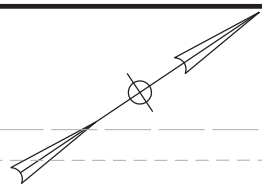
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

GEOMETRIC PLAN 12

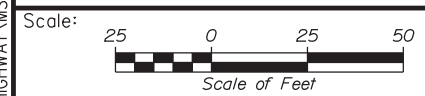
SHEET NUMBER: GM-12
160 OF 503

CONTRACT: 2019.04



Date: 3/20/2019

Filename: ... \00\HIGHWAY\MSTA\GM_14.dgn



Designed by:

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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

By	Date	By	Date
Designed PLP	3\20\19	Checked LEM	3\20\19
Drawn EJB	3\20\19	In Charge of GAE	3\20\19

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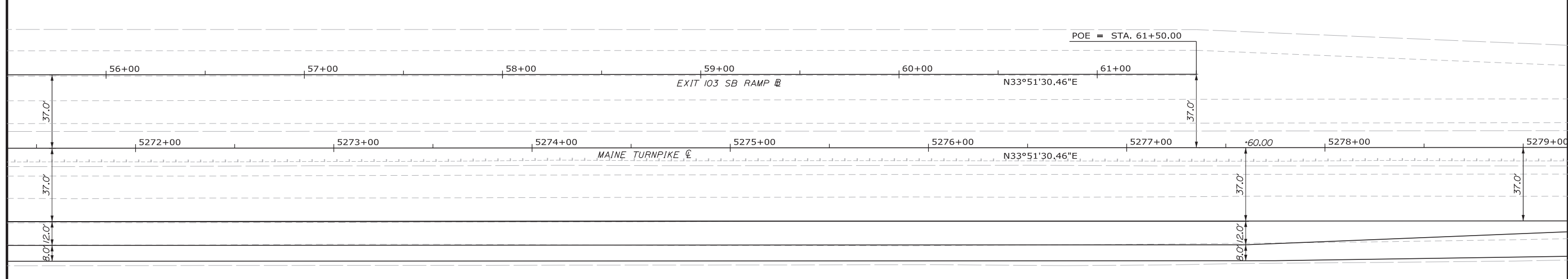
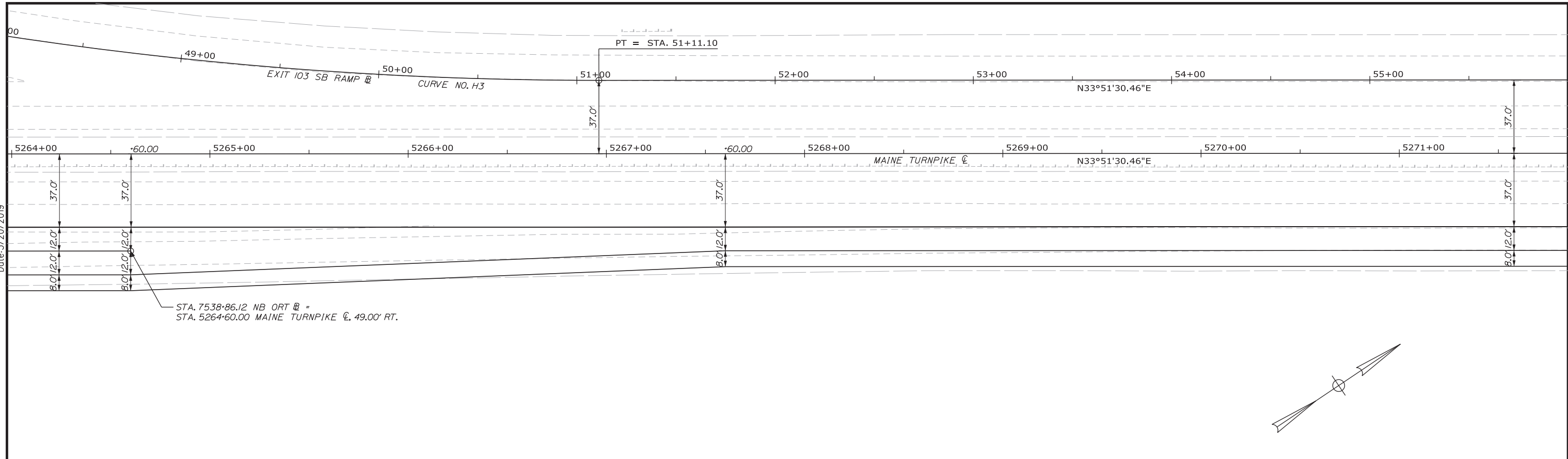
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
GEOMETRIC PLAN 13

SHEET NUMBER: GM-13
161 OF 503

CONTRACT: 2019.04

Date: 3/20/2019



Filename: ...:\00\HIGHWAY\MSTA\..._GM_15.dgn

Scale: Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

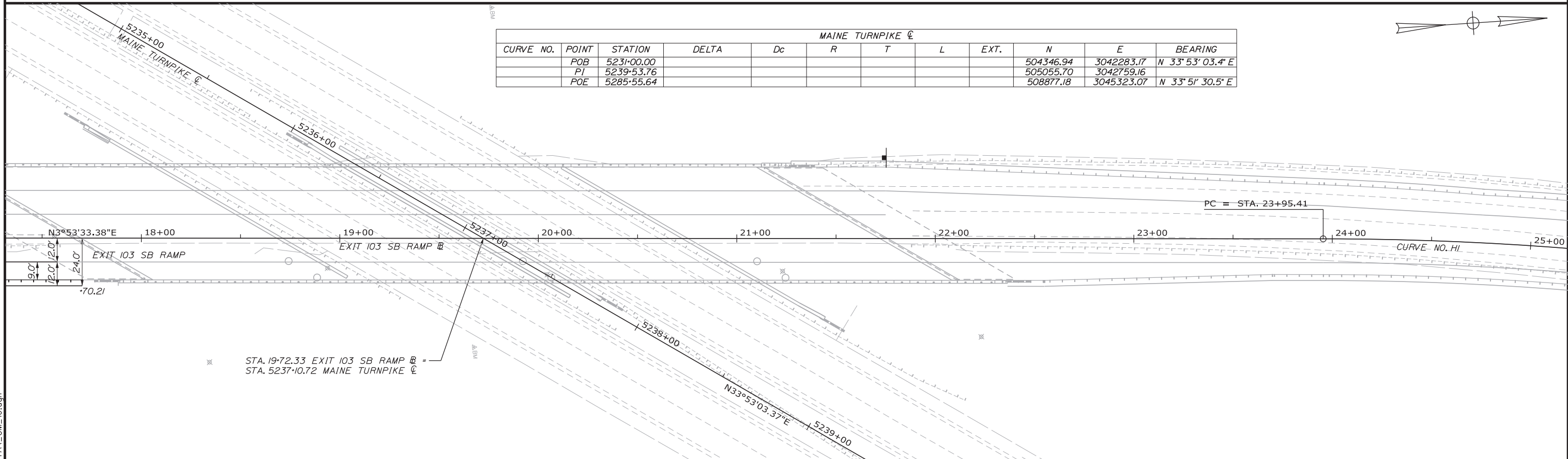
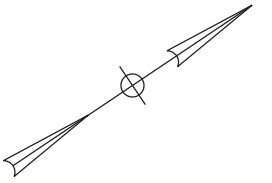
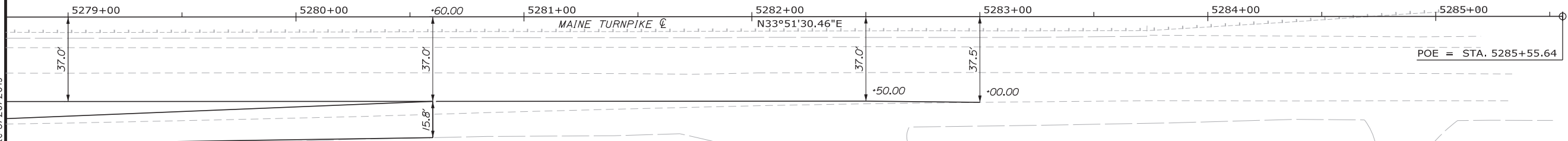
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 GEOMETRIC PLAN 14

SHEET NUMBER: GM-14
 162 OF 503

CONTRACT: 2019.04

Date: 3/20/2019



MAINE TURNPIKE C												
CURVE NO.	POINT	STATION	DELTA	Dc	R	T	L	EXT.	N	E	BEARING	
	POB	5231+00.00							504346.94	3042283.17	N 33° 53' 03.4\"/>	
	PI	5239+53.76							505055.70	3042759.16	N 33° 51' 30.5\"/>	
	POE	5285+55.64							508877.18	3045323.07	N 33° 51' 30.5\"/>	

Filename: ...00\HIGHWAY\MSTA\GM_16.dgn

Scale: Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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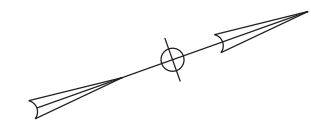
**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

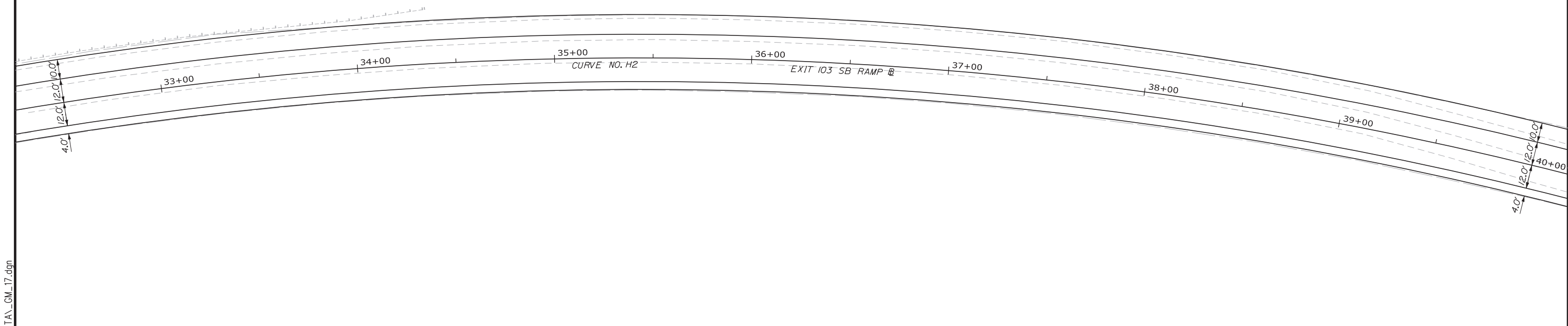
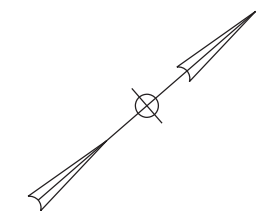
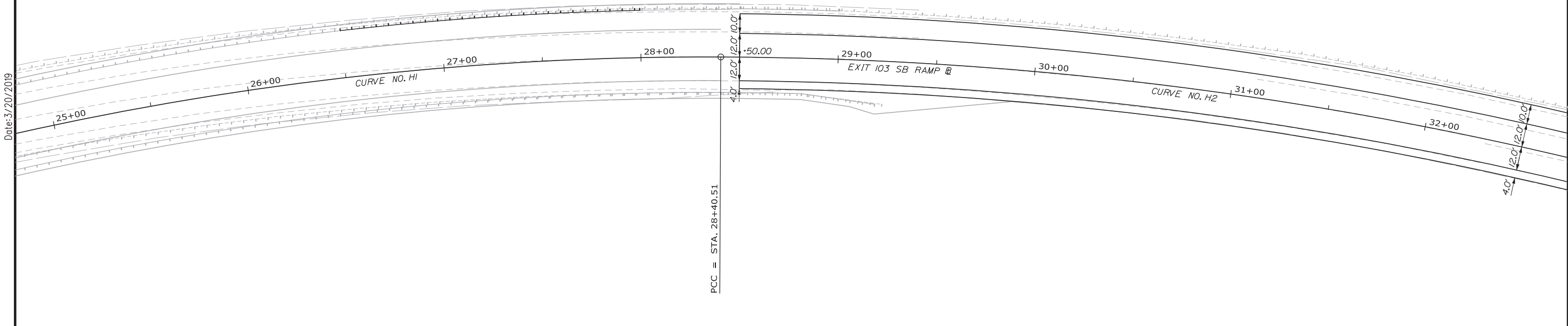
INTERCHANGE 103
ORT CONVERSION
GEOMETRIC PLAN 15

SHEET NUMBER: GM-15
163 OF 503

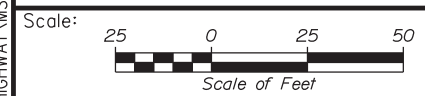
CONTRACT: 2019.04



Date: 3/20/2019



Filename: ...:\00\HIGHWAY\MSTA\GM_17.dgn



Designed by:

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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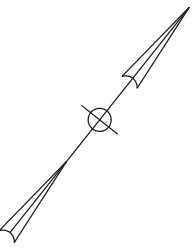
**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

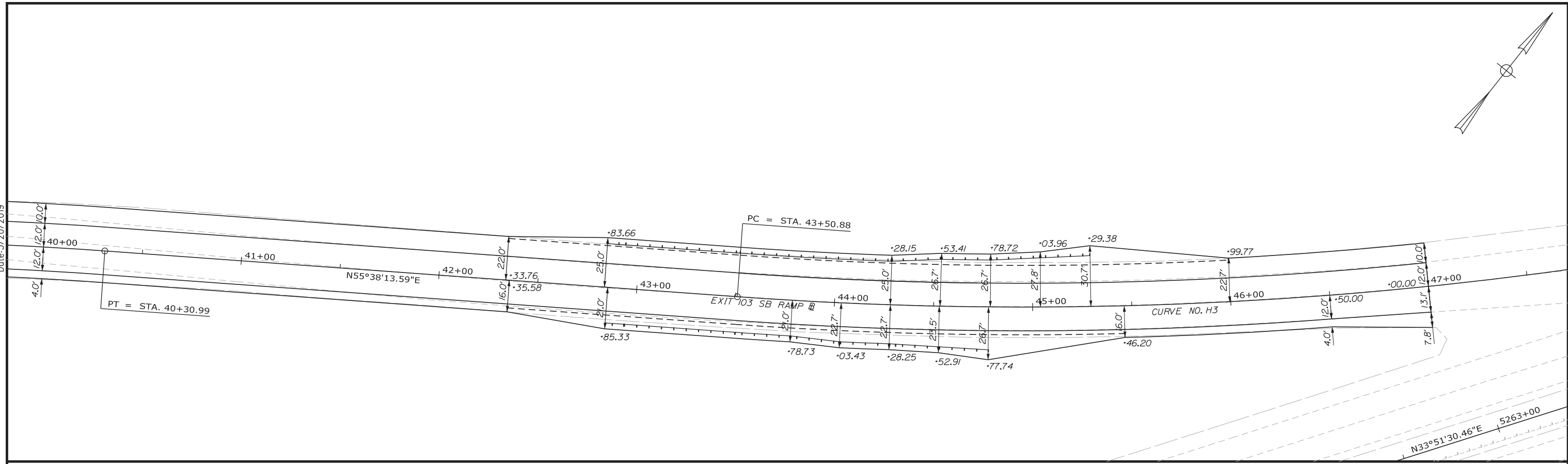
INTERCHANGE 103
ORT CONVERSION
GEOMETRIC PLAN 16

SHEET NUMBER: GM-16
164 OF 503

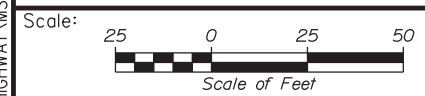
CONTRACT: 2019.04



Date: 3/20/2019



Filename: ...\\00\HIGHWAY\MSTA\GM_18.dgn



Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

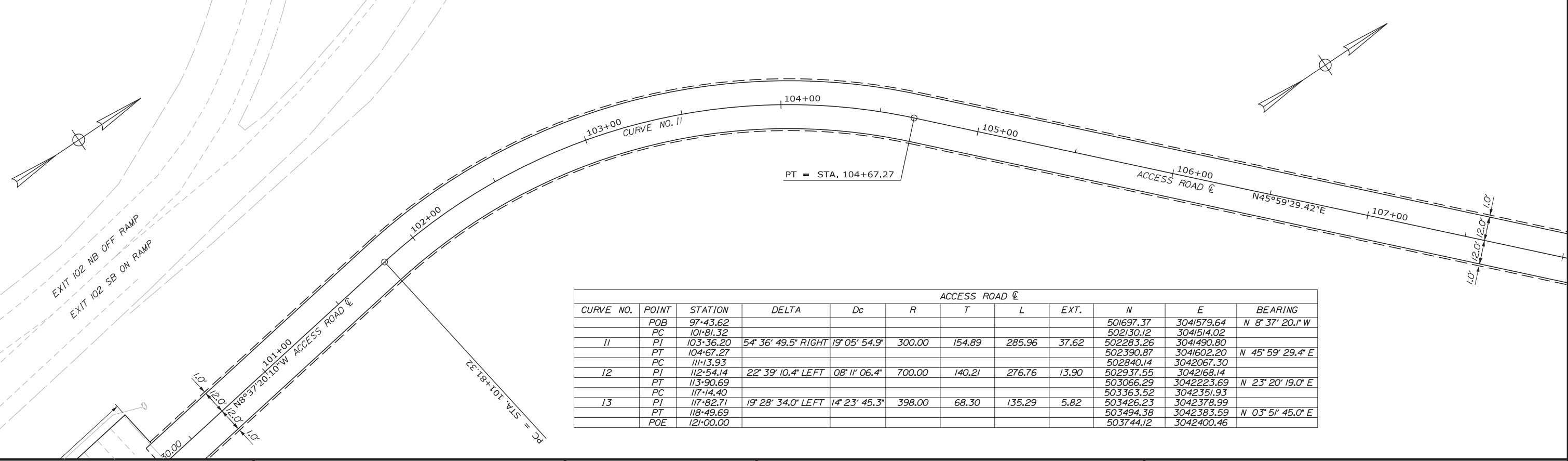
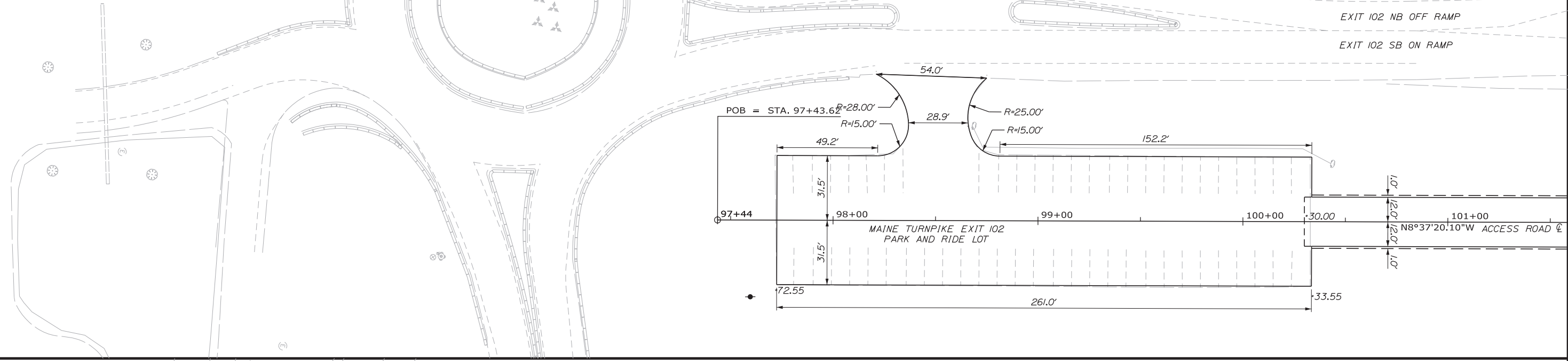
INTERCHANGE 103
ORT CONVERSION

GEOMETRIC PLAN 17

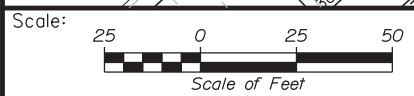
SHEET NUMBER: GM-17
CONTRACT: 2019.04
165 OF 503

Date: 3/20/2019

Filename: ...00.HIGHWAY.MSTA_GM_19.dgn



ACCESS ROAD											
CURVE NO.	POINT	STATION	DELTA	Dc	R	T	L	EXT.	N	E	BEARING
	POB	97+43.62							501697.37	3041579.64	N 8° 37' 20.1\"/>
	PC	101+81.32							502130.12	3041514.02	
11	PI	103+36.20	54° 36' 49.5\"/>								
	PT	104+67.27							502283.26	3041490.80	
	PC	111+13.93							502390.87	3041602.20	N 45° 59' 29.4\"/>
12	PI	112+54.14	22° 39' 10.4\"/>								
	PT	113+90.69							502840.14	3042067.30	
	PC	117+14.40							502937.55	3042168.14	N 23° 20' 19.0\"/>
13	PI	117+82.71	19° 28' 34.0\"/>								
	PT	118+49.69							503066.29	3042223.69	
	PC	121+00.00							503363.52	3042351.93	
	POE	121+00.00							503426.23	3042378.99	N 03° 51' 45.0\"/>
									503494.38	3042383.59	
									503744.12	3042400.46	



Designed by:

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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
By	Date	By	Date
Designed	PLP 3\20\19	Checked	LEM 3\20\19
Drawn	EJB 3\20\19	In Charge of	GAE 3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

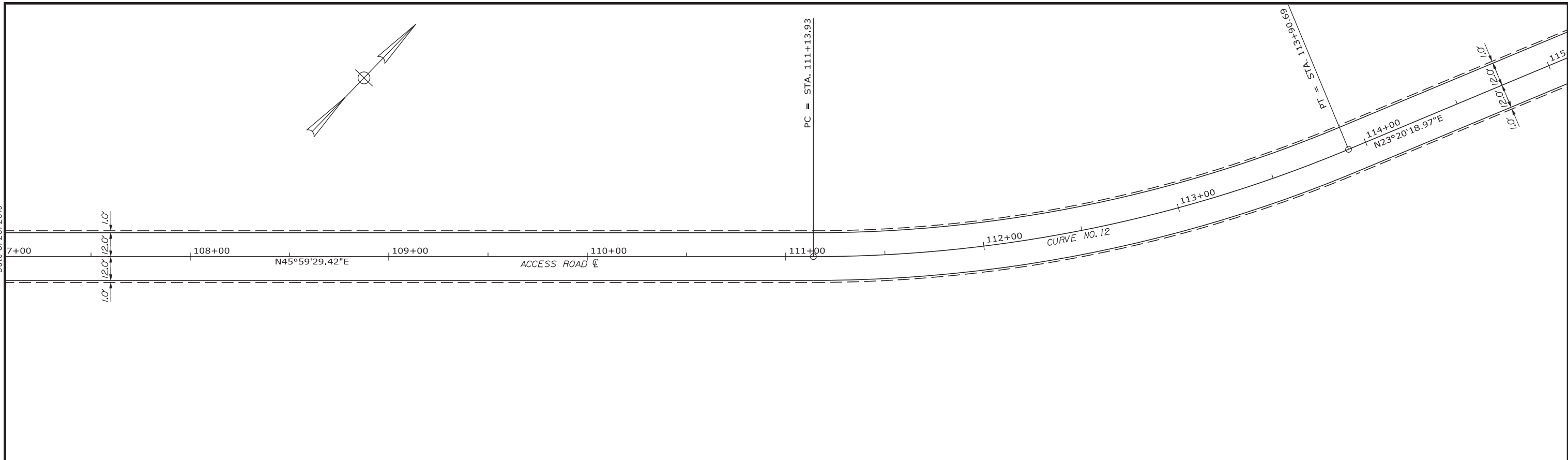
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 GEOMETRIC PLAN 18

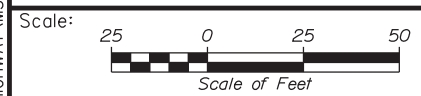
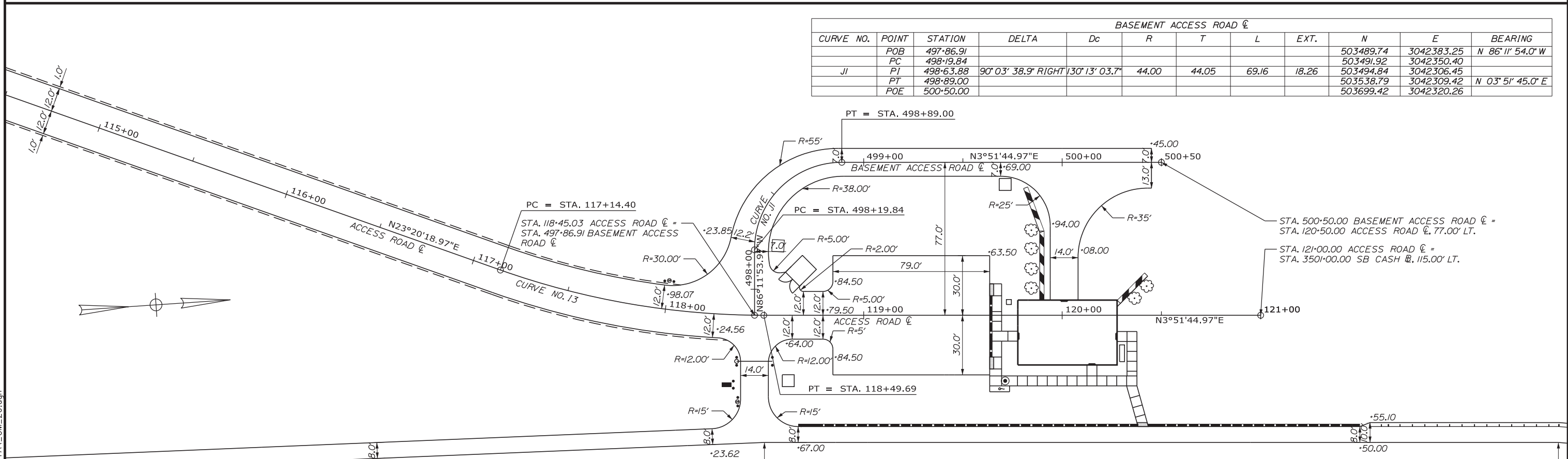
SHEET NUMBER: GM-18
 166 OF 503

CONTRACT: 2019.04

Date: 3/20/2019



BASEMENT ACCESS ROAD											
CURVE NO.	POINT	STATION	DELTA	Dc	R	T	L	EXT.	N	E	BEARING
JI	POB	497-86.91							503489.74	3042383.25	N 86° 11' 54.0" W
	PC	498-19.84							503491.92	3042350.40	
	PI	498-63.88	90° 03' 38.9" RIGHT	130' 13" 03.7"	44.00	44.05	69.16	18.26	503494.84	3042306.45	
	PT	498-89.00							503538.79	3042309.42	N 03° 51' 45.0" E
	POE	500-50.00							503699.42	3042320.26	



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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

No.	Revision	By	Date

By	Date	Checked	By	Date
PLP	3\20\19	LEM	3\20\19	
EJB	3\20\19	GAE	3\20\19	

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

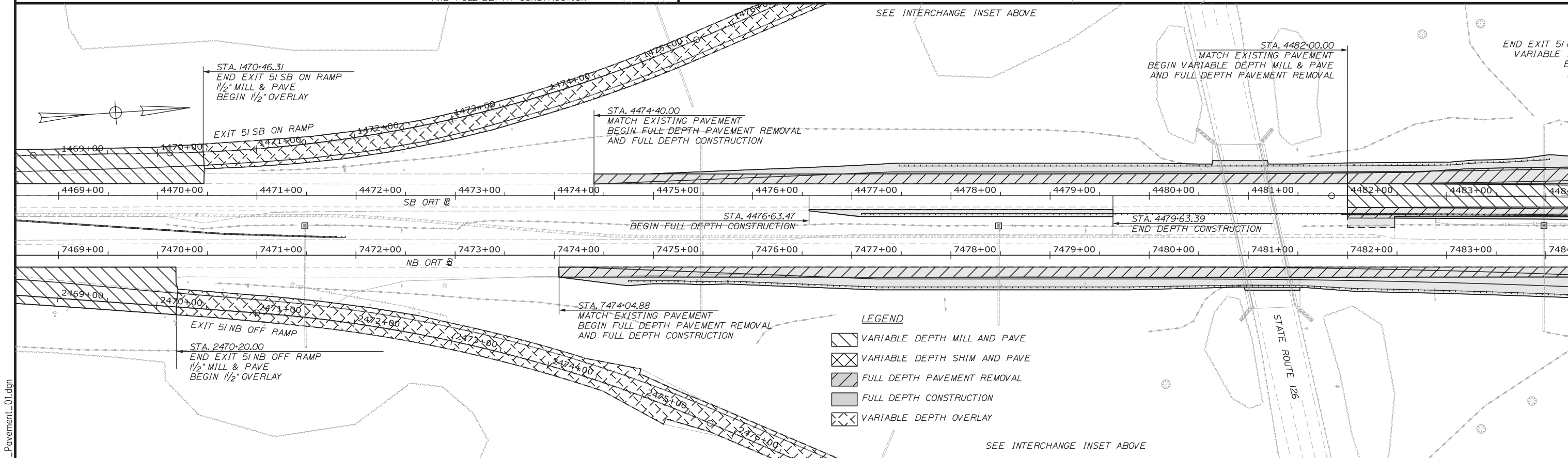
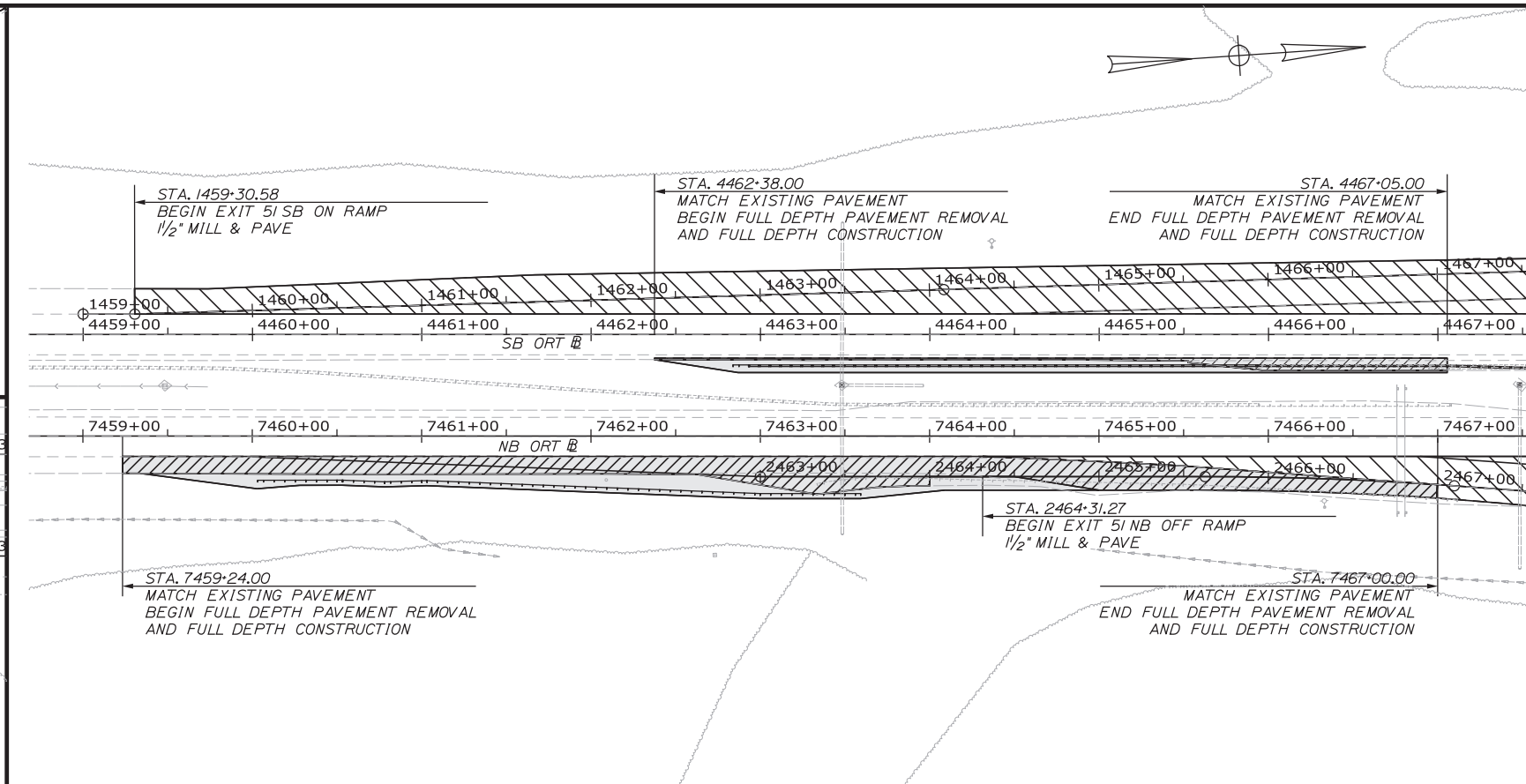
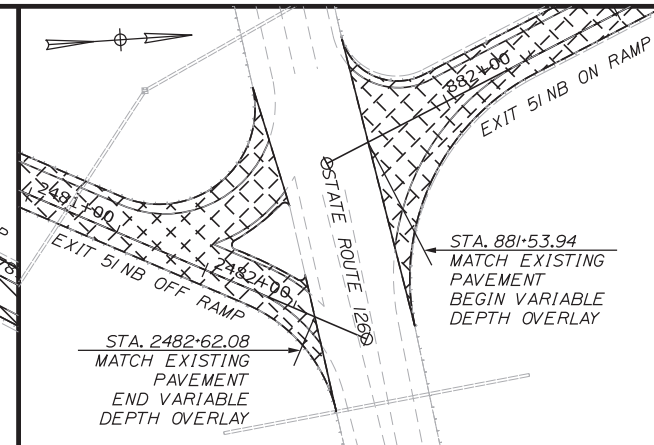
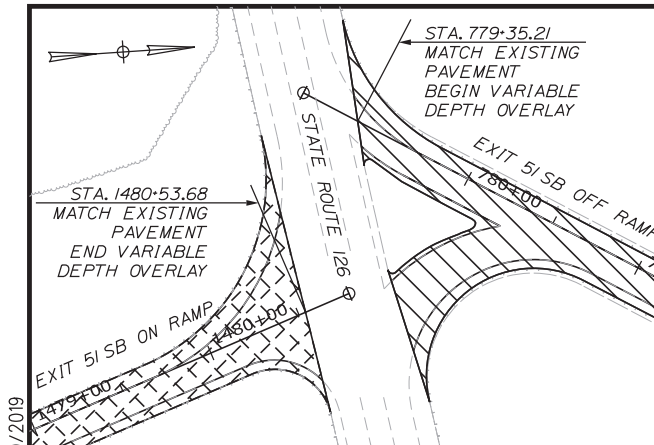
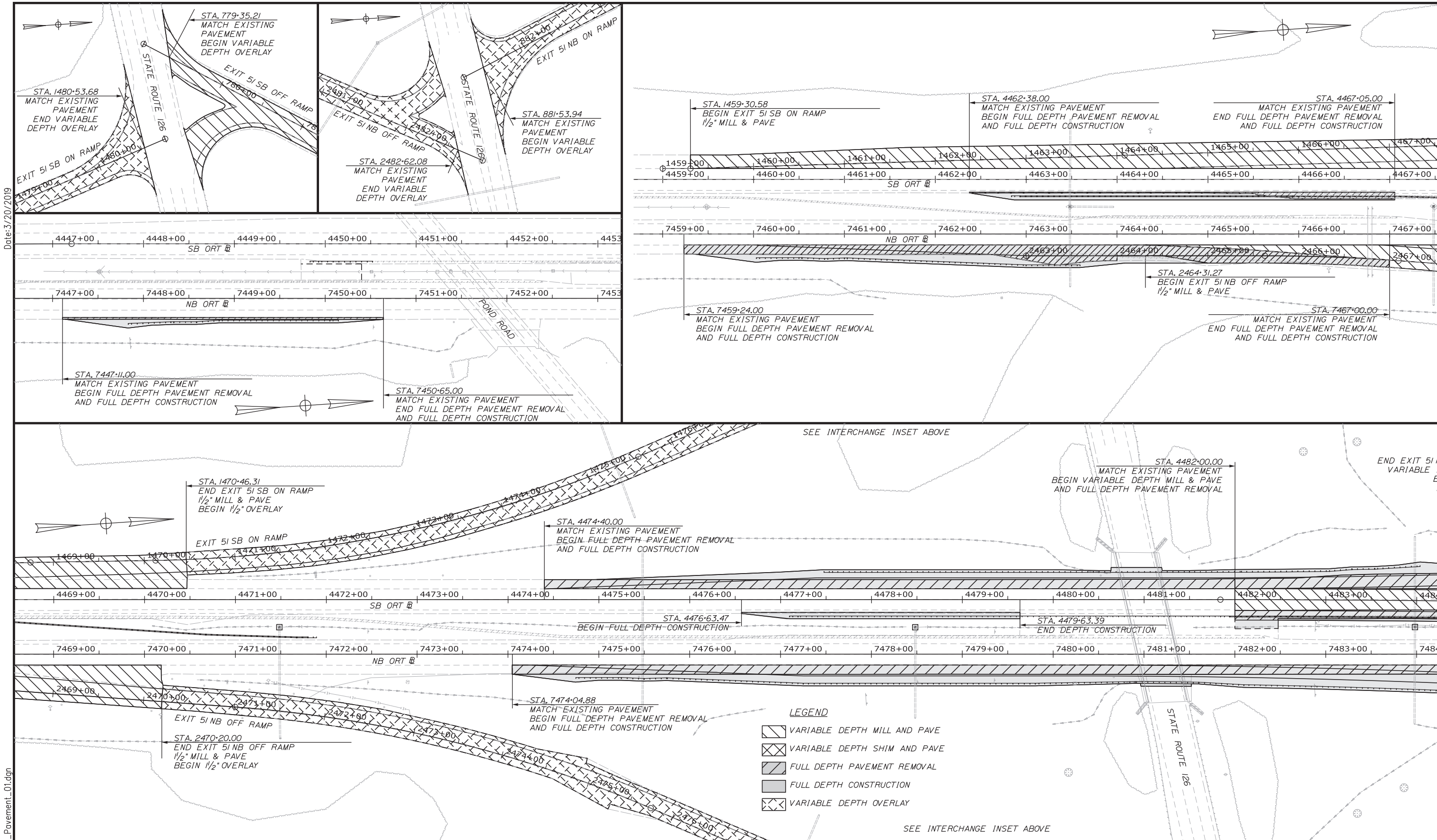
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 GEOMETRIC PLAN 19

SHEET NUMBER: GM-19
 167 OF 503

CONTRACT: 2019.04

Filename: ...:\00\HIGHWAY\MSTA\GM_20.dgn



Scale: 50 0 50 100
Scale of Feet

No.	Revision	By	Date

Designed by: **Stantec**

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date	By	Date	
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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FAX (207) 883-3376

MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

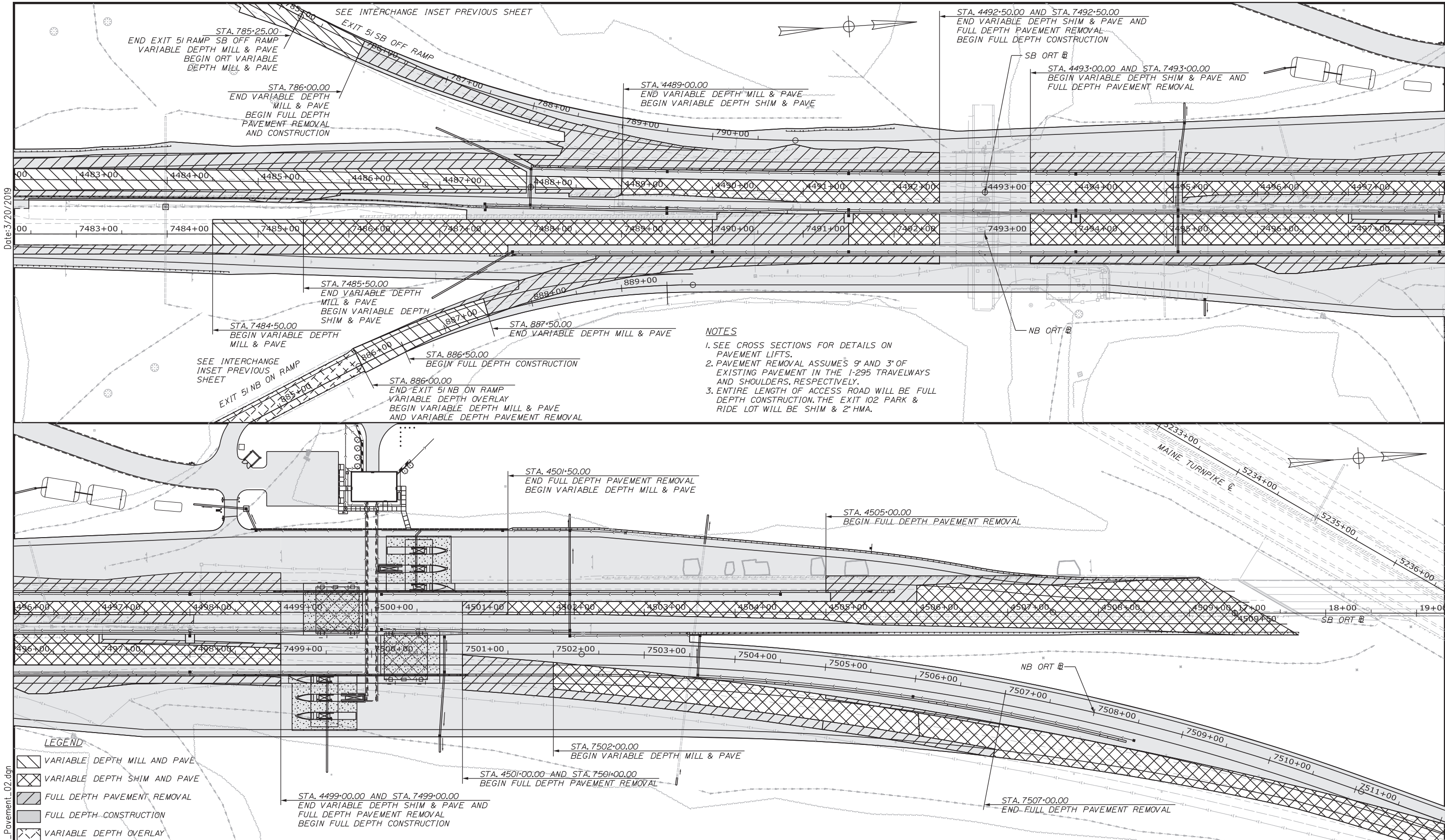
INTERCHANGE 103 ORT CONVERSION

PAVEMENT PLAN 1

SHEET NUMBER: PP-01
CONTRACT: 2019.04
168 OF 503

Date: 3/20/2019
Filename: ...HIGHWAY.MSTA_Pavement_01.dgn

Date: 3/20/2019



Filename: ...HIGHWAY\MSTA_Pavement_02.dgn



No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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FAX (207) 883-3376

MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

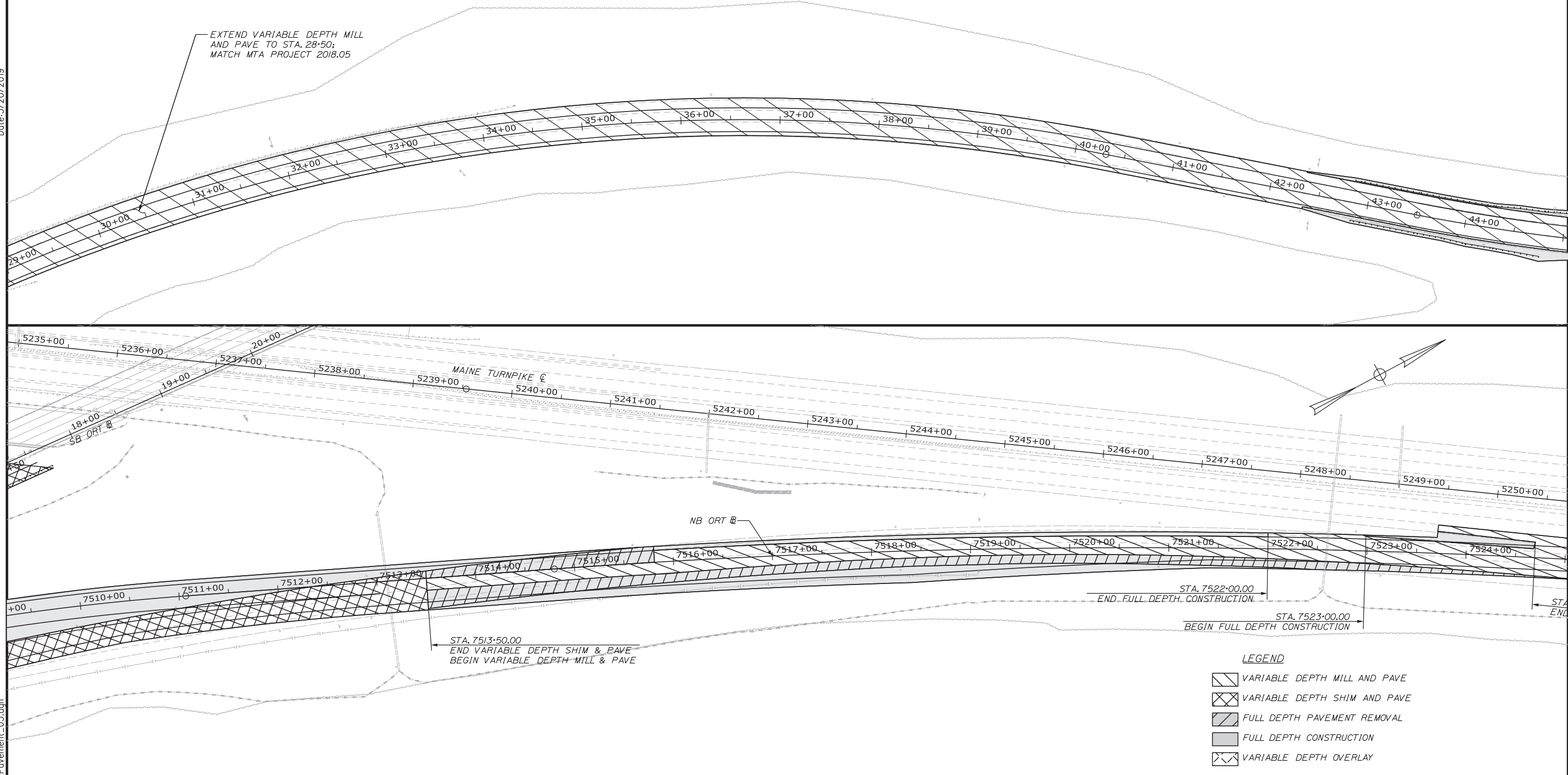
PAVEMENT PLAN 2

SHEET NUMBER: PP-02
169 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...HIGHWAY\MSTA\Pavement_03.dgn



LEGEND

- VARIABLE DEPTH MILL AND PAVE
- VARIABLE DEPTH SHIM AND PAVE
- FULL DEPTH PAVEMENT REMOVAL
- FULL DEPTH CONSTRUCTION
- VARIABLE DEPTH OVERLAY

Scale: Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date	Checked	By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

STANTEC CONSULTING SERVICES INC.
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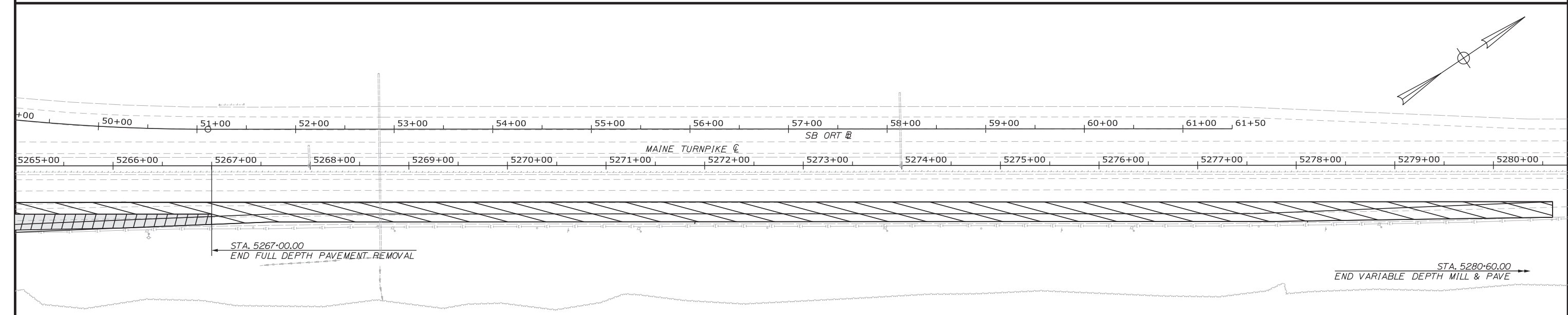
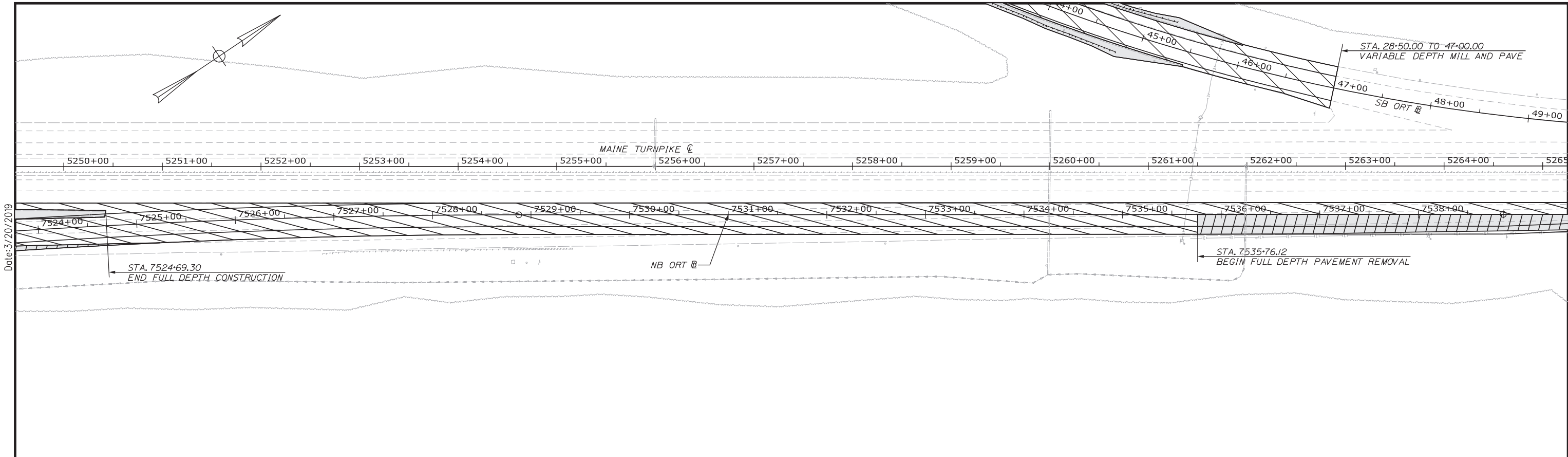
**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
PAVEMENT PLAN 3

SHEET NUMBER: PP-03
170 OF 503

CONTRACT: 2019.04



- LEGEND**
- VARIABLE DEPTH MILL AND PAVE
 - VARIABLE DEPTH SHIM AND PAVE
 - FULL DEPTH PAVEMENT REMOVAL
 - FULL DEPTH CONSTRUCTION
 - VARIABLE DEPTH OVERLAY

Scale: Scale of Feet

No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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 FAX (207) 883-3376



THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

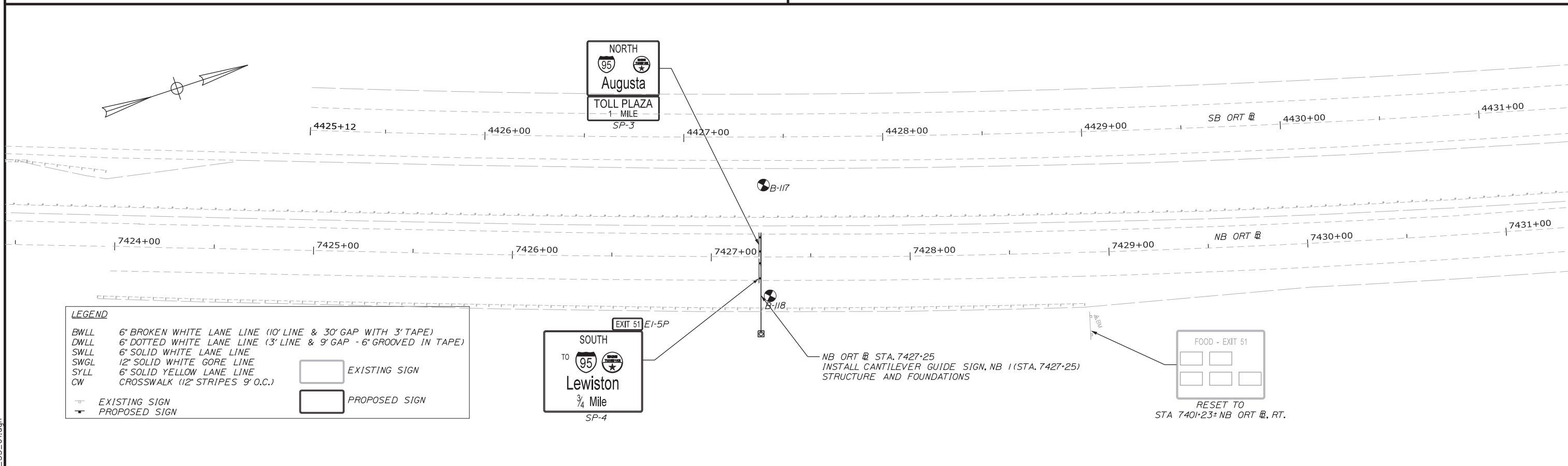
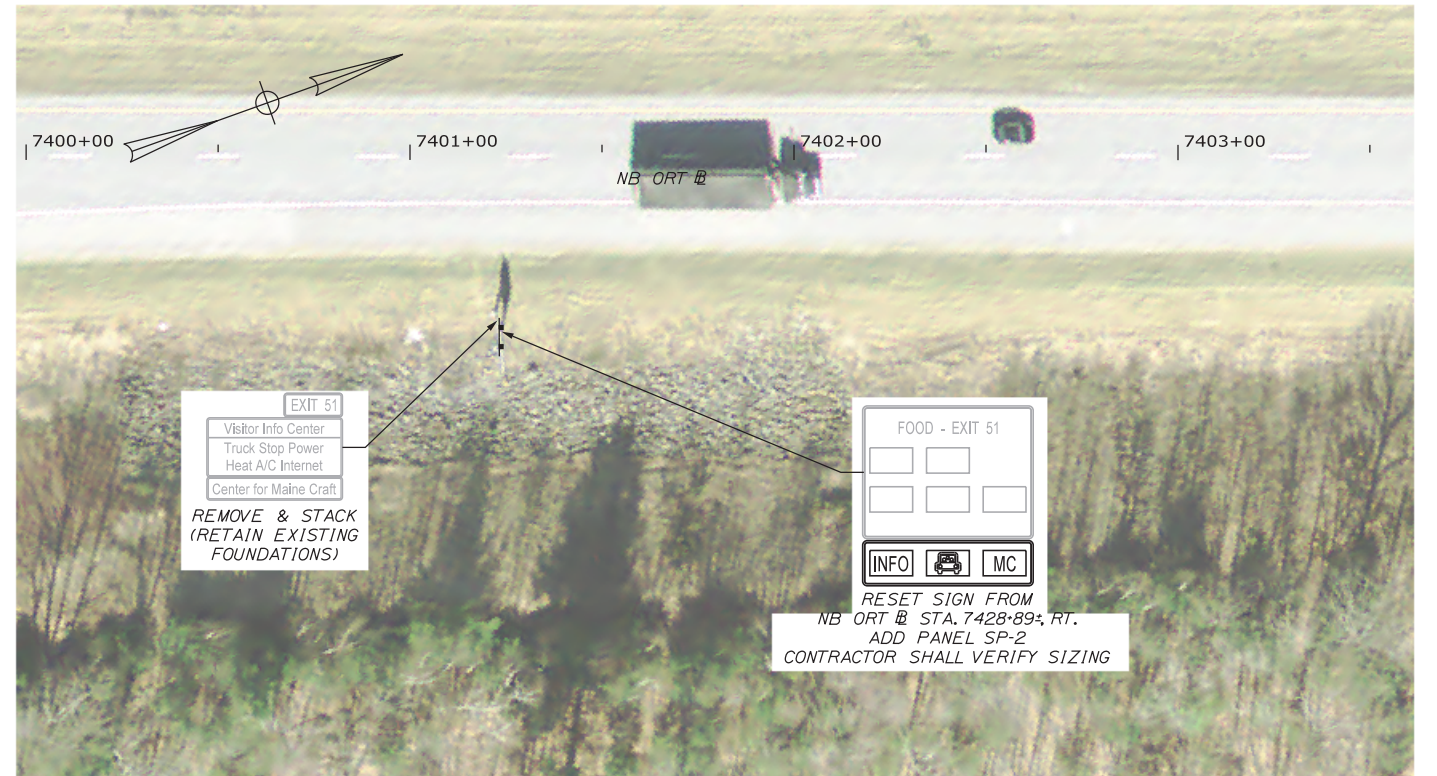
PAVEMENT PLAN 4

SHEET NUMBER: PP-04
171 OF 503

CONTRACT: 2019.04

Date: 3/20/2019
Filename: ...HIGHWAY\MSTA\Pavement_04.dgn

Date: 2/12/2019



LEGEND

BWLL	6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)		EXISTING SIGN
DWLL	6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)		PROPOSED SIGN
SWLL	6" SOLID WHITE LANE LINE		
SWGL	12" SOLID WHITE GORE LINE		
SYLL	6" SOLID YELLOW LANE LINE		
CW	CROSSWALK (12" STRIPES 9' O.C.)		

Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE

	By	Date	Checked	By	Date
Designed	BMH	02\15\19		DGE	02\15\19
Drawn	MPC	02\15\19	In Charge of	GAE	02\15\19

GORRILL PALMER
707 SABLE OAKS DRIVE, SUITE 30
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www.gorrillpalmer.com
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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH C. NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

SIGNING AND STRIPING PLAN 1

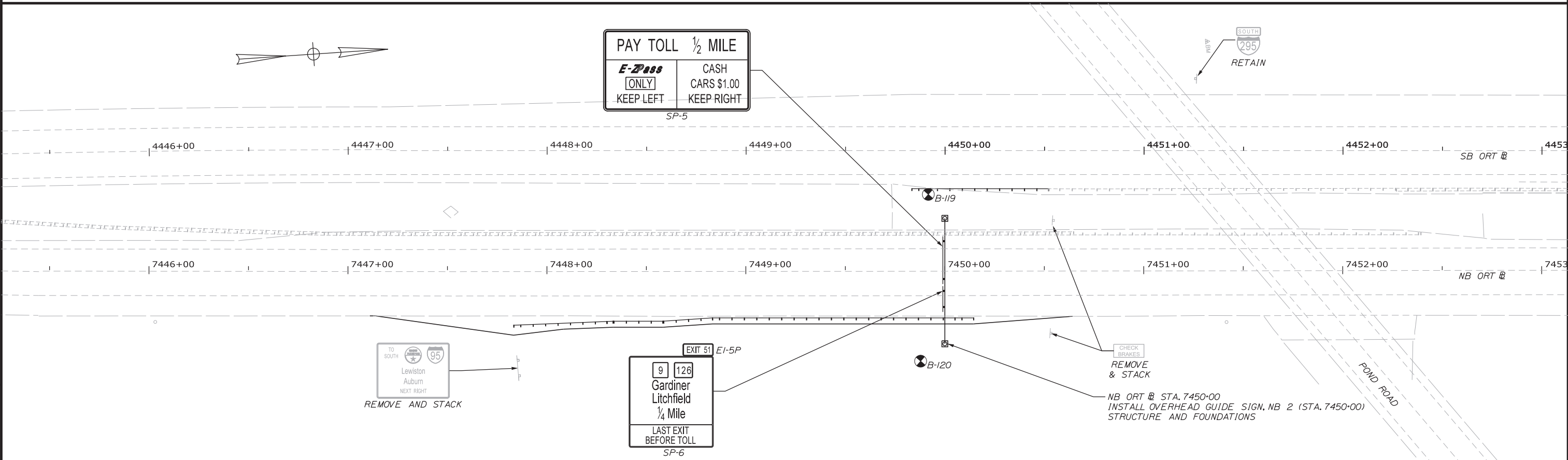
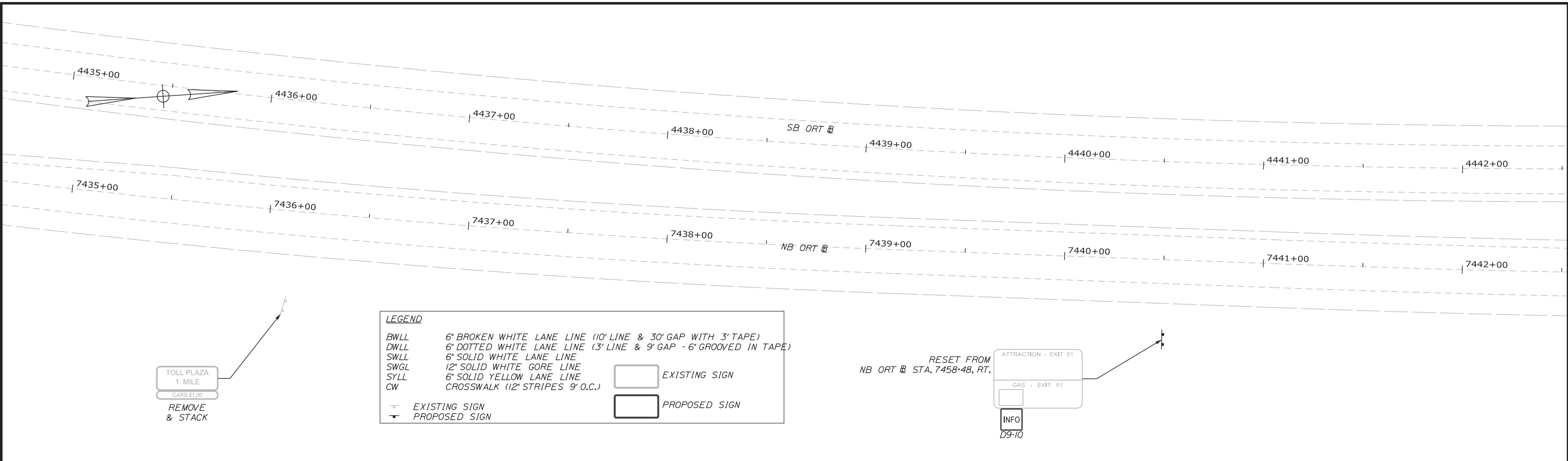
SHEET NUMBER: SS-01
172 OF 503

CONTRACT: 2019.04

Filename: ...:\00\Highway\172_SS_01.dgn

Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\173_SS_02.dgn



Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
By	Date	Checked	Date
BMH	3\20\19	DGE	3\20\19
Drawn	MPC	In Charge of	GAE
	3\20\19		3\20\19

GORRILL PALMER
707 SABLE OAKS DRIVE, SUITE 30
SOUTH PORTLAND, MAINE 04106
www.gorrillpalmer.com
207.772.2515

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

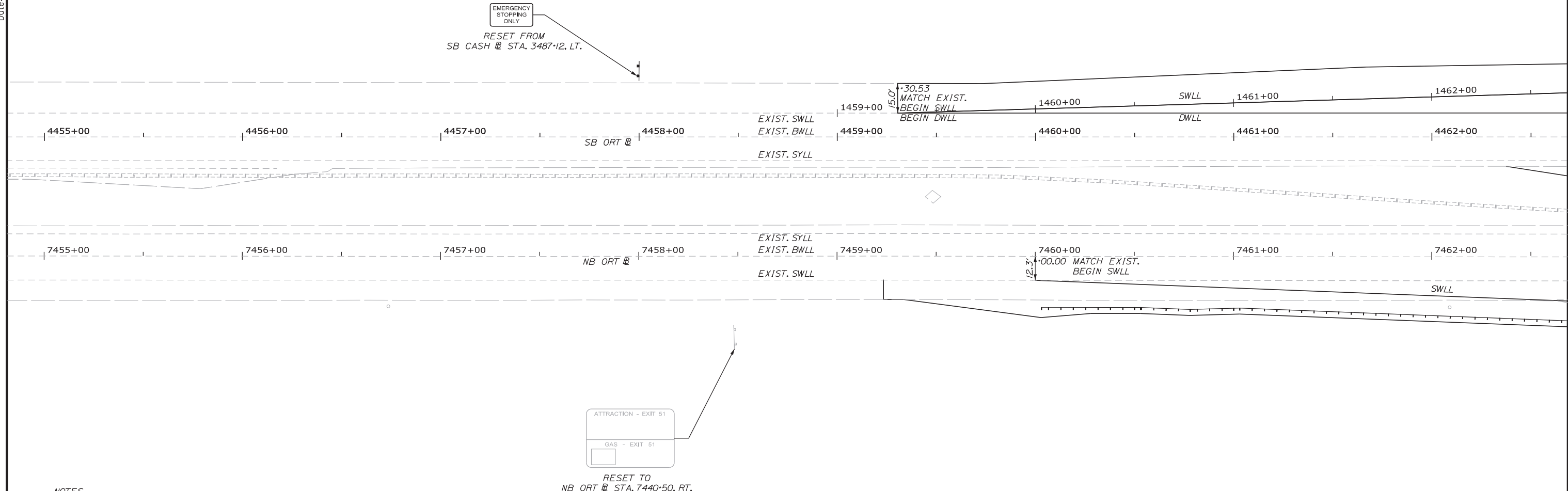
SIGNING AND STRIPING PLAN 2

SHEET NUMBER: SS-02
173 OF 503

CONTRACT: 2019.04



Date: 3/20/2019



NOTES

1. PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH MUTCD.
2. SEE GEOMETRIC PLANS FOR DIMENSIONS ON PAVEMENT MARKINGS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MILLING A GROOVE IN THE PAVEMENT FOR EACH TAPE LENGTH OF THE DOTTED WHITE LANE LINE (DWLL). MILLING SHALL BE INCIDENTAL TO THE PAVEMENT MARKING TAPE.

LEGEND

BWLL	6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)		EXISTING SIGN
DWLL	6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)		PROPOSED SIGN
SWLL	6" SOLID WHITE LANE LINE		
SWGL	12" SOLID WHITE GORE LINE		
SYLL	6" SOLID YELLOW LANE LINE		
CW	CROSSWALK (12" STRIPES 9' O.C.)		



Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

SIGNING AND STRIPING PLAN 3

SHEET NUMBER: SS-03
174 OF 503

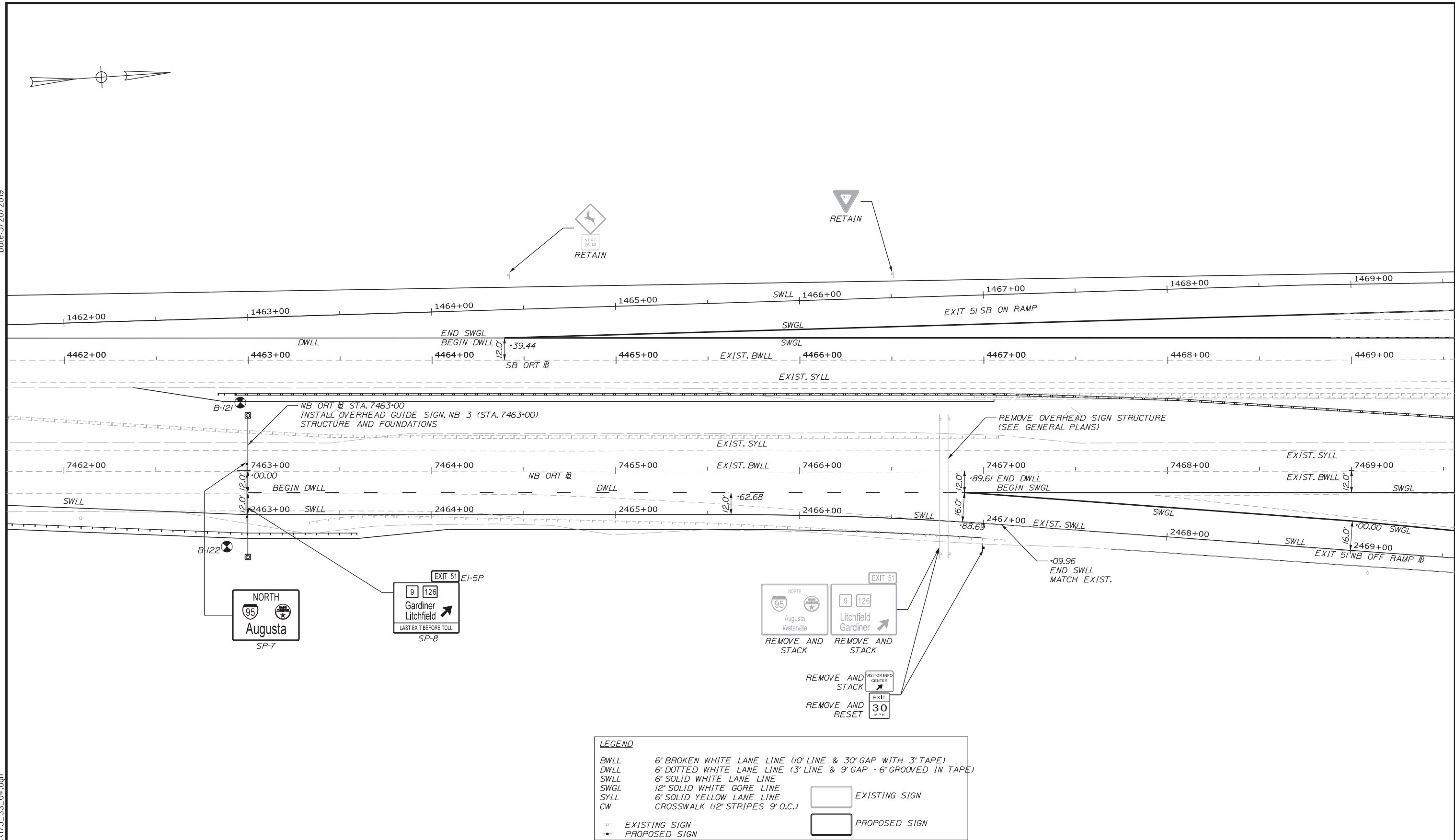
CONTRACT: 2019.04

Filename: ...:\00\HIGHWAY\MSTA\174_SS_03.dgn



Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\175_SS_04.dgn



LEGEND

BWLL	6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)		EXISTING SIGN
DWLL	6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)		PROPOSED SIGN
SWLL	6" SOLID WHITE LANE LINE		
SWGL	12" SOLID WHITE GORE LINE		
SYLL	6" SOLID YELLOW LANE LINE		
CW	CROSSWALK (12" STRIPES 9' O.C.)		
	EXISTING SIGN		
	PROPOSED SIGN		

Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

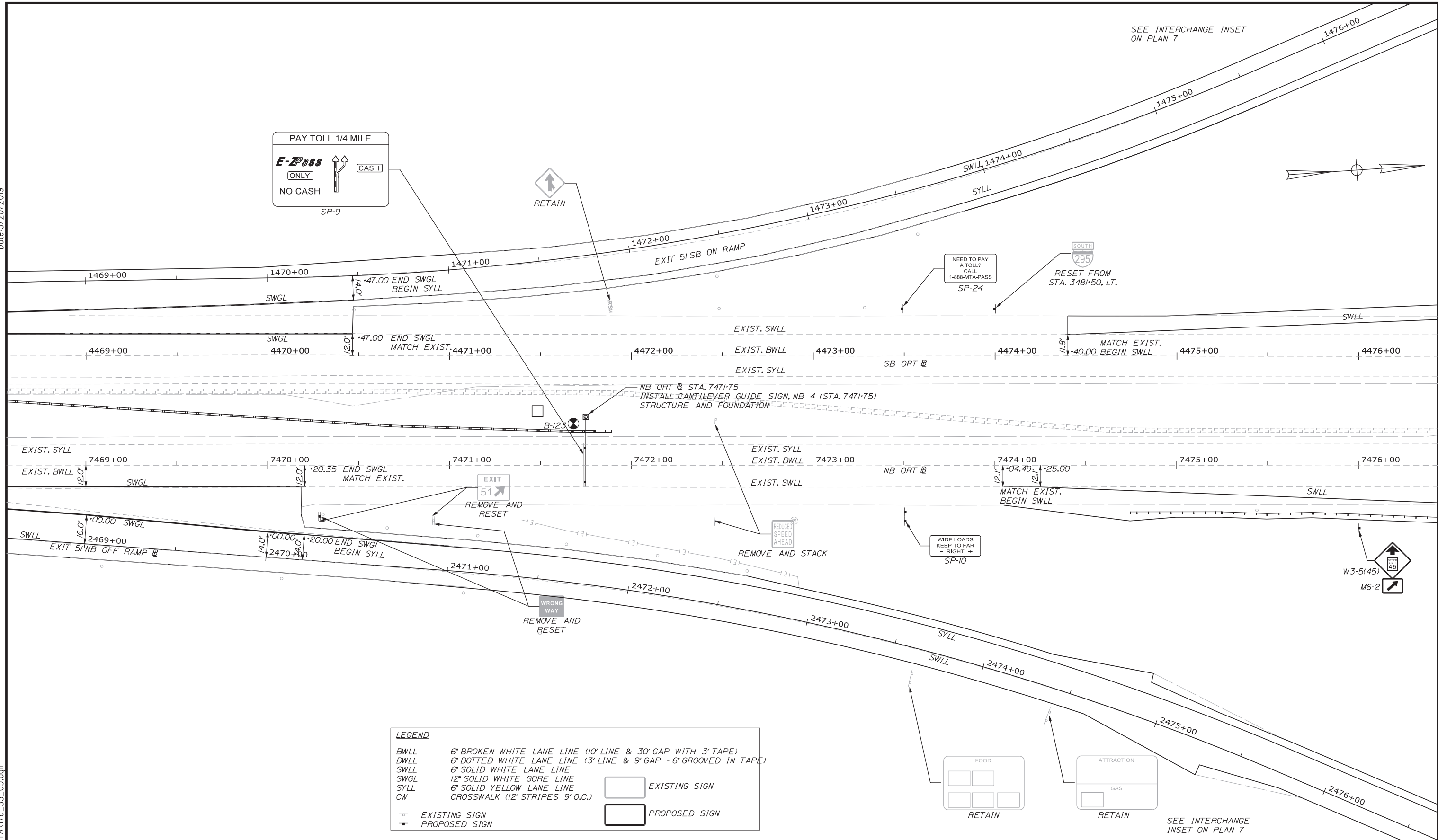
SIGNING AND STRIPING PLAN 4

SHEET NUMBER: SS-04
175 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA176_SS_05.dgn



LEGEND

BWLL	6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)		EXISTING SIGN
DWLL	6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)		PROPOSED SIGN
SWLL	6" SOLID WHITE LANE LINE		
SWGL	12" SOLID WHITE GORE LINE		
SYLL	6" SOLID YELLOW LANE LINE		
CW	CROSSWALK (12" STRIPES 9' O.C.)		
	EXISTING SIGN		
	PROPOSED SIGN		

Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by: **GORRILL PALMER**

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

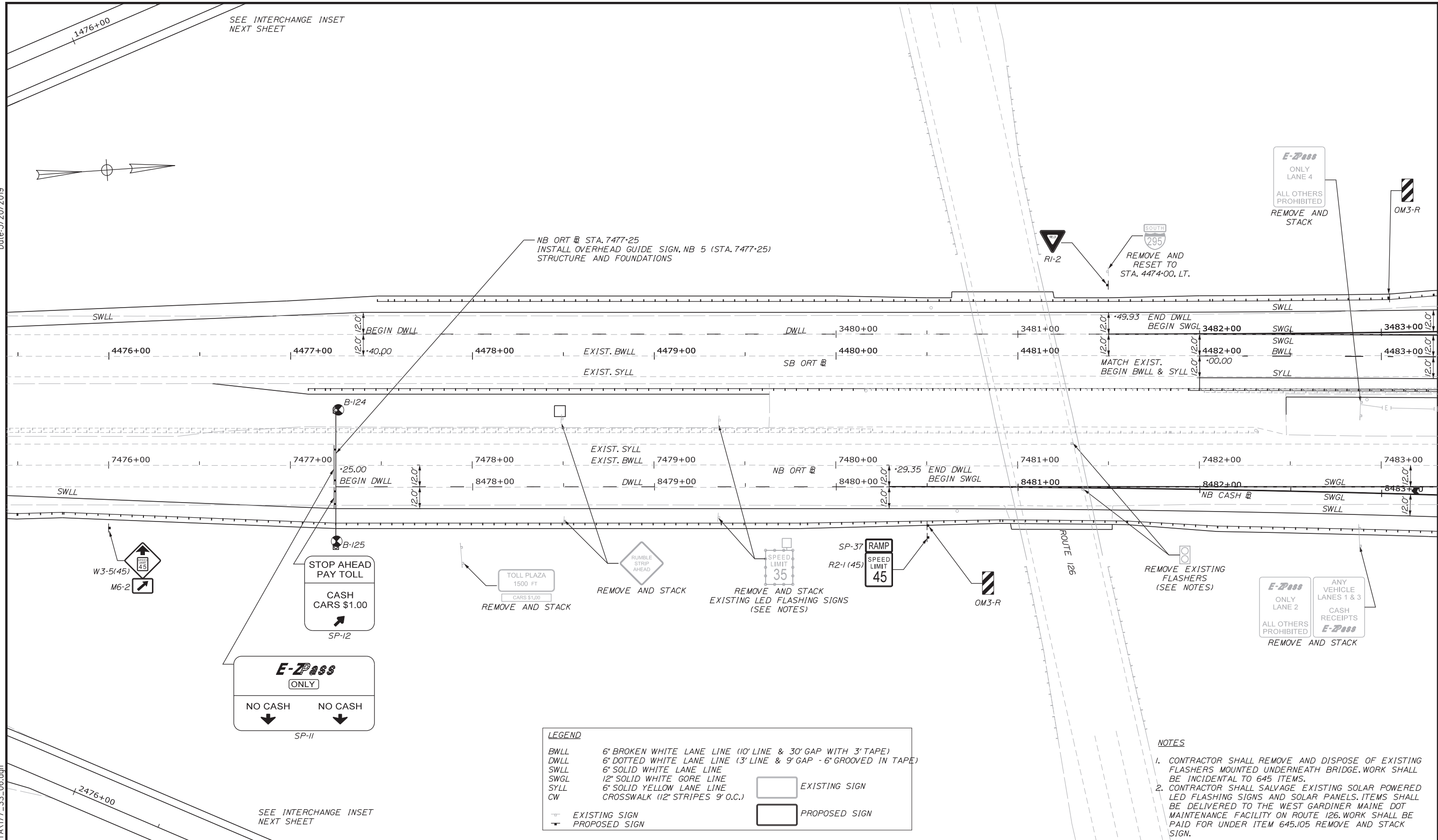
SIGNING AND STRIPING PLAN 5

SHEET NUMBER: SS-05
176 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...00.HIGHWAY\MSTA1177_SS_06.dgn



LEGEND

BWLL	6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)		EXISTING SIGN
DWLL	6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)		PROPOSED SIGN
SWLL	6" SOLID WHITE LANE LINE		
SWGL	12" SOLID WHITE GORE LINE		
SYLL	6" SOLID YELLOW LANE LINE		
CW	6" SOLID YELLOW LANE LINE CROSSWALK (12" STRIPES 9' O.C.)		
	EXISTING SIGN		
	PROPOSED SIGN		

- NOTES**
- CONTRACTOR SHALL REMOVE AND DISPOSE OF EXISTING FLASHERS MOUNTED UNDERNEATH BRIDGE. WORK SHALL BE INCIDENTAL TO 645 ITEMS.
 - CONTRACTOR SHALL SALVAGE EXISTING SOLAR POWERED LED FLASHING SIGNS AND SOLAR PANELS. ITEMS SHALL BE DELIVERED TO THE WEST GARDINER MAINE DOT MAINTENANCE FACILITY ON ROUTE 126. WORK SHALL BE PAID FOR UNDER ITEM 645.105 REMOVE AND STACK SIGN.

Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE

	By	Date		By	Date
Designed	BMH	3\20\19	Checked	DGE	3\20\19
Drawn	MPC	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

SIGNING AND STRIPING PLAN 6

SHEET NUMBER: SS-06
177 OF 503

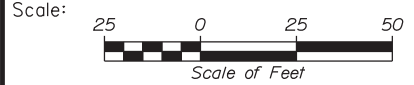
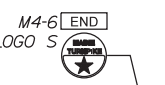
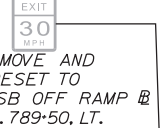
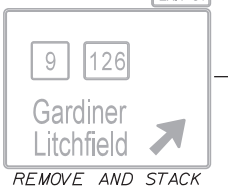
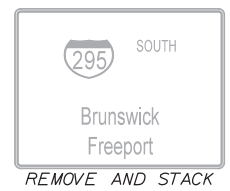
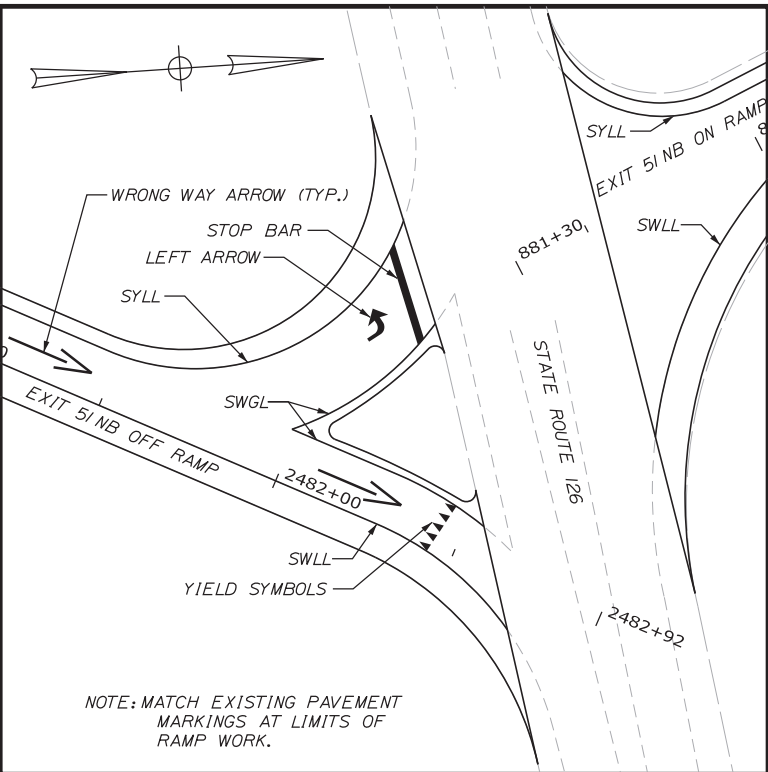
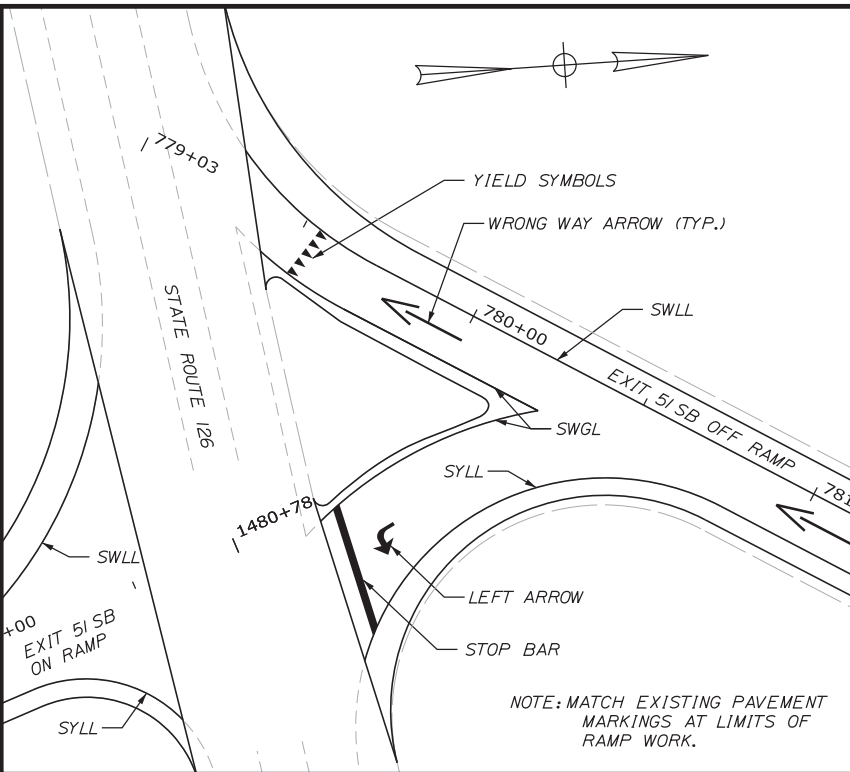
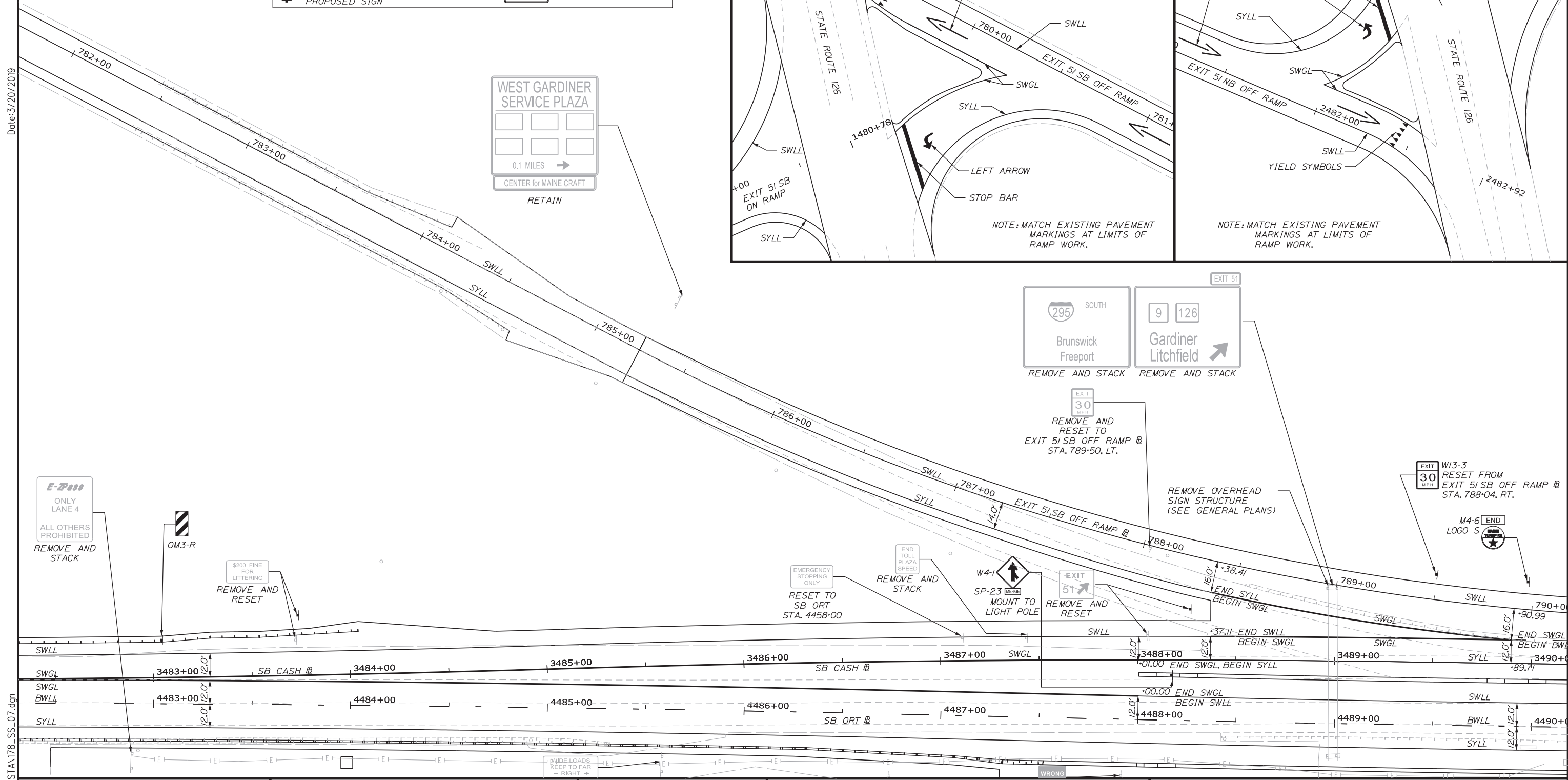
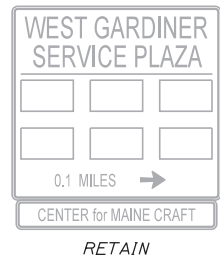
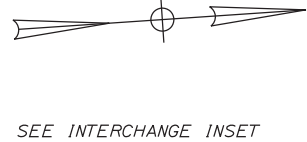
CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...000\HIGHWAY\MST\178_SS_07.dgn

LEGEND

BWLL	6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)		EXISTING SIGN
DWLL	6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)		PROPOSED SIGN
SWLL	6" SOLID WHITE LANE LINE		
SWGL	12" SOLID WHITE GORE LINE		
SYLL	6" SOLID YELLOW LANE LINE		
CW	CROSSWALK (12" STRIPES 9' O.C.)		



Designed by:

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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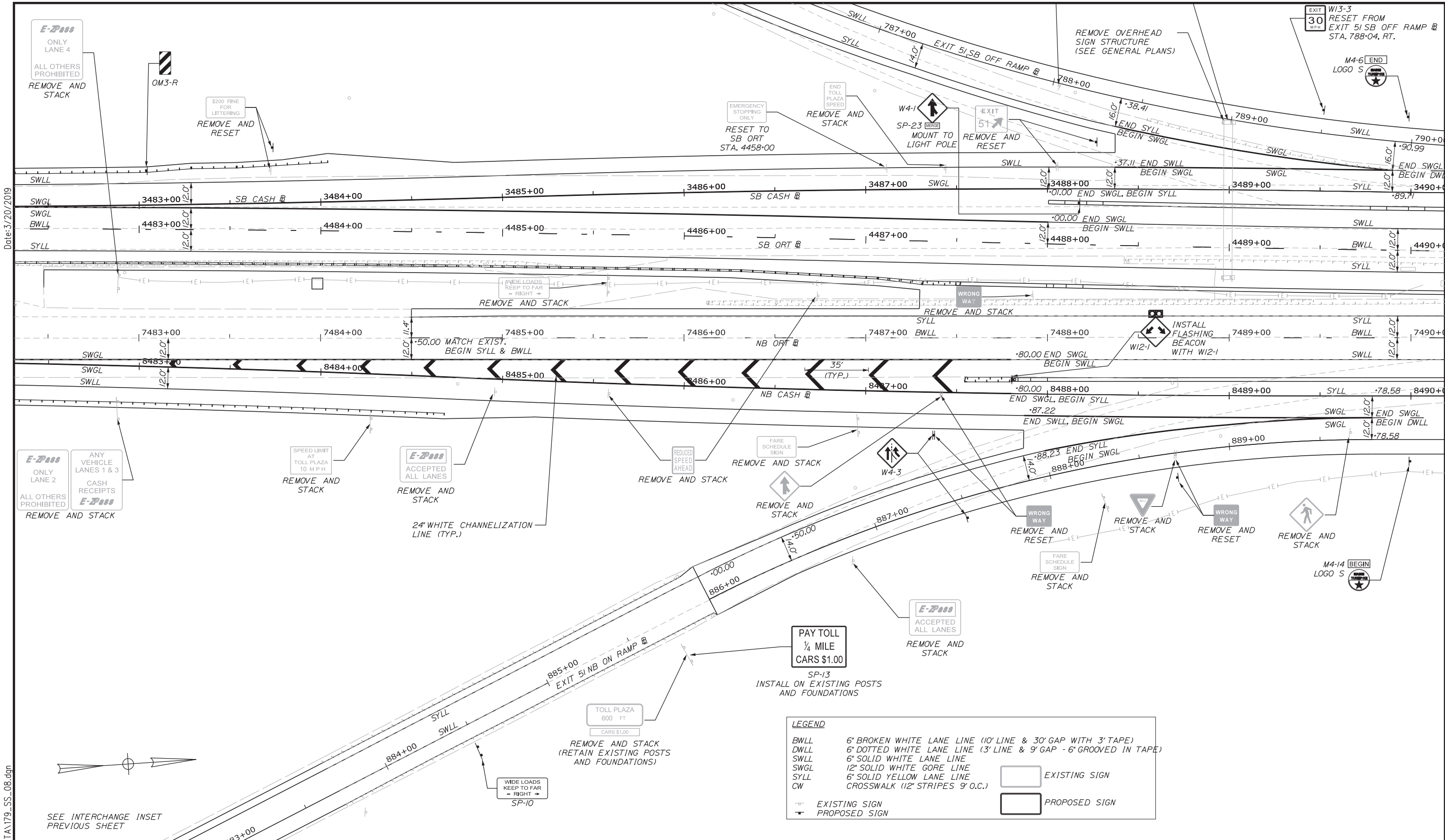
THE GOLD STAR MEMORIAL HIGHWAY

INTERCHANGE 103
ORT CONVERSION
SIGNING AND STRIPING PLAN 7

SHEET NUMBER: SS-07
178 OF 503

CONTRACT: 2019.04

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE



Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA179_SS_08.dgn

E-Pass
ONLY LANE 2
ALL OTHERS PROHIBITED
REMOVE AND STACK

ANY VEHICLE LANES 1 & 3
CASH RECEIPTS
E-Pass

E-Pass
ONLY LANE 4
ALL OTHERS PROHIBITED
REMOVE AND STACK

\$200 FINE FOR LITTERING
REMOVE AND RESET

SPEED LIMIT AT TOLL PLAZA 10 MPH
REMOVE AND STACK

E-Pass
ACCEPTED ALL LANES
REMOVE AND STACK

REDUCED SPEED AHEAD
REMOVE AND STACK

FARE SCHEDULE SIGN
REMOVE AND STACK

E-Pass
ACCEPTED ALL LANES
REMOVE AND STACK

PAY TOLL 1/4 MILE
CARS \$1.00

SP-13
INSTALL ON EXISTING POSTS AND FOUNDATIONS

TOLL PLAZA 600 FT
CARS \$1.00
REMOVE AND STACK (RETAIN EXISTING POSTS AND FOUNDATIONS)

WIDE LOADS KEEP TO FAR - RIGHT ->
SP-10

LEGEND

BWLL	6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)		EXISTING SIGN
DWLL	6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)		PROPOSED SIGN
SWLL	6" SOLID WHITE LANE LINE		
SWGL	12" SOLID WHITE GORE LINE		
SYLL	6" SOLID YELLOW LANE LINE		
CW	6" CROSSWALK (12" STRIPES 9" O.C.)		



SEE INTERCHANGE INSET PREVIOUS SHEET



Designed by:

GORRILL PALMER			
707 SABLE OAKS DRIVE, SUITE 30 SOUTH PORTLAND, MAINE 04106 www.gorrillpalmer.com 207.772.2515			
CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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707 SABLE OAKS DRIVE, SUITE 30
SOUTH PORTLAND, MAINE 04106
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THE GOLD STAR MEMORIAL HIGHWAY

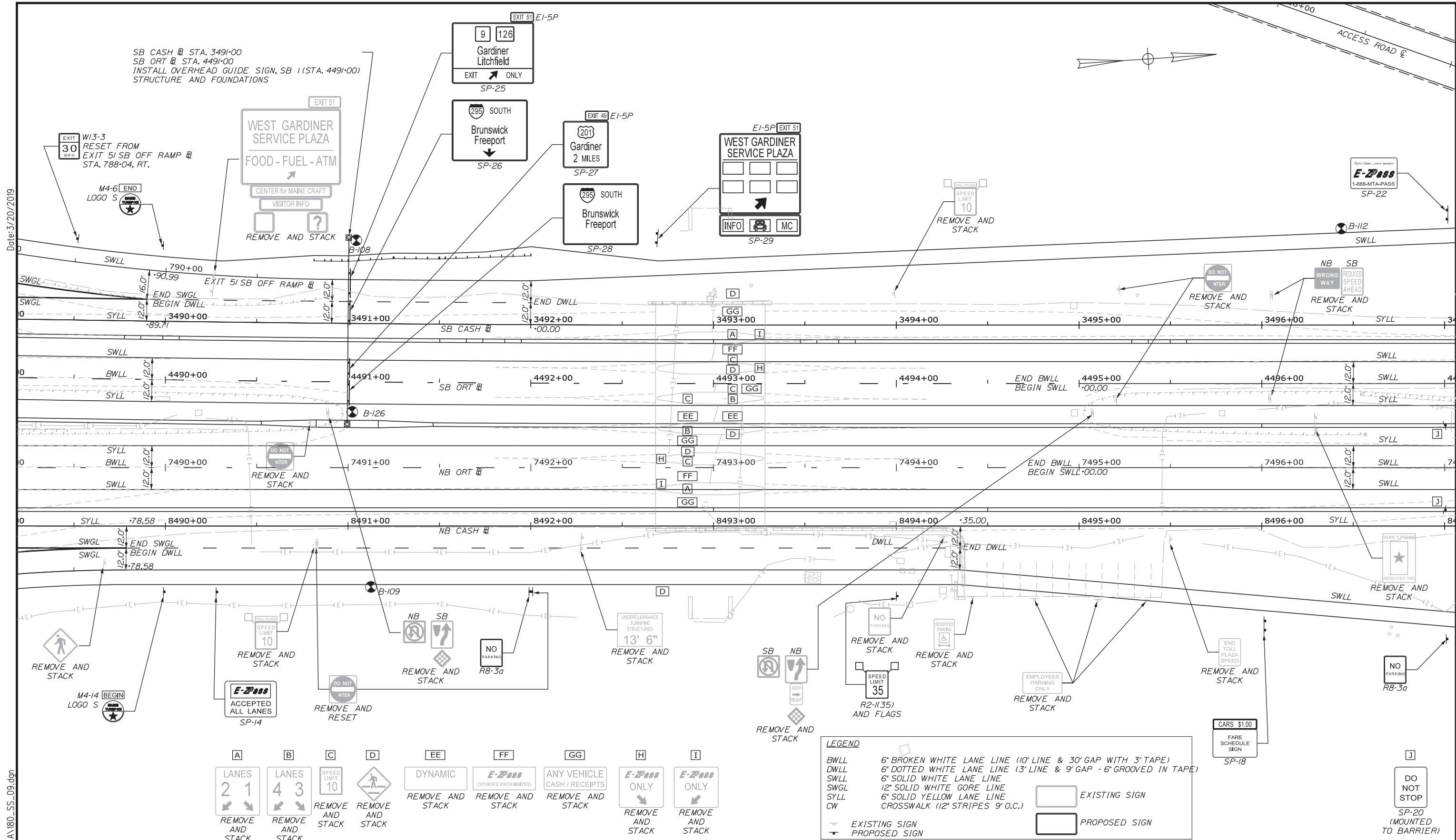
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

SIGNING AND STRIPING PLAN 8

SHEET NUMBER: SS-08
179 OF 503

CONTRACT: 2019.04



Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\180_SS_09.dgn

Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE

	By	Date		By	Date
Designed	BMH	3\20\19	Checked	DGE	3\20\19
Drawn	MPC	3\20\19	In Charge of	GAE	3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

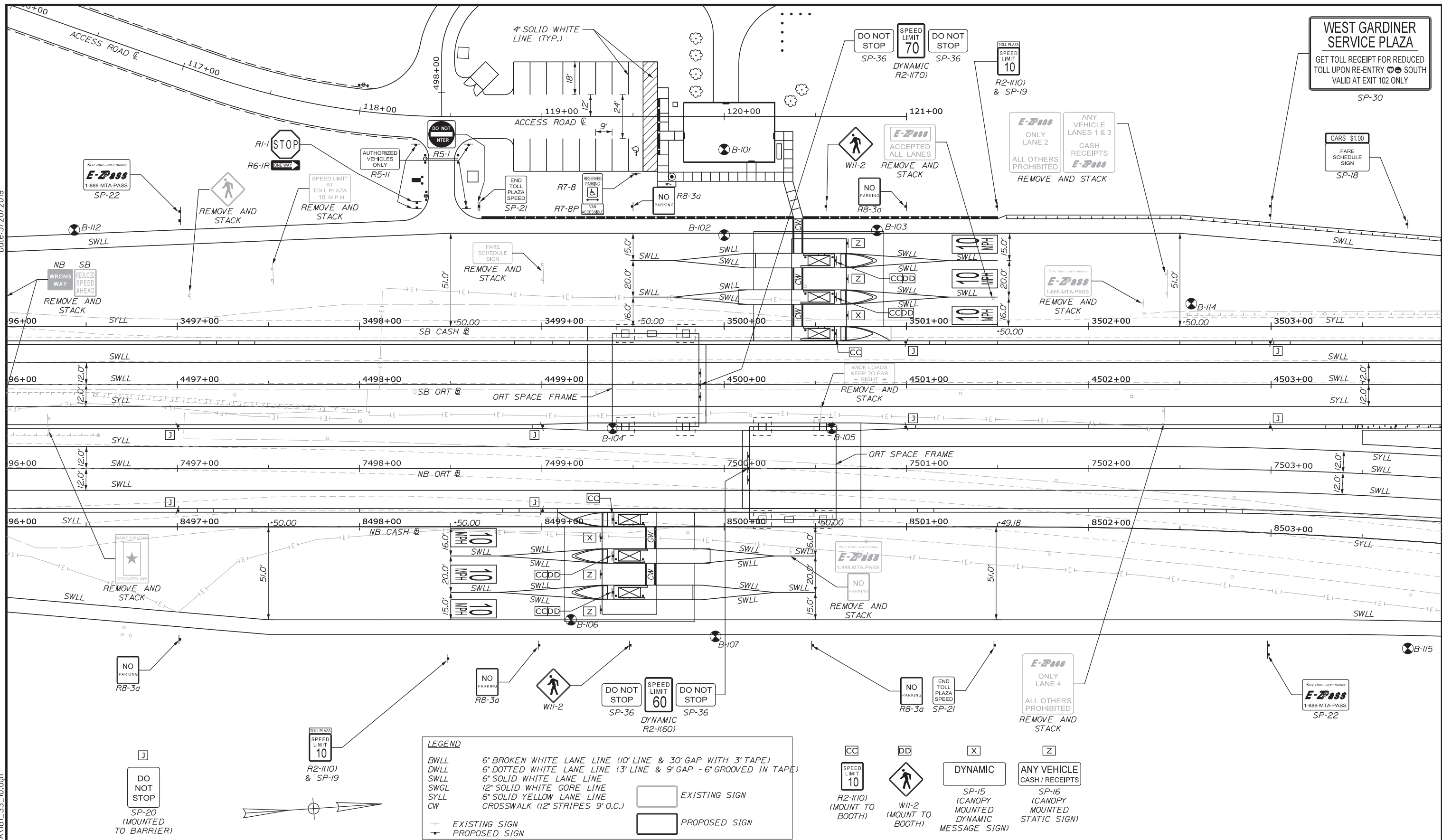
SIGNING AND STRIPING PLAN 9

SHEET NUMBER: SS-09
180 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\181_SS_10.dgn



LEGEND

BWLL 6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)
 DWLL 6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)
 SWLL 6" SOLID WHITE LANE LINE
 SWGL 12" SOLID WHITE GORE LINE
 SYLL 6" SOLID YELLOW LANE LINE
 CW CROSSWALK (12" STRIPES 9" O.C.)

EXISTING SIGN
 PROPOSED SIGN



Designed by: **GORRILL PALMER**

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION

SIGNING AND STRIPING PLAN 10

SHEET NUMBER: SS-10
 181 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\182_SS_11.dgn

WEST GARDINER SERVICE PLAZA

GET TOLL RECEIPT FOR REDUCED TOLL UPON RE-ENTRY SOUTH VALID AT EXIT 102 ONLY

SP-30

CARS \$1.00

FARE SCHEDULE SIGN

SP-18

NO PARKING

R8-3a

SPEED LIMIT 35

REMOVE AND STACK

OVER HEIGHT VEHICLE DO NOT PROCEED PULL OVER TO RIGHT

OVER HEIGHT VEHICLE SIGN AND BEACON

SPEED LIMIT 35

R2-(135) AND FLAGS

INSTALL FLASHING BEACON WITH W12-1

E-Pass ACCEPTED ALL LANES

SP-14

TOLL PLAZA 1500 FT

CARS \$1.00 REMOVE AND STACK

SB ORT @ STA. 4508+75. INSTALL OVERHEAD GUIDE SIGN, SB-2 (STA. 4508+75) STRUCTURE AND FOUNDATIONS

Gardiner Litchfield

USE CASH LANES

SP-31

STOP AHEAD PAY TOLL CASH CARS \$1.00

SP-12

STOP AHEAD PAY TOLL

CASH CARS \$1.00

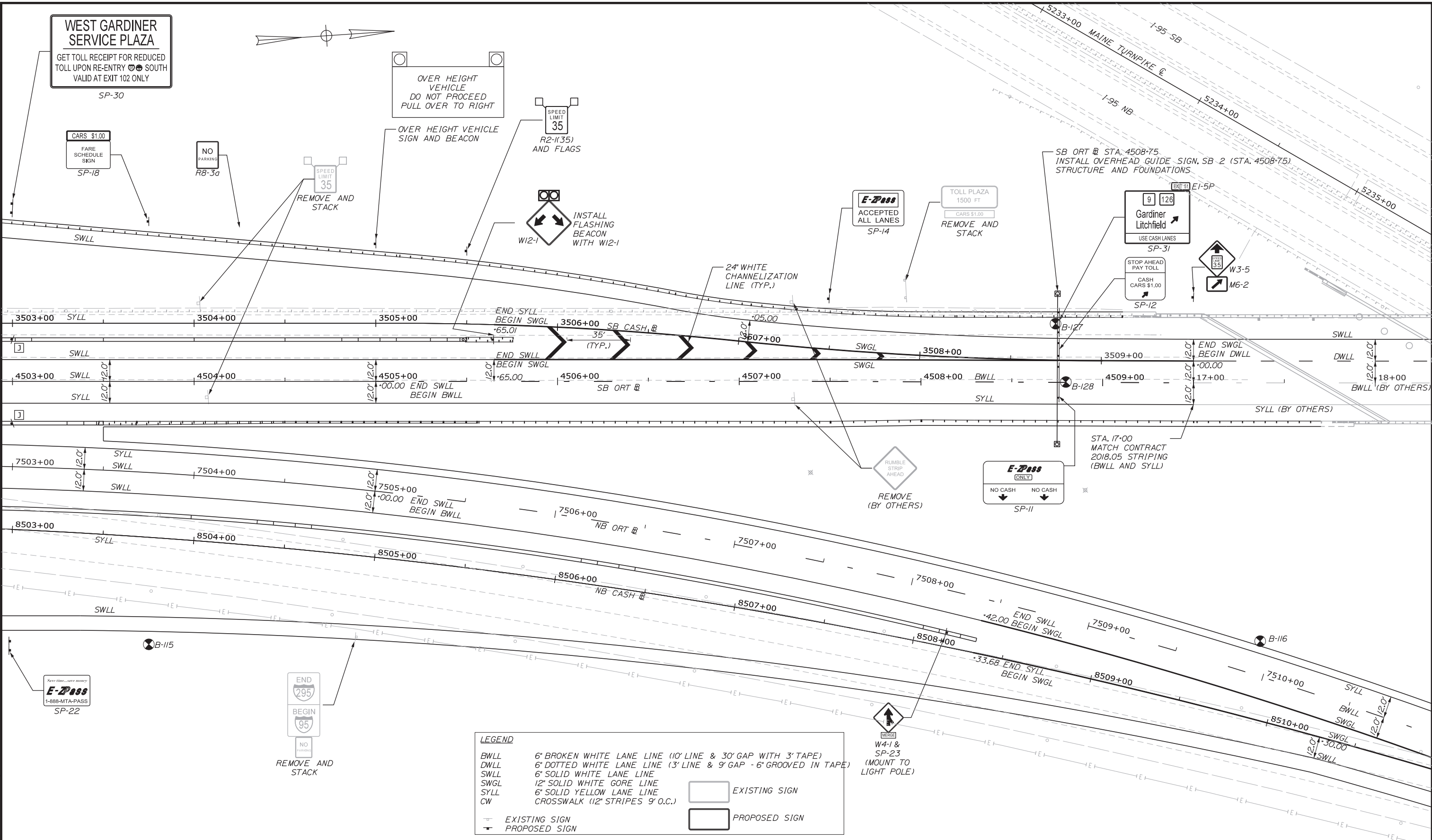
SP-12

E-Pass ONLY NO CASH NO CASH

SP-11

LEGEND

BWLL	6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)	EXISTING SIGN
DWLL	6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)	EXISTING SIGN
SWLL	6" SOLID WHITE LANE LINE	EXISTING SIGN
SWGL	12" SOLID WHITE GORE LINE	EXISTING SIGN
SYLL	6" SOLID YELLOW LANE LINE	EXISTING SIGN
CW	CROSSWALK (12" STRIPES 9' O.C.)	EXISTING SIGN
		PROPOSED SIGN



Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER:	BRANDON M. HAVU, PE		
By	Date	Checked	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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SOUTH PORTLAND, MAINE 04106
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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

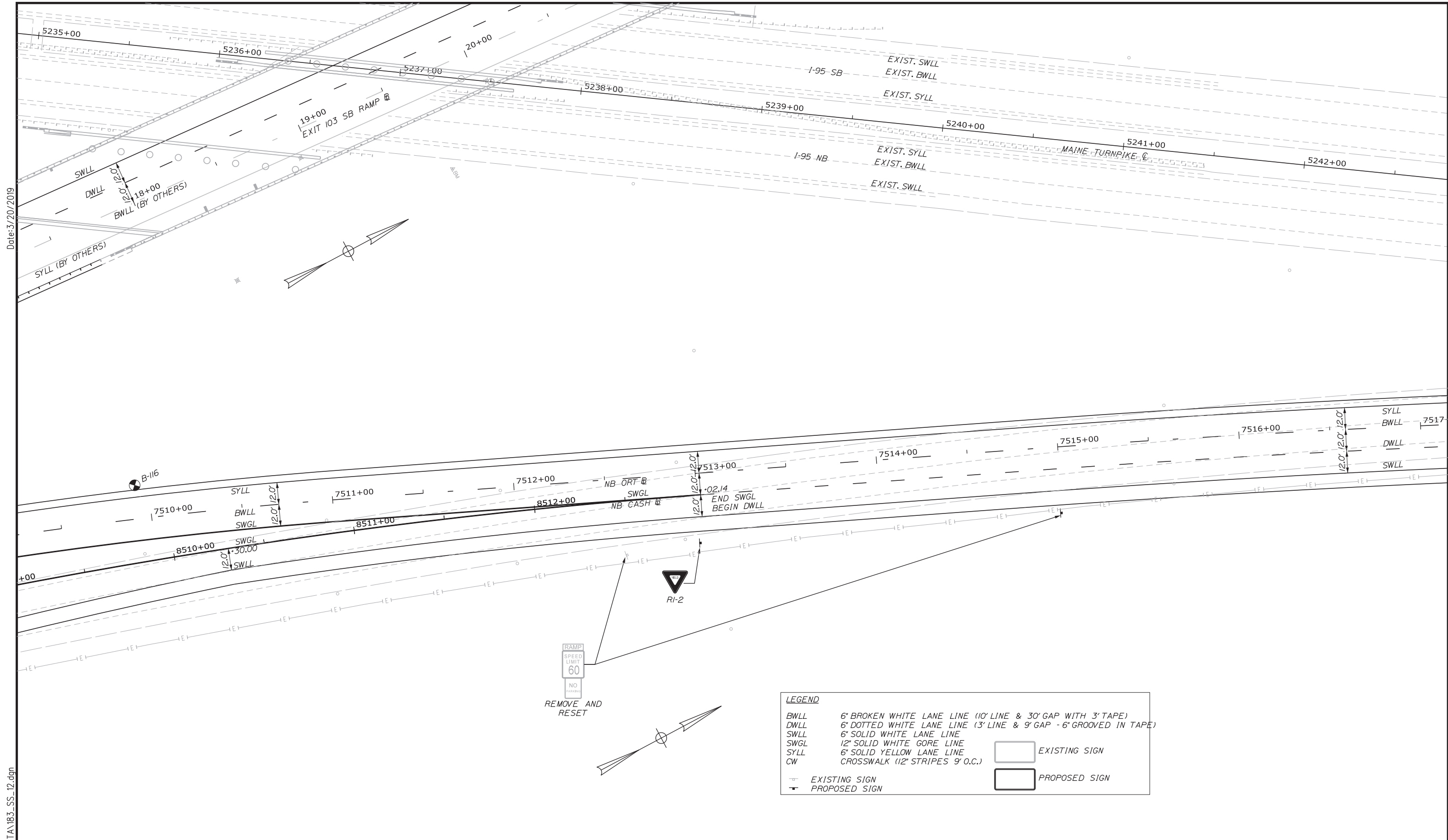
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103 ORT CONVERSION

SIGNING AND STRIPING PLAN 11

SHEET NUMBER: SS-11
182 OF 503

CONTRACT: 2019.04




Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\183_SS_12.dgn

Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:



GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
By	Date	Checked	Date
BMH	3\20\19	DGE	3\20\19
Drawn	MPC	In Charge of	GAE
	3\20\19		3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

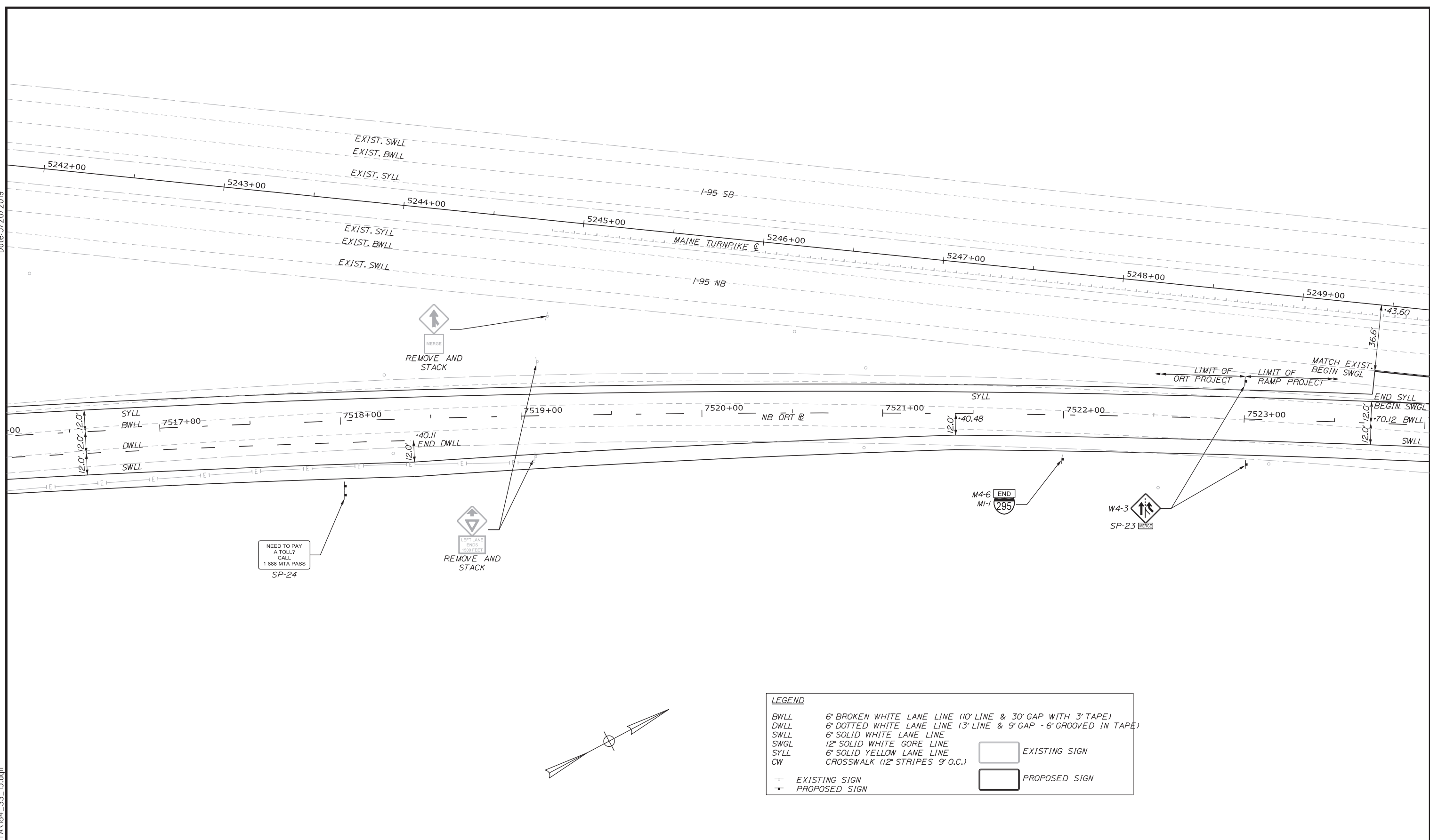
SIGNING AND STRIPING PLAN 12

SHEET NUMBER: SS-12
183 OF 503

CONTRACT: 2019.04

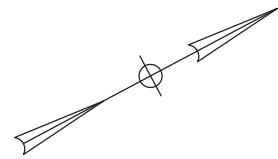
Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\184_SS_13.dgn



LEGEND

BWLL	6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)	
DWLL	6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)	
SWLL	6" SOLID WHITE LANE LINE	
SWGL	12" SOLID WHITE GORE LINE	
SYLL	6" SOLID YELLOW LANE LINE	EXISTING SIGN
CW	CROSSWALK (12" STRIPES 9' O.C.)	PROPOSED SIGN
	EXISTING SIGN	
	PROPOSED SIGN	



Scale:

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
By	Date	Checked	Date
BMH	3\20\19	DGE	3\20\19
Drawn	MPC	In Charge of	GAE
	3\20\19		3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION

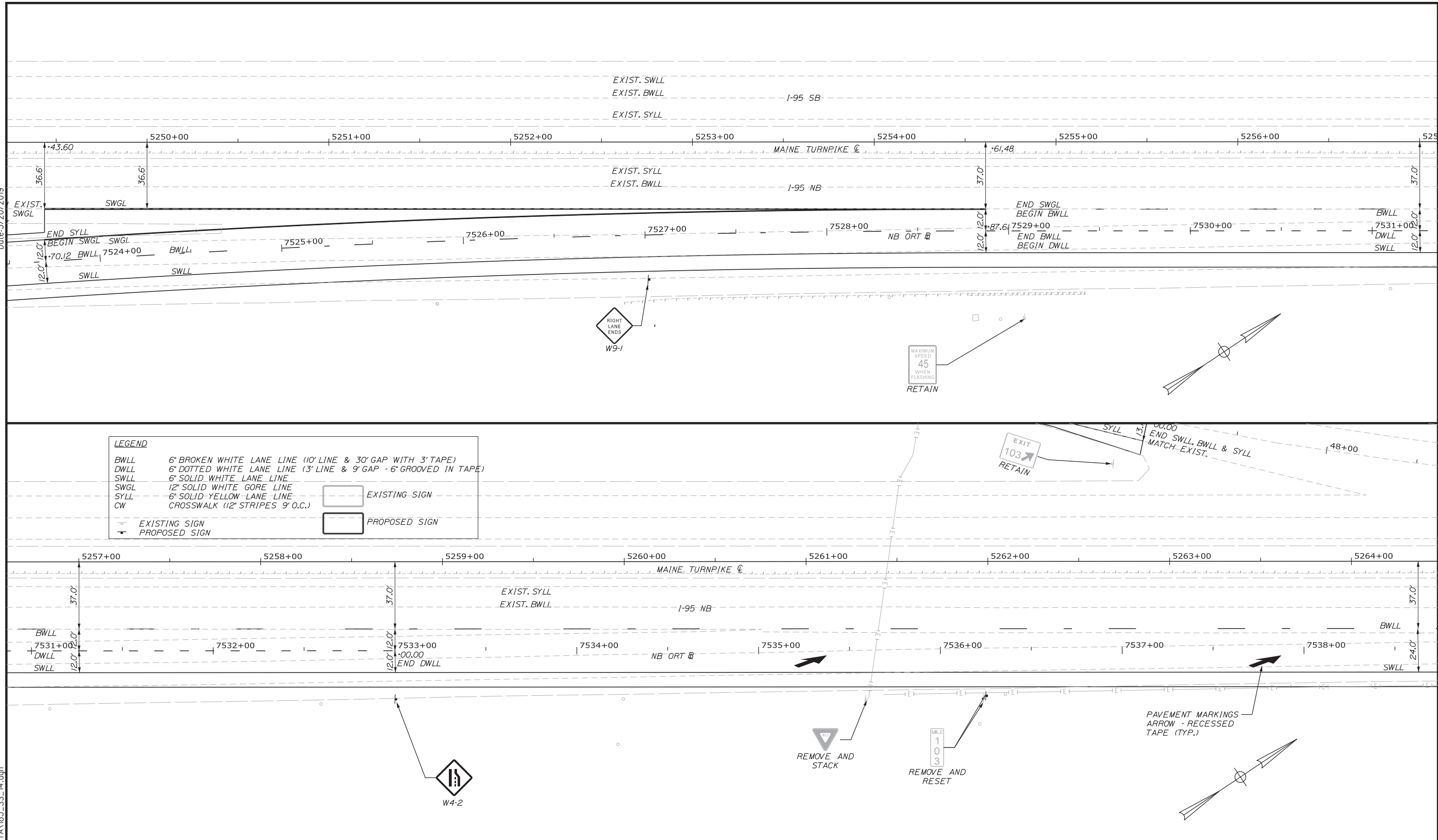
SIGNING AND STRIPING PLAN 13

SHEET NUMBER: SS-13
 184 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\185_SS_14.dgn



LEGEND

BWLL 6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)
 DWLL 6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)
 SWGL 6" SOLID WHITE LANE LINE
 SYLL 12" SOLID WHITE GORE LINE
 CW 6" SOLID YELLOW LANE LINE
 CROSSWALK (12" STRIPES 9" O.C.)

EXISTING SIGN (dashed line)
 PROPOSED SIGN (solid line)

EXISTING SIGN (dashed box)
 PROPOSED SIGN (solid box)



Designed by: **GORRILL PALMER**

CONSULTANT	PROJECT MANAGER: BRANDON M. HAVU, PE		
By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

GORRILL PALMER
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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION

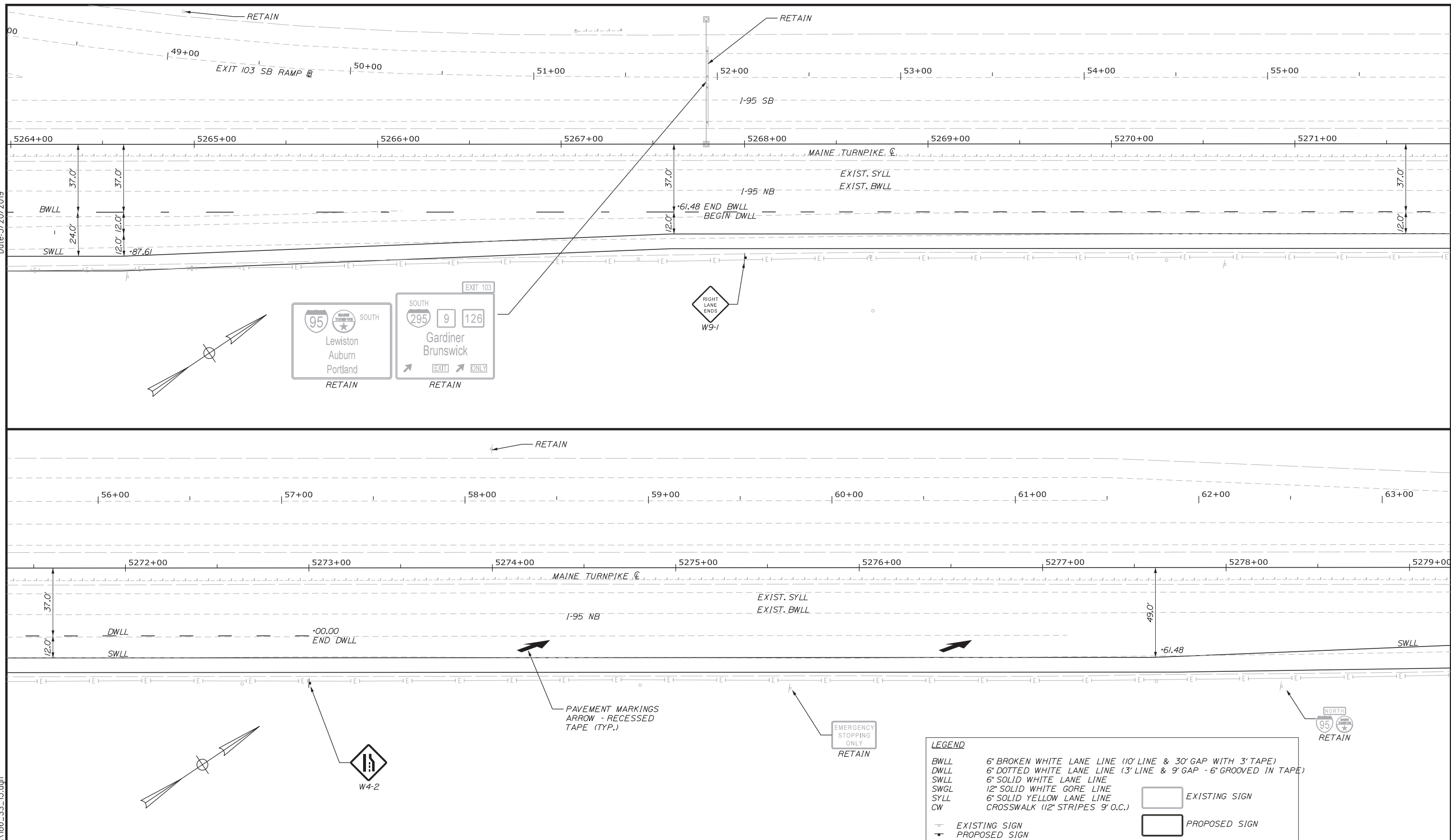
SIGNING AND STRIPING PLAN 14

SHEET NUMBER: SS-14
 185 OF 503

CONTRACT: 2019.04

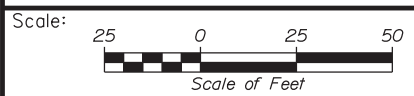
Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\186_SS_15.dgn



LEGEND

BWLL	6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)		EXISTING SIGN
DWLL	6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)		PROPOSED SIGN
SWLL	6" SOLID WHITE LANE LINE		
SWGL	12" SOLID WHITE GORE LINE		
SYLL	6" SOLID YELLOW LANE LINE		
CW	CROSSWALK (12" STRIPES 9' O.C.)		



Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE

No.	Revision	By	Date

By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

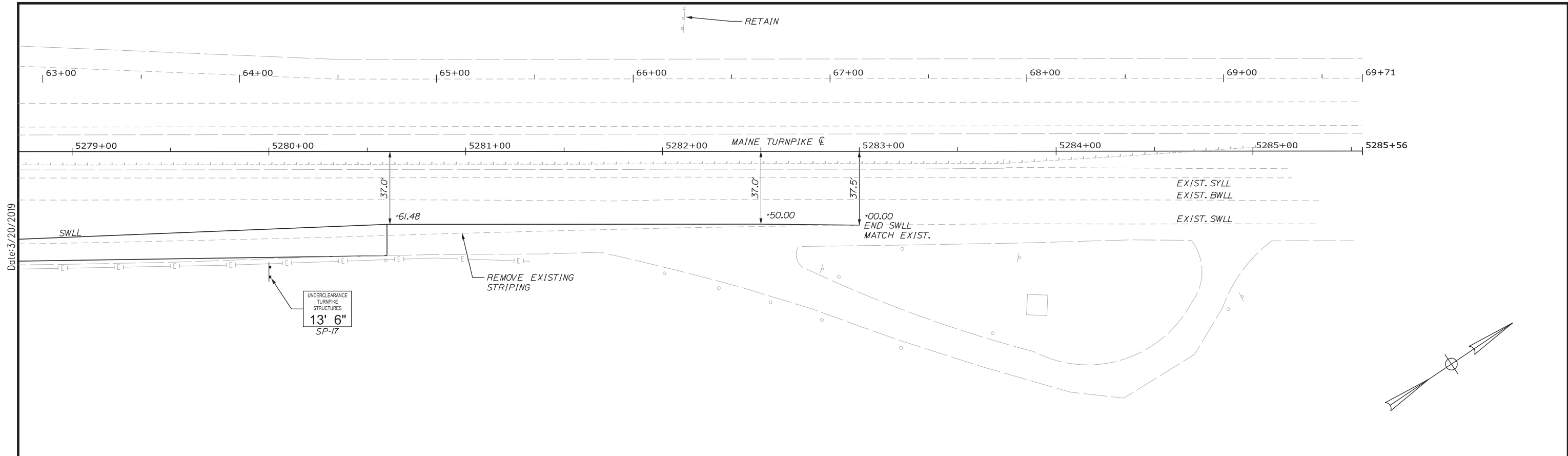
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

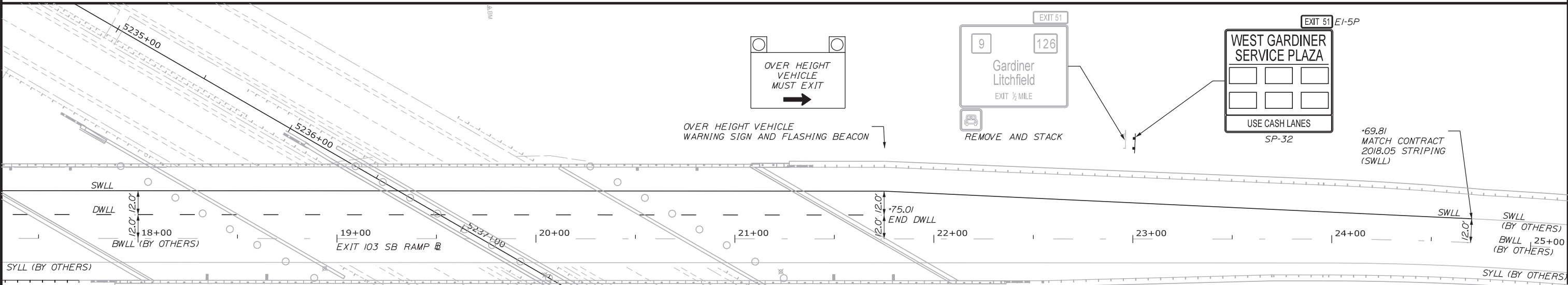
SIGNING AND STRIPING PLAN 15

SHEET NUMBER: SS-15
186 OF 503

CONTRACT: 2019.04



Date: 3/20/2019



LEGEND

BWLL	6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)		EXISTING SIGN
DWLL	6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)		PROPOSED SIGN
SWLL	6" SOLID WHITE LANE LINE		
SWGL	12" SOLID WHITE GORE LINE		
SYLL	6" SOLID YELLOW LANE LINE		
CW	CROSSWALK (12" STRIPES 9' O.C.)		

Filename: ...:\00\HIGHWAY\MSTA\187_SS_16.dgn

Scale:

No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

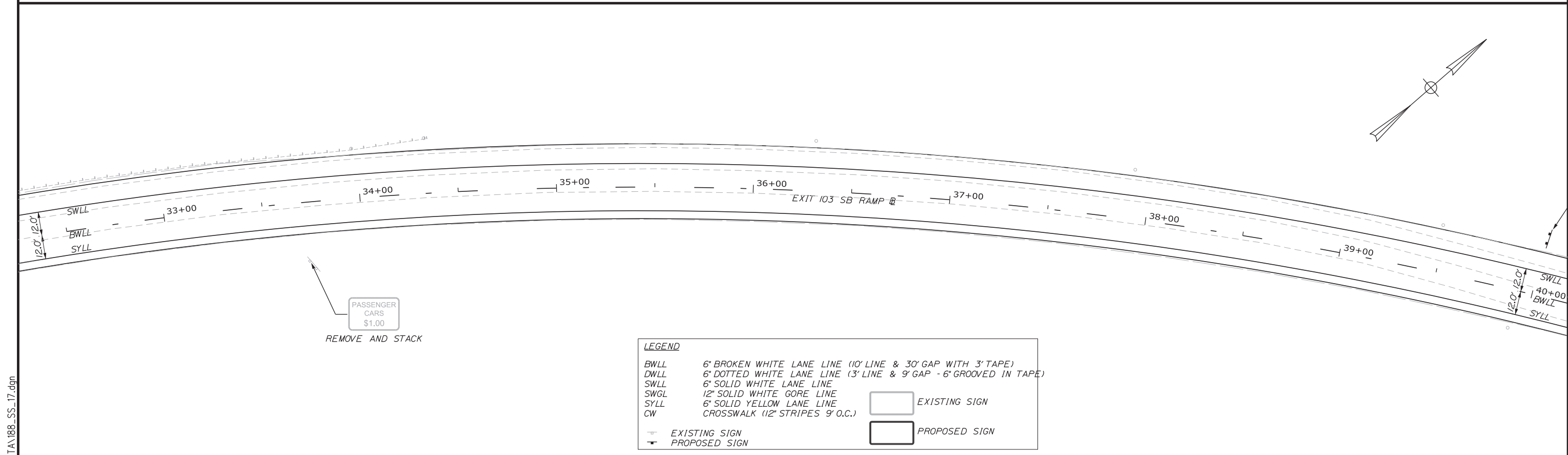
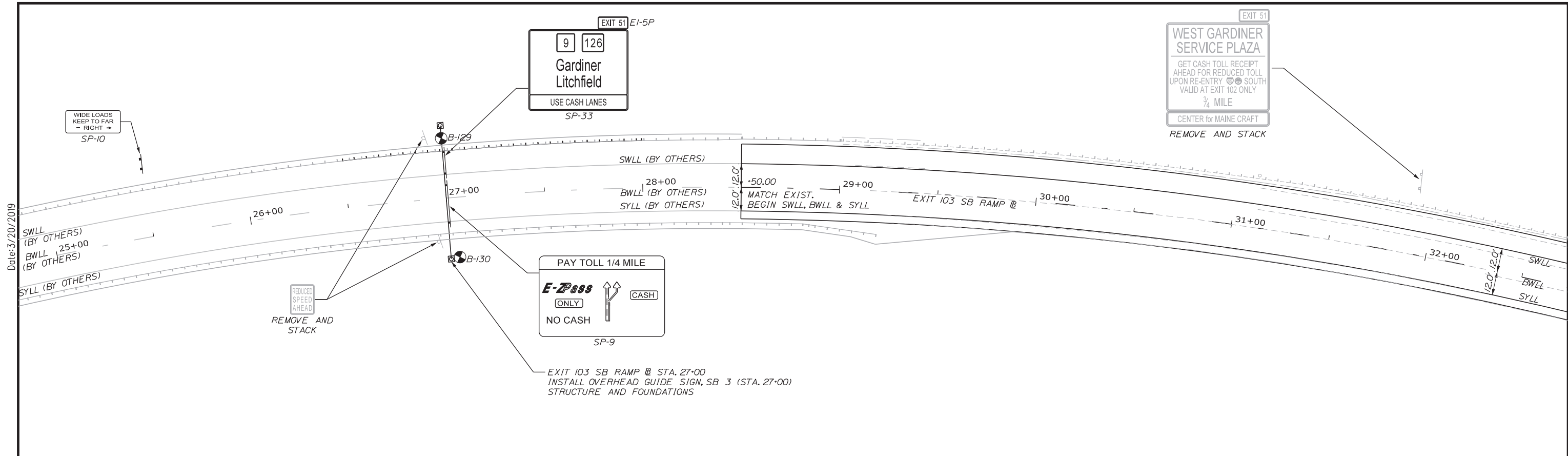
**INTERCHANGE 103
 ORT CONVERSION**

SIGNING AND STRIPING PLAN 16

SHEET NUMBER: SS-16
 187 OF 503

CONTRACT: 2019.04

Date: 3/20/2019



LEGEND

BWLL	6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)	
DWLL	6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)	
SWLL	6" SOLID WHITE LANE LINE	
SWGL	12" SOLID WHITE GORE LINE	
SYLL	6" SOLID YELLOW LANE LINE	
CW	CROSSWALK (12" STRIPES 9' O.C.)	
	EXISTING SIGN	
	PROPOSED SIGN	

Filename: ...:\00\HIGHWAY\MSTA\188_SS_17.dgn

Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

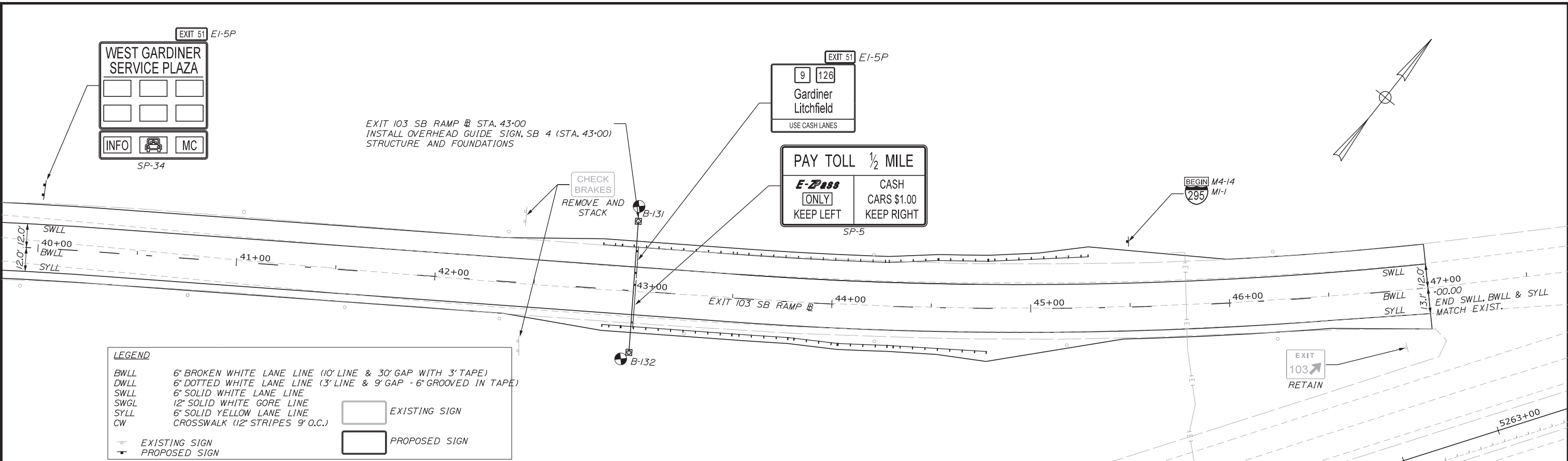
INTERCHANGE 103
ORT CONVERSION

SIGNING AND STRIPING PLAN 17

SHEET NUMBER: SS-17
188 OF 503

CONTRACT: 2019.04

Date: 3/20/2019



LEGEND

- BWLL 6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3" TAPE)
- DWLL 6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)
- SWLL 6" SOLID WHITE LANE LINE
- SWGL 12" SOLID WHITE GORE LINE
- SYLL 6" SOLID YELLOW LANE LINE
- CW CROSSWALK (12" STRIPES 9' O.C.)

EXISTING SIGN
 PROPOSED SIGN

EXISTING SIGN
 PROPOSED SIGN

Filename: ...:\00\HIGHWAY\MSTA\189_SS_18.dgn



No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE

	By	Date	Checked	By	Date
Designed	BMH	3\20\19	Checked	DGE	3\20\19
Drawn	MPC	3\20\19	In Charge of	GAE	3\20\19

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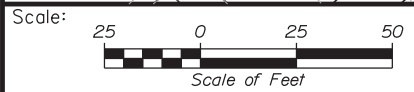
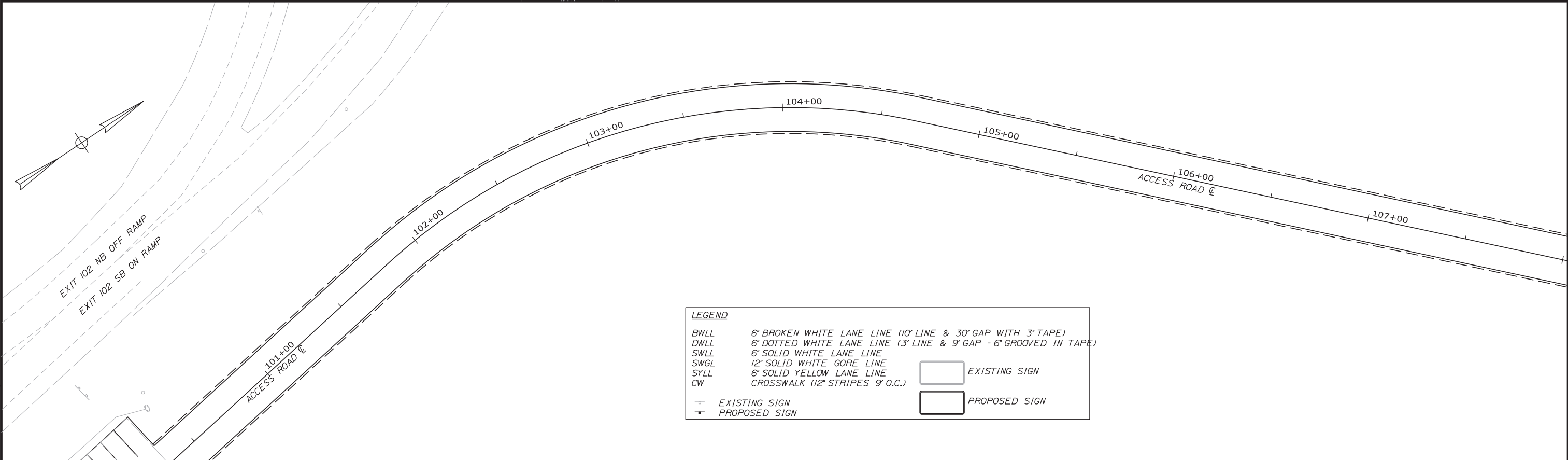
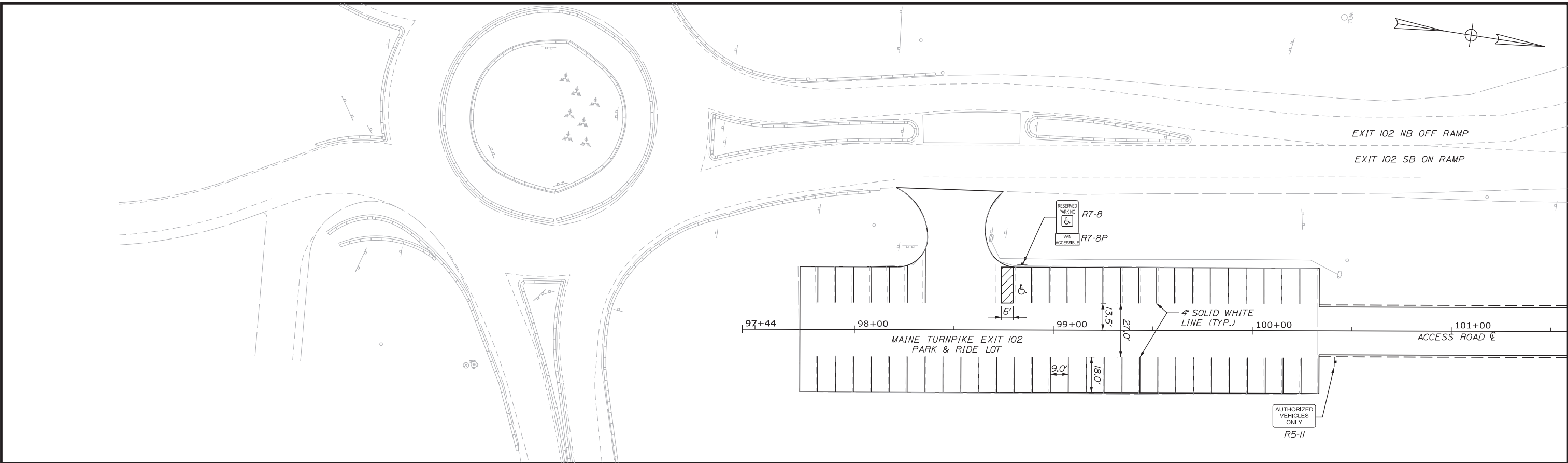
INTERCHANGE 103
 ORT CONVERSION

SIGNING AND STRIPING PLAN 18

SHEET NUMBER: SS-18
 189 OF 503

CONTRACT: 2019.04

Date: 3/20/2019



No.	Revision	By	Date

Designed by:

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CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE

	By	Date	Checked	By	Date
Designed	BMH	3\20\19		DGE	3\20\19
Drawn	MPC	3\20\19	In Charge of	GAE	3\20\19

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MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION

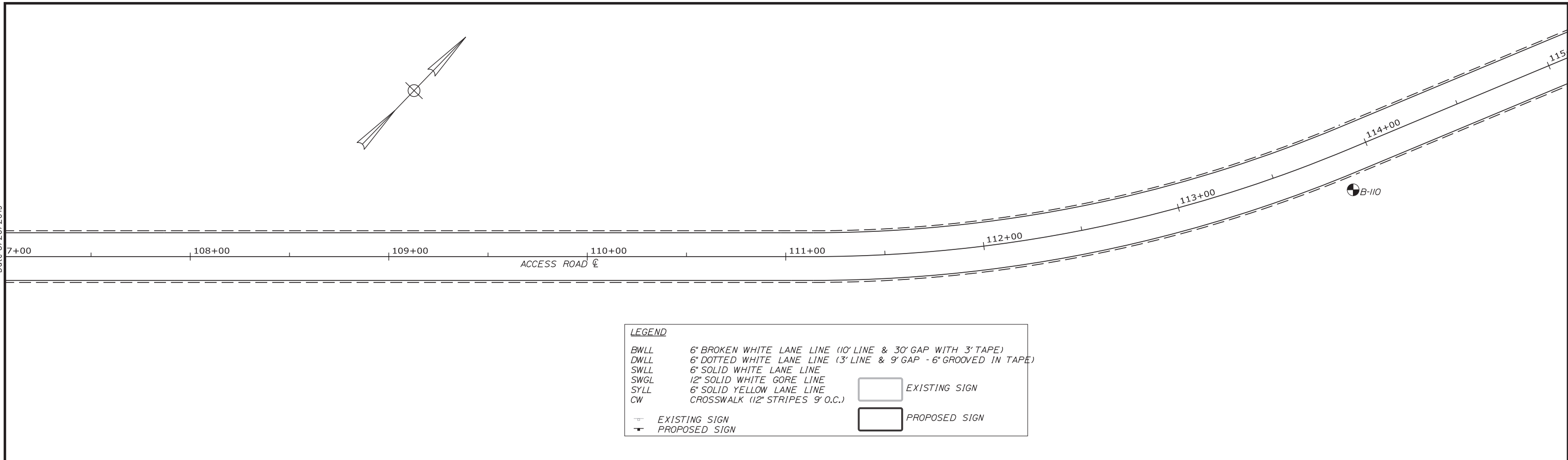
SIGNING AND STRIPING PLAN 19

SHEET NUMBER: SS-19
 190 OF 503

CONTRACT: 2019.04

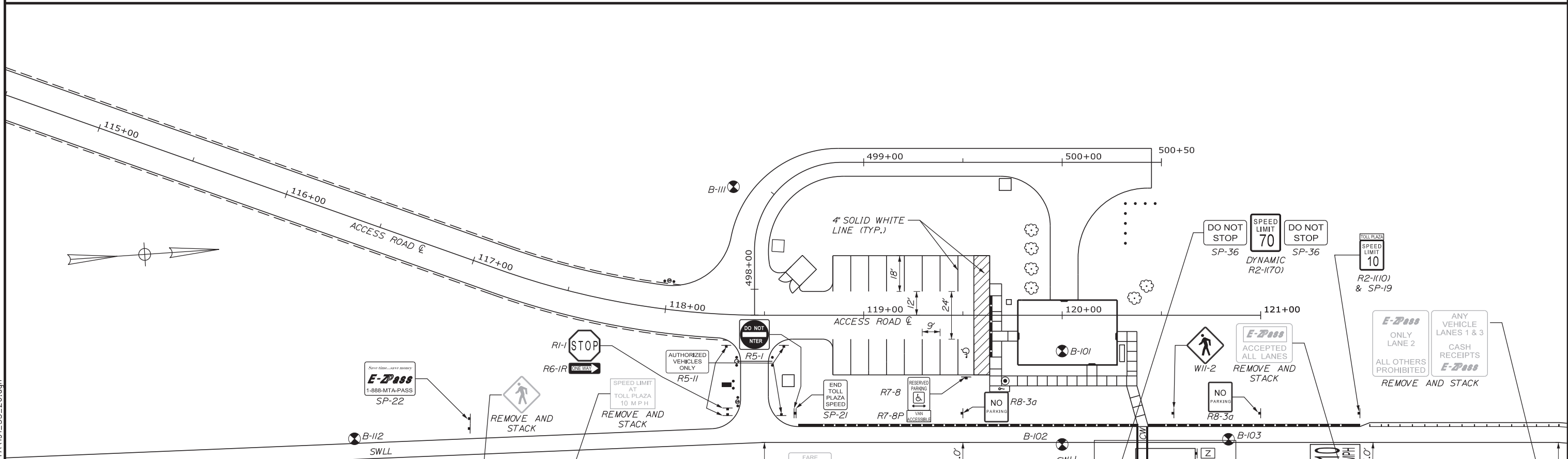
Filename: ...00.HIGHWAY\MSTA\190_SS_19.dgn

Date: 3/20/2019



LEGEND

BWLL	6" BROKEN WHITE LANE LINE (10' LINE & 30' GAP WITH 3' TAPE)	
DWLL	6" DOTTED WHITE LANE LINE (3' LINE & 9' GAP - 6" GROOVED IN TAPE)	
SWLL	6" SOLID WHITE LANE LINE	
SWGL	12" SOLID WHITE GORE LINE	
SYLL	6" SOLID YELLOW LANE LINE	
CW	CROSSWALK (12' STRIPES 9' O.C.)	
	EXISTING SIGN	
	PROPOSED SIGN	



Filename: ...:\00\HIGHWAY\MSTA\191_SS_20.dgn

Scale:

No.	Revision	By	Date

Designed by:

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CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
By	Date	Checked	Date
BMH	3\20\19	DGE	3\20\19
Drawn	MPC	In Charge of	GAE
	3\20\19		3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

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**INTERCHANGE 103
 ORT CONVERSION**

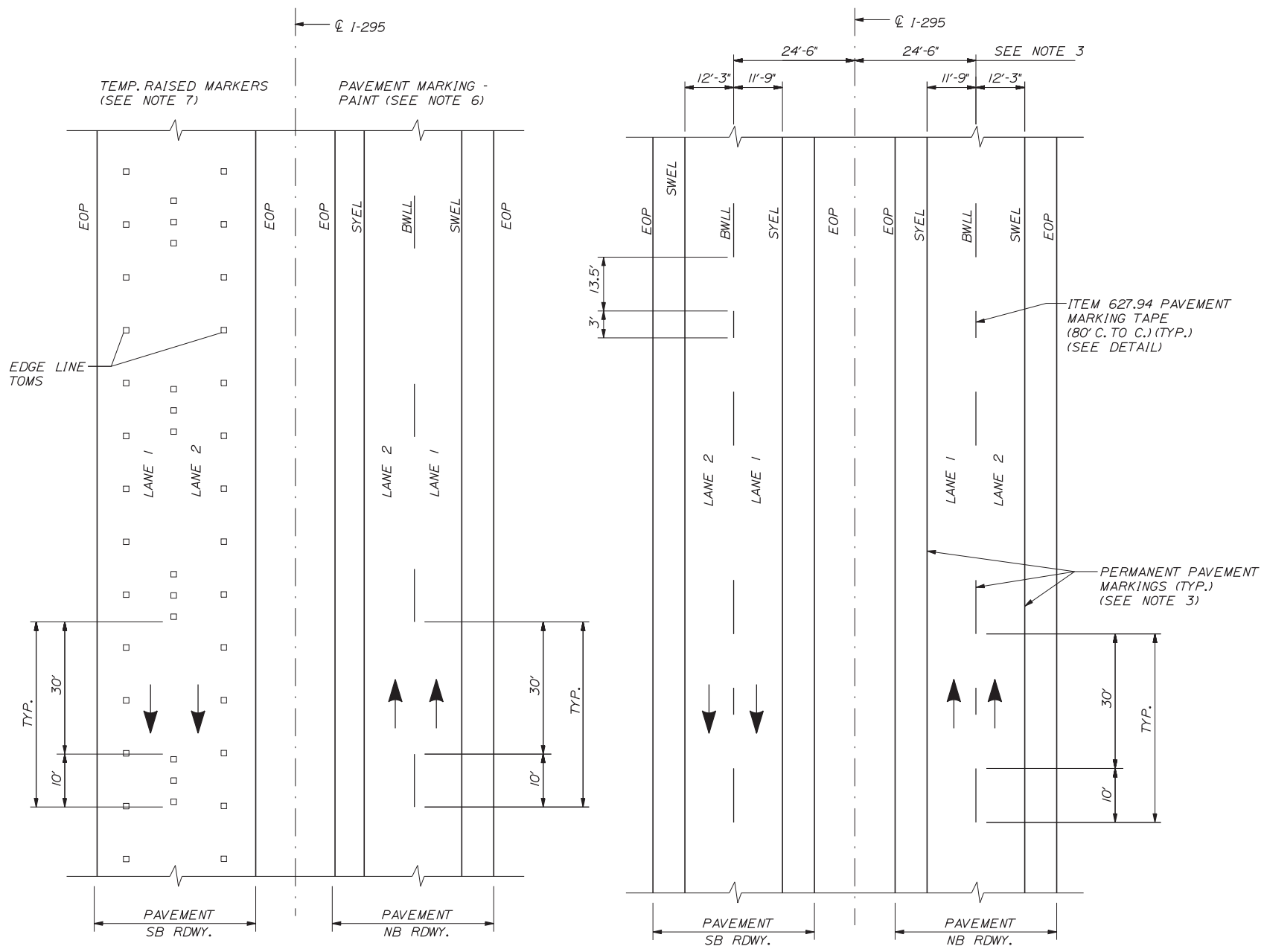
SIGNING AND STRIPING PLAN 20

SHEET NUMBER: SS-20
 191 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...MSTA192_...Stripping_Details.dgn

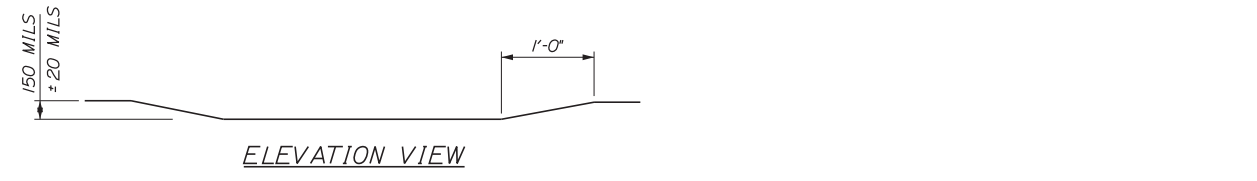


TEMP. PAVEMENT MARKINGS
2 LANE RDWY

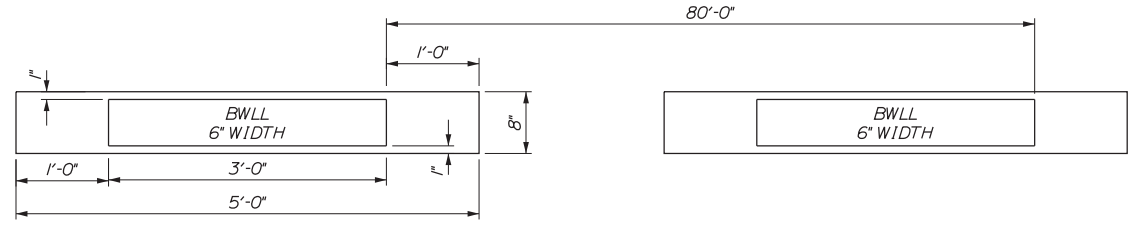
PERMANENT PAVEMENT MARKINGS
2 LANE RDWY

LEGEND

- SYEL = SOLID YELLOW EDGE LINE (6" PAINT)
- SWEL = SOLID WHITE EDGE LINE (6" PAINT)
- BWLL = BROKEN WHITE LANE LINE (6" PAINT)
- EOP = EDGE OF PAVEMENT

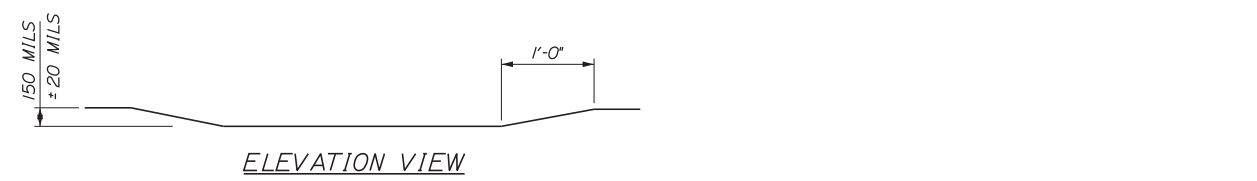


ELEVATION VIEW

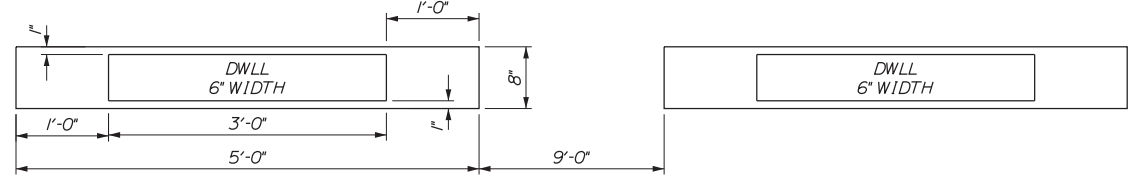


PLAN VIEW

ITEM 627.94 - PERMANENT PAVEMENT MARKINGS WITH TAPE
GROOVING DETAIL
N.T.S.



ELEVATION VIEW



PLAN VIEW

ITEM 627.94I - PAVEMENT MARKING TAPE - DOTTED WHITE LANE LINE (DWLL), 6 INCH WIDTH
GROOVING DETAIL
N.T.S.

NOTES:

1. PERMANENT PAVEMENT MARKINGS SHALL BE 6" WIDE LINES.
2. PERMANENT PAVEMENT MARKINGS SHALL BE INSTALLED ON BOTH ROADWAYS.
3. THE OFFSET DISTANCE SHOWN ARE MEASURED FROM THE CENTER OF THE 6" PAINT LINES.
4. ITEM 627.94I PAVEMENT MARKING TAPE FOR DWLL AND ITEM 627.94 PAVEMENT MARKING TAPE SHALL BE INSTALLED IN AN "IN AND OUT" GROOVING PATTERN. SEE DETAILS THIS SHEET.
5. ALL EXISTING PAINT MARKINGS SHALL BE REMOVED PRIOR TO THE INSTALLATION OF ITEM 627.94I PAVEMENT MARKING TAPE (DWLL) AND SHALL BE INCIDENTAL TO THE INSTALLATION OF THE TAPE.
6. TEMPORARY PAVEMENT MARKINGS ON MILLED PAVEMENT SHALL BE 6" SOLID WHITE, BROKEN WHITE AND SOLID YELLOW PAINT LINES.
7. TEMPORARY PAVEMENT MARKINGS ON NEW HMA 12.5 MM, OR SHIM MATERIAL, SHALL BE TEMPORARY RAISED PAVEMENT MARKERS. "SOLID" WHITE AND "SOLID" YELLOW LANE LINES SHALL BE TEMPORARY RAISED PAVEMENT MARKERS SPACED AT 10'-0" C. TO C. "BROKEN" WHITE LANE LINES SHALL BE TEMPORARY RAISED PAVEMENT MARKERS SPACED 5'-0" C. TO C. EVERY 40'-0". TEMPORARY 6" PAVEMENT MARKING TAPE SHALL BE USED ON RADII ON SURFACE PAVEMENT, A 3' PIECE EVERY 40'.

Scale: Not to Scale			
No.	Revision	By	Date

Designed by:			
CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
	By	Date	
	BMH	3\20\19	
	Checked	DGE	3\20\19
		In Charge of	GAE
			3\20\19

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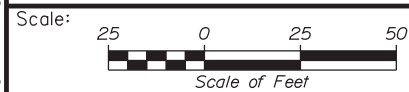
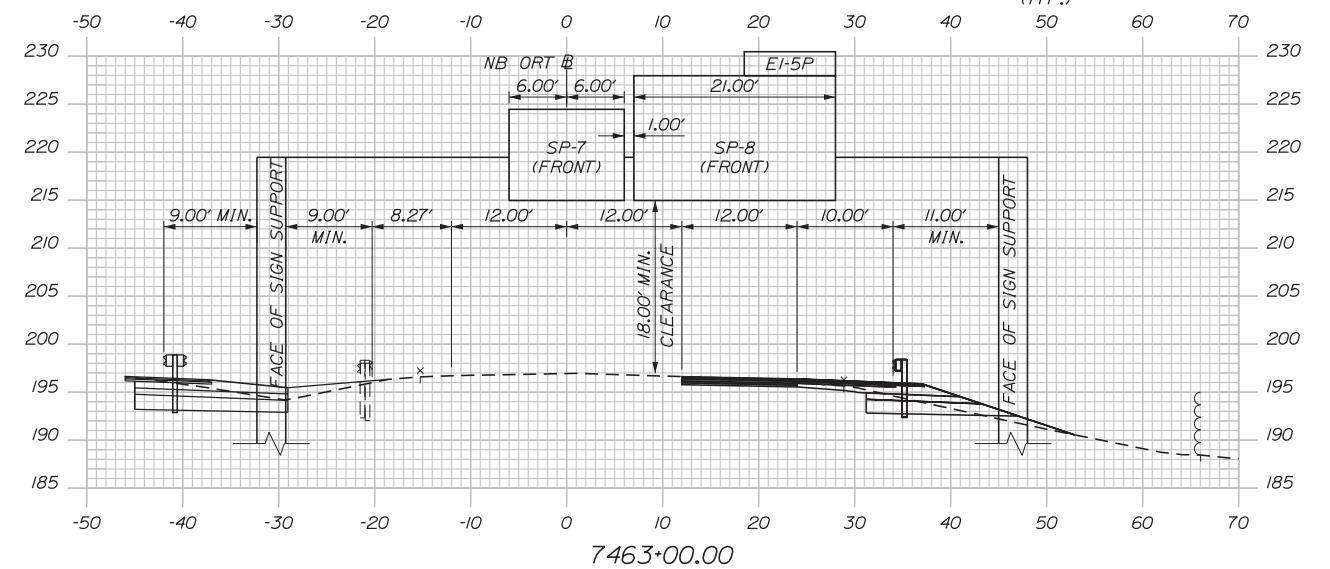
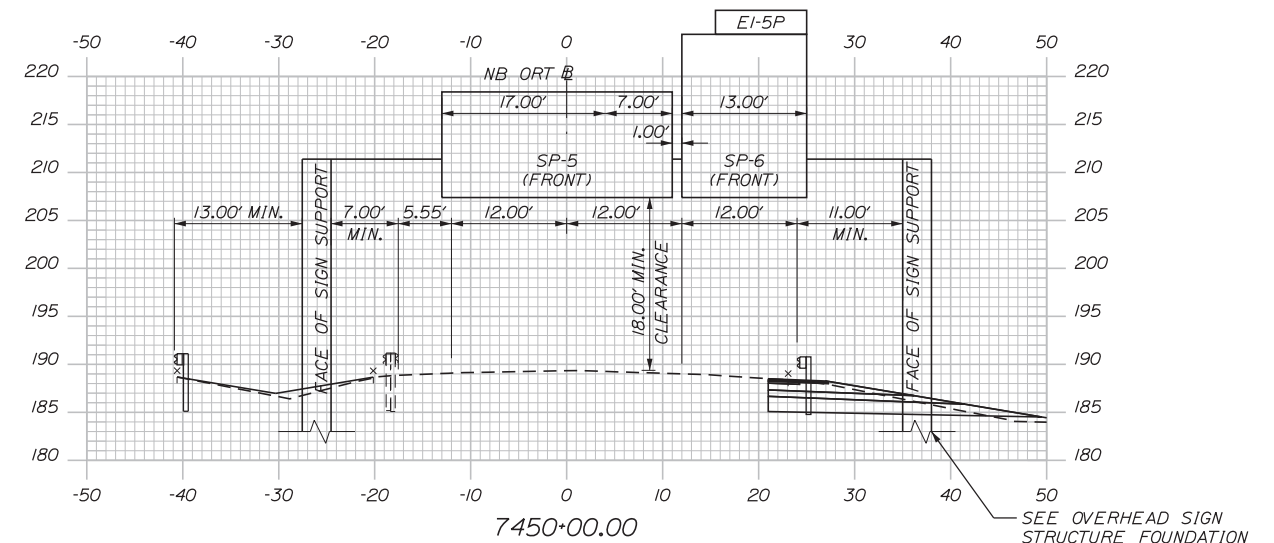
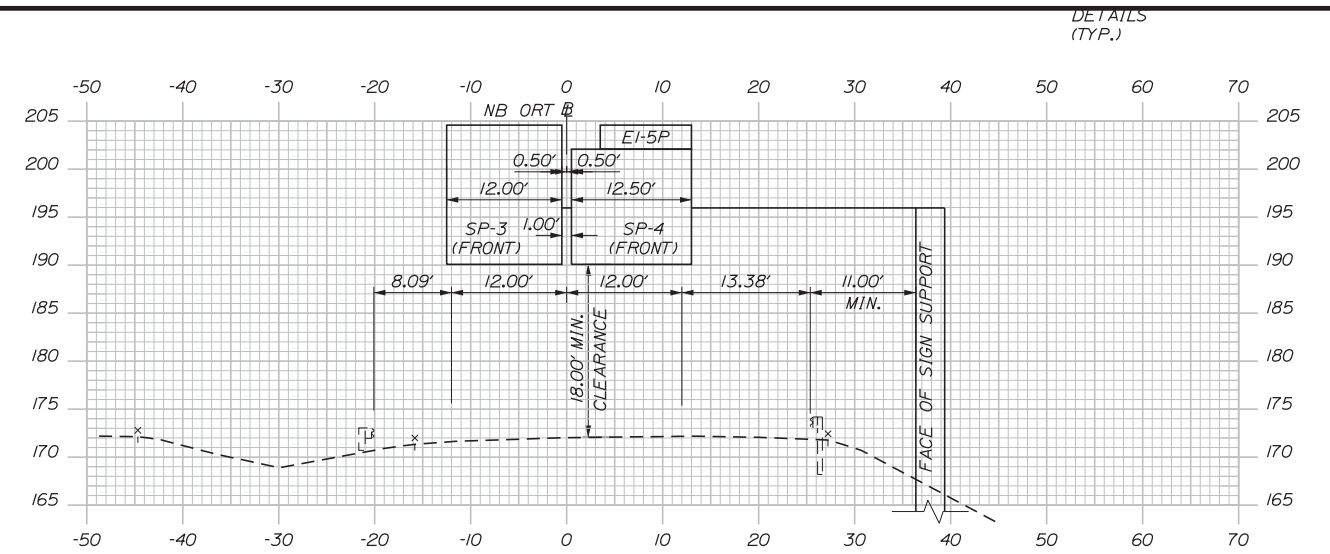
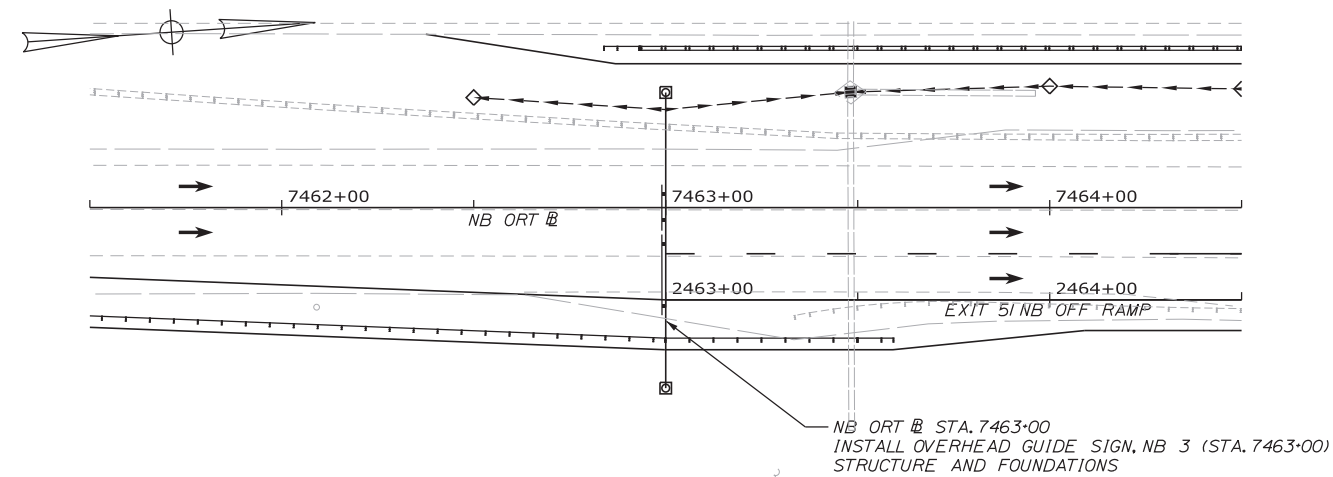
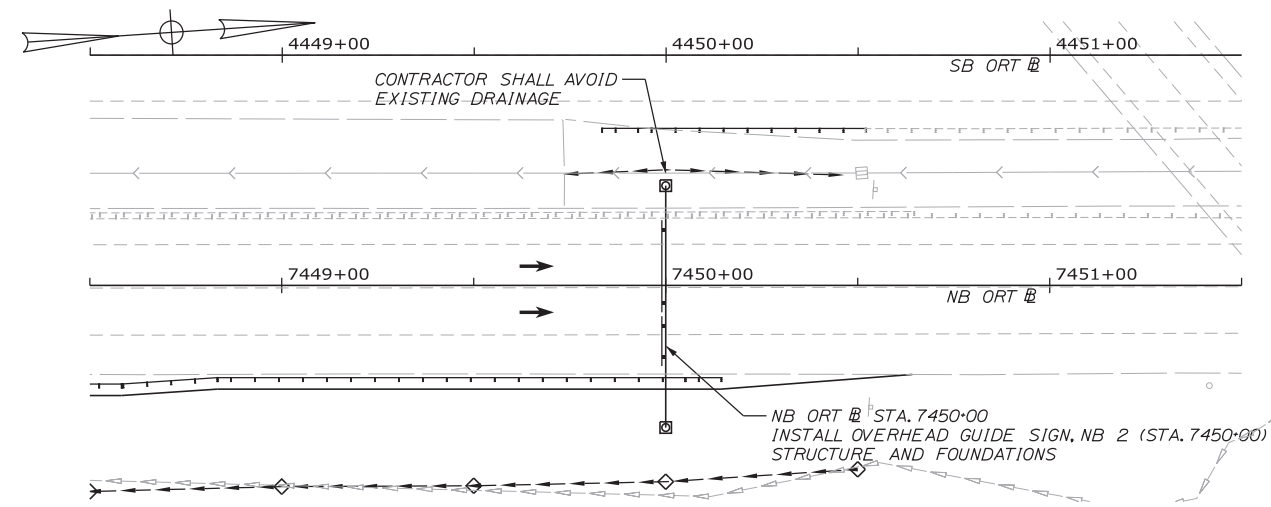
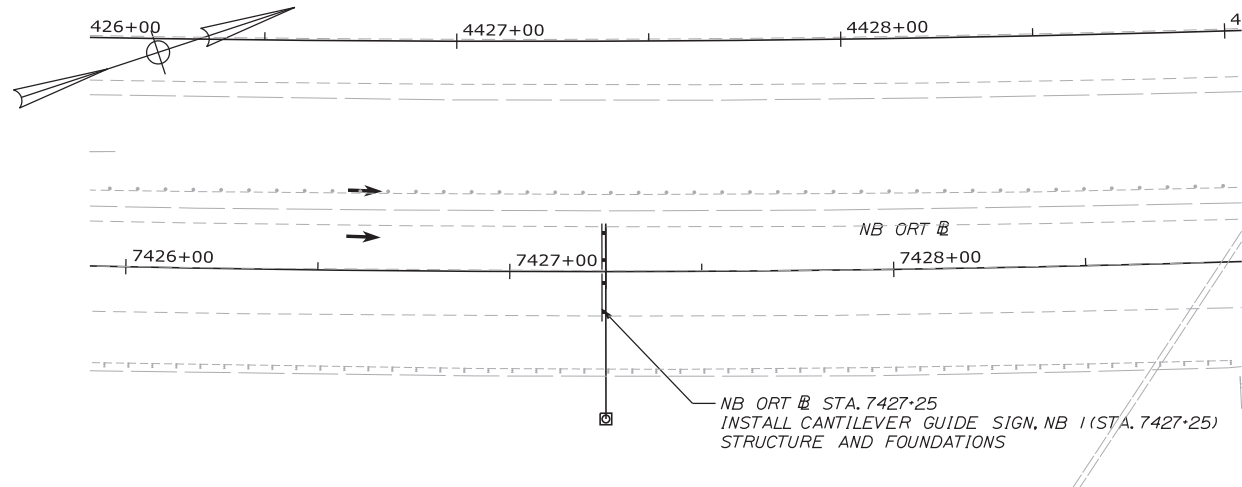
**THE GOLD STAR
MEMORIAL HIGHWAY**

INTERCHANGE 103
ORT CONVERSION
STRIPING DETAILS

SHEET NUMBER: SS-21
CONTRACT: 2019.04
192 OF 503

Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\193_SS_22.dgn



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THE GOLD STAR
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INTERCHANGE 103
ORT CONVERSION
NORTHBOUND OVERHEAD SIGNS 1

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER:	BRANDON M. HAVU, PE		
Designed	By	Date	Checked
Drawn	By	Date	In Charge of
	BMH	3\20\19	DGE
	MPC	3\20\19	GAE

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

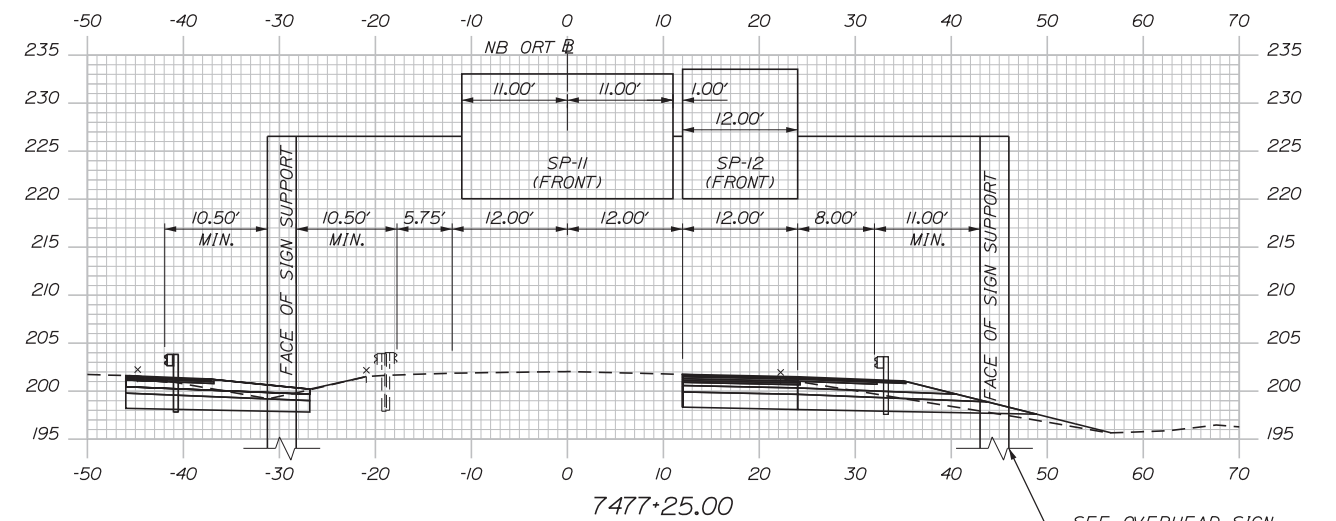
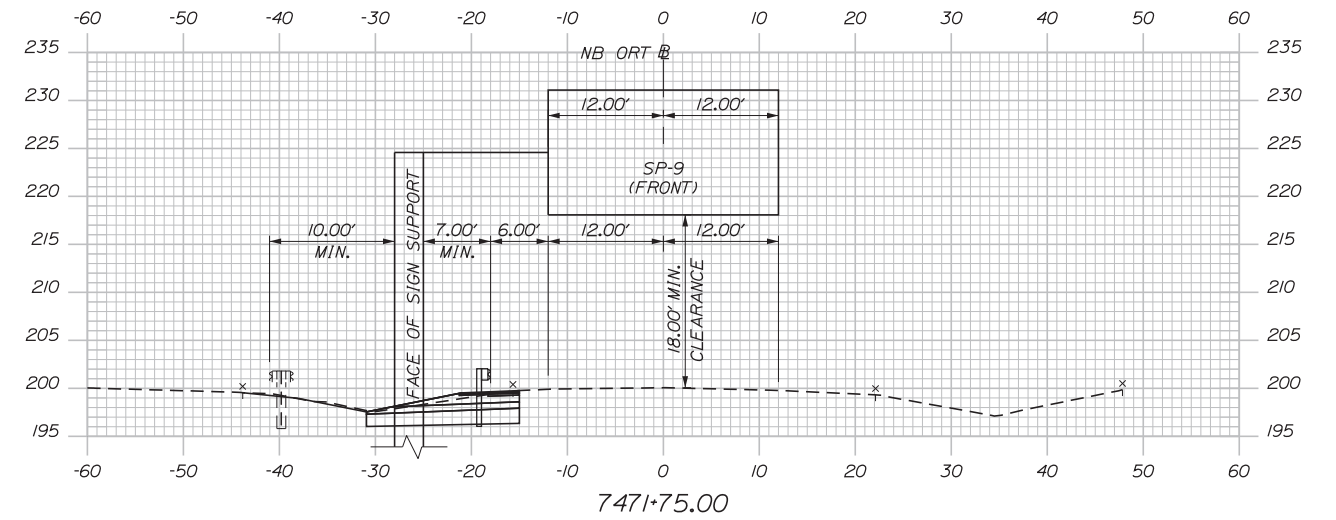
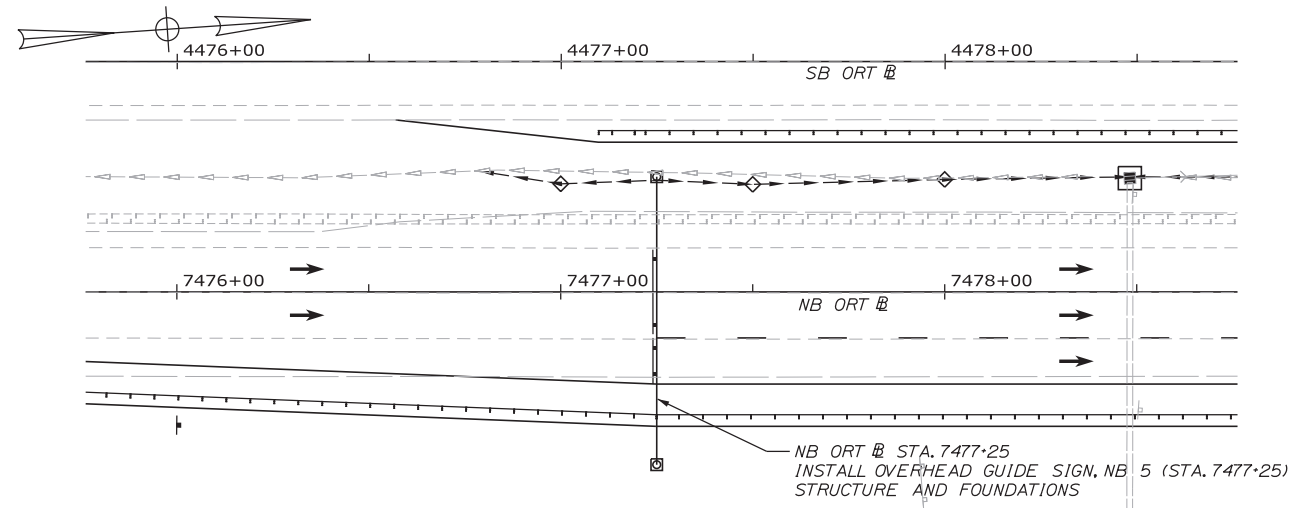
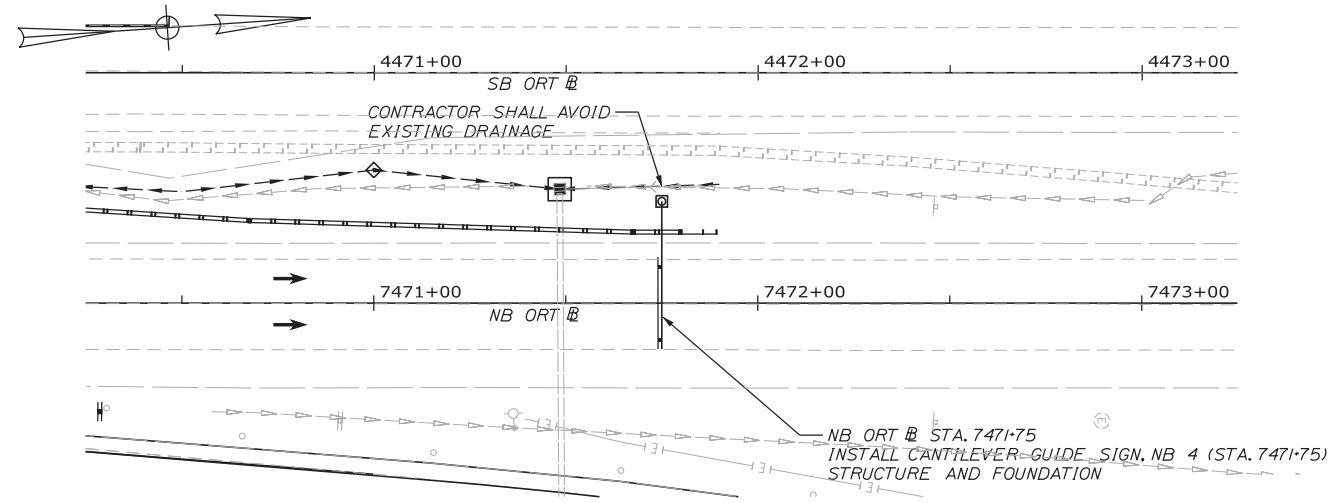
CONTRACT: 2019.04

SHEET NUMBER: SS-22

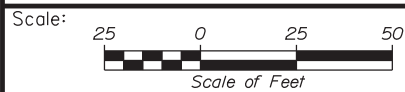
193 OF 503

Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\194_SS_23.dgn



SEE OVERHEAD SIGN STRUCTURE FOUNDATION DETAILS (TYP.)



No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT	PROJECT MANAGER:	BRANDON M. HAVU, PE	
Designed	By	Date	Checked
Drawn	By	Date	In Charge of
	BMH	3\20\19	DGE 3\20\19
	MPC	3\20\19	GAE 3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION

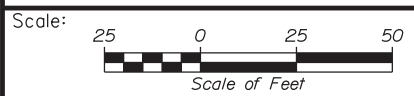
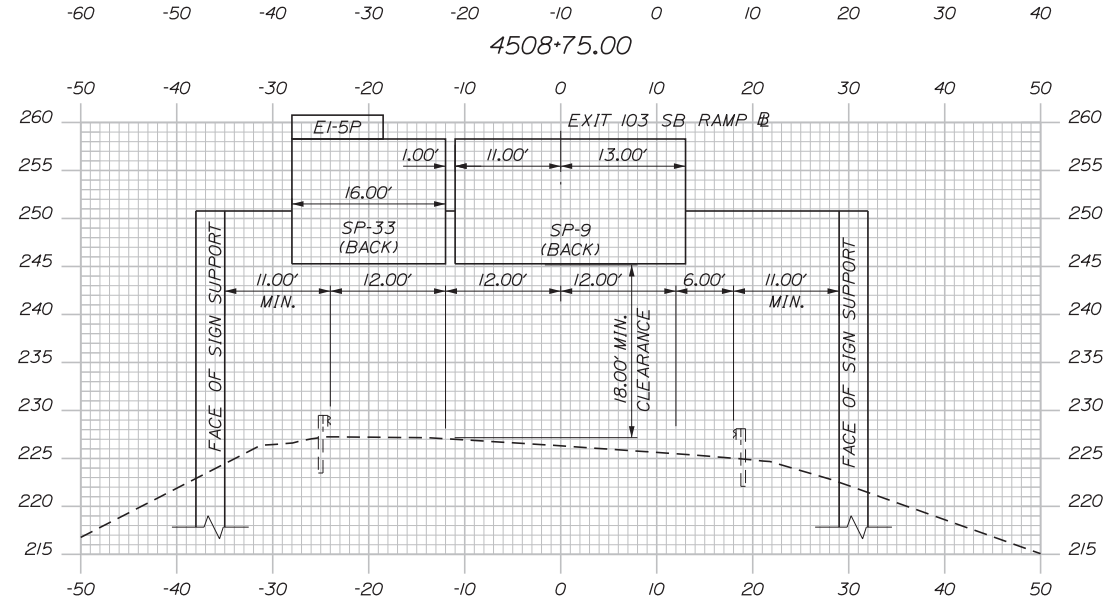
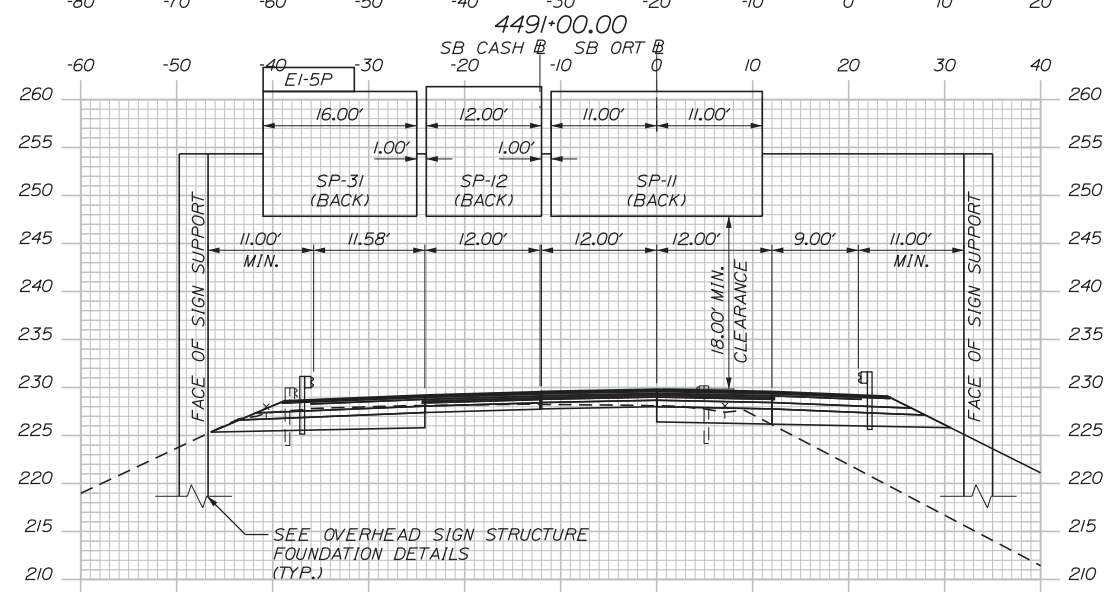
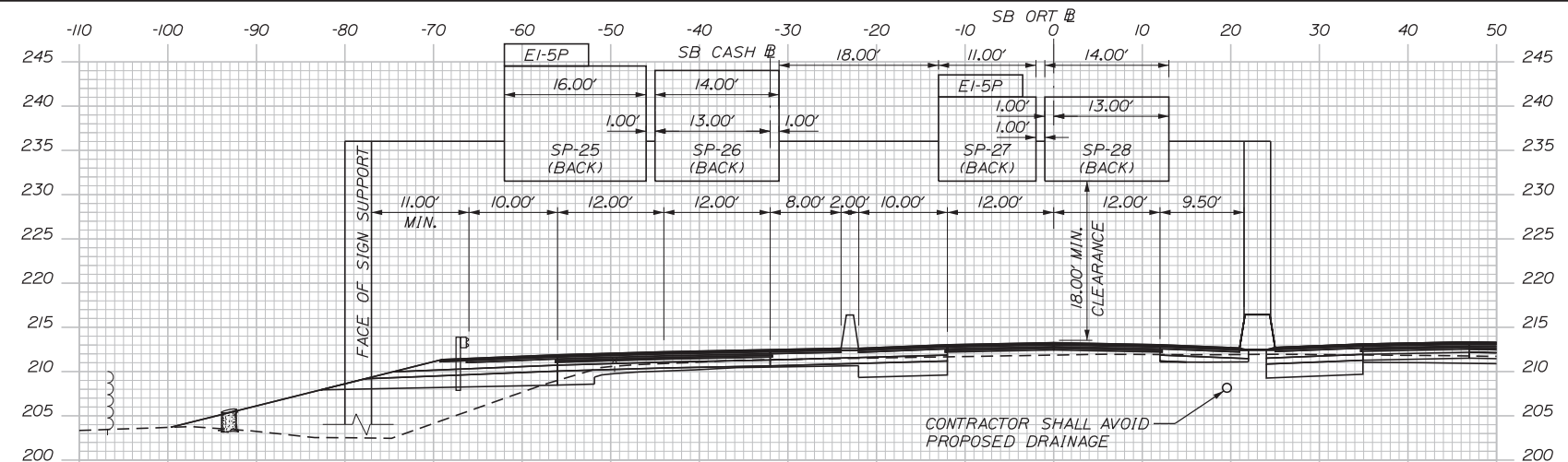
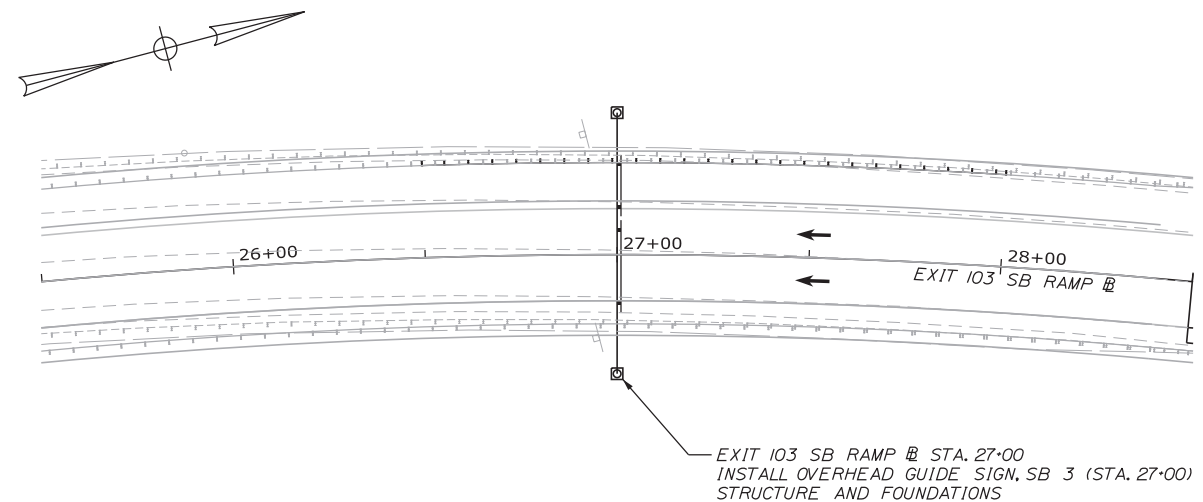
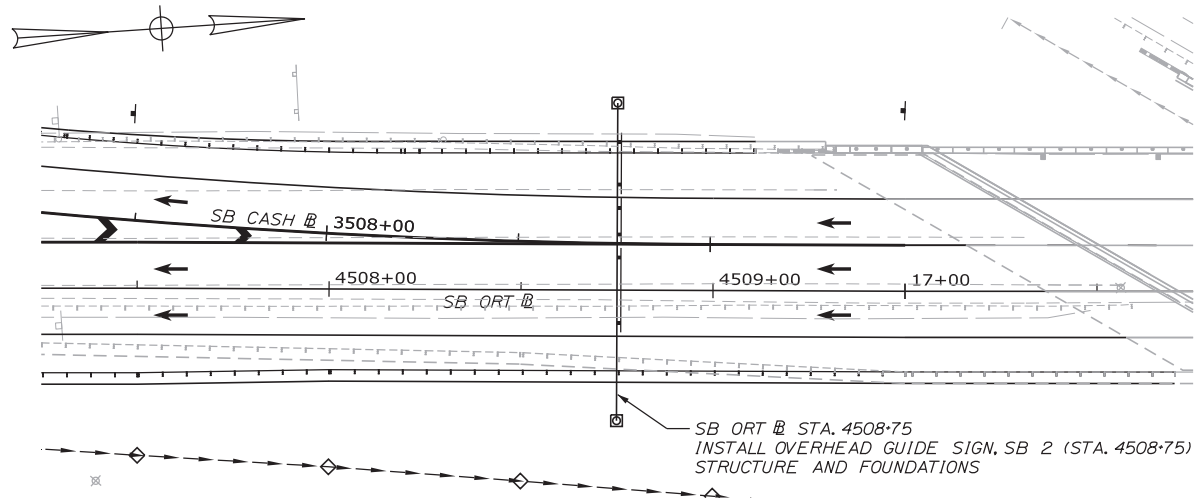
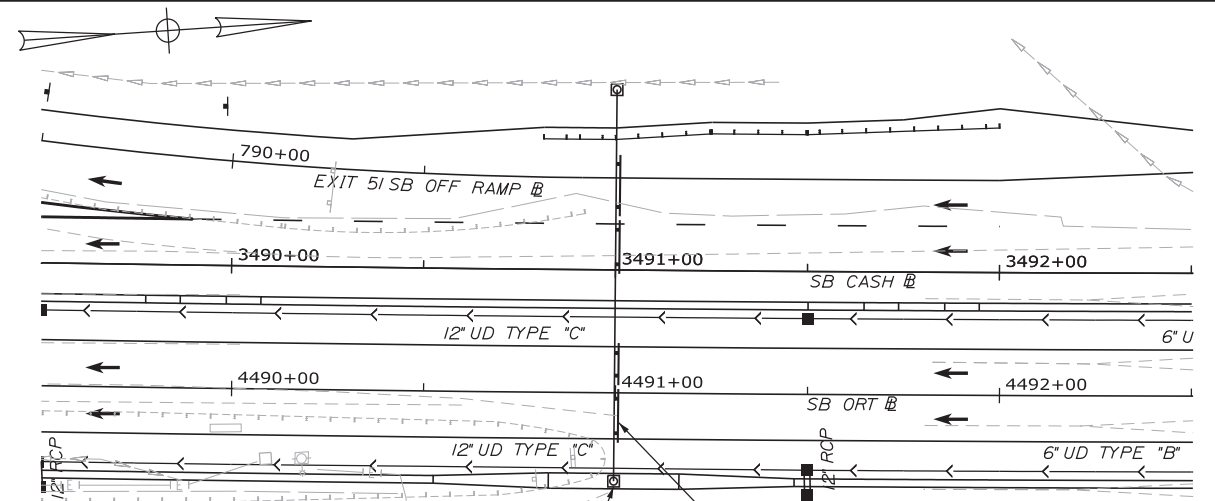
NORTHBOUND OVERHEAD SIGNS 2

SHEET NUMBER: SS-23
 194 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\195_SS_24.dgn



No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

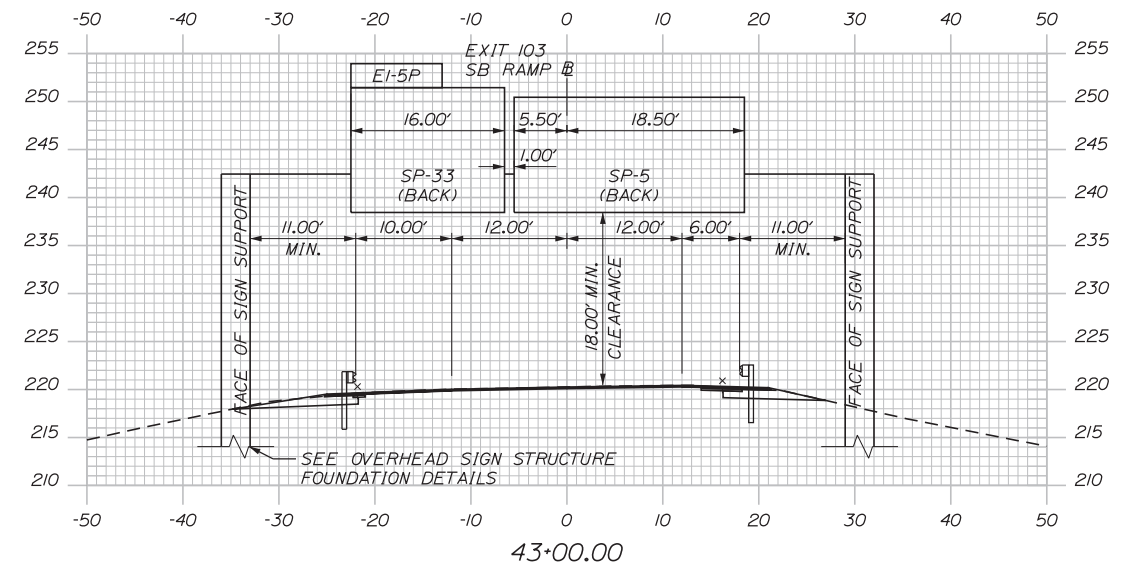
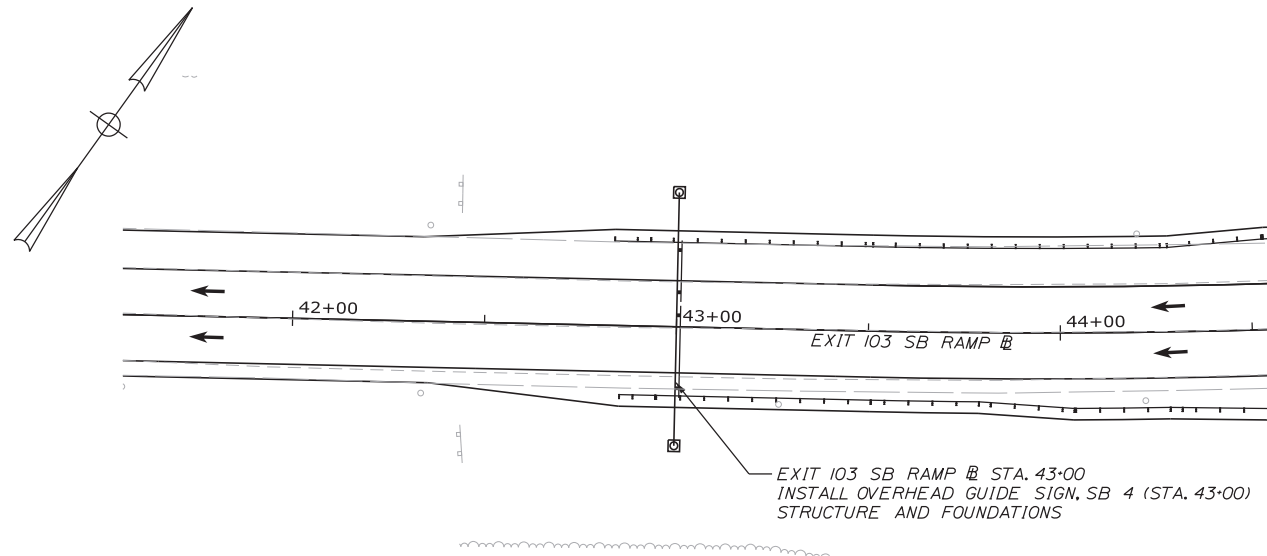
INTERCHANGE 103
ORT CONVERSION

SOUTHBOUND OVERHEAD SIGNS 1

SHEET NUMBER: SS-24
195 OF 503

CONTRACT: 2019.04

Date: 3/20/2019



Filename: ...00\HIGHWAY\MSTA\196_SS_25.dgn



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THE GOLD STAR
MEMORIAL HIGHWAY

INTERCHANGE 103
ORT CONVERSION
SOUTHBOUND OVERHEAD SIGNS 2

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE			
By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

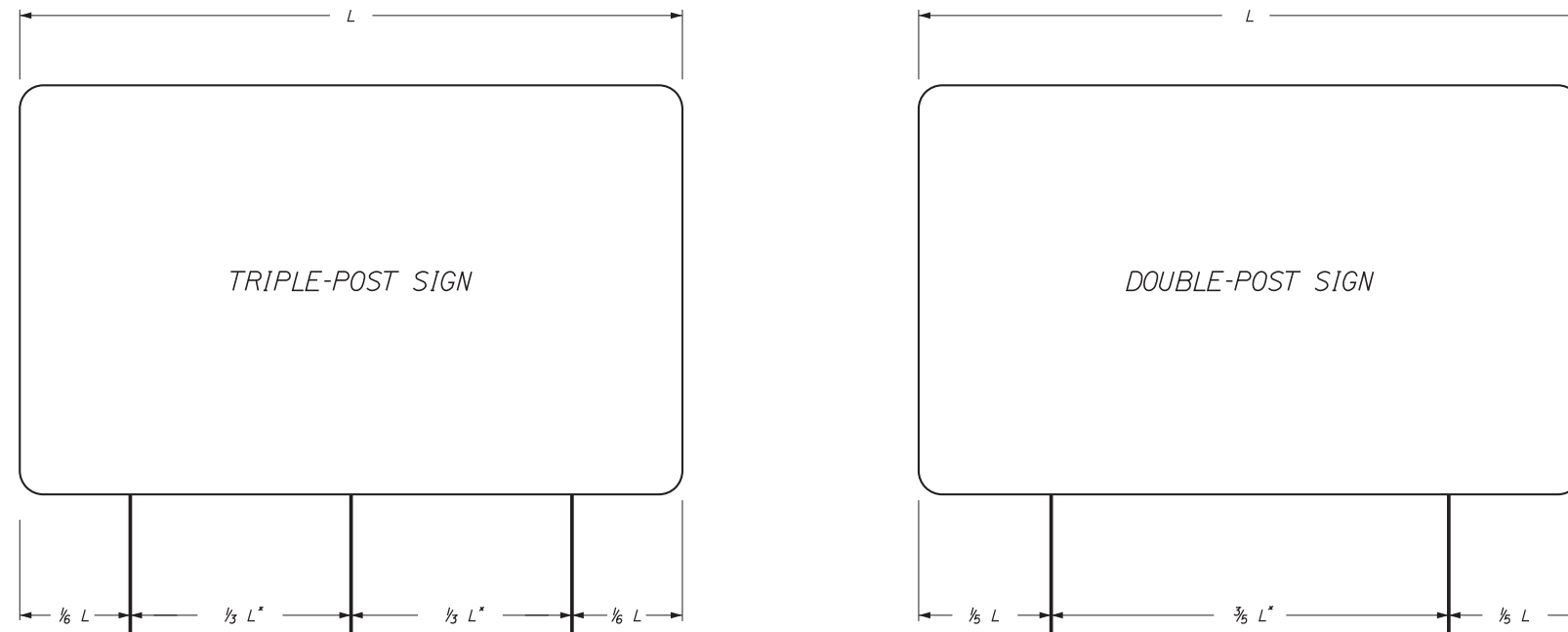
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

CONTRACT: 2019.04

SHEET NUMBER: SS-25

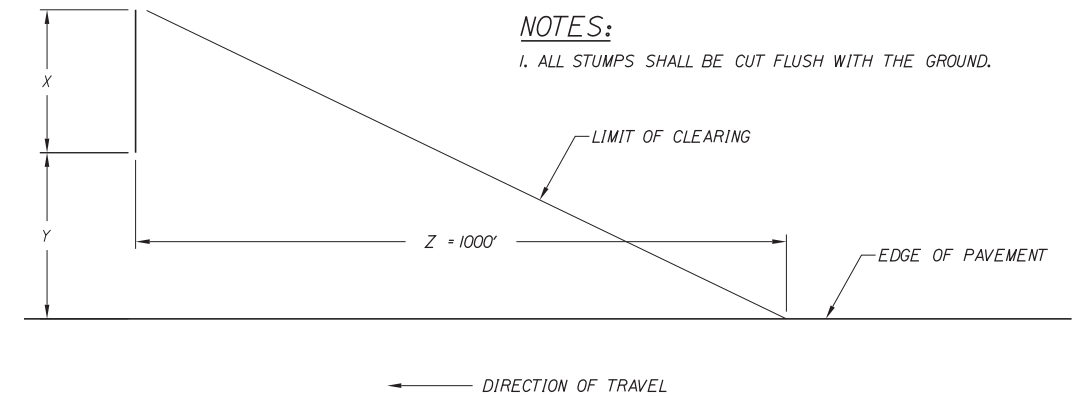
196 OF 503

Date: 3/20/2019



* - A MINIMUM 7 FEET REQUIRED WHEN INSTALLED ON BREAKAWAY DEVICES

POLE SPACING DETAIL
N.T.S.



NOTES:

1. ALL STUMPS SHALL BE CUT FLUSH WITH THE GROUND.

WHERE:

X = WIDTH OF SIGN + 10'
Y = OFFSET TO EDGE OF SIGN FROM EDGE OF PAVEMENT
Z = DISTANCE FROM FACE OF SIGN TO A POINT ON THE EDGE OF PAVEMENT 1000' BACK

CLEARING DETAIL
FOR
MAINLINE SIGN INSTALLATION
N.T.S.

SINGLE SUPPORT SIGNS								
FOUNDATION DIAMETER	SIGN AREA (A)	SIGN WIDTH (W)	POST SIZE	BASE PLATE (1) (3)	MATERIAL	ANCHOR BOLTS (2)	BOLT CIRCLE	MAXIMUM MOUNTING HEIGHT
	$A < 4 \text{ FT}^2$		U CHANNEL POST					12 FEET TO SIGN CENTER
	$4 < A \leq 10 \text{ FT}^2$		WOOD POST 6"X6"					
18"	$10 < A \leq 16 \text{ FT}^2$	W=4'-0" MAX. BUT INCLUDES 5'-0" FIELD SIGN	W6X9	12"X12"X1" 41 LBS.	A36	1" ϕ X 3'-0"	12"	
18"	$16 < A \leq 25 \text{ FT}^2$	W=5'-0" MAX	W6X15	12"X12"X1" 41 LBS.	A36	1" ϕ X 3'-0"	12"	
24"	$25 < A \leq 42 \text{ FT}^2$	W=7'-0" MAX	W8X24	14"X14"X1" 55 LBS.	A36	1-1/4" ϕ X 3'-6"	14"	

MULTIPLE SUPPORT SIGNS								
FOUNDATION DIAMETER	SIGN AREA (A)	SIGN WIDTH (W)	POST SIZE	BASE PLATE (1) (3)	MATERIAL	ANCHOR BOLTS (2)	BOLT CIRCLE	MAXIMUM MOUNTING HEIGHT
24"	TO 60 FT^2 /POST		W8X18	14"X14"X1" 55 LBS.	A36	1-1/4" ϕ X 3'-6"	14"	20 FEET TO SIGN CENTER
24"	60-85 FT^2 /POST		W10X22	12"X17"X1-1/4" 72 LBS.	A36	1-1/4" ϕ X 3'-6"	15"	
30"	85-110 FT^2 /POST		W12X26	13"X19"X1-1/4" 87 LBS.	A36	1-1/2" ϕ X 4'-0"	17"	
30"	110-135 FT^2 /POST		W14X30	14"X21"X1-1/4" 104 LBS.	A36	1-1/2" ϕ X 4'-0"	19"	

GENERAL NOTES

- BOLTS TO BE 50,000 PSI MINIMUM YIELD STRENGTH.
- POST TO BASE PLATE WELD SHALL BE 5/16" FILLET WELD.
- BASE PLATES AND H-BEAMS SHALL BE HOT DIPPED GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH SECTION 720.06.

BREAKAWAY NOTES

- ON 6"X6" WOOD POSTS, DRILL 3/4" DIAMETER HOLES AT RIGHT ANGLES TO ONE ANOTHER 4 INCHES ABOVE GROUND LEVEL TO MEET BREAKAWAY STANDARDS.
- ON BREAKAWAY SUPPORTS, THE HINGE SHOULD BE A MINIMUM OF 7 FEET ABOVE THE FOUNDATION. NO PERMANENT SUPPLEMENTARY SIGNS SHALL BE ATTACHED BELOW THE HINGE. SIGN SHALL BE MOUNTED ABOVE HINGE.
- DIAMETER OF THE FOUNDATION SHALL BE INCREASED IF NEEDED TO LOCATE BREAK-AWAY DEVICE ANCHOR BOLTS WITHIN THE FOUNDATION REINFORCING. ANY NECESSARY INCREASE IN SIZE SHOULD BE CONSIDERED INCIDENTAL TO THE ORIGINAL CONCRETE FOUNDATION SIZE SPECIFIED ON THE PLANS.

REMOVE AND RESET GUIDE SIGN NOTES

- EXISTING SIGN POST LENGTHS ARE APPROXIMATE AND SHALL BE FIELD VERIFIED BY THE CONTRACTOR IF THE POSTS ARE TO BE REUSED.
- PROPOSED POST LENGTHS ARE MEASURED FROM THE TOP OF THE FOUNDATION TO THE TOP OF SIGN AND ARE APPROXIMATE. FINAL POST LENGTHS SHALL BE DETERMINED BY THE CONTRACTOR ONCE FOUNDATIONS ARE PLACED.

Filename: ...MSTA\197_Signing-Details.dgn

Scale: *Not to Scale*

No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE

By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

SIGNING DETAILS

SHEET NUMBER: SS-26
197 OF 503

CONTRACT: 2019.04

REMOVE AND STACK SIGN SUMMARY - POST MOUNTED

DESCRIPTION	QUANTITY	SIGN SIZE (CONTRACTOR TO VERIFY)			COLOR		DIRECTION (SB-SOUTHBOUND) (NB-NORTHBOUND)	LOCATION
		HEIGHT (IN.)	WIDTH (IN.)	AREA (SQ. FT.)	BACKGROUND	LEGEND		
EXIT 51	1	30	66	13.75 (13.75)				
Visitor Info Center	1	66	108	49.50 (49.50)	BLUE	WHITE	NB	STA. 7401+25, RT.
Truck Stop Power	1	24	108	18.00 (18.00)	BLUE	WHITE		
Heat A/C Internet	1	24	108	18.00 (18.00)	BROWN	WHITE		
Center for Maine Craft	1	24	108	18.00 (18.00)	BROWN	WHITE		
TOLL PLAZA 1 MILE	1	72	150	75.00 (75.00)	YELLOW	BLACK	NB	STA. 7436+10, RT.
CARS \$1.00	1	24	120	20.00 (20.00)	YELLOW	BLACK		
TO SOUTH Lewiston Auburn NEXT RIGHT	1	120	168	140.00 (140.00)	GREEN	WHITE	NB	STA. 7447+85, RT.
CHECK BRAKES	4	36	42	10.50 (42.00)	YELLOW	BLACK	NB NB SB SB	STA. 7450+53, LT. STA. 7450+53, RT. STA. 42+45, LT. STA. 42+45, RT.
REDUCED SPEED AHEAD	8	30	24	5.00 (40.00)	WHITE	BLACK	NB NB NB SB SB SB	STA. 7472+46, LT. & RT. STA. 8485+58, RT. STA. 7486+73, LT. STA. 3496+05, LT. & RT. STA. 26+90, LT. & RT.
TOLL PLAZA 1500 FT	2	72	150	75.00 (150.00)	YELLOW	BLACK	NB SB	STA. 7477+95, RT. STA. 3507+90, LT.
CARS \$1.00	2	24	120	20.00 (40.00)	YELLOW	BLACK		
RUMBLE STRIP AHEAD	2	48	48	16.00 (32.00)	YELLOW	BLACK	NB	STA. 7478+50, LT. STA. 7478+50, RT.
SPEED LIMIT 35	2	60	48	20.00 (40.00)	WHITE	BLACK	NB	STA. 7479+35, LT. STA. 7479+35, RT.
E-Pass ONLY LANE 4 ALL OTHERS PROHIBITED	2	120	96	80.00 (160.00)	PURPLE	WHITE	NB SB	STA. 7482+88, LT. STA. 4502+41, RT.
E-Pass ONLY LANE 2 ALL OTHERS PROHIBITED ANY VEHICLE LANES 1 & 3 CASH RECEIPTS E-Pass	2	120	192	160.00 (320.00)	PURPLE/ WHITE	WHITE/ BLACK	NB SB	STA. 7482+88, RT. STA. 3502+43, LT.
END TOLL PLAZA SPEED	2	60	48	20.00 (40.00)	WHITE	BLACK	SB NB	STA. 3487+45, LT. STA. 8495+50, RT.
SPEED LIMIT AT TOLL PLAZA 10 MPH	2	84	108	63.00 (126.00)	YELLOW	BLACK	NB SB	STA. 8484+28, RT. STA. 3497+53, LT.
E-Pass ACCEPTED ALL LANES	3	96	120	80.00 (240.00)	GREEN/ PURPLE	WHITE	NB EXIT 51 NB ON RAMP SB	STA. 8484+96, RT. STA. 886+81, RT. STA. 3501+48, LT.

REMOVE AND STACK SIGN SUMMARY - POST MOUNTED

DESCRIPTION	QUANTITY	SIGN SIZE (CONTRACTOR TO VERIFY)			COLOR		DIRECTION (SB-SOUTHBOUND) (NB-NORTHBOUND)	LOCATION
		HEIGHT (IN.)	WIDTH (IN.)	AREA (SQ. FT.)	BACKGROUND	LEGEND		
TOLL PLAZA 600 FT	1	72	156	78.00 (78.00)	YELLOW	BLACK	EXIT 51 NB ON RAMP	STA. 885+75, RT.
CARS \$1.00	1	24	120	20.00 (20.00)	YELLOW	BLACK		
FARE SCHEDULE SIGN	3	84	144	84.00 (252.00)	WHITE	BLACK	NB EXIT 51 NB ON RAMP SB	STA. 8486+96, RT. STA. 888+27, RT. STA. 3499+01, LT.
↑	2	48	48	16.00 (32.00)	YELLOW	BLACK	NB NB	STA. 8487+41, RT. STA. 5244+85, RT.
WRONG WAY	3	30	42	8.75 (26.25)	RED	WHITE	SB NB NB	STA. 4487+92, RT. STA. 3496+05, LT. STA. 3496+05, RT.
YIELD	2	60	60	25.00 (50.00)	RED/ WHITE	RED	EXIT 51 NB ON RAMP NB	STA. 888+69, RT. STA. 7535+60, RT.
PEDESTRIAN	2	48	48	16.00 (32.00)	YELLOW	BLACK	EXIT 51 NB ON RAMP SB	STA. 889+66, LT. STA. 3497+07, LT.
EXIT 51 WEST GARDINER SERVICE PLAZA FOOD - FUEL - ATM	1	30	60	12.50 (12.50)	BLUE	WHITE		
CENTER FOR MAINE CRAFT	1	84	204	119.00 (119.00)	BLUE	WHITE	SB	STA. 3490+25, LT.
VISITOR INFO	1	24	192	32.00 (32.00)	BROWN	WHITE		
?	1	12	156	13.00 (13.00)	BLUE	WHITE		
?	2	24	24	4.00 (8.00)	BLUE	WHITE		
TOLL PLAZA SPEED LIMIT 10	2	12	48	4.00 (8.00)	YELLOW	BLACK	NB SB	STA. 8490+82, RT. STA. 3494+00, LT.
DO NOT ENTER	3	48	48	16.00 (48.00)	WHITE/ RED	WHITE	SB NB NB	STA. 7490+79, LT. STA. 3495+20, LT. STA. 3495+20, RT.
NO U-TURN	2	36	36	9.00 (18.00)	WHITE	RED/ BLACK	NB SB	STA. 7490+89, LT. STA. 4495+08, RT.
↑	1	60	48	20.00 (20.00)	WHITE	BLACK	SB	STA. 4490+89, RT.
↑	1	18	18	2.25 (2.25)	YELLOW	BLACK/ YELLOW		
↑	1	60	48	20.00 (20.00)	WHITE	BLACK		
↑	1	48	36	12.00 (12.00)	WHITE	BLACK	NB	STA. 7495+08, LT.
↑	1	18	18	2.25 (2.25)	YELLOW	BLACK/ YELLOW		
UNDERCLEARANCE TURNPIKE STRUCTURES 13' 6"	1	48	60	20.00 (20.00)	YELLOW	BLACK	NB	STA. 8492+27, RT.

Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\198_SS_27.dgn

Scale:

No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE

By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
SIGN SUMMARY -
REMOVE AND STACK 1

SHEET NUMBER: SS-27
198 OF 503

CONTRACT: 2019.04

REMOVE AND STACK SIGN SUMMARY - POST MOUNTED

REMOVE AND STACK SIGN SUMMARY - COLUMN AND BOOTH MOUNTED

DESCRIPTION	QUANTITY	SIGN SIZE (CONTRACTOR TO VERIFY)			COLOR		DIRECTION (SB-SOUTHBOUND) (NB-NORTHBOUND)	LOCATION
		HEIGHT (IN.)	WIDTH (IN.)	AREA (SQ. FT.)	BACKGROUND	LEGEND		
	3	30	24	5.00 (15.00)	WHITE	BLACK	NB NB NB	STA. 8494*26, RT. STA. 8500*36, RT. STA. 8504*94, RT.
	1	18	12	1.50 (1.50)	BLUE/ WHITE	WHITE/ GREEN	NB	STA. 8494*43, RT.
	3	18	24	3.00 (9.00)	GREEN	WHITE	NB NB NB	STA. 8494*76, RT. STA. 8495*11, RT. STA. 8495*37, RT.
	1	108	72	54.00 (54.00)	RED/ WHITE	BLACK/ GOLD/ WHITE	NB	STA. 7496*29, LT.
	2	48	60	20.00 (40.00)	PURPLE	WHITE	NB SB	STA. 8500*36, RT. STA. 3502*30, RT.
	2	48	96	32.00 (64.00)	WHITE	BLACK	NB SB	STA. 7485*58, LT. STA. 4500*53, RT.
	2	60	48	20.00 (40.00)	WHITE	BLACK	SB	STA. 3504*03, LT. STA. 3504*05, RT.
	1	96	48	32.00 (32.00)	GREEN	WHITE	NB	STA. 8504*94, RT.
	1	12	36	3.00 (3.00)	YELLOW	BLACK	NB	STA. 5244*85, RT.
	2	48	48	16.00 (32.00)	YELLOW	BLACK/ RED BLACK	NB	STA. 7519*10, LT.&RT.
	1	24	60	10.00 (10.00)	GREEN	WHITE	SB	STA. 22*96, LT.
	1	144	144	144.00 (144.00)	GREEN	WHITE	SB	STA. 22*96, LT.
	1	36	36	9.00 (9.00)	GREEN	WHITE	SB	STA. 22*96, LT.
	1	24	60	10.00 (10.00)	BLUE	WHITE	SB	STA. 31*91, LT.
	1	150	168	175.00 (175.00)	BLUE	WHITE	SB	STA. 31*91, LT.
	1	24	168	28.00 (28.00)	BROWN	WHITE	SB	STA. 31*91, LT.
	1	48	72	24.00 (24.00)	YELLOW	BLACK	SB	STA. 33*72, RT.

DESCRIPTION	QUANTITY	SIGN SIZE (CONTRACTOR TO VERIFY)			COLOR		DIRECTION (SB-SOUTHBOUND) (NB-NORTHBOUND)	LOCATION
		HEIGHT (IN.)	WIDTH (IN.)	AREA (SQ. FT.)	BACKGROUND	LEGEND		
	2	72	54	27.00 (54.00)	YELLOW	BLACK	NB SB	STA. 7492*92, 12' RT. STA. 4493*05, 25' LT.
	2	72	54	27.00 (54.00)	YELLOW	BLACK	NB SB	STA. 7492*92, 21' LT. STA. 4493*05, 8' RT.
	4	36	30	7.50 (30.00)	WHITE	BLACK	NB NB SB SB	STA. 7492*92, 3' LT. STA. 7492*92, 38' LT. STA. 4493*05, 10' LT. STA. 4493*05, 6' RT.
	5	36	36	9.00 (45.00)	YELLOW	BLACK	NB NB SB SB SB	STA. 7492*92, 7' LT. STA. 8492*80, 5' RT. STA. 4493*05, 6' LT. STA. 4493*05, 27' RT. STA. 3493*20, 15' LT.
	2	36	36	9.00 (18.00)	PURPLE	WHITE	NB SB	STA. 7492*70, 5' LT. STA. 4493*26, 9' LT.
	2	36	36	9.00 (18.00)	PURPLE	BLACK	NB SB	STA. 7492*70, 12' RT. STA. 4493*26, 25' LT.

REMOVE AND STACK SIGN SUMMARY - CANOPY/BRIDGE MOUNTED

DESCRIPTION	QUANTITY	SIGN SIZE (CONTRACTOR TO VERIFY)			COLOR		DIRECTION (SB-SOUTHBOUND) (NB-NORTHBOUND)	LOCATION
		HEIGHT (IN.)	WIDTH (IN.)	AREA (SQ. FT.)	BACKGROUND	LEGEND		
	4	72	120	60.00 (120.00)	DYNAMIC		NB SB NB SB	STA. 7492*68, 30' LT. STA. 4493*28, 30' RT. WEST GARDNER I-95 TOLL PLAZA
	2	48	144	48.00 (96.00)	PURPLE	WHITE	NB SB	STA. 7492*68, 6' RT. SA. 4493*28, 17' LT.
	4	48	144	48.00 (192.00)	WHITE	BLACK	NB NB SB SB	STA. 7492*68, 12' LT. STA. 7492*68, 24' RT. STA. 4493*28, 0' LT. STA. 4493*28, 34' LT.

Date: 3/20/2019

Filename: ...000.HIGHWAY\MSTA\199_SS_28.dgn

Scale:			
No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE

By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY





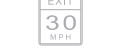







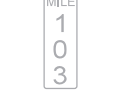
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
SIGN SUMMARY -
REMOVE AND STACK 2

SHEET NUMBER: SS-28
199 OF 503

CONTRACT: 2019.04

REMOVE AND RESET SIGN SUMMARY

DESCRIPTION	EXISTING						PROPOSED							
	SIGN SIZE (CONTRACTOR TO VERIFY)			NUMBER AND TYPE OF POSTS	COLOR		DIRECTION (SB-SOUTHBOUND) (NB-NORTHBOUND)	LOCATION	NUMBER OF POSTS, TYPE AND SIZE (CONTRACTOR TO VERIFY)	FOUNDATION SIZE AND TYPE (CONTRACTOR TO VERIFY)	BREAKAWAY DEVICE	DIRECTION (SB-SOUTHBOUND) (NB-NORTHBOUND)	LOCATION	PAY ITEM NUMBER
	HEIGHT (IN.)	WIDTH (IN.)	AREA (SQ. FT.)		BACKGROUND	LEGEND								
	144	174	174.00	2 STEEL	BLUE	WHITE	NB	STA. 7428+89, RT.	SIGN TO BE PLACED ON EXISTING POSTS AND FOUNDATION (CONTRACTOR TO REVIEW SIZE AND CONDITION)	SIGN TO BE PLACED ON EXISTING POSTS AND FOUNDATION (CONTRACTOR TO REVIEW SIZE AND CONDITION)	YES	NB	STA. 7401+23, RT.	645.501
	120	120	100.00	2 STEEL	BLUE	WHITE	NB	STA. 7458+48, RT.	2 - W8X18	24" DIA.	YES	NB	STA. 7440+50, RT.	645.502
	30	48	100	1 WOOD	WHITE	BLACK	SB	STA. 3487+12, LT.	1 - WOOD POST	N/A	YES	SB	STA. 4458+00, LT.	645.109
	42	48	14.00	MOUNTED TO OVERHEAD SIGN STRUCTURE	BLUE	WHITE	NB	STA. 7466+75, RT.	1 - W8X24	24" DIA.	YES	NB	STA. 7467+00, RT.	645.503
	48	36	12.00		YELLOW	BLACK	NB							
	72	60	30.00	1 STEEL 1 STEEL	GREEN	WHITE	NB SB	STA. 7470+91, RT. STA. 3488+06, LT.	1 - W8X24 1 - W8X24	24" DIA. 24" DIA.	YES YES	NB SB	STA. 7470+29, RT. STA. 3488+27, LT.	645.504 645.505
	30	42	8.75	1 STEEL 1 STEEL 1 STEEL	RED	WHITE	NB NB NB	STA. 888+69, RT. STA. 7470+91, RT. STA. 8487+41, RT.	1 - WOOD POST	N/A	YES YES YES	NB NB NB	STA. 888+69, RT. STA. 7470+29, RT. STA. 8487+38, RT.	645.109
	12 36	24 36	2.00 9.00	1 STEEL	BLUE	WHITE	SB	STA. 3481+50, LT.	1 - W6X9	18" DIA.	YES	SB	STA. 4474+00, LT.	645.506
	30	48	10.00	1 WOOD	WHITE	BLACK	SB	STA. 3483+73, LT.	1 - WOOD POST	N/A	YES	SB	STA. 3483+75, LT.	645.109
	48	36	12.00	1 STEEL	YELLOW	BLACK	EXIT 51 SB OFF RAMP	STA. 788+04	1 - W6X9	18" DIA.	YES	EXIT 51 SB OFF RAMP	STA. 789+50, LT.	645.507
	48	48	16.00	1 STEEL	WHITE/ RED	WHITE	NB	STA. 8490+82, RT.	1 - W6X9	18" DIA.	YES	NB	STA. 8492+00, RT.	645.508
	12 60 18	48 48 12	4.00 20.00 1.50	1 WOOD	YELLOW WHITE WHITE	BLACK BLACK BLACK	NB	STA. 7512+60, RT.	1 - W8X24	24" DIA.	YES	NB	STA. 7515+00, RT.	645.509
	24	12	2.00	1 U-CHANNEL	GREEN	WHITE	NB	STA. 7536+23, RT.	1 - U-CHANNEL POST	N/A	YES	NB	STA. 7536+25, RT.	645.109

Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\200_SS_29.dgn

Scale:

No.	Revision	By	Date

Designed by:



GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE

By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY













MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
SIGN SUMMARY -
REMOVE AND RESET

SHEET NUMBER: SS-29
200 OF 503

CONTRACT: 2019.04

PROPOSED SIGN SUMMARY

IDENTIFICATION NUMBER	DESCRIPTION	QUANTITY	SIGN SIZE			COLOR		NUMBER OF POSTS	POST SIZE	HEIGHT OF POST (CONTRACTOR TO CONFIRM HEIGHT)			FOUNDATION SIZE AND TYPE	BREAKAWAY DEVICE	DIRECTION (SB-SOUTHBOUND) (NB-NORTHBOUND)	LOCATION	PAY ITEM NUMBER
			HEIGHT (IN.)	WIDTH (IN.)	AREA (SQ. FT.)	BACKGROUND	LEGEND			LEFT (FEET)	CENTER (FEET)	RIGHT (FEET)					
M6-2		2	21	30	4.375 (8.75)	YELLOW	BLACK	← MOUNT WITH W3-5 →						NB SB	STA. 7476+00, RT. STA. 3509+50, LT.	645.271 645.271	
R1-1		1	48	48	16.00 (16.00)	RED	WHITE	1	W6X15	-	-	-	18" DIA.	YES	ACCESS ROAD	STA. 118+33, RT.	645.271
R1-2		2	60	60	25.00 (50.00)	WHITE/ RED	RED	1	W6X9	-	-	-	18" DIA.	YES	SB NB	STA. 3481+50, LT. STA. 7513+00, RT.	645.271 645.271
R2-1(10)		2	60	48	20.00 (40.00)	WHITE	BLACK	1	W8X24	-	-	-	24" DIA.	YES	NB SB	STA. 8498+50, RT. STA. 3501+50, LT.	645.271 645.271
R2-1(10)		6	60	48	20.00 (120.00)	WHITE	BLACK	← MOUNT TO BOOTH →						NB NB NB SB SB SB	STA. 8499+38, 16' RT. STA. 8499+38, 36' RT. STA. 8499+38, 4' LT. STA. 3500+61, 16' LT. STA. 3500+61, 36' LT. STA. 3500+61, 4' RT.	645.271 645.271 645.271 645.271 645.271 645.271	
R2-1(35) WITH FLAGS		2	60	48	20.00 (40.00)	WHITE	BLACK	1	W6X15	-	-	-	18" DIA.	YES	NB SB	STA. 8494+00, RT. STA. 3505+50, LT.	645.271 645.271
R2-1(45)		1	60	48	20.00 (20.00)	WHITE	BLACK	1	W8X24	-	-	-	24" DIA.	YES	NB	STA. 8480+50, RT.	645.271
DYNAMIC R2-1(60)		1	60	48	20.00 (20.00)	WHITE	BLACK	← MOUNT TO SPACE FRAME →						NB	STA. 7500+13	645.155	
DYNAMIC R2-1(70)		1	60	48	20.00 (20.00)	WHITE	BLACK	← MOUNT TO SPACE FRAME →						SB	STA. 4499+87	645.155	
R5-1		1	48	48	16.00 (16.00)	WHITE/ RED	WHITE	← MOUNT ON REVERSE SIDE OF SP-21 →						NB	STA. 3498+65, LT.	645.271	
R5-11	AUTHORIZED VEHICLES ONLY	5	24	30	5.00 (25.00)	WHITE	BLACK	1	6X6 WOOD POST	-	-	-	-	YES	ACCESS ROAD ACCESS ROAD ACCESS ROAD ACCESS ROAD ACCESS ROAD	STA. 118+31, RT. STA. 118+32, RT. STA. 118+59, RT. STA. 118+60, RT. STA. 100+41, RT.	645.271 645.271 645.271 645.271 645.271
R6-1R		1	12	36	3.00 (3.00)	BLACK/ WHITE	BLACK	← MOUNT WITH R1-1 →						ACCESS ROAD	STA. 118+33, RT.	645.271	
R7-8		2	18	12	1.50 (3.00)	BLUE/ WHITE	WHITE/ GREEN	1	U-CHANNEL POST	-	-	-	-	YES	EMPLOYEE PARKING PARK & RIDE LOT	STA. 119+50, RT. STA. 98+84, LT.	645.271 645.271
R7-8P	VAN ACCESSIBLE	2	9	18	1.125 (2.25)	WHITE	GREEN	← MOUNT WITH R7-8 →						EMPLOYEE PARKING PARK & RIDE LOT	STA. 119+50, RT. STA. 98+84, LT.	645.271 645.271	

Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\201_SS_30.dgn

Scale:

No.	Revision	By	Date

Designed by:



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CONSULTANT	PROJECT MANAGER:	BRANDON M. HAVU, PE	
By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

GORRILL PALMER
707 SABLE OAKS DRIVE, SUITE 30
SOUTH PORTLAND, MAINE 04106
www.gorrillpalmer.com
207.772.2515



**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
SIGN SUMMARY -
PROPOSED 1

SHEET NUMBER: SS-30
201 OF 503

CONTRACT: 2019.04

PROPOSED SIGN SUMMARY

IDENTIFICATION NUMBER	DESCRIPTION	QUANTITY	SIGN SIZE			COLOR		NUMBER OF POSTS	POST SIZE	HEIGHT OF POST (CONTRACTOR TO CONFIRM HEIGHT)			FOUNDATION SIZE AND TYPE	BREAKAWAY DEVICE	DIRECTION (SB-SOUTHBOUND) (NB-NORTHBOUND)	LOCATION	PAY ITEM NUMBER
			HEIGHT (IN.)	WIDTH (IN.)	AREA (SQ. FT.)	BACKGROUND	LEGEND			LEFT (FEET)	CENTER (FEET)	RIGHT (FEET)					
R8-3a		7	48	48	16.00 (112.00)	WHITE	RED	1	W6X9	-	-	-	18" DIA.	YES	NB NB NB NB SB SB SB	STA. 8492+00, RT. STA. 8497+00, RT. STA. 8499+00, RT. STA. 8500+50, RT. STA. 3499+50, LT. STA. 3501+00, LT. STA. 3504+75, LT.	645.271 645.271 645.271 645.271 645.271 645.271 645.271
SP-1		1	66	96	44.00 (44.00)	YELLOW	BLACK	2	W8X18	-	-	-	24" DIA.	YES	NB	STA. 7377+87, RT.	645.271
SP-2		1	36	174	43.50 (43.50)	BLUE	WHITE	MOUNT ON EXISTING FOUNDATION AND POSTS MOUNT WITH RESET SIGN (CONTRACTOR SHALL VERIFY EXISTING SIGN WIDTH AND MATCH ACCORDINGLY)					NB	STA. 7401+23, RT.	645.251		
SP-3		1	174	144	174.00 (174.00)	GREEN/YELLOW	WHITE/BLACK	MOUNT TO CANTILEVER STRUCTURE					NB	STA. 7427+25, LT.	645.151		
SP-4		1	144	150	150.00 (150.00)	GREEN	WHITE	MOUNT TO CANTILEVER STRUCTURE					NB	STA. 7427+25, RT.	645.151		
SP-5		2	132	288	264.00 (528.00)	YELLOW/PURPLE/GREEN	BLACK/WHITE	MOUNT TO OVERHEAD STRUCTURE					NB SB	STA. 7450+00, LT. STA. 43+00, LT.	645.121 645.127		
SP-6		1	204	156	221.00 (221.00)	GREEN/YELLOW	WHITE/BLACK	MOUNT TO OVERHEAD STRUCTURE					NB	STA. 7450+00, RT.	645.121		
SP-7		1	114	144	114.00 (114.00)	GREEN	WHITE	MOUNT TO OVERHEAD STRUCTURE					NB	STA. 7463+00, LT.	645.122		
SP-8		1	156	252	273.00 (273.00)	GREEN/YELLOW	WHITE/BLACK	MOUNT TO OVERHEAD STRUCTURE					NB	STA. 7463+00, RT.	645.122		
SP-9		2	156	288	312.00 (624.00)	YELLOW/GREEN	BLACK/WHITE	MOUNT TO CANTILEVER STRUCTURE MOUNT TO OVERHEAD STRUCTURE					NB SB	STA. 7471+75 STA. 27+00, RT.	645.152 645.126		
SP-10		3	48	96	32.00 (96.00)	WHITE	BLACK	2	W8X18	-	-	-	24" DIA.	YES	NB EXIT 51 NB ON RAMP SB	STA. 7473+50, RT. STA. 884+50, RT. STA. 25+50, LT.	645.271 645.271 645.271
SP-11		2	156	264	286.00 (572.00)	PURPLE/GREEN	WHITE/BLACK	MOUNT TO OVERHEAD STRUCTURE					NB SB	STA. 7477+25 STA. 4508+75, RT.	645.123 645.125		
SP-12		2	162	144	162.00 (324.00)	YELLOW/GREEN	BLACK/WHITE	MOUNT TO OVERHEAD STRUCTURE					NB SB	STA. 7477+25, RT. STA. 4508+75, LT.	645.123 645.125		

Date: 3/20/2019

Filename: ...000\HIGHWAY\MSTA\202...SS_31.dgn

Scale:

No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE

By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

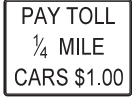



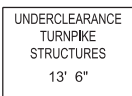









MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
SIGN SUMMARY -
PROPOSED 2

SHEET NUMBER: SS-31
202 OF 503

CONTRACT: 2019.04

PROPOSED SIGN SUMMARY

IDENTIFICATION NUMBER	DESCRIPTION	QUANTITY	SIGN SIZE			COLOR		NUMBER OF POSTS	POST SIZE	HEIGHT OF POST (CONTRACTOR TO CONFIRM HEIGHT)			FOUNDATION SIZE AND TYPE	BREAKAWAY DEVICE	DIRECTION (SB-SOUTHBOUND) (NB-NORTHBOUND)	LOCATION	PAY ITEM NUMBER			
			HEIGHT (IN.)	WIDTH (IN.)	AREA (SQ. FT.)	BACKGROUND	LEGEND			LEFT (FEET)	CENTER (FEET)	RIGHT (FEET)								
SP-13		1	66	96	44.00 (44.00)	YELLOW	BLACK	INSTALL ON EXISTING POSTS AND FOUNDATIONS										NB	STA. 885+75, RT.	645.271
SP-14		2	36	48	12.00 (24.00)	PURPLE/GREEN	WHITE	1	W6X9	-	-	-	18" DIA.	YES	NB SB	STA. 8490+25, RT. STA. 3507+50, LT.	645.271 645.271			
SP-15		4	48	160	53.33 (213.33)	ELECTRONIC		MOUNT TO CANOPY										NB SB NB SB	STA. 8499+32, 6' RT. STA. 3500+68, 6' LT. WEST GARDINER I-95 TOLL PLAZA	645.1092 645.1092 645.1092 645.1092
SP-16		4	48	144	48.00 (192.00)	WHITE	BLACK	MOUNT TO CANOPY										NB NB SB SB	STA. 8499+32, 26' RT. STA. 8499+32, 46' RT. STA. 3500+68, 26' LT. STA. 3500+68, 46' LT.	645.14
SP-17		1	60	72	30.00 (30.00)	YELLOW	BLACK	1	W8x24	-	-	-	24" DIA.	YES	NB	STA. 5280+00, RT.	645.271			
SP-18		2	72	78	39.00 (78.00)	YELLOW/WHITE	BLACK	1	W8x24	-	-	-	24" DIA.	YES	NB SB	STA. 8496+00, RT. STA. 3501+50, LT.	645.271 645.271			
SP-19		2	24	48	8.00 (16.00)	YELLOW	BLACK	MOUNT WITH R2-1(10)										NB SB	STA. 8498+50, RT. STA. 3504+50, LT.	645.271 645.271
SP-20		8	36	24	6.00 (48.00)	WHITE	BLACK	MOUNT TO BARRIER										NB NB SB SB	STA. 7497+00, LT. & RT. STA. 7499+00, LT. & RT. STA. 4501+00, LT. & RT. STA. 4503+00, LT. & RT.	645.271 645.271 645.271 645.271
SP-21		2	36	24	6.00 (12.00)	WHITE	BLACK	1	6X6 WOOD POST	-	-	-	-	YES	SB NB	STA. 3498+65, LT. STA. 8501+50, RT.	645.271 645.271			
SP-22		2	48	60	20.00 (40.00)	PURPLE	WHITE	1	W6X15	-	-	-	18" DIA.	YES	SB NB	STA. 3497+00, LT. STA. 8503+00, RT.	645.271 645.271			
SP-23		4	12	36	3.00 (12.00)	YELLOW	BLACK	MOUNT WITH W4-1 OR W4-3										SB NB NB NB	STA. 4488+16, LT. STA. 7508+25, RT. STA. 7523+00, LT. STA. 7523+00, RT.	645.271 645.271 645.271 645.271
SP-24		2	96	162	108.00 (216.00)	BLUE	WHITE	2	W8X18	-	-	-	24" DIA.	YES	SB NB	STA. 4473+50, LT. STA. 7518+00, RT.	645.251 645.251			
SP-25		1	156	192	208.00 (208.00)	GREEN/YELLOW	WHITE/BLACK	MOUNT TO OVERHEAD STRUCTURE										SB	STA. 3491+00, LT.	645.124
SP-26		1	150	168	175.00 (175.00)	GREEN	WHITE	MOUNT TO OVERHEAD STRUCTURE										SB	STA. 3491+00, LT.	645.124

Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\203_SS_32.dgn

Scale:

No.	Revision	By	Date

Designed by:



GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE

By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

GORRILL PALMER
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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
SIGN SUMMARY -
PROPOSED 3

SHEET NUMBER: SS-32
203 OF 503

CONTRACT: 2019.04

PROPOSED SIGN SUMMARY

IDENTIFICATION NUMBER	DESCRIPTION	QUANTITY	SIGN SIZE			COLOR		NUMBER OF POSTS	POST SIZE	HEIGHT OF POST (CONTRACTOR TO CONFIRM HEIGHT)			FOUNDATION SIZE AND TYPE	BREAKAWAY DEVICE	DIRECTION (SB-SOUTHBOUND) (NB-NORTHBOUND)	LOCATION	PAY ITEM NUMBER
			HEIGHT (IN.)	WIDTH (IN.)	AREA (SQ. FT.)	BACKGROUND	LEGEND			LEFT (FEET)	CENTER (FEET)	RIGHT (FEET)					
SP-27	Gardiner 2 MILES	1	114	132	104.50 (104.50)	GREEN	WHITE			MOUNT TO OVERHEAD STRUCTURE				SB	STA. 4491+00, LT.	645.124	
SP-28	SOUTH Brunswick Freeport	1	114	168	133.00 (133.00)	GREEN	WHITE			MOUNT TO OVERHEAD STRUCTURE				SB	STA. 4491+00, RT.	645.124	
SP-29	WEST GARDINER SERVICE PLAZA	1 1	150 36	150 150	156.25 (156.25) 37.50 (37.50)	BLUE BLUE	WHITE WHITE	2	W12X26	-	-	-	30" DIA.	YES	SB	STA. 3492+70, LT.	645.251
SP-30	WEST GARDINER SERVICE PLAZA GET TOLL RECEIPT FOR REDUCED TOLL UPON RE-ENTRY SOUTH VALID AT EXIT 102 ONLY	1	96	174	116.00 (116.00)	BLUE	WHITE	2	W8X18	-	-	-	24" DIA.	YES	SB	STA. 3503+00, LT.	645.251
SP-31	Gardiner Litchfield USE CASH LANES	1	156	192	208.00 (208.00)	GREEN/YELLOW	WHITE/BLACK			MOUNT TO OVERHEAD STRUCTURE				SB	STA. 4508+75, LT.	645.125	
SP-32	WEST GARDINER SERVICE PLAZA USE CASH LANES	1	192	192	256.00 (256.00)	BLUE/YELLOW	WHITE/BLACK	3	W12X26	-	-	-	30" DIA.	YES	SB	STA. 23+00, LT.	645.251
SP-33	Gardiner Litchfield USE CASH LANES	2	156	192	208.00 (416.00)	GREEN/YELLOW	WHITE/BLACK			MOUNT TO OVERHEAD STRUCTURE				SB SB	STA. 27+00, LT. STA. 43+00, LT.	645.126 645.127	
SP-34	WEST GARDINER SERVICE PLAZA USE CASH LANES	1 1	156 42	192 192	208.00 (208.00) 56.00 (56.00)	BLUE BLUE	WHITE WHITE	3	W12X26	-	-	-	30" DIA.	YES	SB	STA. 40+00, LT.	645.251
SP-36	DO NOT STOP	4	48	84	28.00 (112.00)	WHITE	BLACK			MOUNT TO SPACE FRAME				NB NB SB SB	STA. 7500+13, LT. STA. 7500+13, RT. STA. 4499+87, LT. STA. 4499+87, RT.	645.271 645.271 645.271 645.271	
SP-37	RAMP	1	24	48	8.00 (8.00)	YELLOW	BLACK			MOUNT WITH R2-1(45)				NB	STA. 8480+50, RT.	645.271	
W3-5(35)	AHEAD 35	1	48	48	16.00 (16.00)	YELLOW/WHITE	BLACK	1	W6X15	-	-	-	18" DIA.	YES	SB	STA. 3509+50, LT.	645.271
W3-5(45)	AHEAD 45	1	48	48	16.00 (16.00)	YELLOW/WHITE	BLACK	1	W6X9	-	-	-	18" DIA.	YES	NB	STA. 7476+00, RT.	645.271
W4-1	AHEAD	2	48	48	16.00 (32.00)	YELLOW	BLACK			MOUNT TO LIGHT POLE				SB NB	STA. 4488+16, LT. STA. 7508+25, RT.	645.271 645.271	
W4-2	AHEAD	2	48	48	16.00 (32.00)	YELLOW	BLACK	1	W6X9	-	-	-	18" DIA.	YES	NB NB	STA. 7533+00, RT. STA. 5273+00, RT.	645.271 645.271

Date: 3/20/2019

Filename: ...00\HIGHWAY\MSTA\204_SS_33.dgn

Scale:

No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE

By	Date	By	Date
Designed	BMH 3\20\19	Checked	DGE 3\20\19
Drawn	MPC 3\20\19	In Charge of	GAE 3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

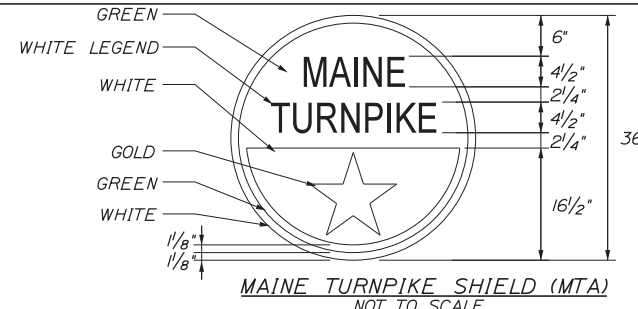
INTERCHANGE 103
ORT CONVERSION
SIGN SUMMARY -
PROPOSED 4

SHEET NUMBER: SS-33
204 OF 503

CONTRACT: 2019.04

PROPOSED SIGN SUMMARY

IDENTIFICATION NUMBER	DESCRIPTION	QUANTITY	SIGN SIZE			COLOR		NUMBER OF POSTS	POST SIZE	HEIGHT OF POST (CONTRACTOR TO CONFIRM HEIGHT)			FOUNDATION SIZE AND TYPE	BREAKAWAY DEVICE	DIRECTION (SB-SOUTHBOUND) (NB-NORTHBOUND)	LOCATION	PAY ITEM NUMBER
			HEIGHT (IN.)	WIDTH (IN.)	AREA (SQ. FT.)	BACKGROUND	LEGEND			LEFT (FEET)	CENTER (FEET)	RIGHT (FEET)					
W4-3		4	48	48	16.00 (64.00)	YELLOW	BLACK	1	W6X9	-	-	-	18" DIA.	YES	EXIT 51 NB ON RAMP NB NB NB	STA. 887+50, RT. STA. 8487+38, RT. STA. 7523+00, LT. STA. 7523+00, RT.	645.271 645.271 645.271 645.271
W9-1		2	48	48	16.00 (32.00)	YELLOW	BLACK	1	W6X9	-	-	-	18" DIA.	YES	NB NB	STA. 7527+00, RT. STA. 5268+00, RT.	645.271 645.271
W11-2		6	36	36	9.00 (54.00)	YELLOW	BLACK	1	W6X9	-	-	-	18" DIA.	YES	NB NB SB SB NB SB	STA. 8499+38, 16' RT. STA. 8499+38, 36' RT. STA. 3500+61, 16' LT. STA. 3500+61, 36' LT. STA. 8499+50, 65' RT. STA. 3500+56, 65' LT.	645.271 645.271 645.271 645.271 645.271 645.271
W12-1		2	48	48	16.00 (32.00)	YELLOW	BLACK	1	W6X9	-	-	-	18" DIA.	YES	NB SB	STA. 7487+80, RT. STA. 4505+65, LT.	645.271 645.271
M4-6		2	18	36	4.50 (9.00)	WHITE	BLACK	1	W6X9	-	-	-	18" DIA.	YES	SB NB	STA. 4489+99, LT. STA. 7522+00, RT.	645.271 645.271
M4-14		2	18	36	4.50 (9.00)	WHITE	BLACK	1	W6X9	-	-	-	18" DIA.	YES	NB SB	STA. 7489+99, RT. STA. 45+50, LT.	645.271 645.271
MI-1		2	36	45	11.25 (22.50)	RED/ BLUE	WHITE	1	W6X9	-	-	-	18" DIA.	YES	NB SB	STA. 7522+00, RT. STA. 45+50, LT.	645.271 645.271
LOGO S		2	36	36	9.00 (18.00)	SEE DETAIL THIS SHEET		1	W6X9	-	-	-	18" DIA.	YES	NB SB	STA. 7489+99, RT. STA. 4489+99, LT.	645.271 645.271
E1-5P(49)		1	30	114	23.75 (23.75)	GREEN	WHITE	1	U-CHANNEL POST	-	-	-	-	YES	SB	STA. 4491+00, LT.	645.124
E1-5P(51)		10	30	114	23.75 (237.50)	GREEN	WHITE	1	U-CHANNEL POST	-	-	-	-	YES	NB NB NB SB SB SB SB SB SB SB	STA. 7427+25, RT. STA. 7450+00, RT. STA. 7463+00, RT. STA. 4491+00, LT. STA. 3492+70, LT. STA. 4508+75, LT. STA. 23+00, LT. STA. 27+00, LT. STA. 40+00, LT. STA. 43+00, LT.	645.151 645.121 645.122 645.124 645.251 645.125 645.251 645.126 645.251 645.251 645.127
OM3-R		2	36	12	3.00 (6.00)	BLACK/YELLOW		1	U-CHANNEL POST	-	-	-	-	YES	NB SB	STA. 8480+50, RT. STA. 3483+00, LT.	645.271 645.271
D9-10		1	30	30	6.25 (6.25)	BLUE	WHITE	1	U-CHANNEL POST	-	-	-	-	YES	NB	STA. 7440+50, RT.	645.271



NOTE: FOR 24" SHIELD REDUCE ALL MEASUREMENTS BY 1/3.

Date: 3/20/2019

Filename: ...:\00\HIGHWAY\MSTA\205_SS_34.dgn

Scale:			
No.	Revision	By	Date

Designed by:

GORRILL PALMER

CONSULTANT PROJECT MANAGER: BRANDON M. HAVU, PE

By	Date	By	Date
BMH	3\20\19	DGE	3\20\19
MPC	3\20\19	GAE	3\20\19

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SOUTH PORTLAND, MAINE 04106
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THE GOLD STAR MEMORIAL HIGHWAY

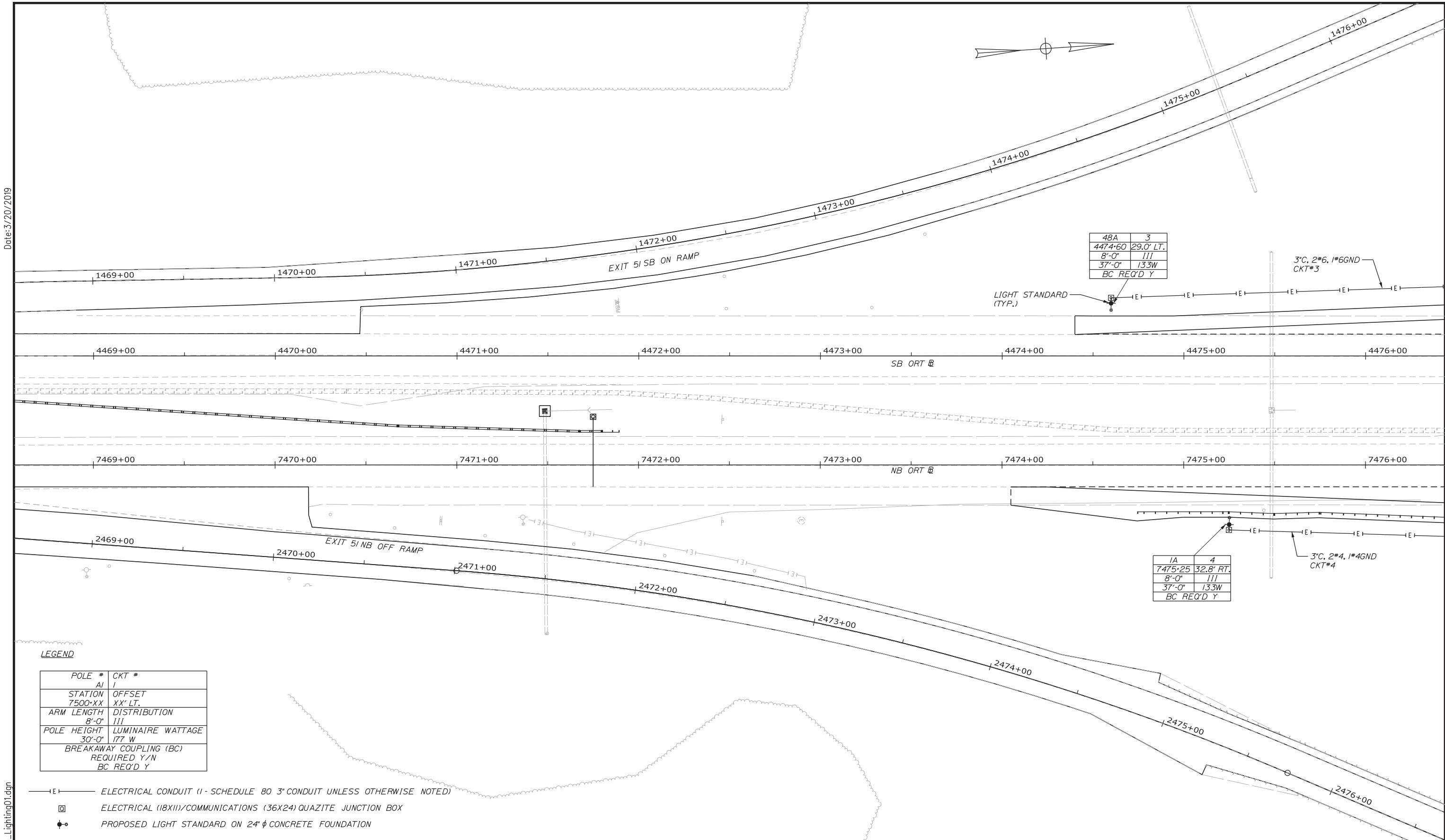
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
SIGN SUMMARY -
PROPOSED 5

SHEET NUMBER: SS-34
205 OF 503

CONTRACT: 2019.04

Date: 3/20/2019



48A	3
4474+60	29.0' LT.
8'-0"	133W
37'-0"	133W
BC REQ'D Y	

1A	4
7475+25	32.8' RT.
8'-0"	133W
37'-0"	133W
BC REQ'D Y	

LEGEND

POLE #	CKT #
AI	I
STATION	OFFSET
7500+XX	XX' LT.
ARM LENGTH	DISTRIBUTION
8'-0"	133
POLE HEIGHT	LUMINAIRE WATTAGE
30'-0"	177 W
BREAKAWAY COUPLING (BC)	
REQUIRED Y/N	
BC REQ'D Y	

- E— ELECTRICAL CONDUIT (1 - SCHEDULE 80 3" CONDUIT UNLESS OTHERWISE NOTED)
- ⊠ ELECTRICAL (18X11)/COMMUNICATIONS (36X24) QUARTZITE JUNCTION BOX
- ⊙ PROPOSED LIGHT STANDARD ON 24" φ CONCRETE FOUNDATION

Filename: ... \HIGHWAY\MSTA\Lighting01.dgn

Scale: Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

By	Date	By	Date
Designed	TJM 3\20\19	Checked	LEM 3\20\19
Drawn	EJB 3\20\19	In Charge of	GAE 3\20\19

STANTEC CONSULTING SERVICES INC.
 482 PAYNE ROAD
 SCARBOROUGH, ME 04074
 TEL (207) 887-3448
 FAX (207) 883-3376

**THE GOLD STAR
 MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

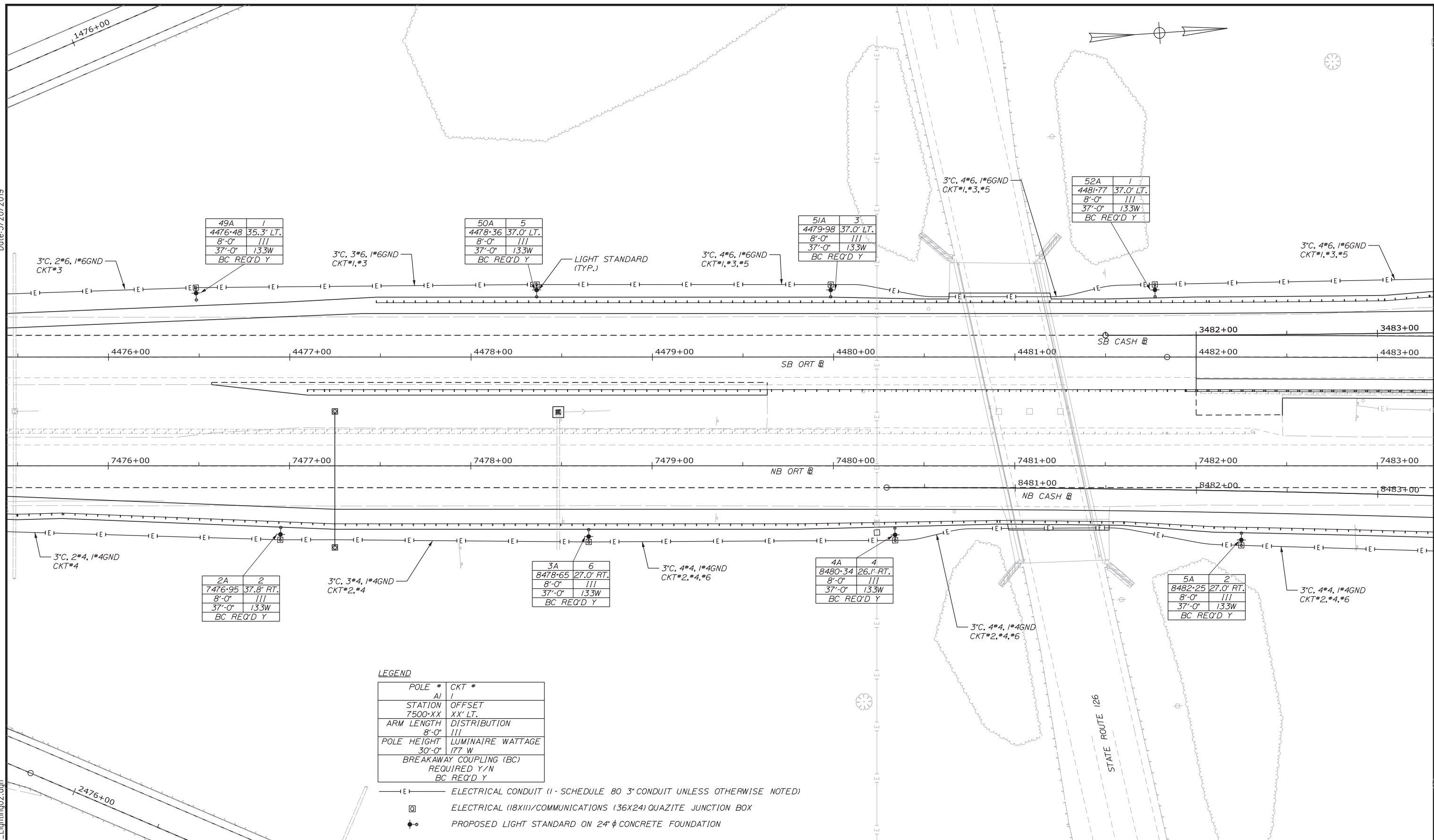
INTERCHANGE 103
 ORT CONVERSION
 LIGHTING PLAN 1

SHEET NUMBER: LP-01
 206 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ... \HIGHWAY\MSTA\Lighting02.dgn



49A	1
4476+48	35.3' LT.
8'-0"	III
37'-0"	133W
BC REQ'D Y	

50A	5
4478+36	37.0' LT.
8'-0"	III
37'-0"	133W
BC REQ'D Y	

51A	3
4479+98	37.0' LT.
8'-0"	III
37'-0"	133W
BC REQ'D Y	

52A	1
4481+77	37.0' LT.
8'-0"	III
37'-0"	133W
BC REQ'D Y	

2A	2
7476+95	37.8' RT.
8'-0"	III
37'-0"	133W
BC REQ'D Y	

3A	6
8478+65	27.0' RT.
8'-0"	III
37'-0"	133W
BC REQ'D Y	

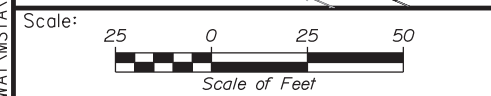
4A	4
8480+34	26.1' RT.
8'-0"	III
37'-0"	133W
BC REQ'D Y	

5A	2
8482+25	27.0' RT.
8'-0"	III
37'-0"	133W
BC REQ'D Y	

LEGEND

POLE #	CKT #
AI	I
STATION	OFFSET
7500+XX	XX' LT.
ARM LENGTH	DISTRIBUTION
8'-0"	III
POLE HEIGHT	LUMINAIRE WATTAGE
30'-0"	177 W
BREAKAWAY COUPLING (BC)	
REQUIRED Y/N	
BC REQ'D Y	

- E— ELECTRICAL CONDUIT (1 - SCHEDULE 80 3" CONDUIT UNLESS OTHERWISE NOTED)
- ELECTRICAL (18X11)/COMMUNICATIONS (36X24) QUAZITE JUNCTION BOX
- ⊕ PROPOSED LIGHT STANDARD ON 24" φ CONCRETE FOUNDATION



Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

By	Date	By	Date
Designed	TJM 3\20\19	Checked	LEM 3\20\19
Drawn	EJB 3\20\19	In Charge of	GAE 3\20\19

STANTEC CONSULTING SERVICES INC.
 482 PAYNE ROAD
 SCARBOROUGH, ME 04074
 TEL (207) 887-3448
 FAX (207) 883-3376

MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

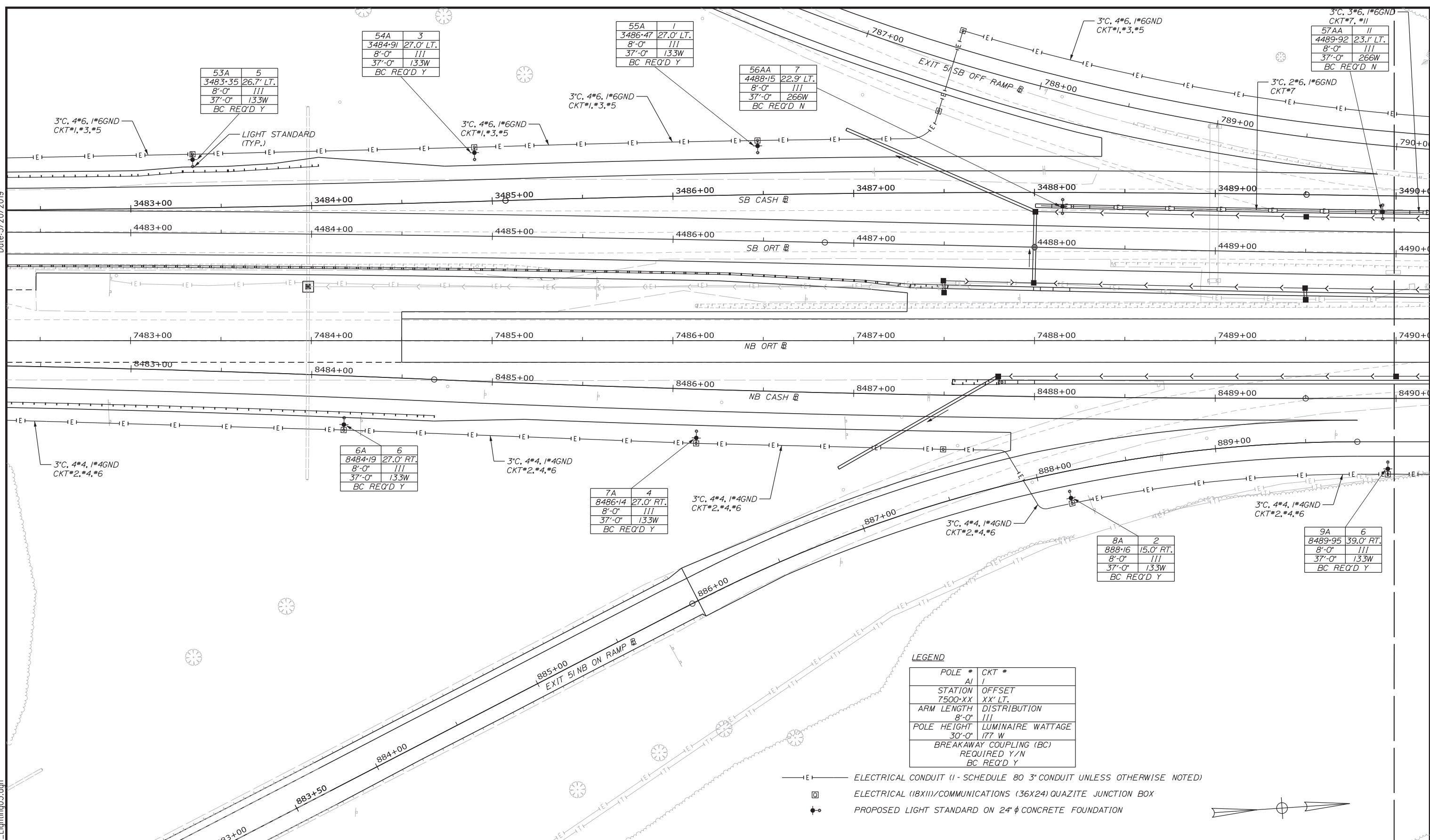
INTERCHANGE 103
 ORT CONVERSION
 LIGHTING PLAN 2

SHEET NUMBER: LP-02
 207 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...HIGHWAY_MSTA...Lighting03.dgn



LEGEND

POLE #	CKT #
A1	1
STATION	OFFSET
7500-XX	XX' LT.
ARM LENGTH	DISTRIBUTION
8'-0"	III
POLE HEIGHT	LUMINAIRE WATTAGE
30'-0"	177 W
BREAKAWAY COUPLING (BC)	
REQUIRED Y/N	
BC REQ'D Y	

- E— ELECTRICAL CONDUIT (1 - SCHEDULE 80 3" CONDUIT UNLESS OTHERWISE NOTED)
- ELECTRICAL (IBXII)/COMMUNICATIONS (36X24) QUAZITE JUNCTION BOX
- ⊕ PROPOSED LIGHT STANDARD ON 24" φ CONCRETE FOUNDATION



Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

STANTEC CONSULTING SERVICES INC.
482 PAYNE ROAD
SCARBOROUGH, ME 04074
TEL (207) 887-3448
FAX (207) 883-3376

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE					
Designed	TJM	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

**THE GOLD STAR
MEMORIAL HIGHWAY**

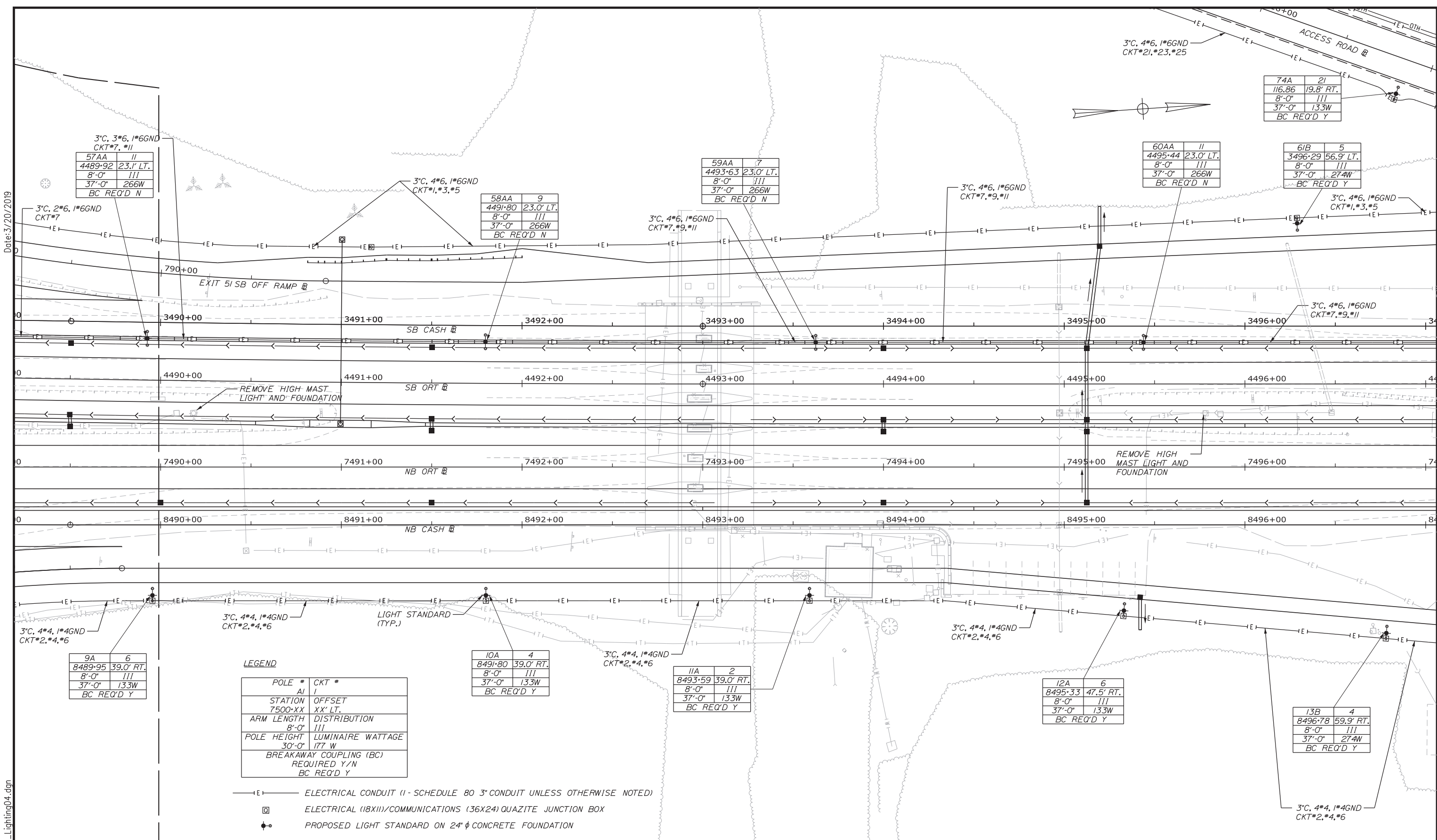
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
LIGHTING PLAN 3

SHEET NUMBER: LP-03
CONTRACT: 2019.04
208 OF 503

Date: 3/20/2019

Filename: ...HIGHWAY.MSTA...Lighting04.dgn



LEGEND

POLE #	AI	CKT #
STATION	7500-XX	OFFSET XX' LT.
ARM LENGTH	8'-0"	DISTRIBUTION 111
POLE HEIGHT	30'-0"	LUMINAIRE WATTAGE 177 W
BREAKAWAY COUPLING (BC) REQUIRED Y/N BC REQ'D Y		

- E— ELECTRICAL CONDUIT (1 - SCHEDULE 80 3" CONDUIT UNLESS OTHERWISE NOTED)
- ELECTRICAL (18X11)/COMMUNICATIONS (36X24) QUAZITE JUNCTION BOX
- ⊙ PROPOSED LIGHT STANDARD ON 24" φ CONCRETE FOUNDATION

Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

Stantec

STANTEC CONSULTING SERVICES INC.
482 PAYNE ROAD
SCARBOROUGH, ME 04074
TEL (207) 887-3448
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CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE					
Designed	TJM	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103 ORT CONVERSION

LIGHTING PLAN 4

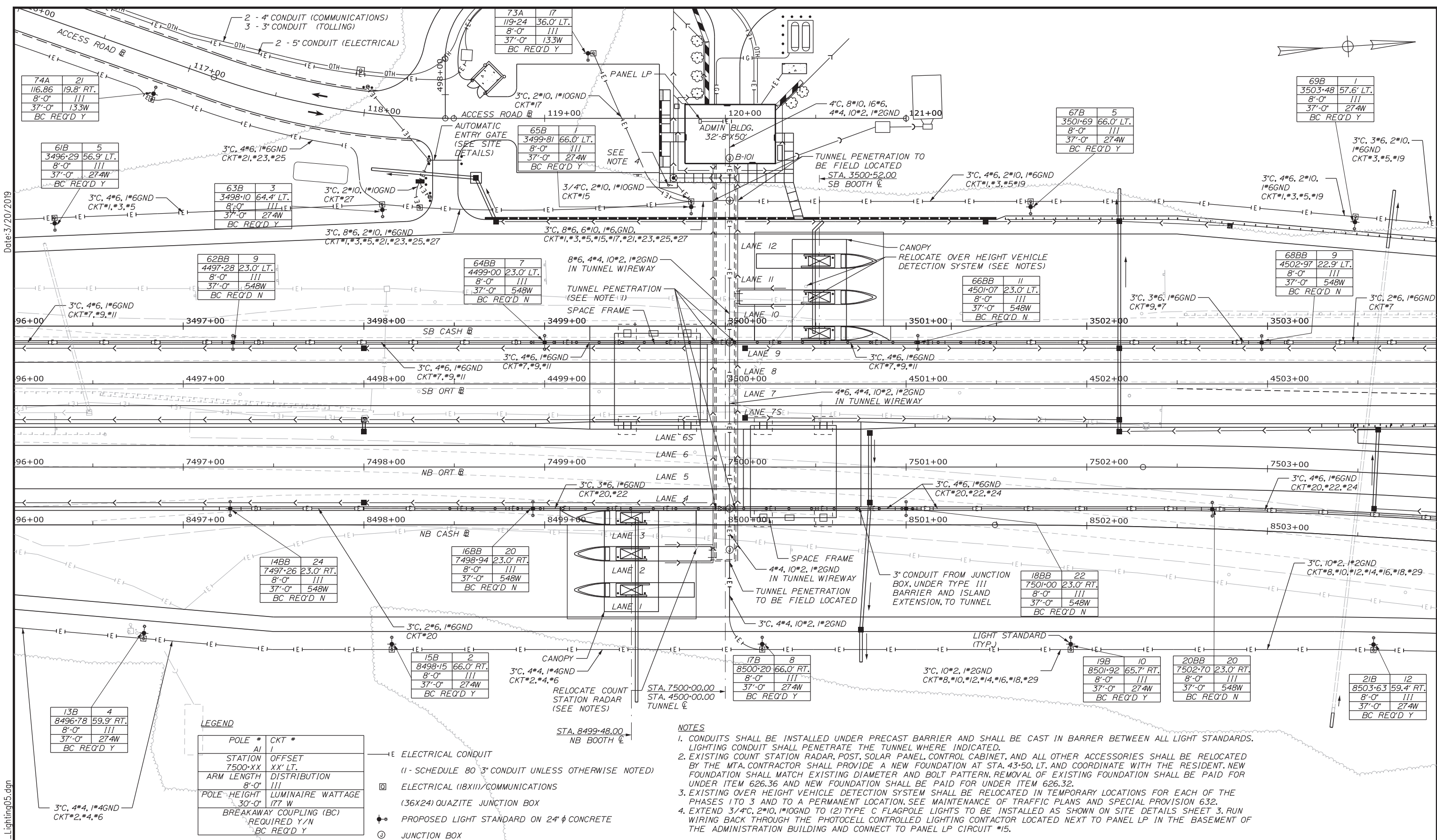
SHEET NUMBER: LP-04

209 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...HIGHWAY.MSTA...Lighting05.dgn



Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by: **Stantec**

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FAX (207) 883-3376

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

By	Date	By	Date
Designed	TJM 3\20\19	Checked	LEM 3\20\19
Drawn	EJB 3\20\19	In Charge of	GAE 3\20\19

MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

**INTERCHANGE 103
ORT CONVERSION**

LIGHTING PLAN 5

SHEET NUMBER: LP-05
210 OF 503

CONTRACT: 2019.04

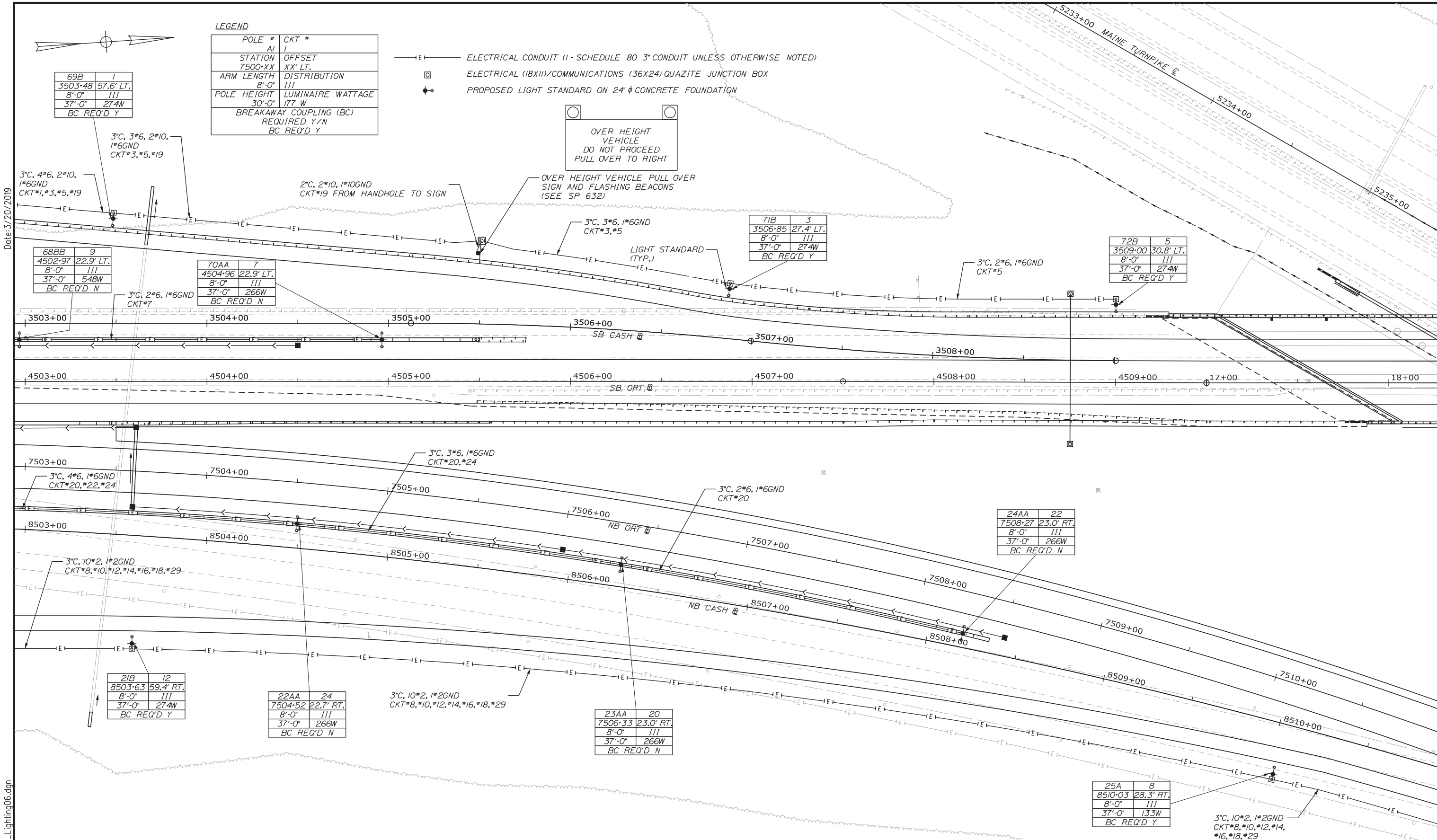
LEGEND

POLE #	AI	CKT #	J
STATION	7500+XX	OFFSET	XX' LT.
ARM LENGTH	8'-0"	DISTRIBUTION	III
POLE HEIGHT	30'-0"	LUMINAIRE WATTAGE	177 W
BREAKAWAY COUPLING (BC) REQUIRED Y/N			
BC REQ'D Y			

- E ELECTRICAL CONDUIT
(1 - SCHEDULE 80 3" CONDUIT UNLESS OTHERWISE NOTED)
- ⊠ ELECTRICAL (18XII)/COMMUNICATIONS
(36X24) QUAZITE JUNCTION BOX
- ⦿ PROPOSED LIGHT STANDARD ON 24" φ CONCRETE
- ⊙ JUNCTION BOX

NOTES

1. CONDUITS SHALL BE INSTALLED UNDER PRECAST BARRIER AND SHALL BE CAST IN BARRER BETWEEN ALL LIGHT STANDARDS. LIGHTING CONDUIT SHALL PENETRATE THE TUNNEL WHERE INDICATED.
2. EXISTING COUNT STATION RADAR, POST, SOLAR PANEL, CONTROL CABINET, AND ALL OTHER ACCESSORIES SHALL BE RELOCATED BY THE MTA CONTRACTOR SHALL PROVIDE A NEW FOUNDATION AT STA. 43+50, LT. AND COORDINATE WITH THE RESIDENT. NEW FOUNDATION SHALL MATCH EXISTING DIAMETER AND BOLT PATTERN. REMOVAL OF EXISTING FOUNDATION SHALL BE PAID FOR UNDER ITEM 626.36 AND NEW FOUNDATION SHALL BE PAID FOR UNDER ITEM 626.32.
3. EXISTING OVER HEIGHT VEHICLE DETECTION SYSTEM SHALL BE RELOCATED IN TEMPORARY LOCATIONS FOR EACH OF THE PHASES 1 TO 3 AND TO A PERMANENT LOCATION. SEE MAINTENANCE OF TRAFFIC PLANS AND SPECIAL PROVISION 632.
4. EXTEND 3/4" C, 2" I, 1" IOGND TO (2) TYPE C FLAGPOLE LIGHTS TO BE INSTALLED AS SHOWN ON SITE DETAILS SHEET 3. RUN WIRING BACK THROUGH THE PHOTOCELL CONTROLLED LIGHTING CONTRACTOR LOCATED NEXT TO PANEL LP IN THE BASEMENT OF THE ADMINISTRATION BUILDING AND CONNECT TO PANEL LP CIRCUIT #15.



LEGEND

POLE #	CKT #
AI	I
STATION	OFFSET
7500-XX	XX' LT.
ARM LENGTH	DISTRIBUTION
8'-0"	III
POLE HEIGHT	LUMINAIRE WATTAGE
30'-0"	177 W
BREAKAWAY COUPLING (BC) REQUIRED Y/N	
BC REQ'D Y	

- E— ELECTRICAL CONDUIT (1- SCHEDULE 80 3" CONDUIT UNLESS OTHERWISE NOTED)
- ELECTRICAL (18X11)/COMMUNICATIONS (36X24) QUAZITE JUNCTION BOX
- ⊕ PROPOSED LIGHT STANDARD ON 24" φ CONCRETE FOUNDATION

OVER HEIGHT VEHICLE DO NOT PROCEED PULL OVER TO RIGHT

OVER HEIGHT VEHICLE PULL OVER SIGN AND FLASHING BEACONS (SEE SP 632)

LIGHT STANDARD (TYP.)

69B	1
3503-48	57.6' LT.
8'-0"	III
37'-0"	274W
BC REQ'D Y	

68BB	9
4502-97	22.9' LT.
8'-0"	III
37'-0"	548W
BC REQ'D N	

70AA	7
4504-96	22.9' LT.
8'-0"	III
37'-0"	266W
BC REQ'D N	

71B	3
3506-85	27.4' LT.
8'-0"	III
37'-0"	274W
BC REQ'D Y	

72B	5
3509-00	30.8' LT.
8'-0"	III
37'-0"	274W
BC REQ'D Y	

24AA	22
7508-27	23.0' RT.
8'-0"	III
37'-0"	266W
BC REQ'D N	

21B	12
8503-63	59.4' RT.
8'-0"	III
37'-0"	274W
BC REQ'D Y	

22AA	24
7504-52	22.7' RT.
8'-0"	III
37'-0"	266W
BC REQ'D N	

23AA	20
7506-33	23.0' RT.
8'-0"	III
37'-0"	266W
BC REQ'D N	

25A	8
8510-03	28.3' RT.
8'-0"	III
37'-0"	133W
BC REQ'D Y	

3" C, 10*2, 1*2GND CKT*8,*10,*12,*14,*16,*18,*29

Date: 3/20/2019

Filename: ... \HIGHWAY\MSTA\Lighting06.dgn

Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

STANTEC CONSULTING SERVICES INC.
482 PAYNE ROAD
SCARBOROUGH, ME 04074
TEL (207) 887-3448
FAX (207) 883-3376

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE					
Designed	TJM	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

MAINE TURNPIKE

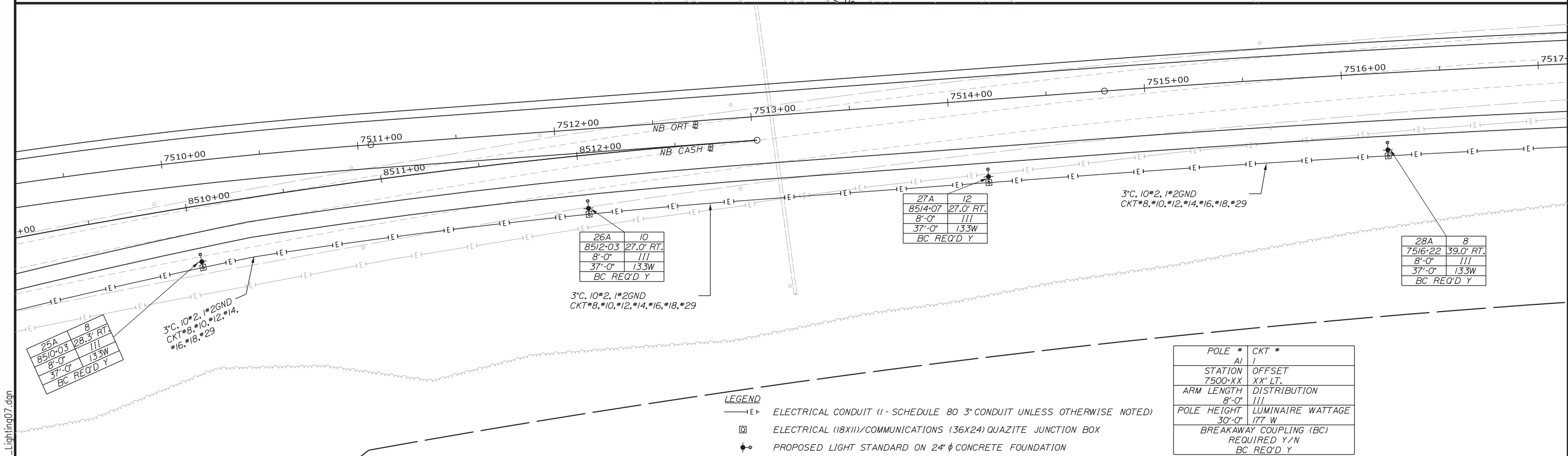
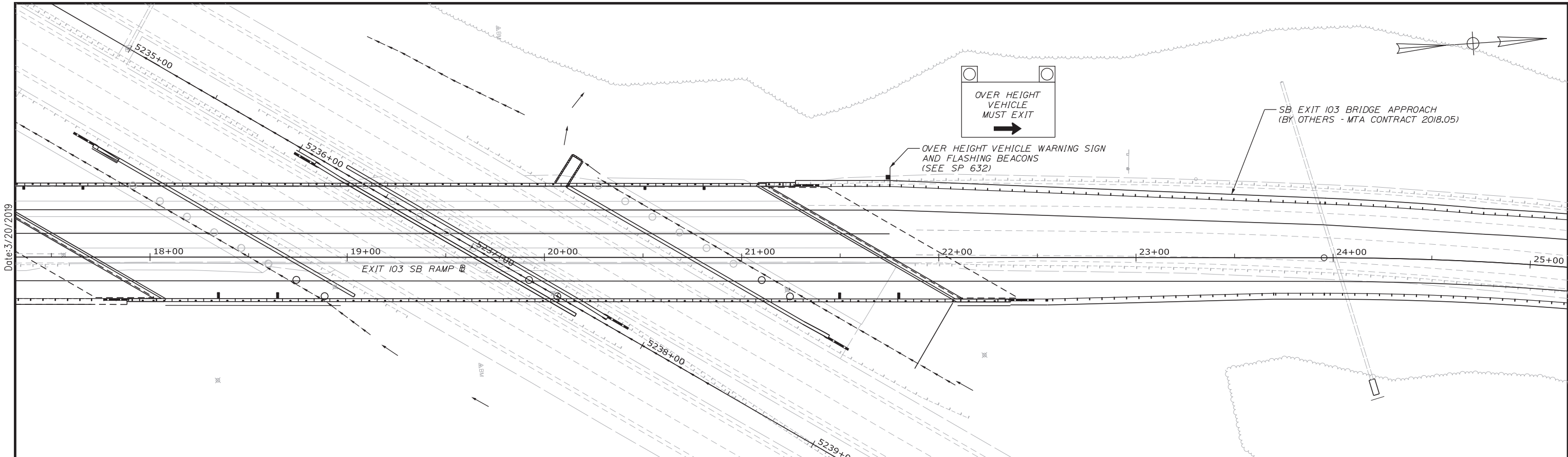
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
LIGHTING PLAN 6

SHEET NUMBER: LP-06
211 OF 503

CONTRACT: 2019.04



Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by: **Stantec**

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	TJM	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

STANTEC CONSULTING SERVICES INC.
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FAX (207) 883-3376



**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

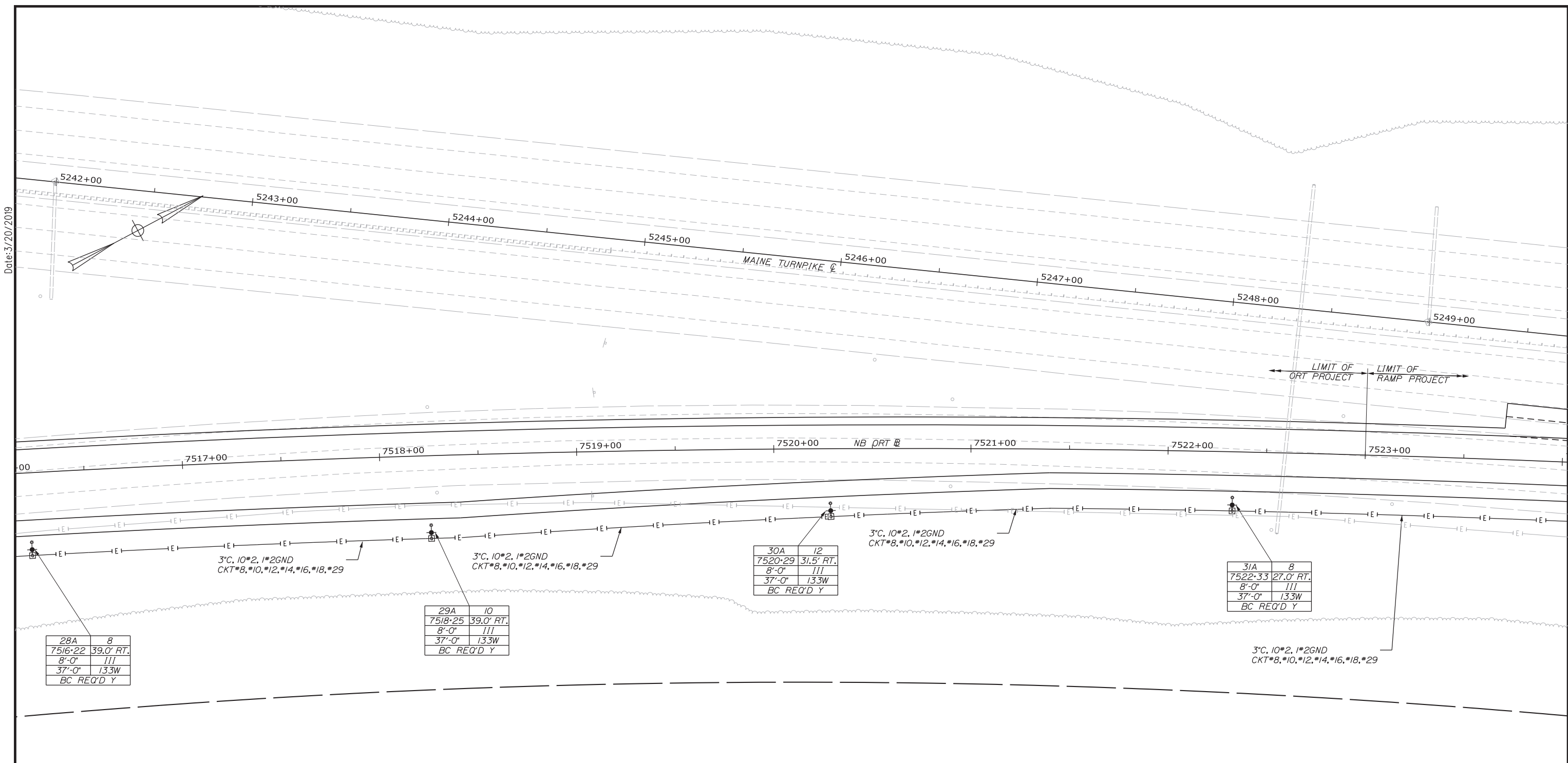
INTERCHANGE 103
ORT CONVERSION
LIGHTING PLAN 7

SHEET NUMBER: LP-07
212 OF 503

CONTRACT:2019.04

Date: 3/20/2019
Filename: ...HIGHWAY_MSTA...Lighting07.dgn

Date: 3/20/2019



28A	8
7516-22	39.0' RT.
8'-0"	111
37'-0"	133W
BC REQ'D Y	

29A	10
7518-25	39.0' RT.
8'-0"	111
37'-0"	133W
BC REQ'D Y	

30A	12
7520-29	31.5' RT.
8'-0"	111
37'-0"	133W
BC REQ'D Y	

31A	8
7522-33	27.0' RT.
8'-0"	111
37'-0"	133W
BC REQ'D Y	

LEGEND

POLE #	CKT #
AI	I
STATION	OFFSET
7500-XX	XX' LT.
ARM LENGTH	DISTRIBUTION
8'-0"	111
POLE HEIGHT	LUMINAIRE WATTAGE
30'-0"	177 W
BREAKAWAY COUPLING (BC)	
REQUIRED Y/N	
BC REQ'D Y	

- E— ELECTRICAL CONDUIT (1 - SCHEDULE 80 3" CONDUIT UNLESS OTHERWISE NOTED)
- ⊠ ELECTRICAL (18X11)/COMMUNICATIONS (36X24) QUARTZITE JUNCTION BOX
- ⊙ PROPOSED LIGHT STANDARD ON 24" φ CONCRETE FOUNDATION

Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	TJM	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

STANTEC CONSULTING SERVICES INC.
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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
LIGHTING PLAN 8

SHEET NUMBER: LP-08
CONTRACT: 2019.04
213 OF 503

Filename: ... \HIGHWAY \MSTA \Lighting08.dgn

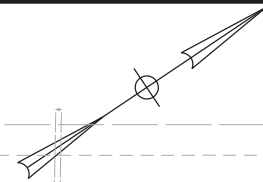
NOTES

1. CONTRACTOR SHALL MAINTAIN EXISTING LIGHT LEVELS; SEE SECTIONS 104.4.6 AND 104.4.6.1.
2. A PHOTOCELL SHALL BE INSTALLED ON EACH LUMINAIRE BEING RELOCATED.

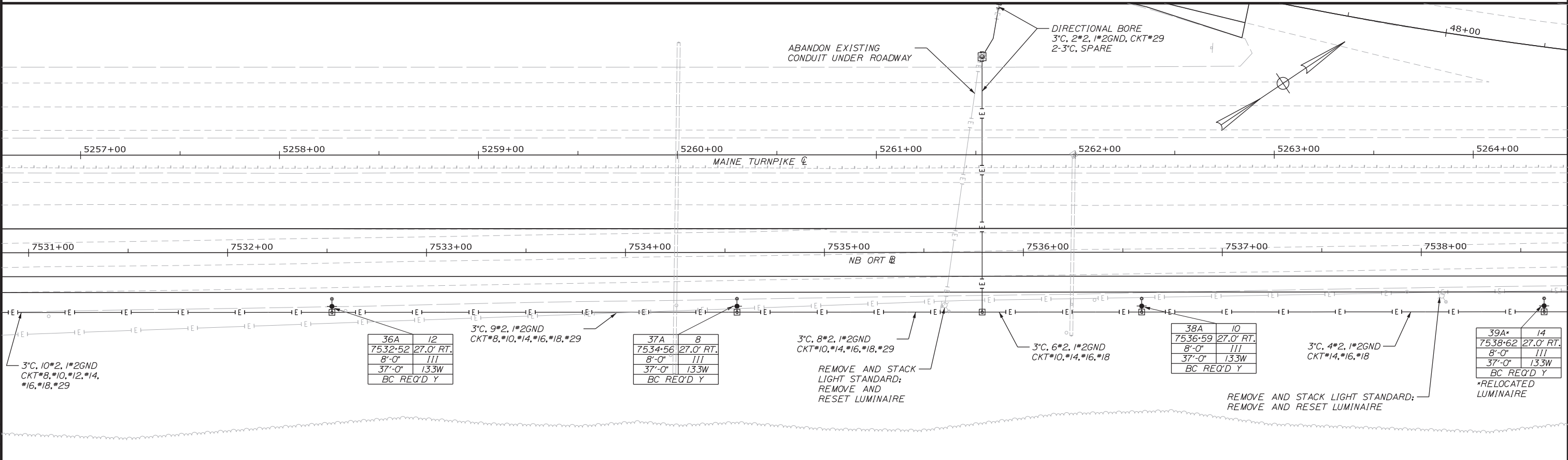
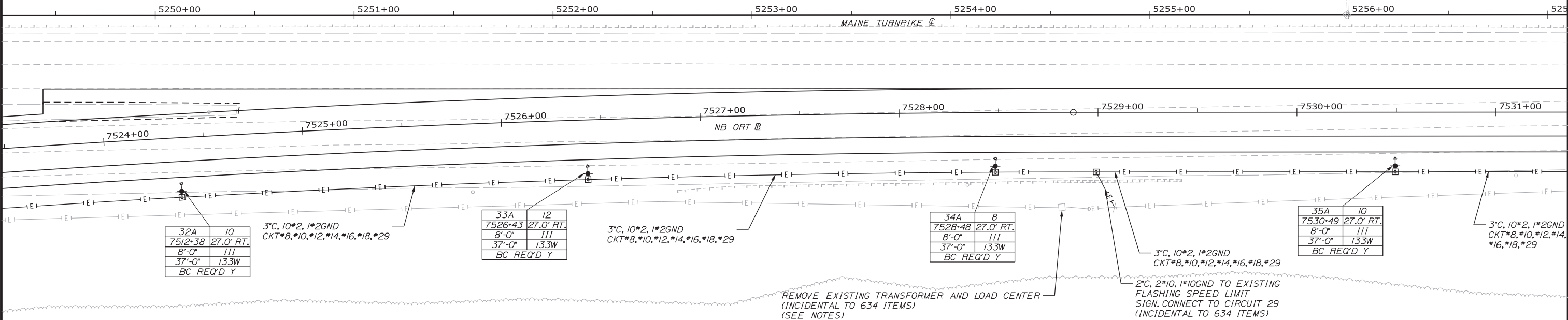
LEGEND

POLE #	AI	CKT #	1
STATION	7500-XX	OFFSET	XX' LT.
ARM LENGTH	8'-0"	DISTRIBUTION	III
POLE HEIGHT	30'-0"	LUMINAIRE WATTAGE	177 W
BREAKAWAY COUPLING (BC)		REQUIRED Y/N	BC REQ'D Y

- E— ELECTRICAL CONDUIT (1 - SCHEDULE 80 3" CONDUIT UNLESS OTHERWISE NOTED)
- ELECTRICAL (18X11)/COMMUNICATIONS (36X24) QUAZITE JUNCTION BOX
- ◆ PROPOSED LIGHT STANDARD ON 24" Ø CONCRETE FOUNDATION



Date: 3/20/2019



Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	TJM	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

STANTEC CONSULTING SERVICES INC.
482 PAYNE ROAD
SCARBOROUGH, ME 04074
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FAX (207) 883-3376

MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

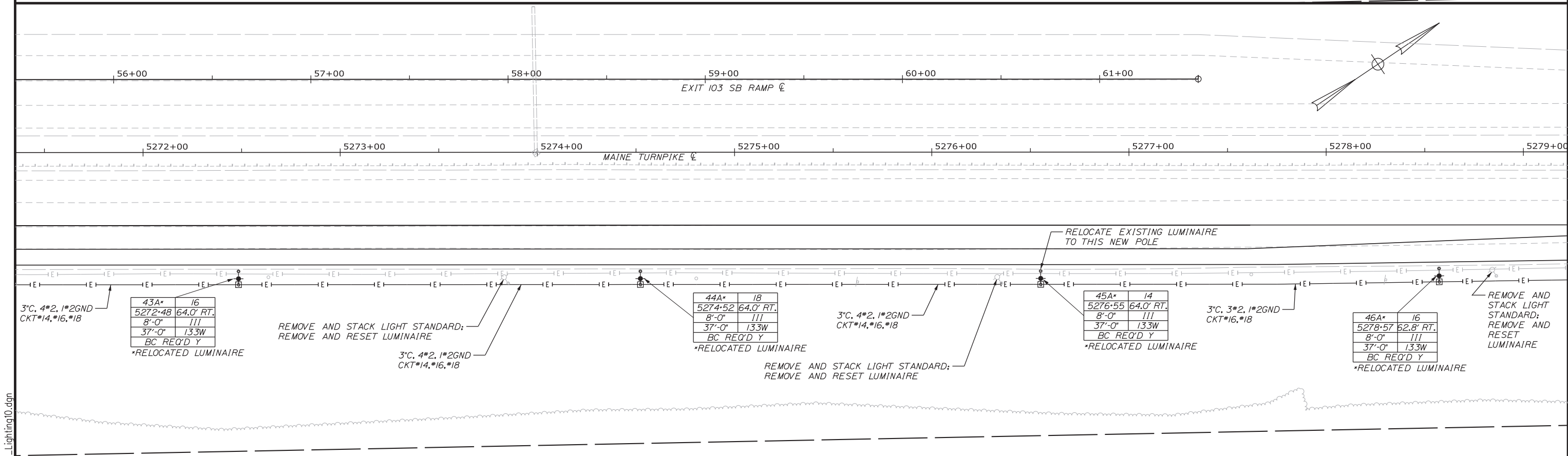
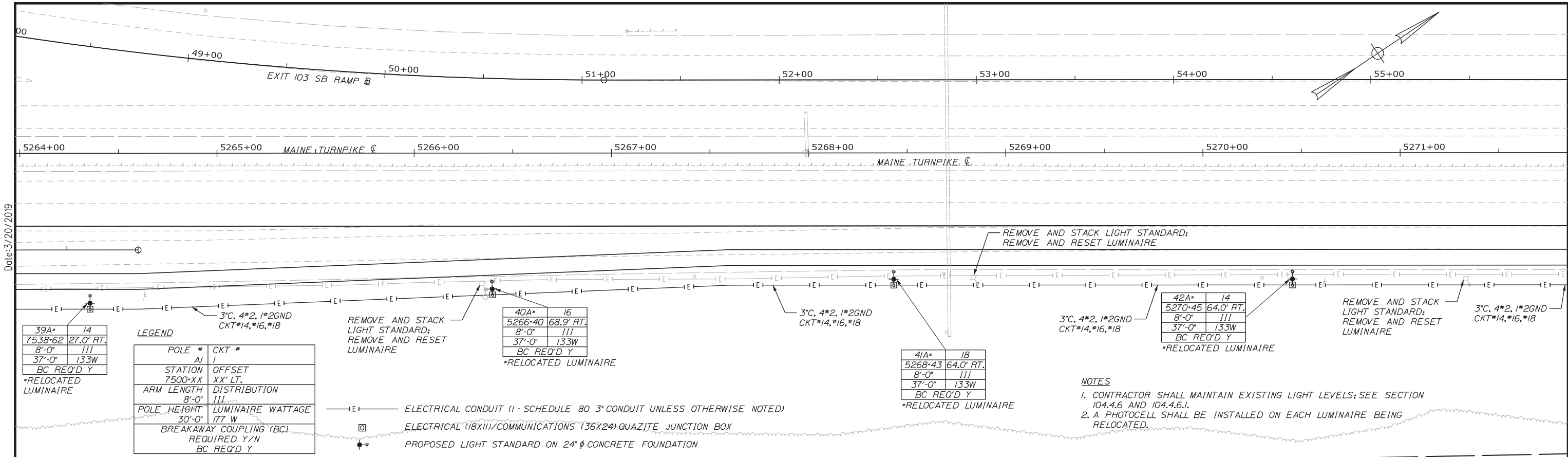
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
LIGHTING PLAN 9

SHEET NUMBER: LP-09
214 OF 503

CONTRACT: 2019.04

Filename: ... \HIGHWAY\MSTA\Lighting09.dgn



Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	TJM	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
LIGHTING PLAN 10

SHEET NUMBER: LP-10
215 OF 503

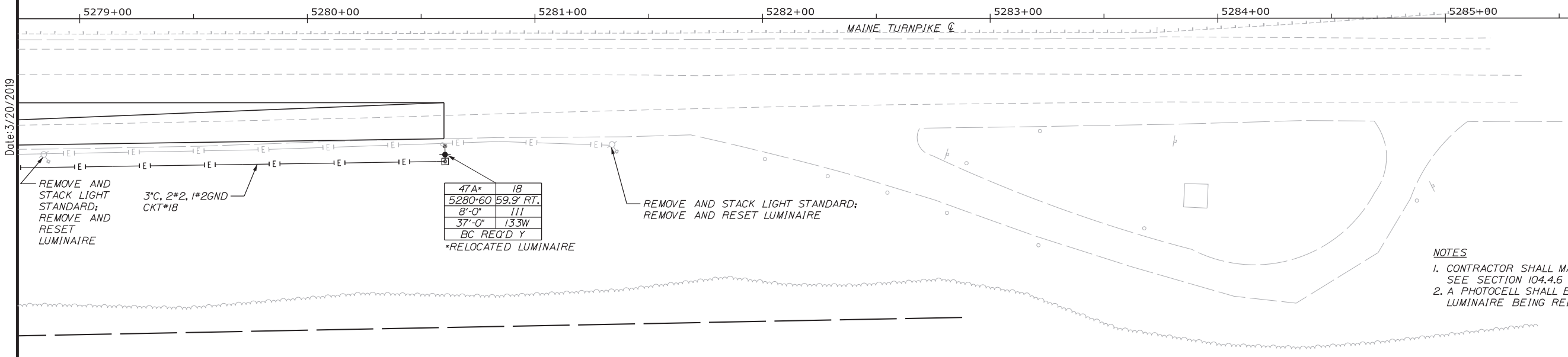
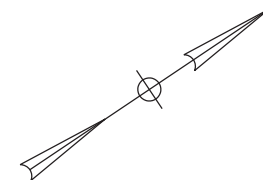
CONTRACT: 2019.04

Date: 3/20/2019
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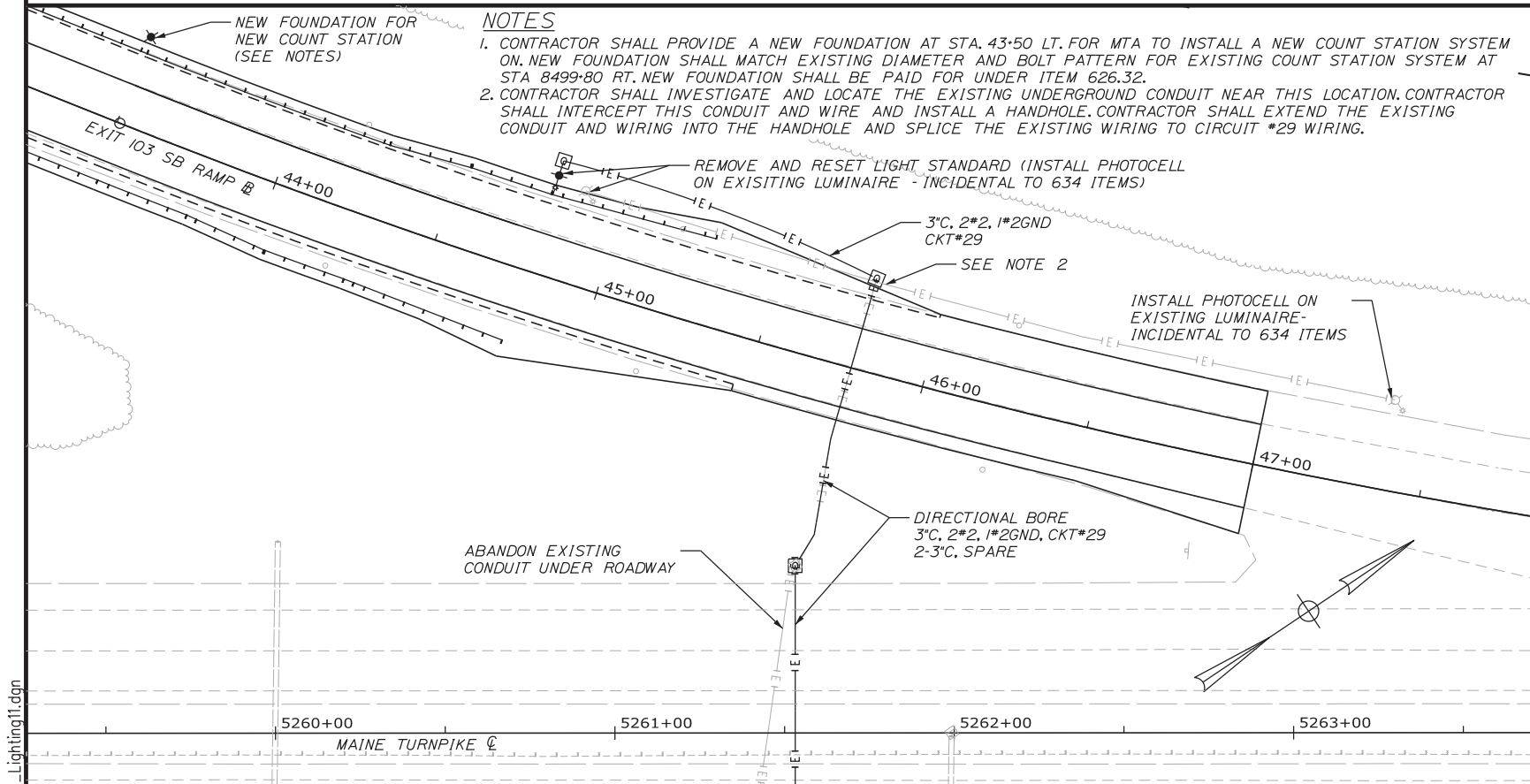
LEGEND

POLE #	CKT #
AI	I
STATION	OFFSET
7500'XX	XX' LT.
ARM LENGTH	DISTRIBUTION
8'-0"	III
POLE HEIGHT	LUMINAIRE WATTAGE
30'-0"	177 W
BREAKAWAY COUPLING (BC)	
REQUIRED Y/N	
BC REQ'D Y	

- |E|— ELECTRICAL CONDUIT (1- SCHEDULE 80 3" CONDUIT UNLESS OTHERWISE NOTED)
- ELECTRICAL (18X11)/COMMUNICATIONS (36X24) QUAZITE JUNCTION BOX
- ◆◆ PROPOSED LIGHT STANDARD ON 24" φ CONCRETE FOUNDATION



- NOTES**
- CONTRACTOR SHALL MAINTAIN EXISTING LIGHT LEVELS; SEE SECTION 104.4.6 AND 104.4.6.1.
 - A PHOTOCELL SHALL BE INSTALLED ON EACH LUMINAIRE BEING RELOCATED.



Scale: 1" = 25'

No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	TJM	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

STANTEC CONSULTING SERVICES INC.
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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

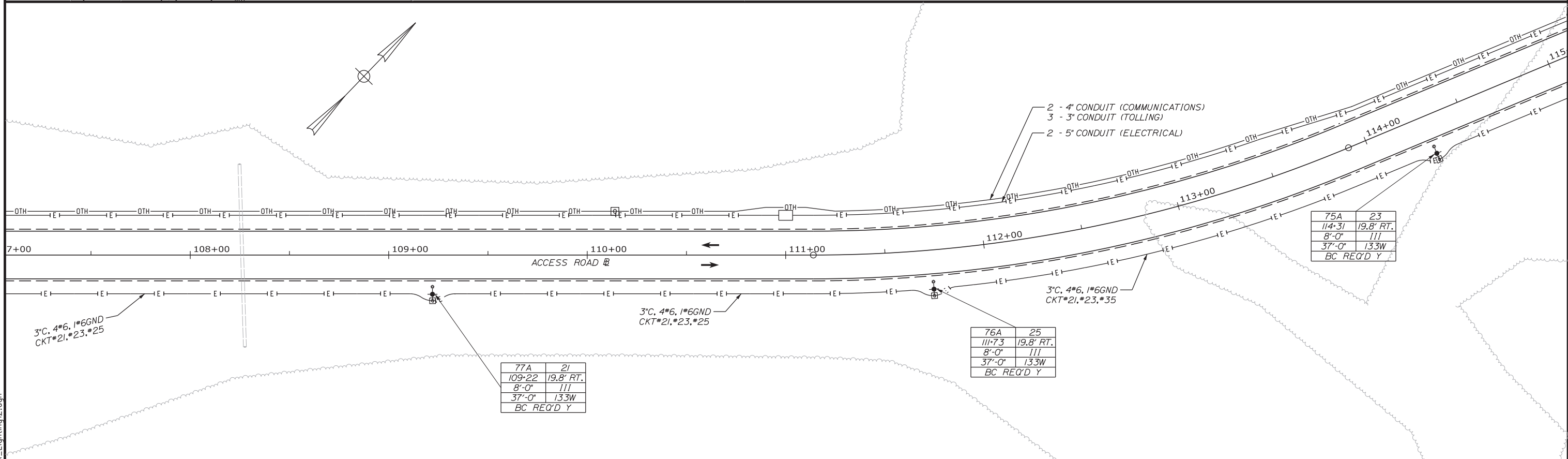
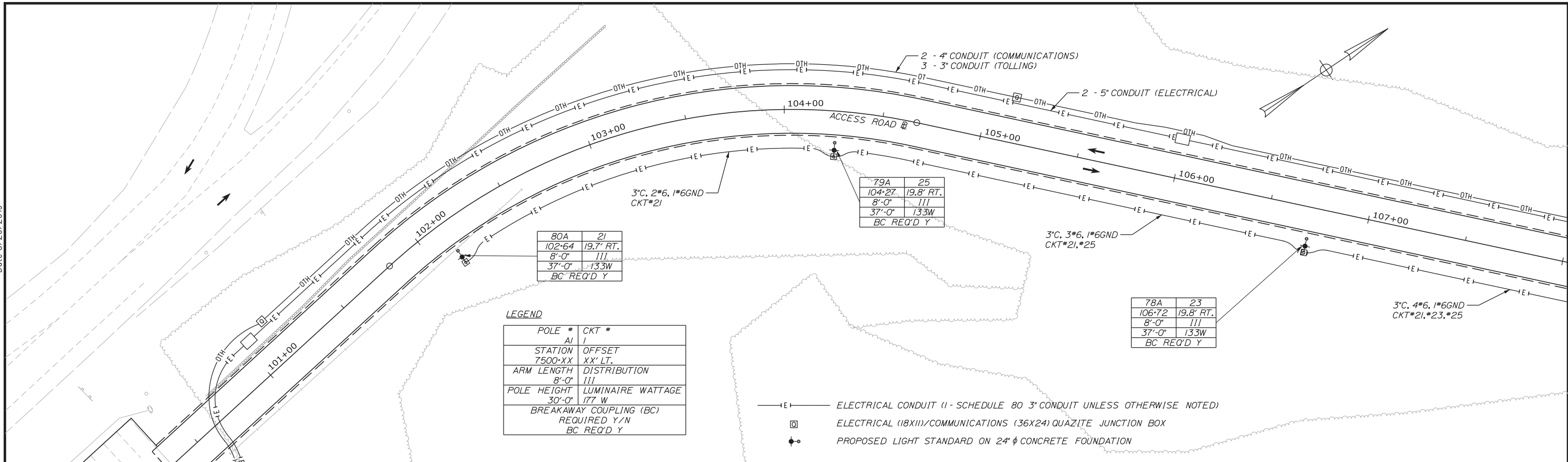
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 LIGHTING PLAN 11

SHEET NUMBER: LP-11
 CONTRACT: 2019.04
 216 OF 503

Date: 3/20/2019
 Filename: ... \HIGHWAY\MSTA\Lighting11.dgn

Date: 3/20/2019



No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	TJM	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
LIGHTING PLAN 12

SHEET NUMBER: LP-12
217 OF 503

CONTRACT: 2019.04

Filename: ... \HIGHWAY\MSTA\Lighting12.dgn

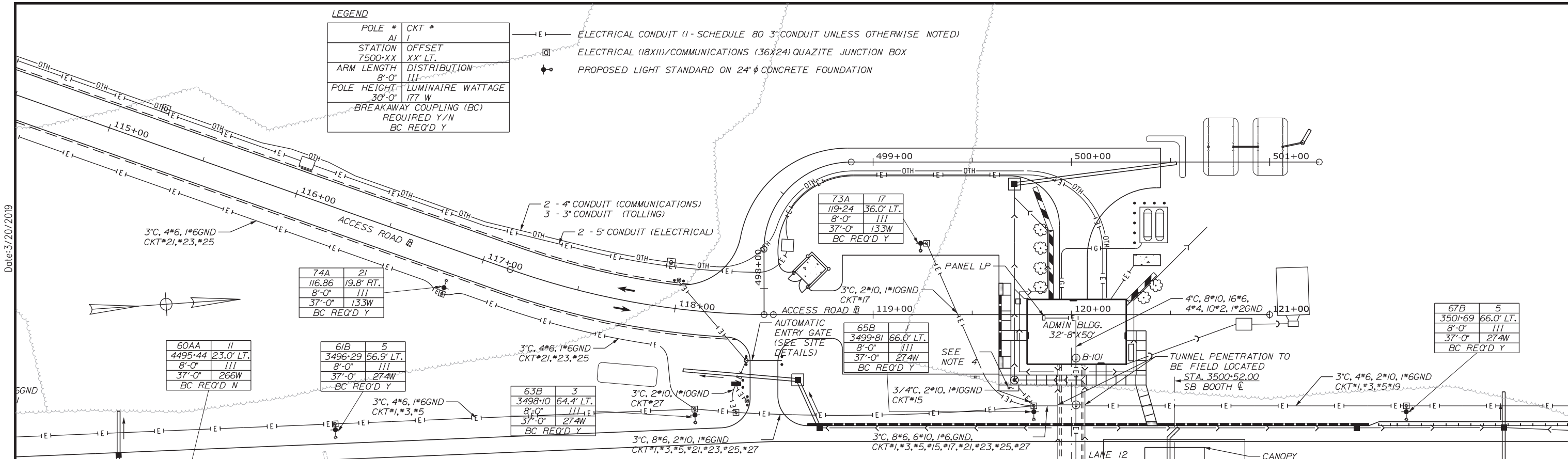
LEGEND

POLE #	CKT #
AI	I
STATION	OFFSET
7500-XX	XX' LT.
ARM LENGTH	DISTRIBUTION
8'-0"	III
POLE HEIGHT	LUMINAIRE WATTAGE
30'-0"	177 W
BREAKAWAY COUPLING (BC)	REQUIRED Y/N
	BC REQ'D Y

—E— ELECTRICAL CONDUIT (1 - SCHEDULE 80 3" CONDUIT UNLESS OTHERWISE NOTED)

□ ELECTRICAL (18X11)/COMMUNICATIONS (36X24) QUAZITE JUNCTION BOX

◆ PROPOSED LIGHT STANDARD ON 24" Ø CONCRETE FOUNDATION



Date: 3/20/2019

Filename: ... \HIGHWAY\MSTA\Lighting13.dgn

Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	TJM	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

STANTEC CONSULTING SERVICES INC.
482 PAYNE ROAD
SCARBOROUGH, ME 04074
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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

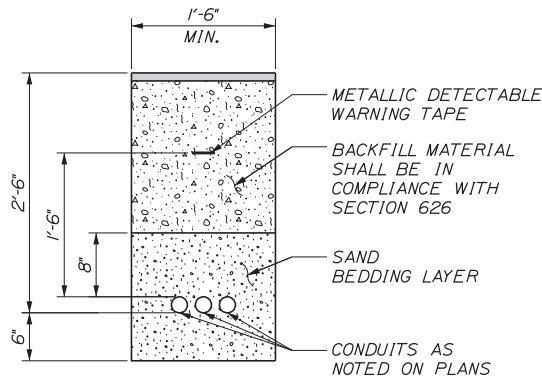
INTERCHANGE 103
ORT CONVERSION

LIGHTING PLAN 13

SHEET NUMBER: LP-13
218 OF 503

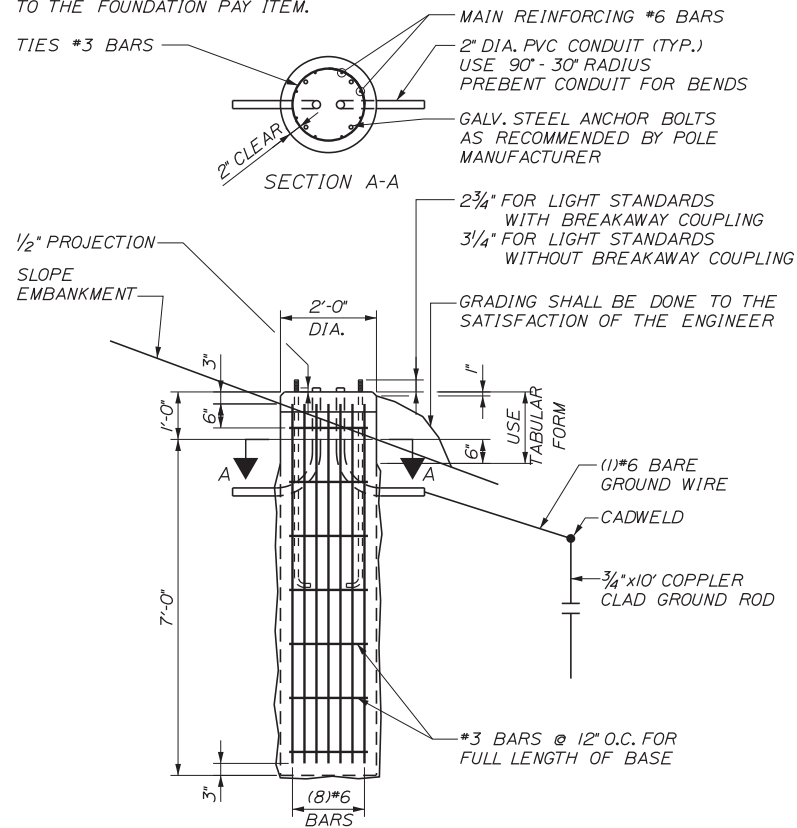
CONTRACT: 2019.04

Date: 3/20/2019

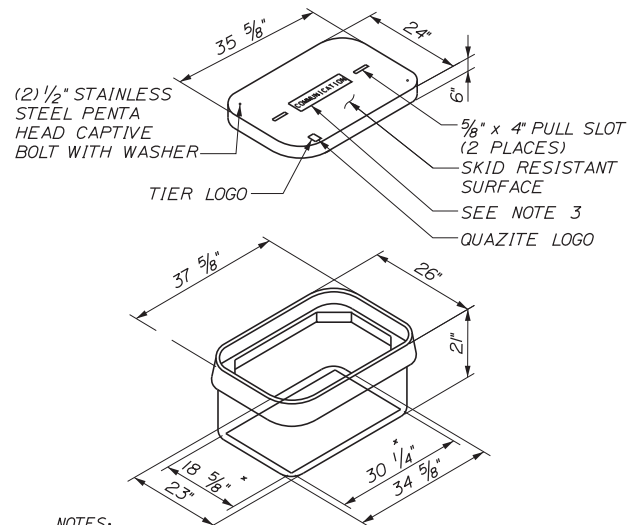


TRENCH CROSS SECTION
NOT TO SCALE

NOTE:
WHERE SOLID ROCK IS ENCOUNTERED AT LESS THAN THE REQUIRED DISTANCE BELOW GROUND LEVEL, REINFORCING STEEL SHALL BE DOWELED INTO LEDGE AS SHOWN ON MAINEDOT STANDARD DETAIL 606(06). PAYMENT SHALL BE INCIDENTAL TO THE FOUNDATION PAY ITEM.

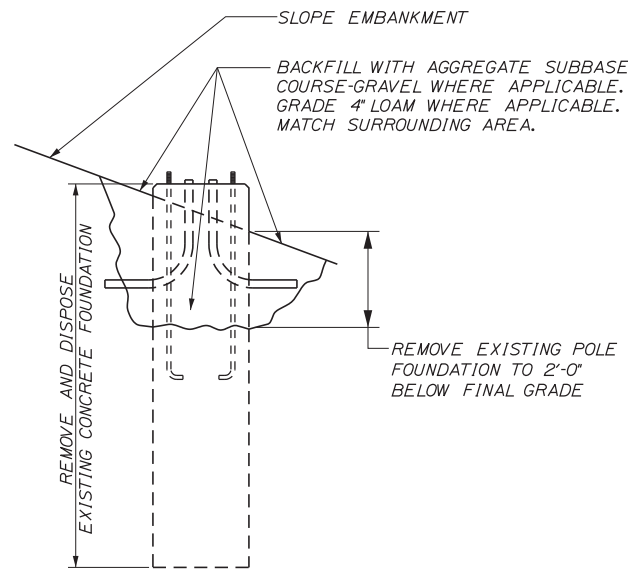


24 INCH FOUNDATION
NOT TO SCALE

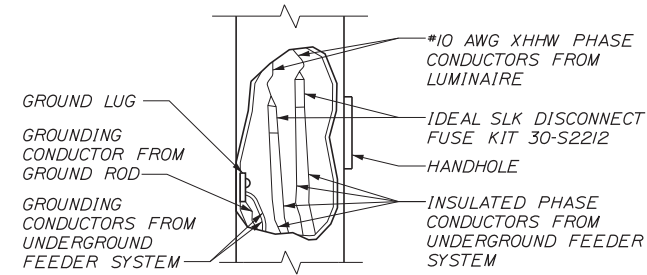


- NOTES:
- SPLICES IN BOX SHALL BE MADE WITH ILSCP USPA-350-SS-DB SAFTEYSUB WATER TIGHT DIRECT BURY SPLICE WIRE RANGE 350MCM-10-STR, ONLY. PROVIDE ENOUGH SLACK IN THE WIRE TO ALLOW REMOVAL OF SPLICES AND NEATLY ARRANGE WIRE IN BOX.
 - PROVIDE 12" CRUSHED STONE OR GRAVEL UNDER BOX.
 - THE COVER SHALL BE LABELED WITH THE FUNCTION DESIGNATION IN 1" MIN TEXT AS FOLLOWS:
- "LIGHTING" FOR LIGHTING CONDUIT
- "COMMUNICATION" FOR COMMUNICATIONS CONDUIT
 - DIMENSIONS SHOWN FOR ITEM 626.121 QUAZITE JUNCTION BOX (36X24). SEE MANUFACTURERS DETAILS FOR DIMENSIONS OF ITEM 626.122 (18X11).

QUAZITE BOX DETAIL
NOT TO SCALE

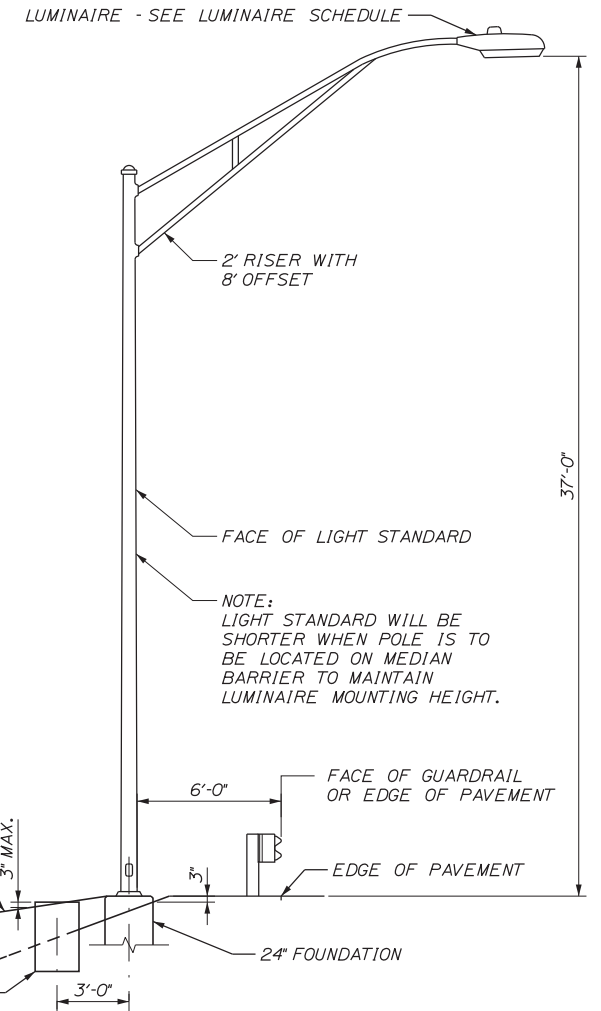


REMOVAL OF CONCRETE FOUNDATION
NOT TO SCALE

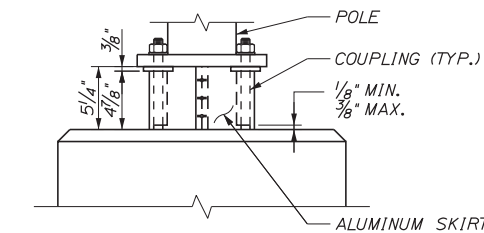


TYPICAL WIRING DETAIL
NOT TO SCALE

NOTE: CONTRACTOR SHALL PROVIDE 18" OF SLACK IN POLES



PLACEMENT OF LIGHT STANDARD
NOT TO SCALE



* BREAKAWAY COUPLINGS SHALL BE REQUIRED ON ALL NEW LIGHT STANDARDS EXCEPT POLES MOUNTED ON BARRIER

BREAKAWAY COUPLINGS AND SKIRT DETAIL
NOT TO SCALE

Filename: ...MSTA...Detail-Lighting_01.dgn

No.	Revision	By	Date

Designed by:			
CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
Designed	TJM	3\20\19	Checked
Drawn	EJB	3\20\19	In Charge of
			By
			Date
			By
			Date

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THE GOLD STAR MEMORIAL HIGHWAY

INTERCHANGE 103
ORT CONVERSION

LIGHTING AND COMMUNICATION DETAILS 1

SHEET NUMBER: LDET-01
CONTRACT: 2019.04
219 OF 503

GENERAL NOTES: LIGHTING

1. THERE SHALL BE AN INDIVIDUAL PHOTOCELL FOR EACH LUMINAIRE.
2. LIGHTING FIXTURE VOLTAGE SHALL BE 120 VOLTS.
3. LIGHTING FIXTURES SHALL BE IES FULL CUTOFF, LIGHT EMITTING DIODE (LED) FIXTURES.
4. ALL FIXTURES SHALL BE GASKETED AND HAVE SURGE PROTECTION AND A DOUBLE FUSE KIT. ALL FIXTURES SHALL BE GRAY. IF DIFFERENT FIXTURES ARE PROPOSED, THEY SHALL BE IES FULL CUTOFF OR CUTOFF, TYPE 3 IES DISTRIBUTION TYPE AS NOTED IN THE LUMINAIRE SCHEDULE. LED LUMINAIRES, B.U.G. RATINGS SHALL BE EQUAL TO OR BETTER THAN THE DESIGNED FIXTURES. THE CONTRACTOR MUST DEMONSTRATE THAT THE PROPOSED FIXTURES WILL REASONABLY EQUAL THE LIGHT LEVELS AND DISTRIBUTIONS SHOWN ON THE PLANS, IN THE OPINION OF MTA.
5. EVALUATION OF ALTERNATIVE LED LUMINAIRES THAT MAY BE PROPOSED BY THE CONTRACTOR FOR SUBSTITUTION WILL REQUIRE SUBMITTAL OF THE FOLLOWING, AT MINIMUM: IES LM-79-08 ABSOLUTE TESTING REPORT FOR THE PROPOSED ALTERNATIVE LUMINAIRE; IES LM-80-15 TESTING REPORT FOR LED CHIPS TO BE USED IN THE ALTERNATIVE LUMINAIRE, DOCUMENTING TESTING FOR A MINIMUM OF 8500 HOURS; IES TM-21-II REPORT FOR PROJECTED LONG TERM LUMEN MAINTENANCE, INCLUDING INCREMENTAL LUMEN DEPRECIATION TABLE AT 25 DEGREES CELSIUS TO A MINIMUM OF 50,000 HOURS; IES PHOTOMETRIC FILE FROM THE MANUFACTURER FOR THE PROPOSED ALTERNATIVE LUMINAIRE; PHOTOMETRIC PLOT, OVERLAID ON THE LAYOUT OF PROJECT, SHOWING LIGHT CONTOURS, ILLUMINATION STATISTICS FOR EACH OF THE LIGHTING GROUPS, AND VALUE OF LIGHT LOSS FACTOR USED IN THE ANALYSIS; VALUES OF LLD, LDD, BALLAST FACTOR AND OTHER FACTORS USED FOR CALCULATION OF THE ASSUMED LIGHT LOSS FACTOR; SPECIFICATION DATA REGARDING OPTICS, CHROMATIC COLOR TEMPERATURE, DRIVER, SURGE PROTECTION, HOUSING AND GASKETING.
6. CONDUIT SHALL BE 3" MINIMUM, PVC SCHEDULE 80. MINIMUM BURIAL DEPTH FOR CONDUIT SHALL BE 30".
7. A JUNCTION BOX SHALL BE INSTALLED AT EACH POLE. THE WIRING IN CONDUITS SHALL BE CONTINUOUS BETWEEN JUNCTION BOXES WITHOUT RUNNING SPLICES IN CONDUITS.
8. UPON COMPLETION OF THIS PROJECT, THE CONTRACTOR SHALL FURNISH TO MTA A SET OF AS-BUILT PLANS FOR FUTURE REFERENCE AND SYSTEM MAINTENANCE.
9. ALL LIGHT BASES SHALL HAVE A GROUND ROD LOCATED ADJACENT TO THE POLE THAT IS BONDED TO THE GROUNDING CONDUCTOR. PAYMENT FOR THE GROUND ROD SHALL BE INCLUDED IN LIGHT POLE ITEM.
10. PAYMENT UNDER ITEM 634.231, CONVENTIONAL LIGHT STANDARD WITH LED FIXTURE, WILL INCLUDE BUT NOT BE LIMITED TO NEW POLES, LED FIXTURE, WIRING, AND BREAKAWAY DEVICES.
11. LIGHTING SERVICE PANEL SHALL BE MARKED WITH ARC FLASH HAZARD TYPE 1, 2, 3 OR 4 AND THE APPROPRIATE PPE REQUIRED.
12. BREAKAWAY DEVICES FOR LIGHT POLES SHALL CONFORM TO THE LATEST VERSION OF "AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS" AND NCHRP REPORT 350. THE BREAKAWAY DEVICE SHALL BE DESIGNED SO THAT THE ANCHOR BOLTS WILL NOT BEND WHEN A VEHICLE HITS THE POLE. A FRANGIBLE COUPLING SUCH AS TRANSPO POLE-SAFE 5000 SERIES (WITH A FEMALE ANCHOR), THE MANITOBA SAFETY BASE WITH REACTION PLATE, OR APPROVED EQUAL SHALL BE USED. BREAKAWAY DEVICES SHALL BE INSTALLED ON ALL POLES EXCEPT THOSE LOCATED ON BARRIERS.
13. WIRE SHALL BE STRANDED COPPER XHHW-2, SIZE AS NOTED ON THE PLANS.
14. FOUNDATIONS SHALL BE PRECAST 24 INCH DIAMETER BY 7' HEIGHT FOUNDATIONS.
15. LIGHT POLES THAT ARE REMOVED SHALL BE CAREFULLY REMOVED AND DELIVERED TO THE MTA SIGN SHOP AT MM 58.3 NB. WHEN REMOVING LIGHT POLES, REMOVE ALL EXISTING CONDUCTORS ASSOCIATED WITH THE LIGHT POLE. WHEN REMOVING HANDHOLES, REMOVE ALL CONDUCTORS IN CONDUITS ENTERING THE HANDHOLE BACK TO NEAREST HANDHOLE OR LIGHTING UNIT TO REMAIN AND ABANDON THE EMPTY CONDUIT IN PLACE. PRIOR TO CONDUCTOR REMOVAL, CONTRACTOR SHALL TEST THAT CIRCUITS THAT SERVICE EXISTING LOADS ARE NOT AFFECTED BY REMOVAL.
16. EXISTING INTERCHANGE LIGHTING IS INTENDED TO REMAIN OPERATIONAL UNTIL NEW LIGHTING IS ACTIVATED. IF NECESSARY TO REMOVE OR DEACTIVATE ANY LUMINAIRE BEFORE NEW LIGHTING IS ACTIVATED, WHEN APPROVED BY THE RESIDENT, TEMPORARY LIGHTING SHALL BE USED TO MAINTAIN ILLUMINATION AND WILL BE PAID UNDER ITEM NO. 800.30 EXISTING TOLL PLAZA DEMOLITION. ALL TEMPORARY LIGHTING SHALL BE PROTECTED OR LOCATED OUTSIDE OF THE CLEAR ZONE.
17. AT EACH POLE, CONTRACTOR SHALL PROVIDE 18" OF EXTRA WIRE SLACK.
18. ALL REMOVED AND RESET LIGHT STANDARDS SHALL HAVE BREAKAWAY BASES/COUPLINGS.
19. FOR ALL POLES TO BE RELOCATED, REMOVE ALL EXISTING WIRING FROM THE EXISTING POLE AND REPLACE PER THE TYPICAL WIRING DETAIL ON SHEET "LDET-0".
20. FOR ALL CONDUIT RUNS FROM HANDHOLE TO POLE, THE CONTRACTOR SHALL INSTALL 2" WITH 2#10 AND 1#10GND.


CALCULATION SUMMARY							
PROJECT: ROADWAY LIGHTING CALCULATIONS							
LABEL	CALCTYPE	UNITS	AVG	MAX	MIN	AVG/MIN	MAX/MIN
PARKING AREA	ILLUMINANCE	FC	1.15	1.8	0.6	1.92	3.00
I-295 - SOUTH OF TOLLS	ILLUMINANCE	FC	0.86	2.5	0.3	2.87	8.33
I-295 NORTH OF TOLLS INCL. I-95	ILLUMINANCE	FC	0.78	3.1	0.3	2.60	10.33
TOLL APPROACH/DEPARTURE AREA	ILLUMINANCE	FC	3.05	13.4	1.1	2.77	12.18

ROADWAY LUMINAIRE SCHEDULE					
LABEL	CATALOG NUMBER	DESCRIPTION	MINIMUM LUMENS	LIGHT LOSS FACTOR	MAXIMUM WATTS
A	AMERICAN ELECTRIC ATB2 60BLEDE70 MVOLT R3 NL PCLL	LED COBRAHEAD, 4000K CCT, IES TYPE 3 DISTRIBUTION	17000	0.67	133
B	AMERICAN ELECTRIC ATB2 80BLEDE10 MVOLT R3 NL PCLL	LED COBRAHEAD, 4000K CCT, IES TYPE 3 DISTRIBUTION	30000	0.67	274
C	HYDREL M9820ALED-WHT53KMVOLTN-SPFLC34BIHLLPDNA	LED IN-GROUND, 5000K CCT, NARROW SPOT DISTRIBUTION	4600	0.67	274

Date: 3/20/2019

Filename: ...MSTA...Detail-Lighting_02.dgn

Scale:				
No.	Revision	By	Date	

Designed by:					
					
CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE					
	By	Date		By	Date
Designed	TJM	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

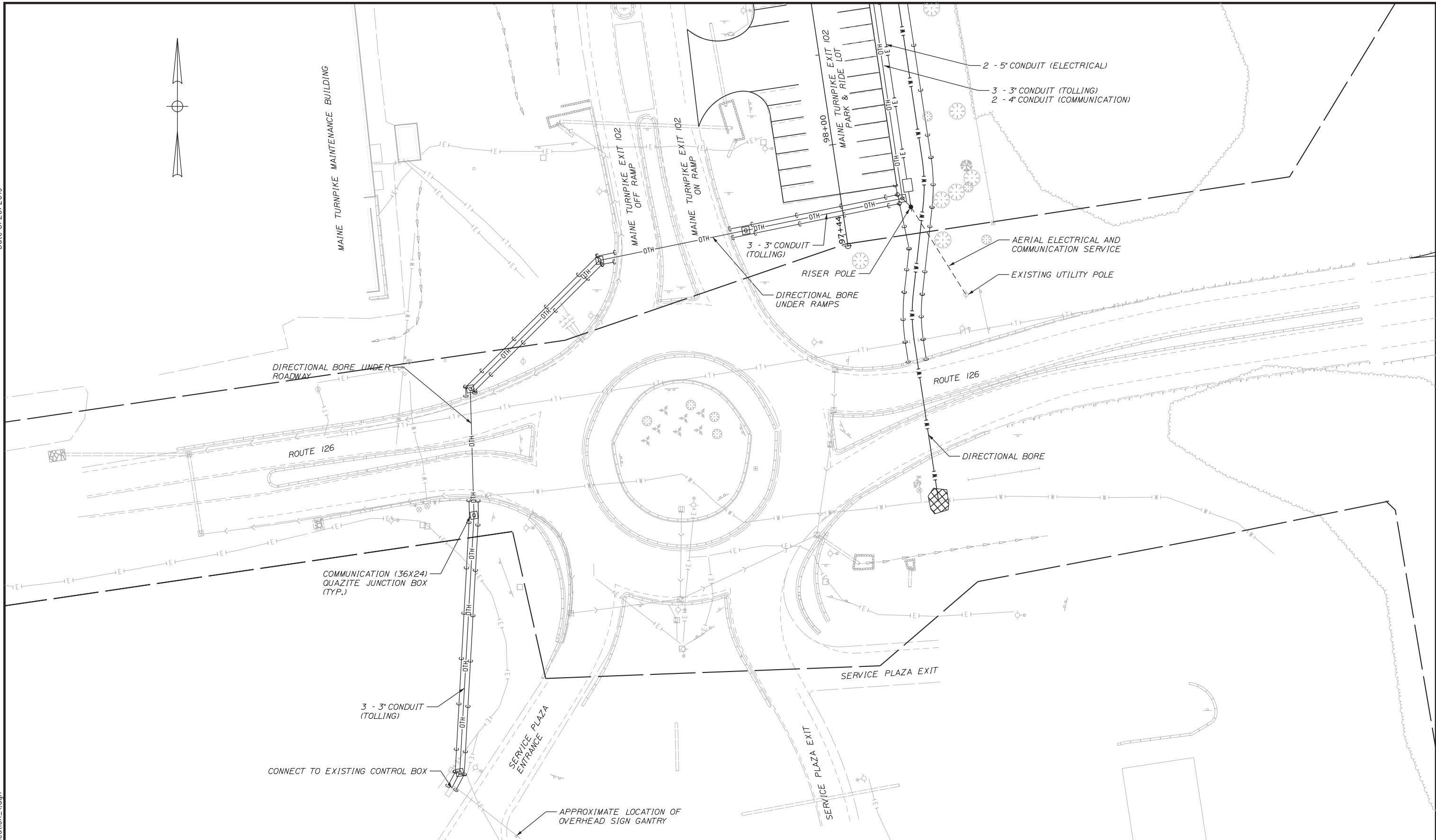
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 LIGHTING NOTES, SCHEDULES
 AND DETAILS

SHEET NUMBER: LDET-02
 CONTRACT: 2019.04
 220 OF 503

Date: 3/20/2019

Filename: ...MSTA_Communication_1.dgn



Scale: 25 0 25 50
Scale of Feet

No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
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Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

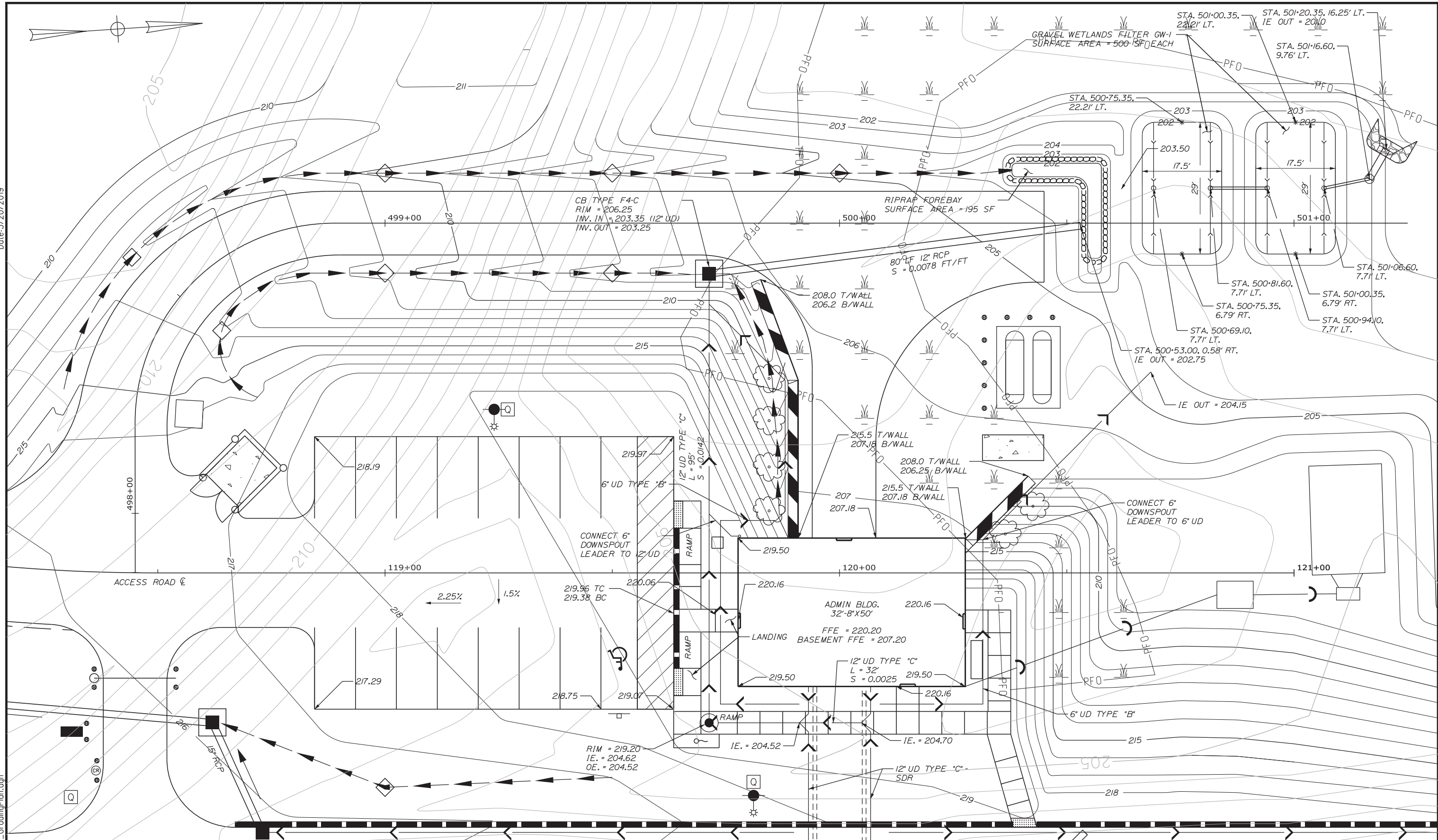
COMMUNICATION PLAN 1

SHEET NUMBER: COMM-01
221 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...HIGHWAY_MSTA...GradingPlan.dgn



Scale: 1" = 20'

No.	Revision	By	Date

Designed by: **Stantec**

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

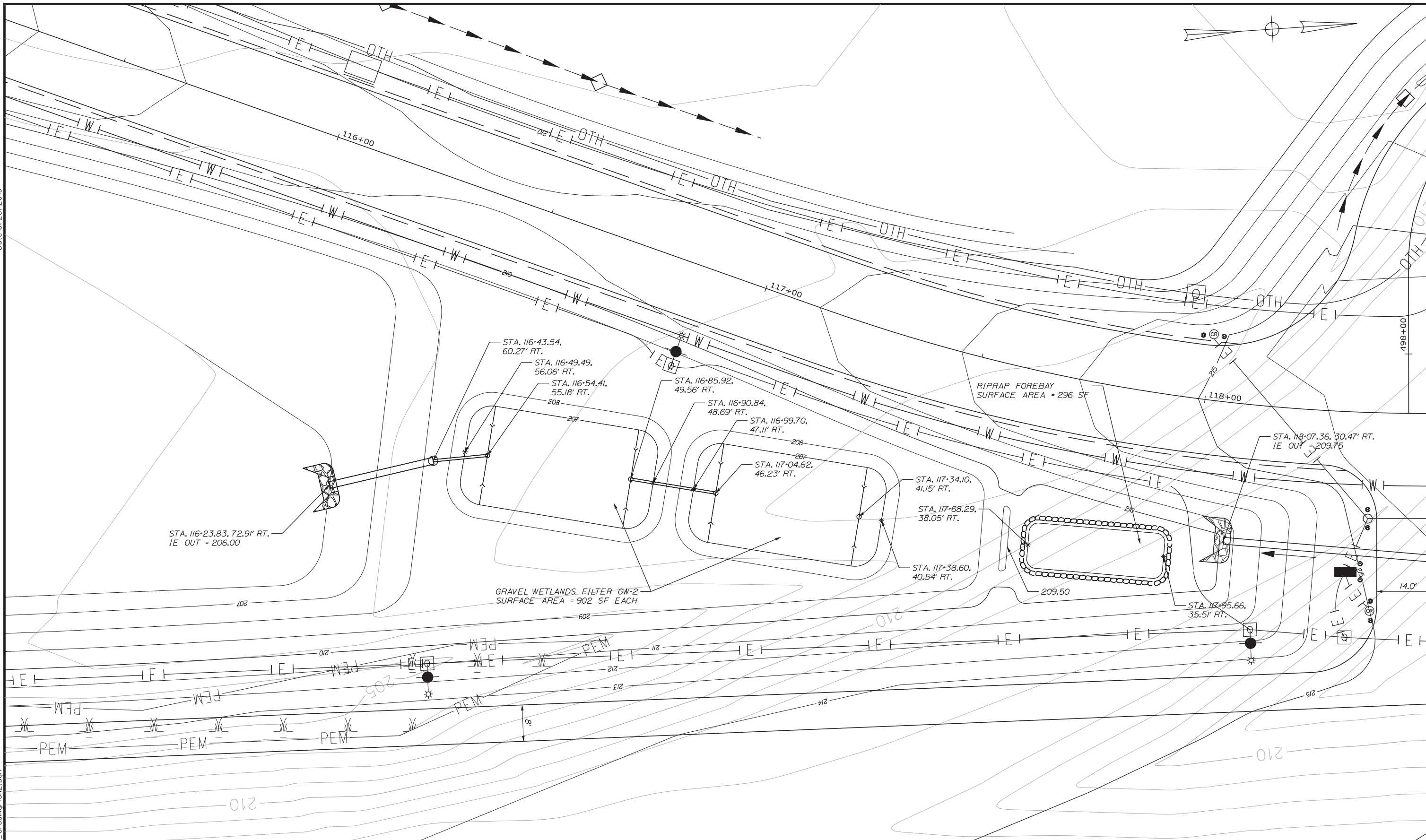
INTERCHANGE 103 ORT CONVERSION ADMIN BUILDING GRADING AND DRAINAGE PLAN 1

SHEET NUMBER: SP-02
 223 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...HIGHWAY_MSTA...GradingPlan2.dgn



Scale: 1" = 20'

No.	Revision	By	Date

Designed by: **Stantec**

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date	Checked	By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

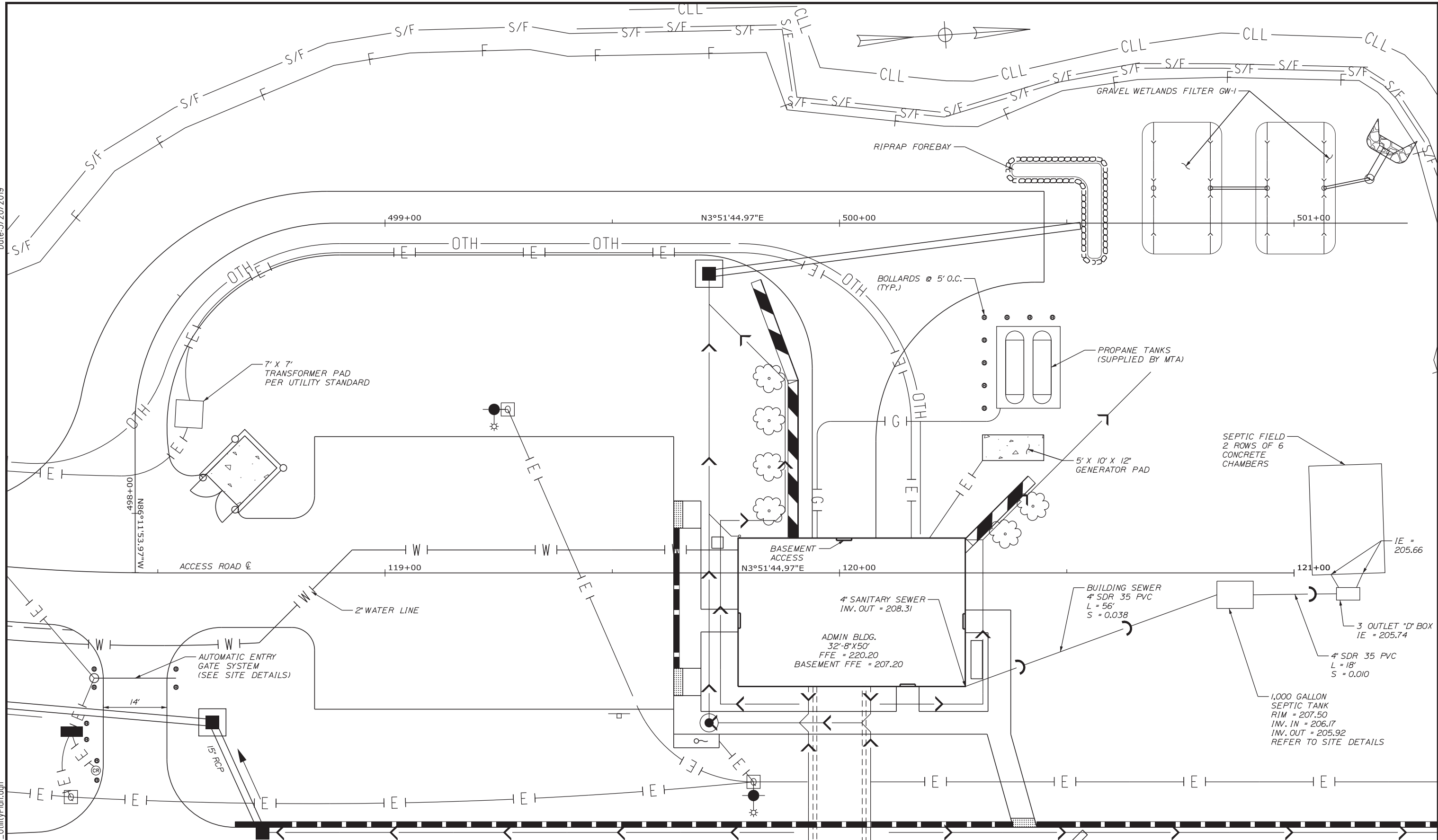
INTERCHANGE 103
 ORT CONVERSION
 ADMIN BUILDING
 GRADING AND DRAINAGE PLAN 2

SHEET NUMBER: SP-03
 224 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ... \HIGHWAY\MSTA\UtilityPlan.dgn



Scale: 10 0 10 20
Scale of Feet

No.	Revision	By	Date

Designed by: **Stantec**

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

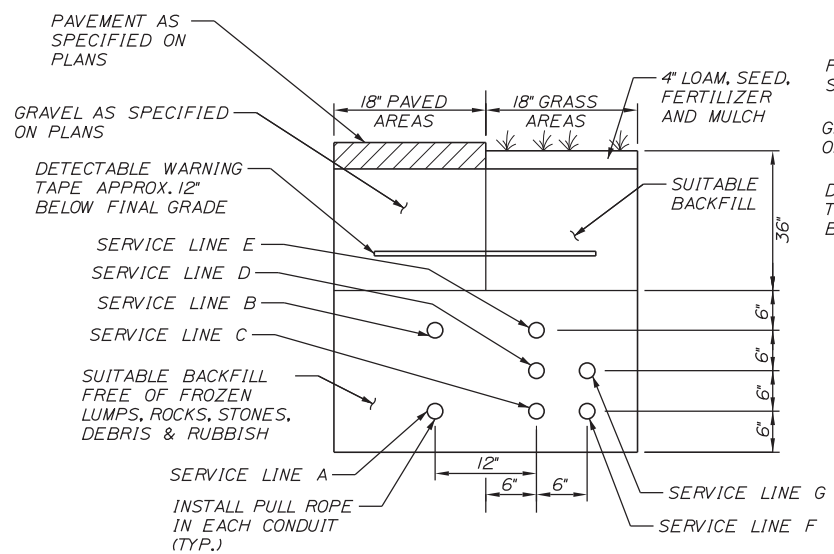
INTERCHANGE 103 ORT CONVERSION ADMIN BUILDING UTILITY PLAN

SHEET NUMBER: SP-04
225 OF 503

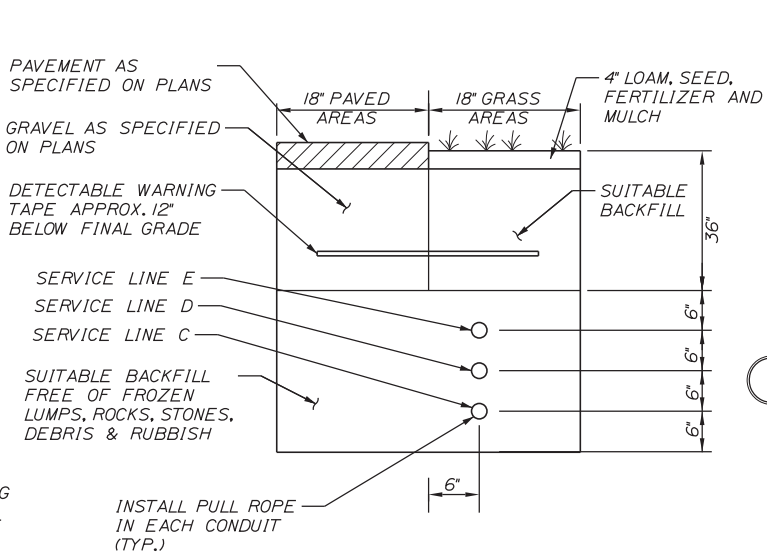
CONTRACT: 2019.04

Date: 3/20/2019

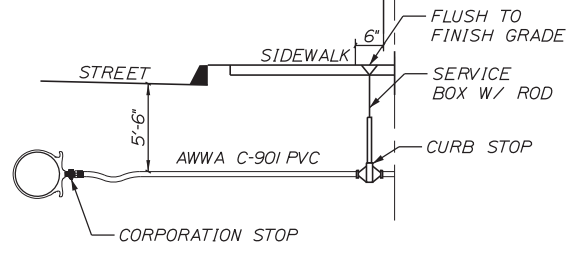
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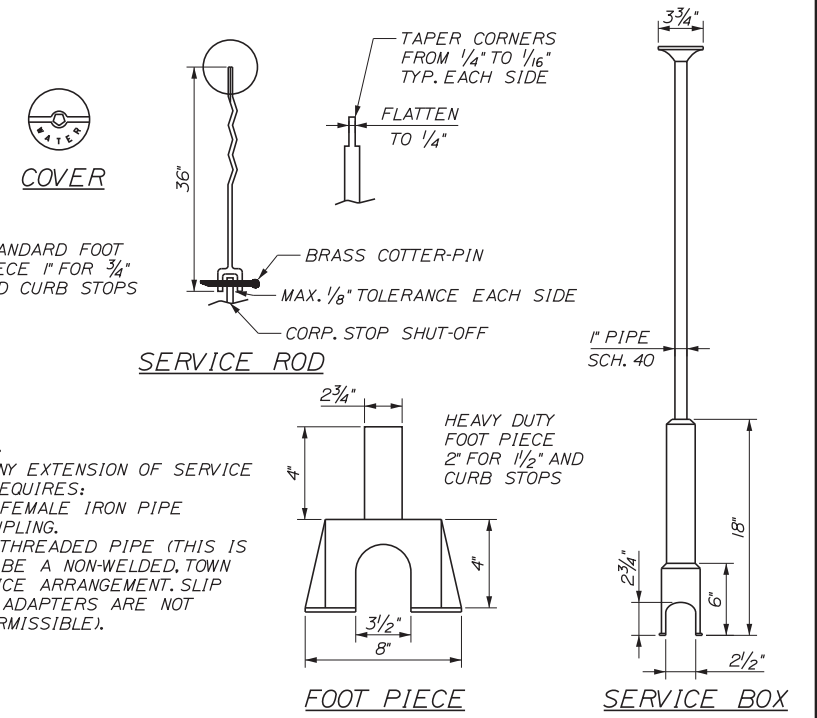
COMBINED UTILITY TRENCH DETAIL
NOT TO SCALE



TOLLING TRENCH DETAIL
NOT TO SCALE



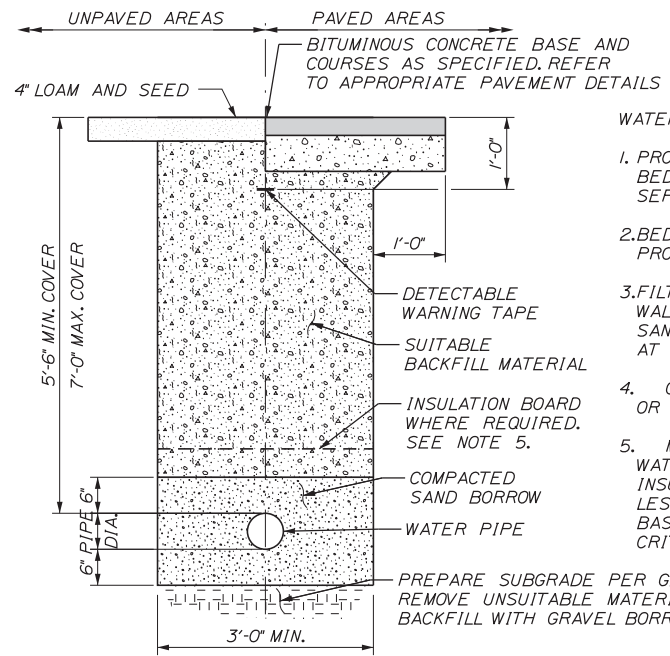
TYPICAL SERVICE CONNECTION
NOT TO SCALE



NOTES:
1. ANY EXTENSION OF SERVICE BOX REQUIRES:
A. 1\"/>

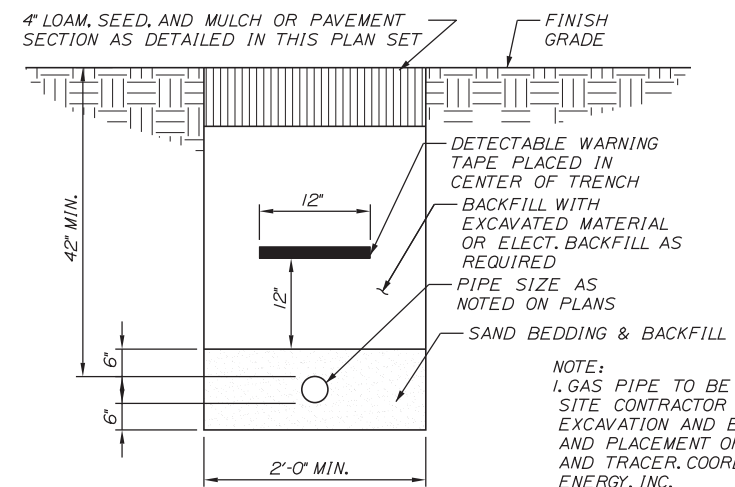
SERVICE	CONDUIT SIZE	CONDUIT TYPE	UTILITY	REMARKS
A	5"	SCHEDULE 80 PVC ELECTRICAL GRADE	POWER	SEE NOTES
B	5"	SCHEDULE 80 PVC ELECTRICAL GRADE	POWER	SEE NOTES
C	3"	SCHEDULE 80 PVC ELECTRICAL GRADE	TOLLING	SEE NOTES
D	3"	SCHEDULE 80 PVC ELECTRICAL GRADE	TOLLING	SEE NOTES
E	3"	SCHEDULE 80 PVC ELECTRICAL GRADE	TOLLING	SEE NOTES
F	4"	SCHEDULE 80 PVC ELECTRICAL GRADE	COMMUNICATION	SEE NOTES
G	4"	SCHEDULE 80 PVC ELECTRICAL GRADE	COMMUNICATION	SEE NOTES

NOTES:
1. ONE CONDUIT CAPPED FOR SPARE, PROVIDE GALVANIZED STEEL LONG SWEEP AT RISER POLE AND EXTEND GALVANIZED CONDUIT TO 10' ABOVE GRADE AT POLE WITH STAND-OFF BRACKETS.
2. CMP STANDARDS SHALL SUPERCEDE DETAIL.



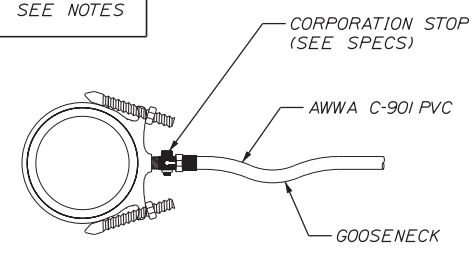
WATER SERVICE DETAIL
NOT TO SCALE

WATER SERVICE NOTES:
1. PROVIDE 7 FOOT COVER WHERE LOCATED UNDER PAVEMENT. IF BEDROCK IS ENCOUNTERED, PROVIDE MINIMUM 1 FOOT SEPARATION AND INSULATE AS NOTED BELOW.
2. BEDDING SHALL BE COMPACTED TO A MINIMUM 95% STANDARD PROCTOR DENSITY.
3. FILTER FABRIC SHALL BE INSTALLED AGAINST THE TRENCH WALLS IN THE HAUNCHING AND BEDDING ZONES WHEN FINE SANDS, SILT, CLAY OR ORGANIC MATERIALS ARE ENCOUNTERED AT THE TRENCH BOTTOM.
4. CONTRACTOR SHALL SHORE TRENCH SIDES WHEN REQUIRED OR AS DIRECTED BY THE RESIDENT.
5. MINIMUM 2\"/>

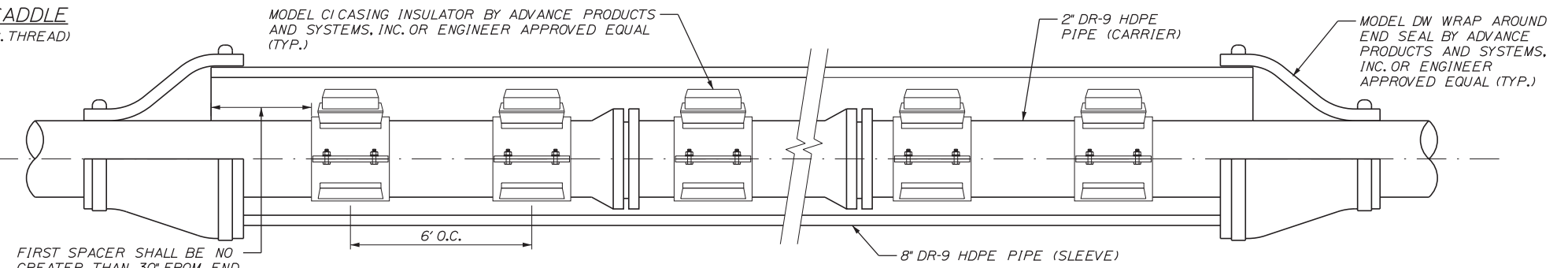


PROPANE SERVICE TRENCH SECTION
NOT TO SCALE

NOTE:
1. GAS PIPE TO BE INSTALLED BY OTHERS. SITE CONTRACTOR IS RESPONSIBLE FOR EXCAVATION AND BACKFILL OF THE GAS LINE AND PLACEMENT OF THE WARNING TAPES AND TRACER. COORDINATE WITH DOWNEAST ENERGY, INC.
2. THE CONTRACTOR SHALL PROVIDE 300 FEET OF OPEN GAS TRENCH EACH DAY. ANY BACKCHARGE FROM AUTHORITY'S CURRENT PROVIDER DUE TO NOT MEETING THIS REQUIREMENT SHALL BE DEDUCTED FROM THE CONTRACTOR'S PAYMENT.



SERVICE SADDLE
(1-1/2\"/>



WATER SERVICE WITHIN CASING PIPE
NOT TO SCALE

No.	Revision	By	Date

Designed by:			
CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
Designed	PLP	3\20\19	Checked
Drawn	EJB	3\20\19	In Charge of

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THE GOLD STAR MEMORIAL HIGHWAY

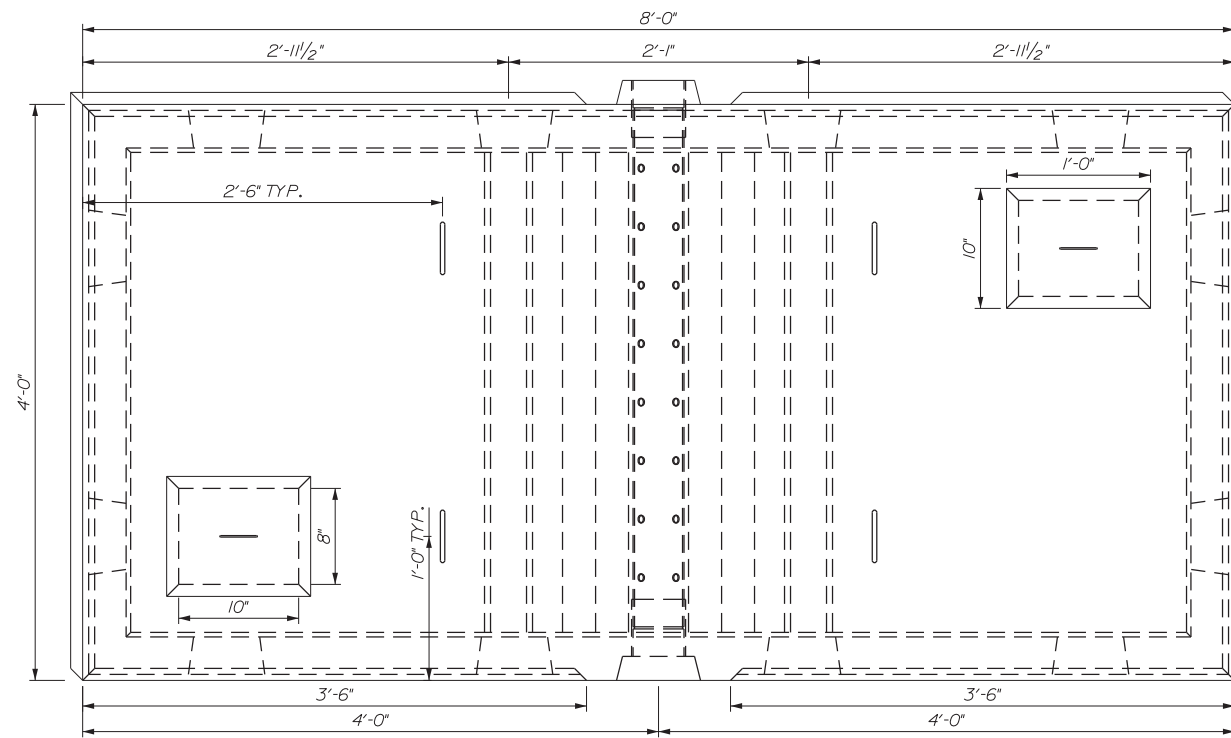
INTERCHANGE 103
ORT CONVERSION
SITE DETAILS 1

SHEET NUMBER: SP-05
226 OF 503

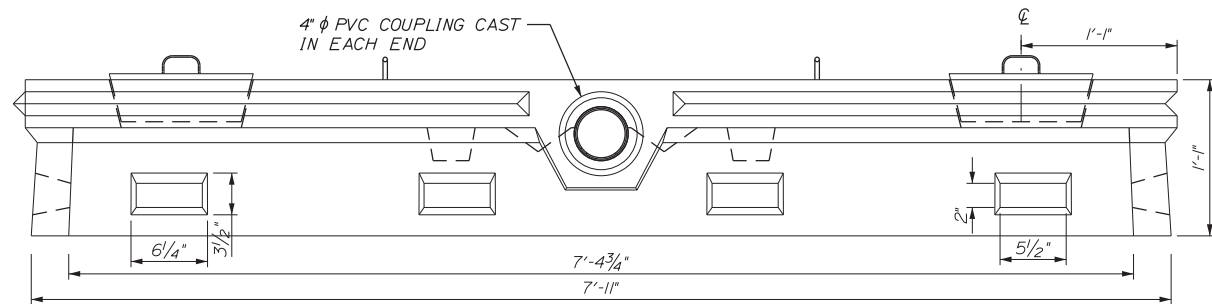
CONTRACT: 2019.04

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

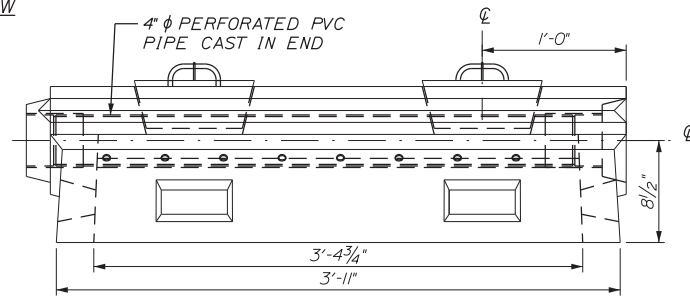
Date: 3/20/2019



PLAN VIEW



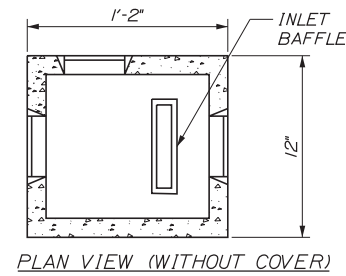
ELEVATION VIEW



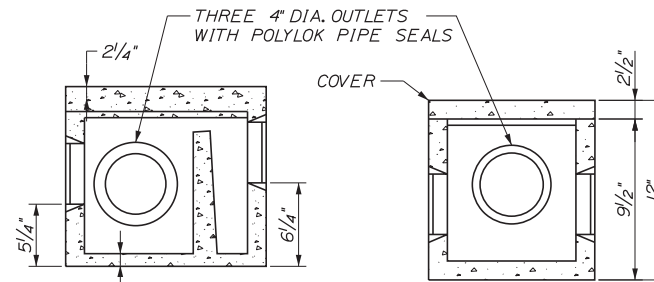
END VIEW

SHORT PIPE LEACHING CHAMBER

- NOTES:
1. CONCRETE 5,000 PSI AT 28 DAYS.
 2. AIR ENTRAINMENT: 4-6%
 3. GRADE 60 REINFORCEMENT.
 4. LEACHING CHAMBER SHALL BE H2O LOADING RATED.



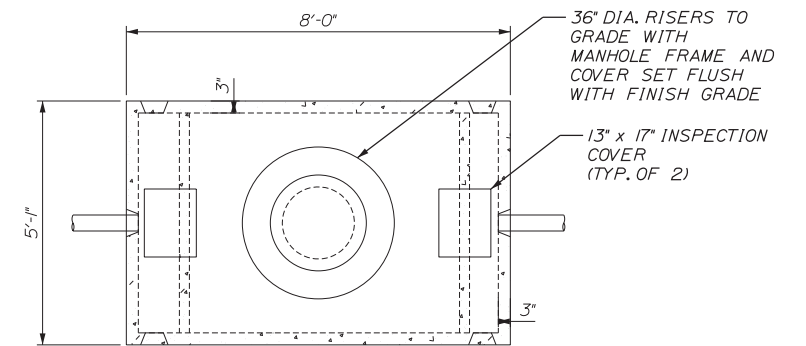
PLAN VIEW (WITHOUT COVER)



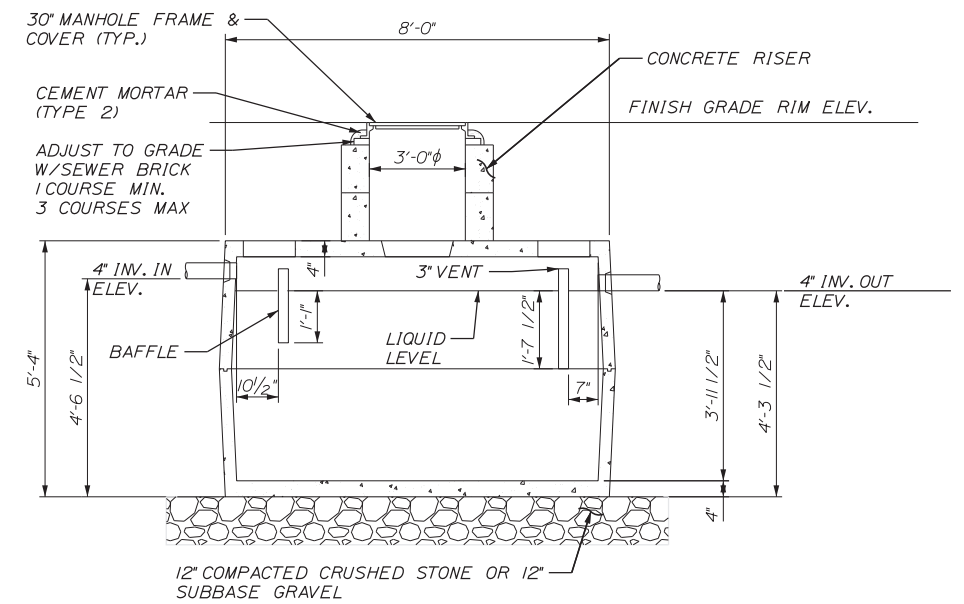
SIDE ELEVATION

END VIEW

3 OUTLET MINI DISTRIBUTION BOX
NOT TO SCALE



PLAN VIEW



SECTION VIEW

TANK CAPACITY GALS.
1000

SEPTIC TANK

- NOTES:
1. CONCRETE: 5000 PSI AFTER 28 DAYS.
 2. REINFORCING: 6X6 10/10 W.W.M. AND REBAR.
 3. KEYED JOINT SEALED WITH BUTYL RUBBER.
 4. SEPTIC TANK SHALL BE H2O LOADING RATED.

No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

By	Date	By	Date
Designed	PLP 3\20\19	Checked	LEM 3\20\19
Drawn	EJB 3\20\19	In Charge of	GAE 3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

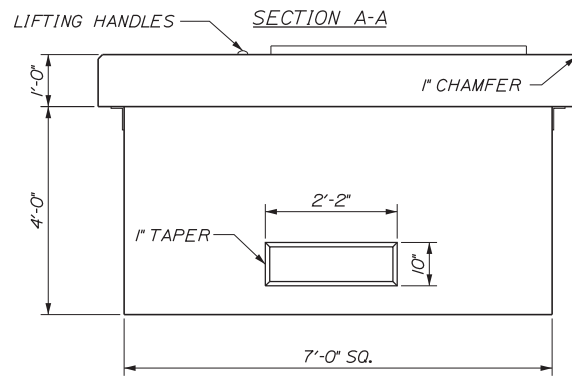
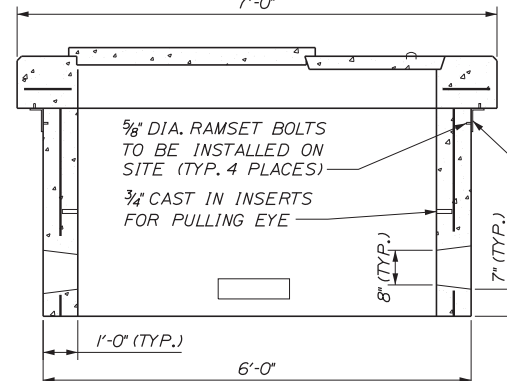
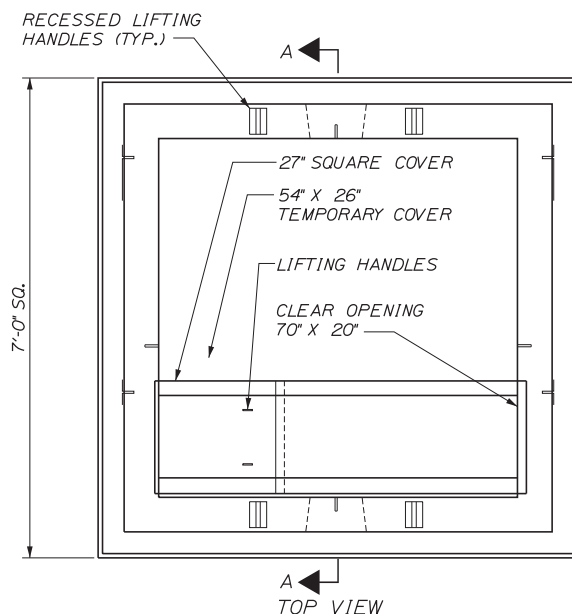
SITE DETAILS 2

SHEET NUMBER: SP-06
227 OF 503

CONTRACT: 2019.04

Filename: ...MSTA...Detail-Site_2.dgn

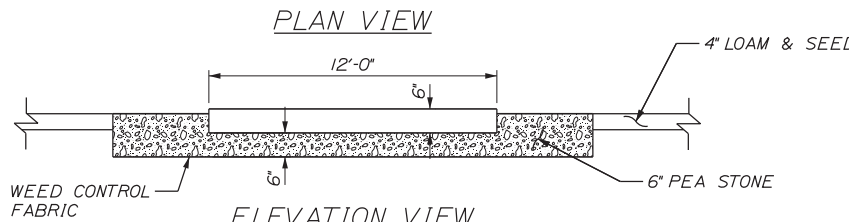
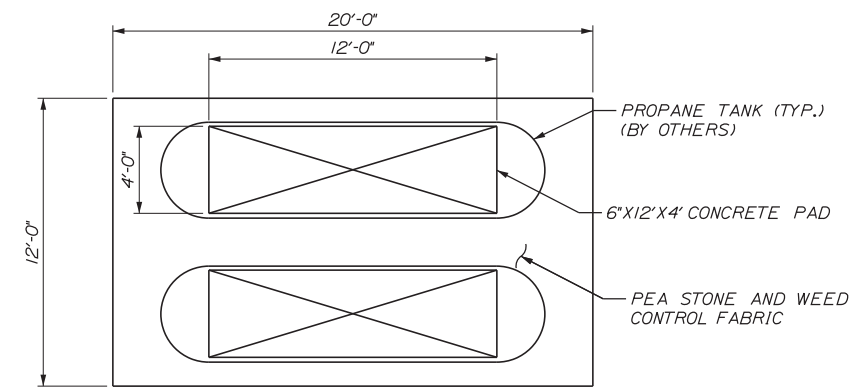
Date: 3/20/2019



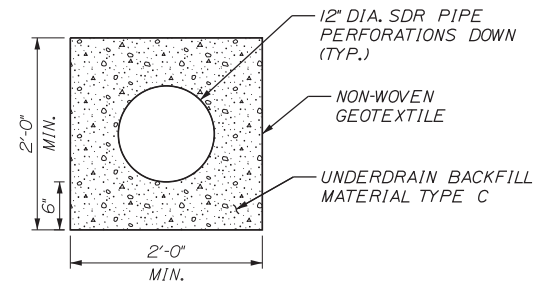
NOTES:
 1. CONCRETE 4,000 PSI AT 28 DAYS.
 2. REINFORCING #4 BARS AT 6" O.C. EACH WAY.
 3. DUCT OPENINGS SHOWN ARE TYPICAL AND CAN BE MODIFIED PER REQUEST.
 4. TRANSFORMER PAD SHALL MEET CENTRAL MAINE POWER COMPANY SPECIFICATIONS.
 5. TEMPORARY COVER HAS (2) 5/8" THREADED LIFTING INSERTS CAST IN.

NOTE:
 CONFIRM TRANSFORMER PAD SIZES WITH OWNER. DIVISION 2 CONTRACTOR IS RESPONSIBLE FOR FURNISHING AND INSTALLING THE TRANSFORMER PAD.

7'-0" TRANSFORMER PAD
NOT TO SCALE

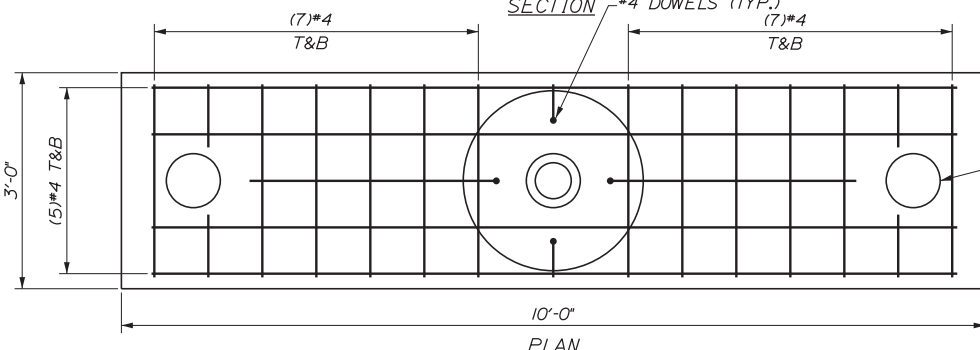
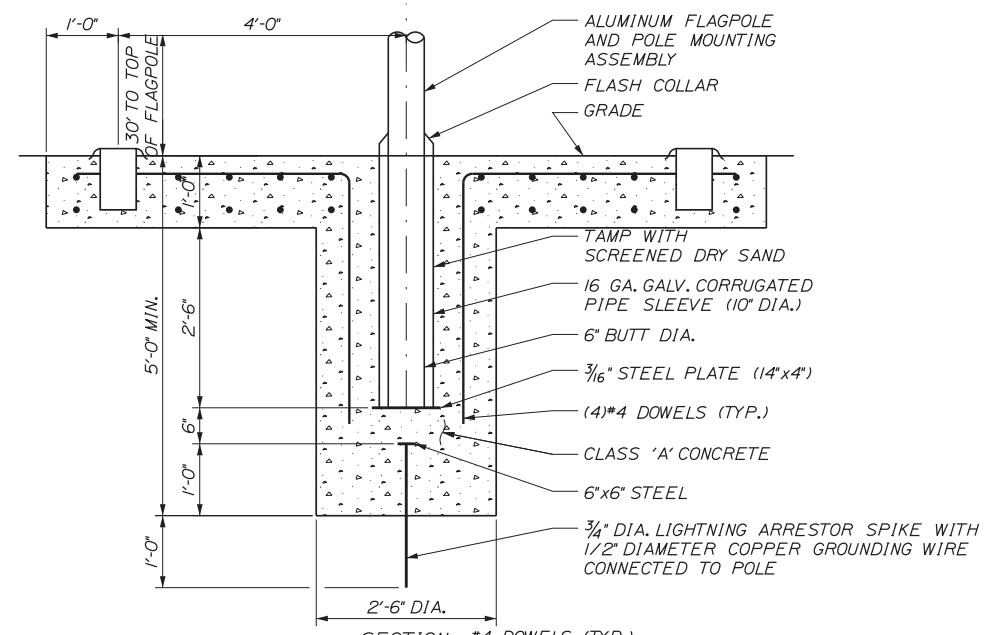


PROPANE TANK PAD
N.T.S.



TYPICAL UNDERDRAIN AT BUILDING FOUNDATION
NOT TO SCALE

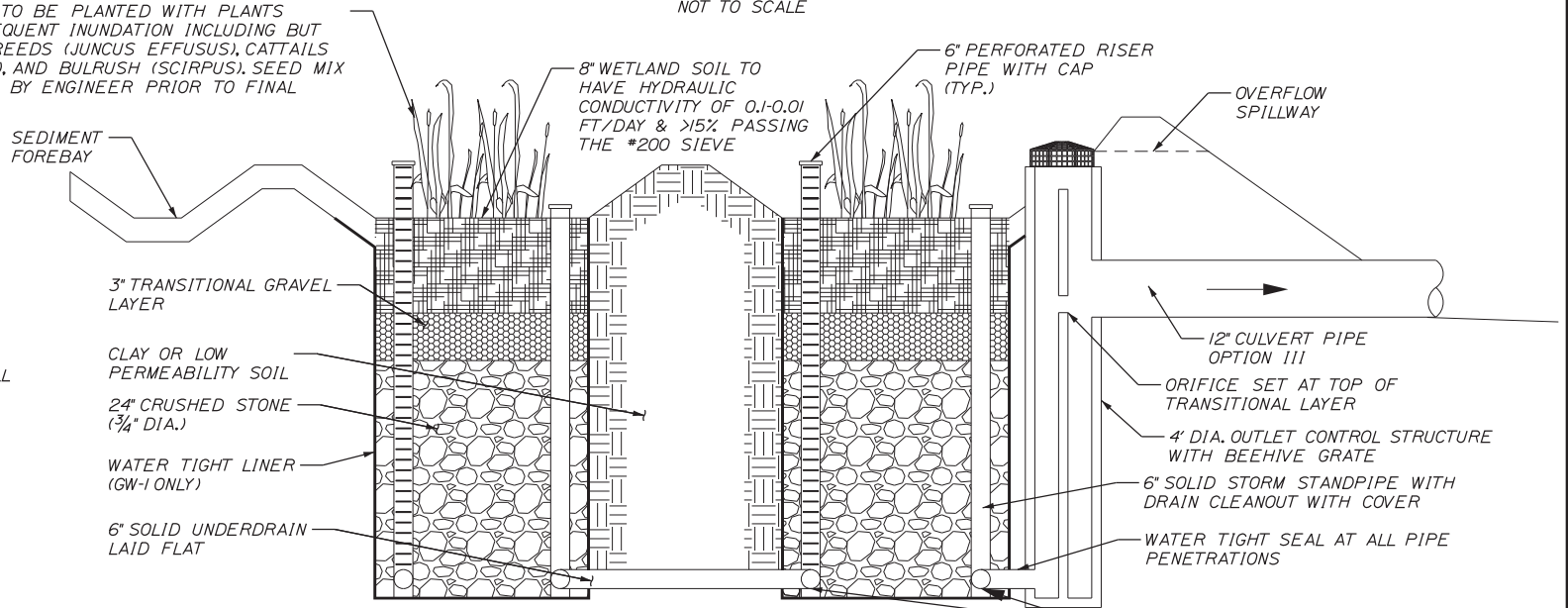
NOTES:
 1. DISTRIBUTION BOX SHALL BE H2O LOADING RATED



FLAGPOLE FOUNDATION DETAILS
NOT TO SCALE

TYPE C LED IN GRADE FLAG LUMINAIRE - INSTALL WITH 6" OF CONCRETE WITH REBAR ALL ROUND. INSTALL PER LUMINAIRE MANUFACTURERS RECOMMENDATIONS. FOR ELECTRICAL INSTALLATION NOTES, SEE SHEET LP-05.

GRAVEL WETLAND TO BE PLANTED WITH PLANTS TOLERANT OF FREQUENT INUNDATION INCLUDING BUT NOT LIMITED TO: REEDS (JUNCUS EFFUSUS), CATTAILS (TYPHA LATIFOLIA), AND BULRUSH (SCIRPUS). SEED MIX TO BE APPROVED BY ENGINEER PRIOR TO FINAL SEEDING



TYPICAL SECTION FOR GRAVEL WETLAND
NOT TO SCALE

No.	Revision	By	Date

Designed by:			
CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
Designed	PLP	3\20\19	Checked
Drawn	EJB	3\20\19	In Charge of

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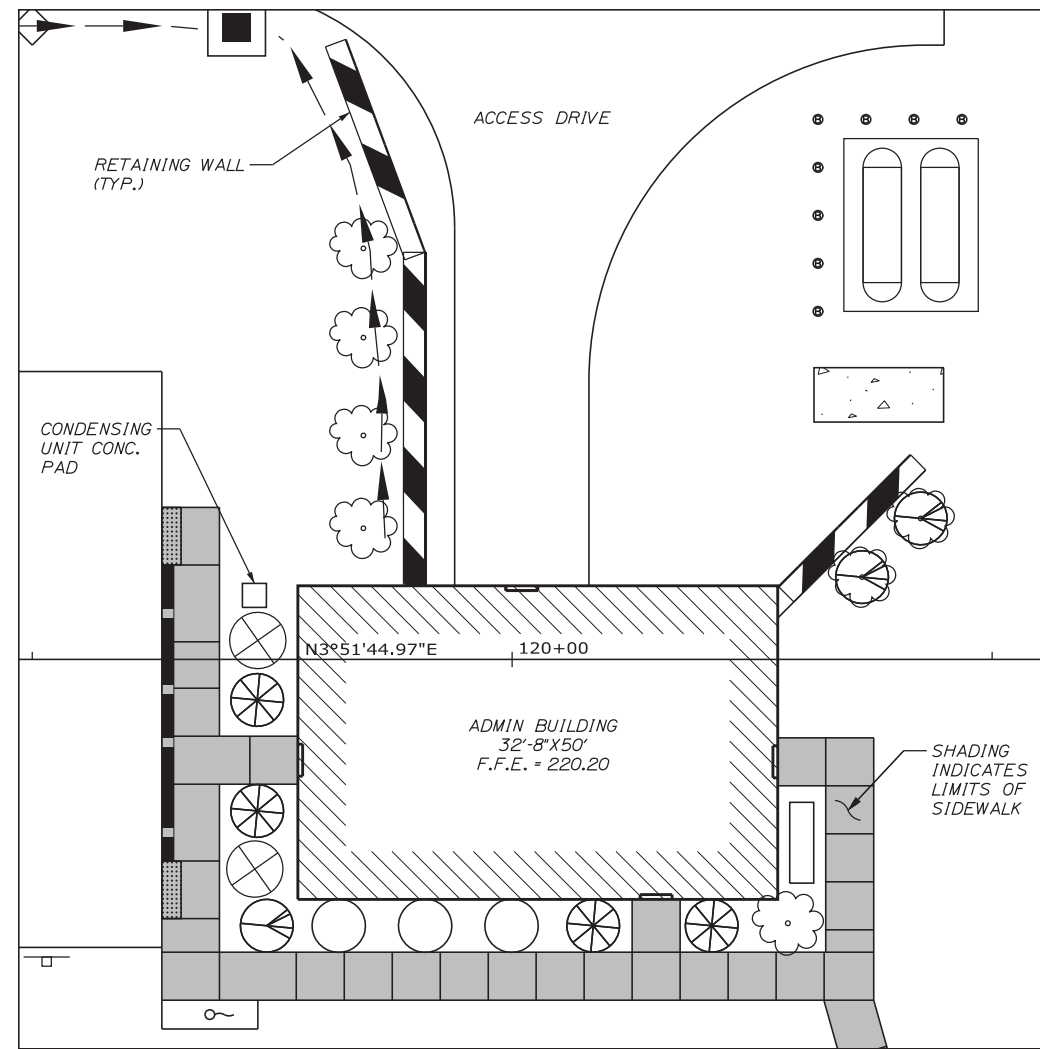
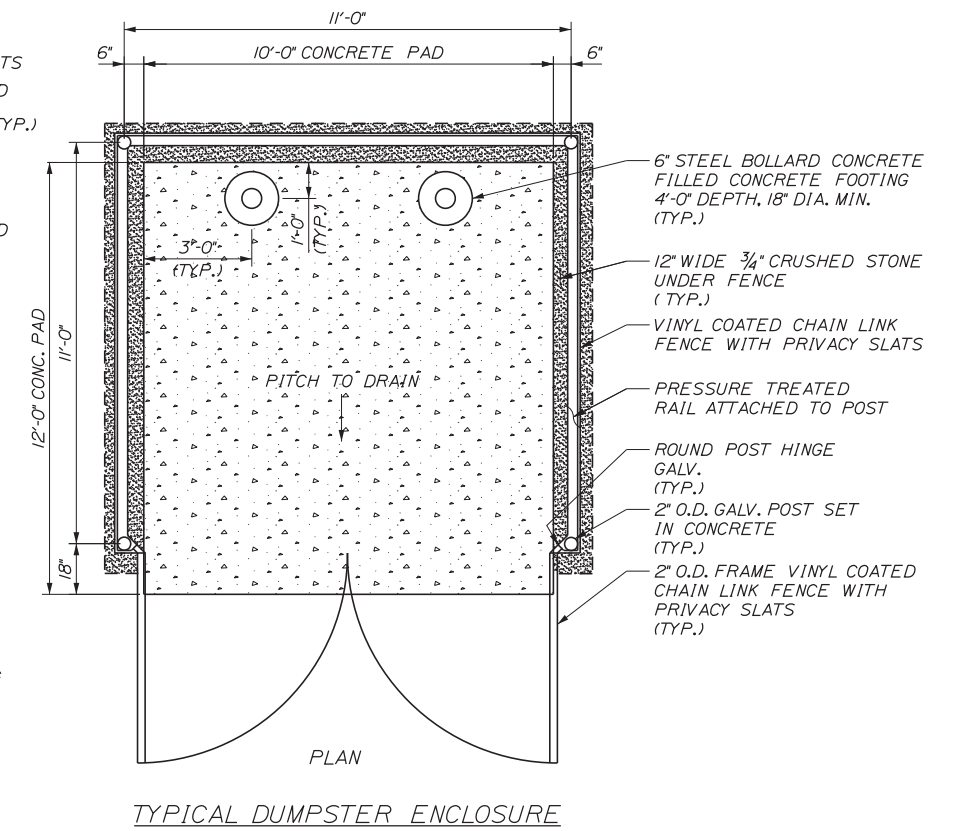
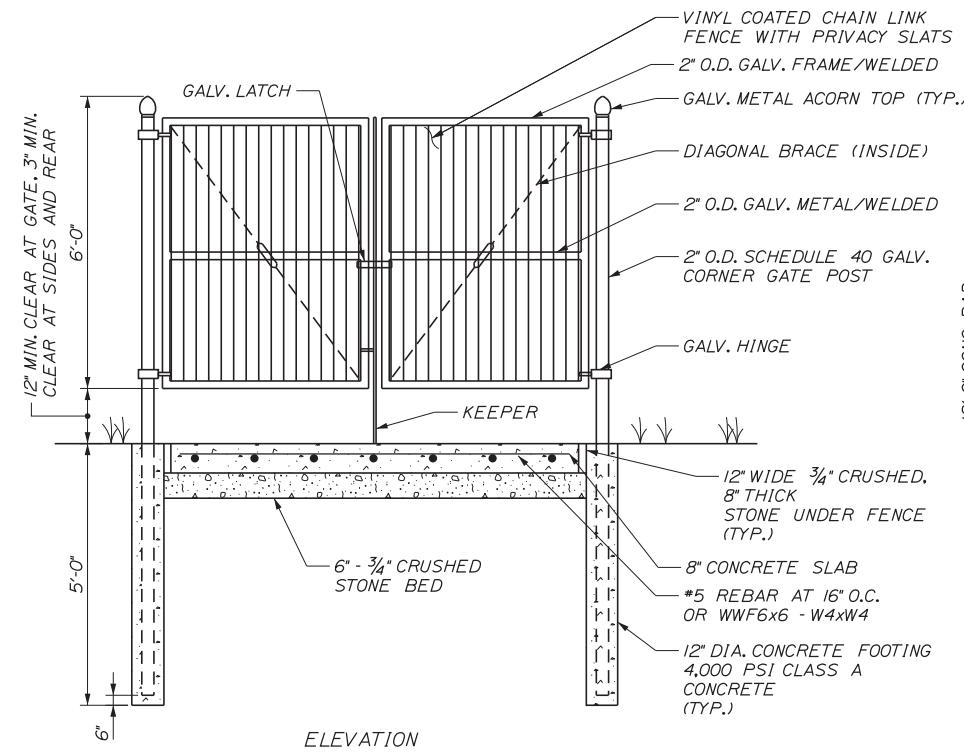
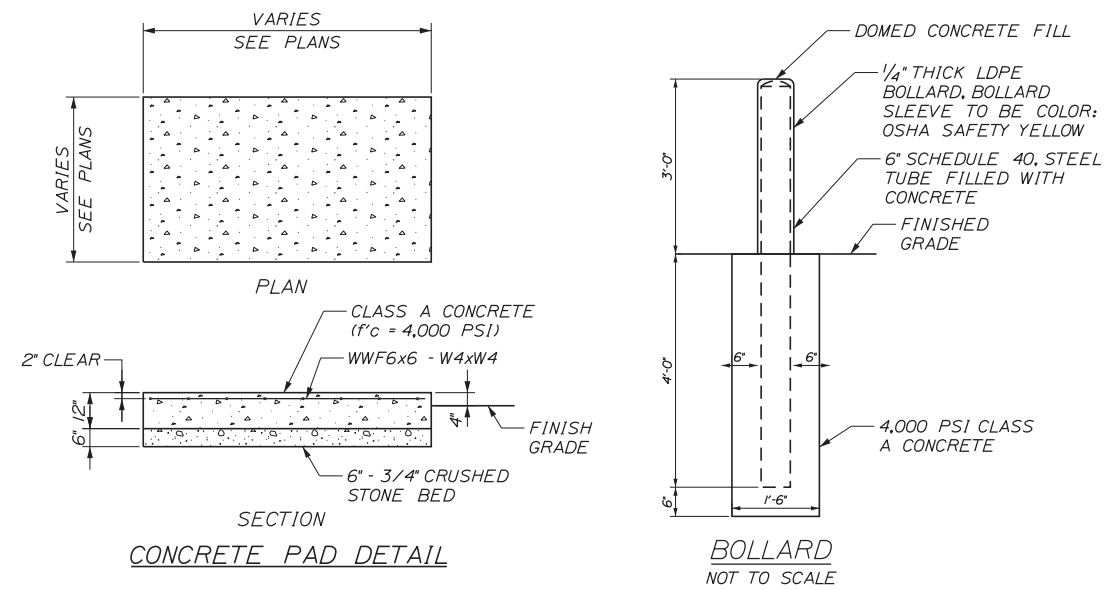
THE GOLD STAR MEMORIAL HIGHWAY

INTERCHANGE 103
 ORT CONVERSION
 SITE DETAILS 3

SHEET NUMBER: SP-07
 228 OF 503

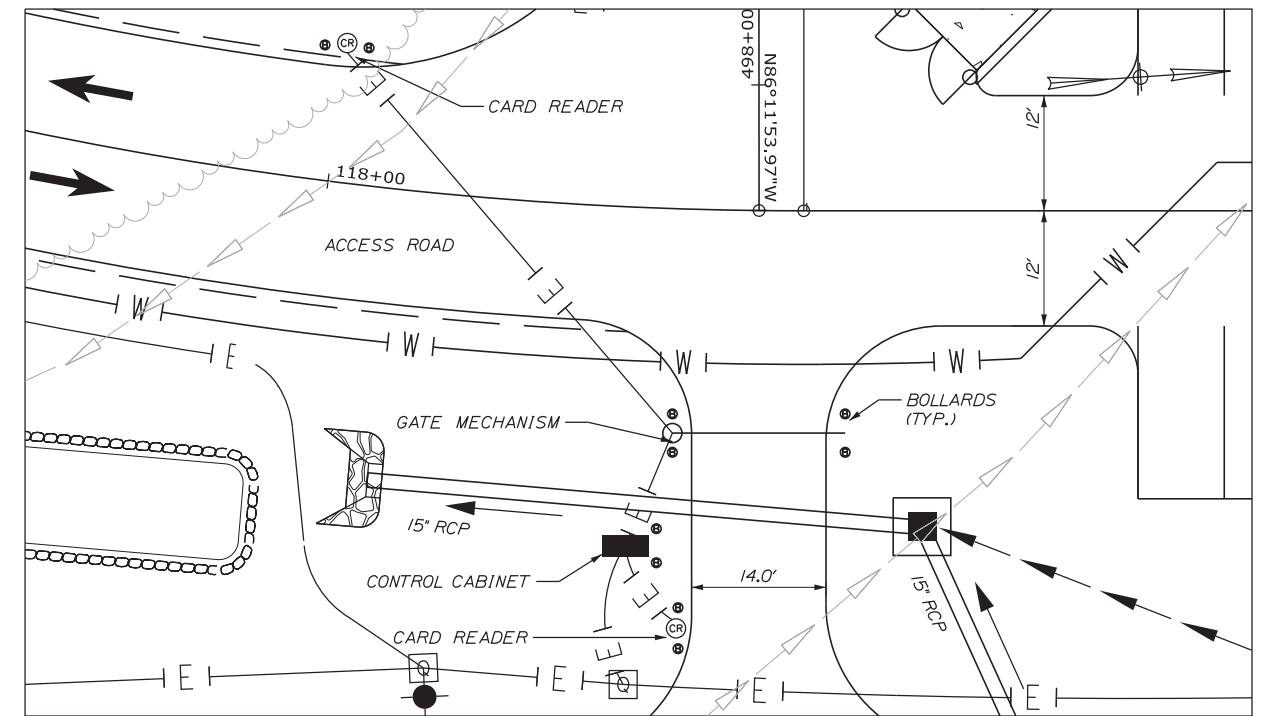
Filename: ...MSTA...Detail-Site_3.dgn

Date: 3/20/2019



LANDSCAPING LEGEND

- (4) DW = CORNUS SERICEA "KELSEY" KELSEY DOGWOOD 3 GAL.
- (3) RH = DWARF RHODODENDRON 2'-2.5' BB
- (7) J = JUNIPERUS CHINENSIS "SERGENT" SARGENT JUNIPER 18'-24" BB
- (1) TY = TAUNTONI YEW 18'-24" BB
- (2) NC = THUJA OCCIDENTALIS NORTHERN WHITE-CEDAR 5'-6' BB



Filename: ...MSTA...Detail-Site_4.dgn

No.	Revision	By	Date

Designed by:

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**THE GOLD STAR
MEMORIAL HIGHWAY**

INTERCHANGE 103
ORT CONVERSION

SITE DETAILS 4

SHEET NUMBER: SP-08

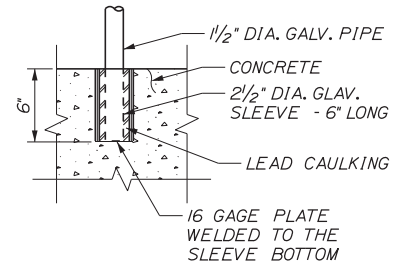
229 OF 503

CONTRACT: 2019.04

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

Date: 3/20/2019

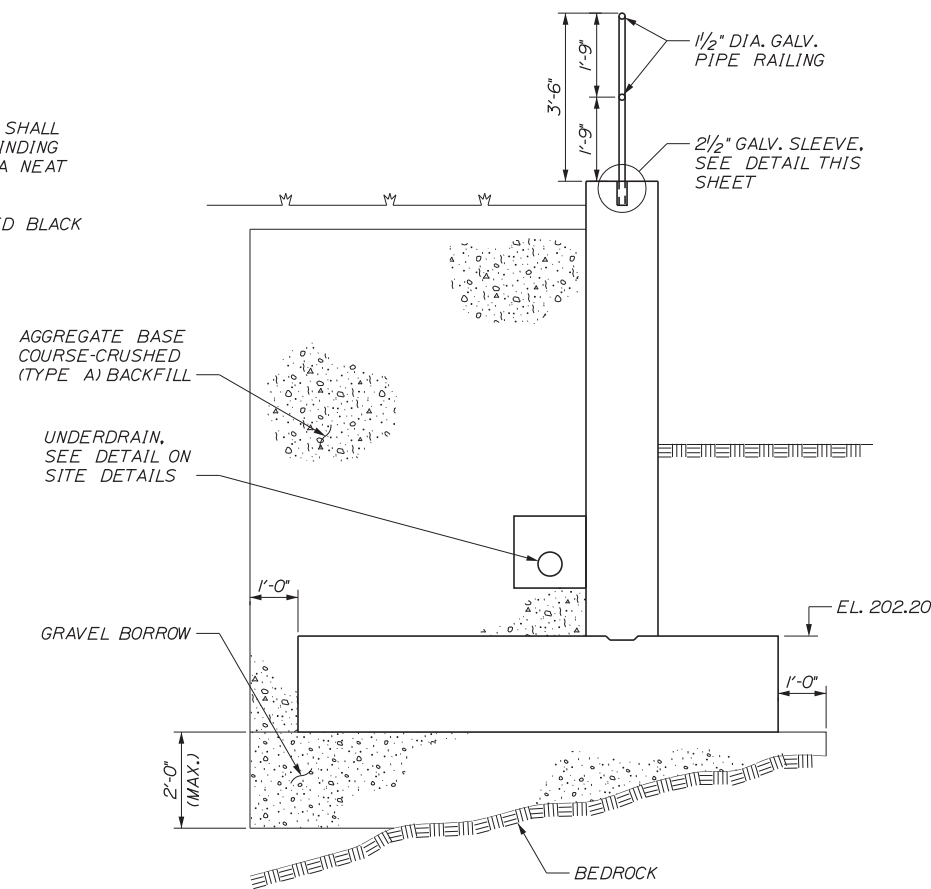
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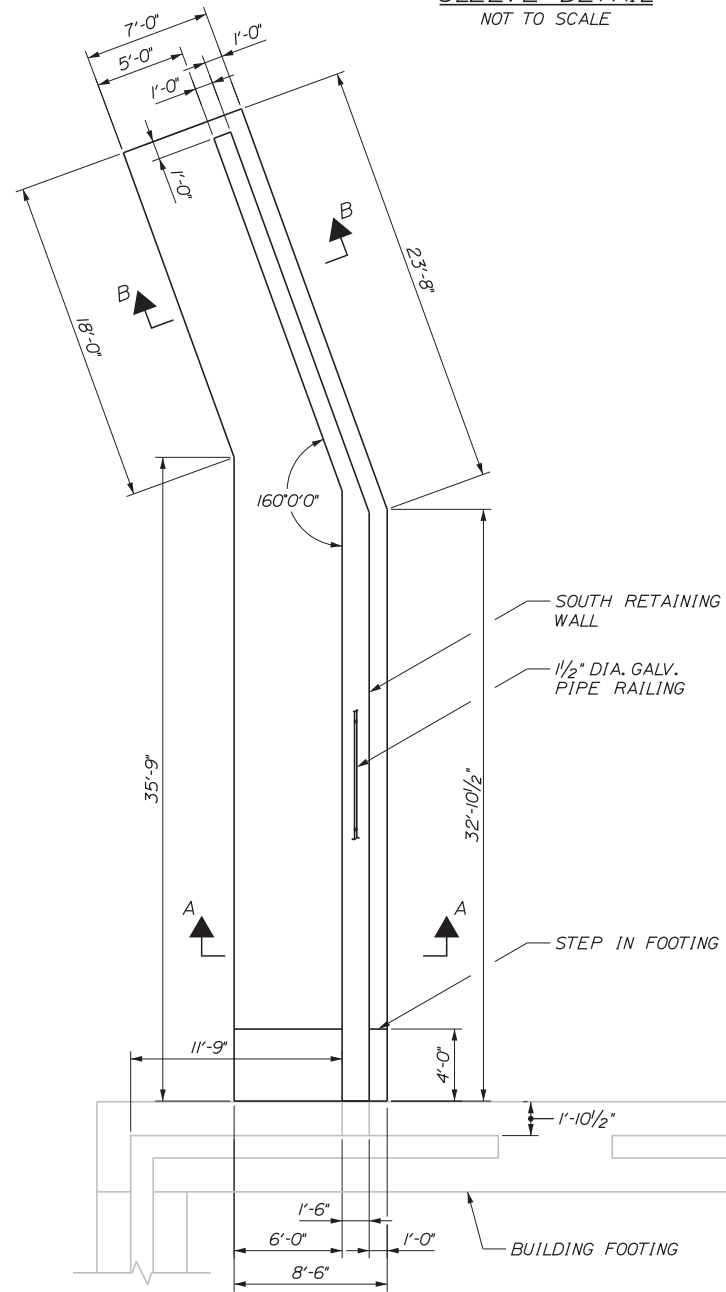
SLEEVE DETAIL
NOT TO SCALE

SLEEVE NOTES

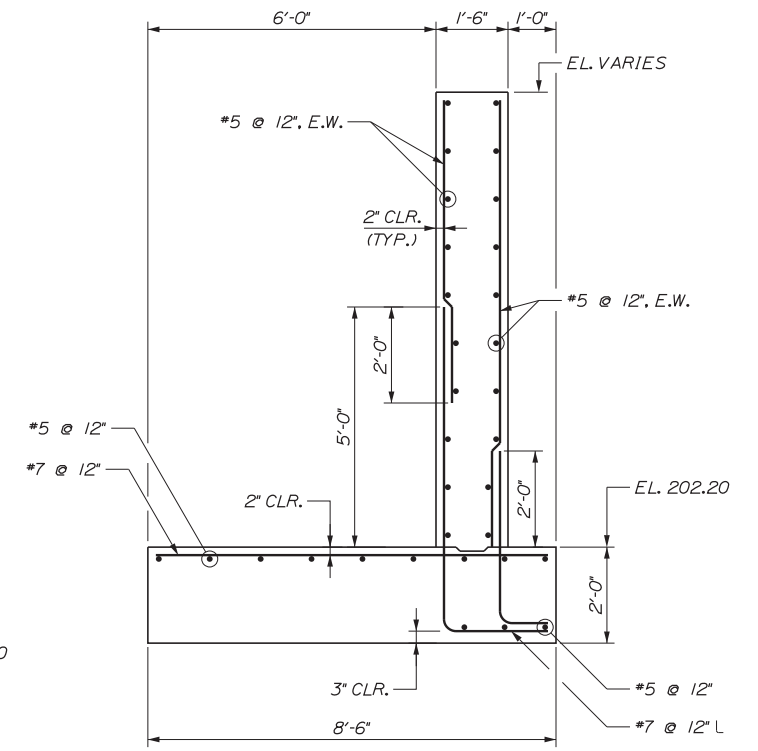
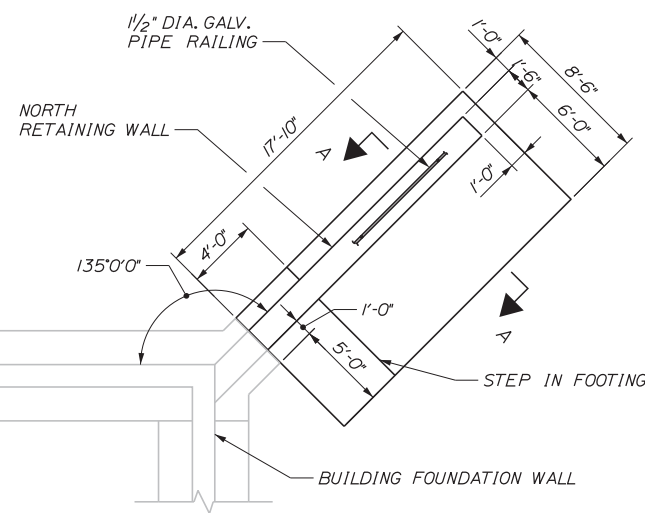
1. ALL WELDED JOINTS SHALL BE FINISHED BY GRINDING OR FILING TO GIVE A NEAT APPEARANCE.
2. RAIL TO BE PAINTED BLACK AFTER ERECTION.



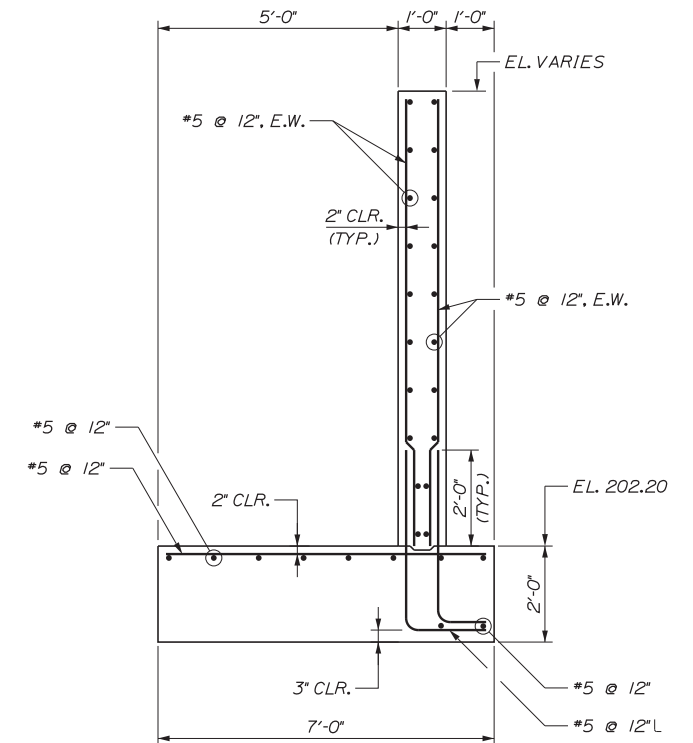
TYPICAL RETAINING WALL EARTHWORKS SECTION
NOT TO SCALE



PLAN - RETAINING WALLS AT ADMINISTRATION BUILDING
SCALE: 3/16" = 1'-0"



SECTION A-A - TYPE A RETAINING WALL
(RAILING NOT SHOWN FOR CLARITY)
SCALE: 1/2" = 1'-0"



SECTION B-B - TYPE B RETAINING WALL
(RAILING NOT SHOWN FOR CLARITY)
SCALE: 1/2" = 1'-0"

Scale:			
No.	Revision	By	Date

Designed by:			
CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE			
Designed	MJC	3\20\19	Checked
Drawn	JTB	3\20\19	In Charge of
			GAB
			GAE

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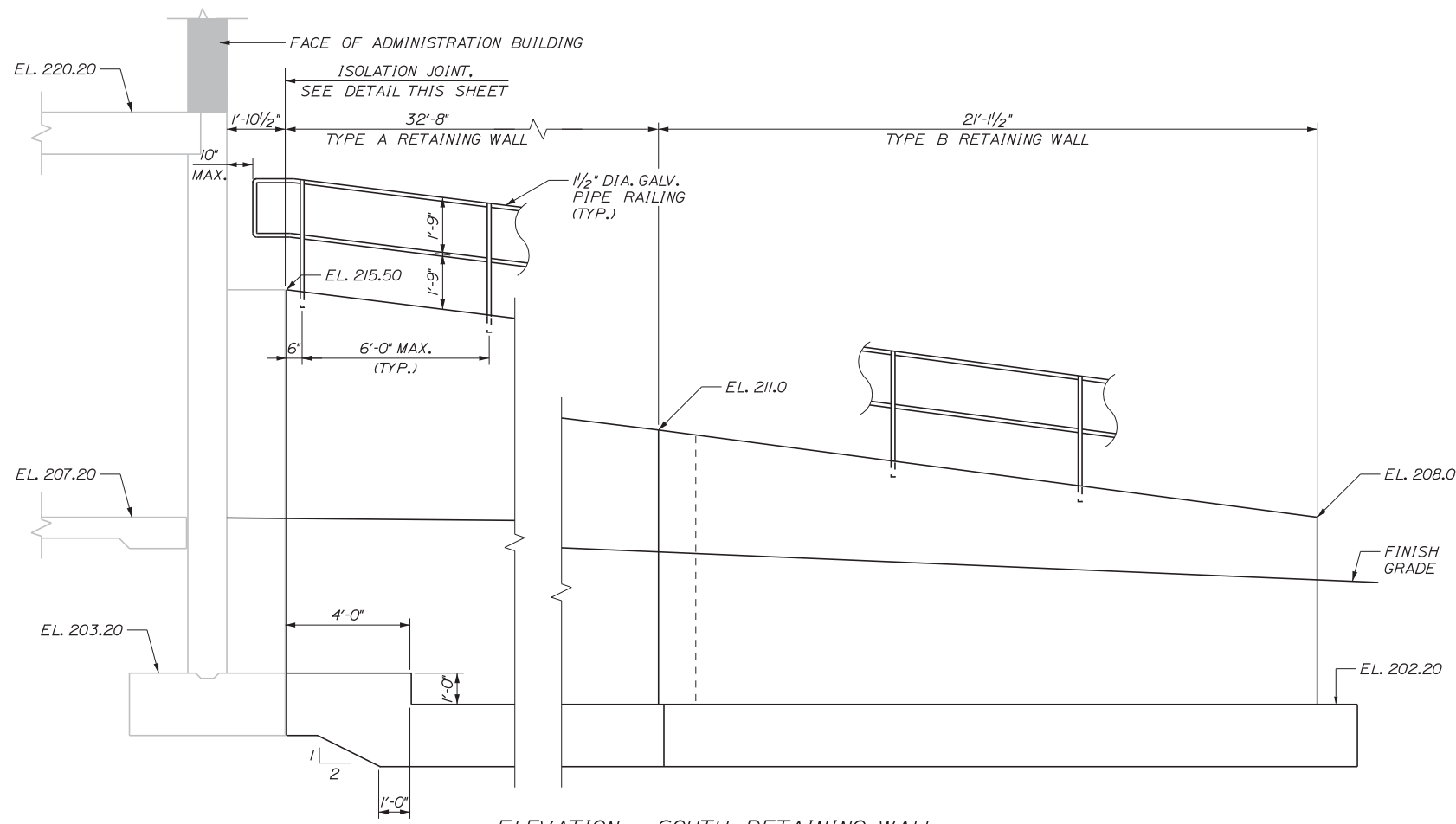
**THE GOLD STAR
MEMORIAL HIGHWAY**

INTERCHANGE 103
ORT CONVERSION
ADMINISTRATION BUILDING
RETAINING WALL DETAILS 1

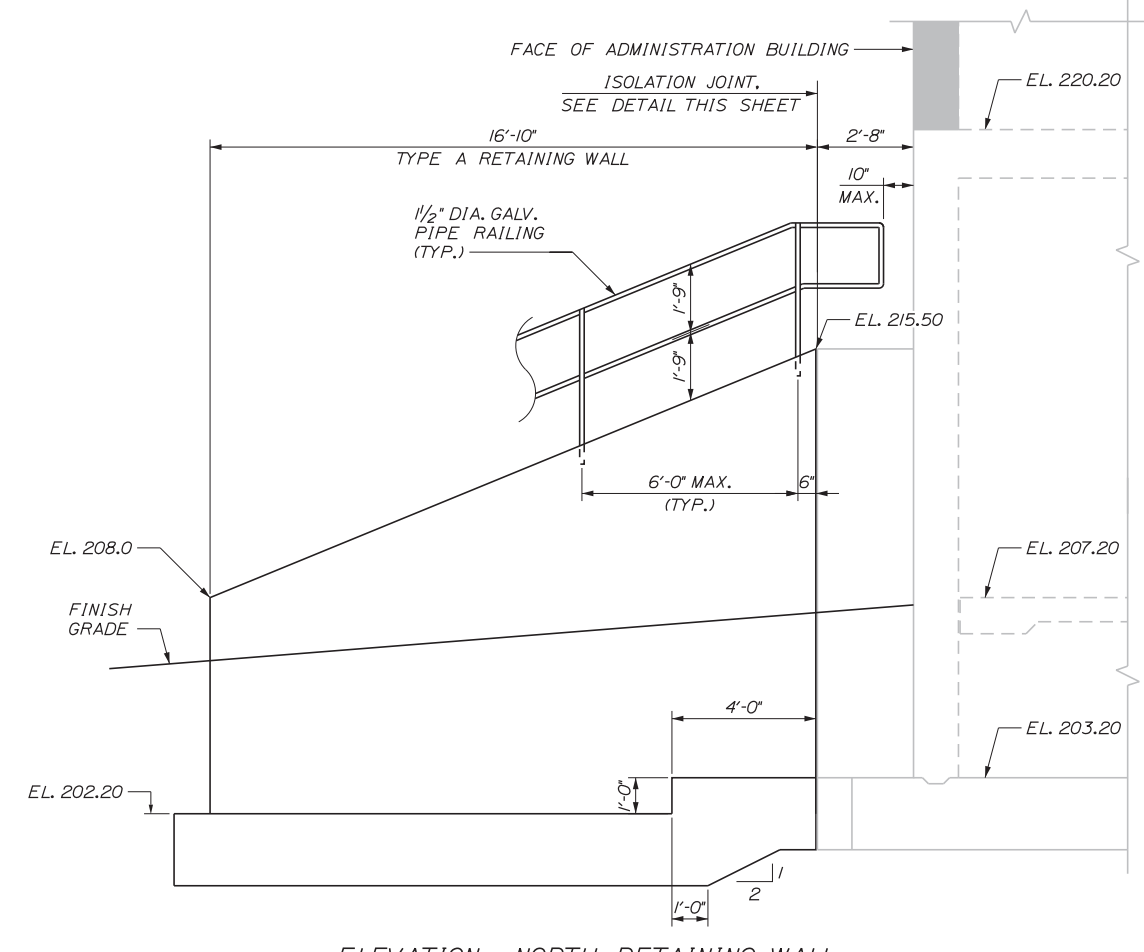
SHEET NUMBER: RW-01
CONTRACT: 2019.04
230 OF 503

Date: 3/20/2019

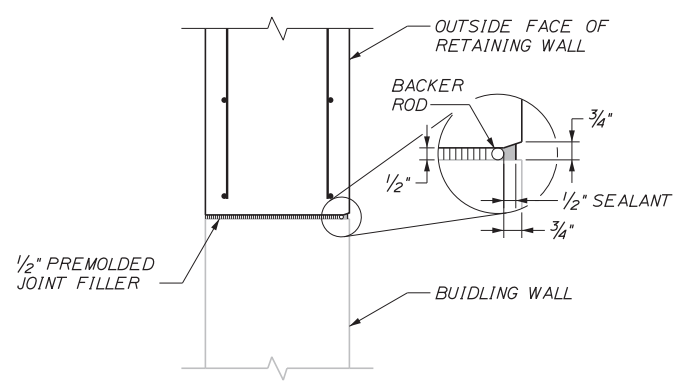
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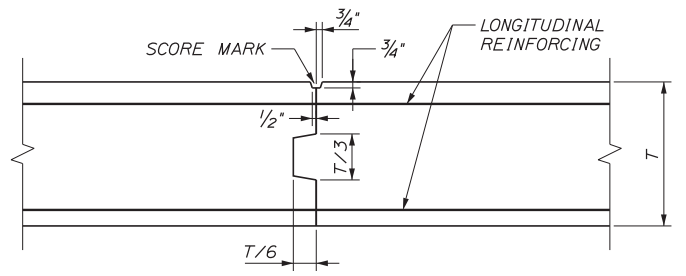
ELEVATION - SOUTH RETAINING WALL
SCALE: 3/8" = 1'-0"



ELEVATION - NORTH RETAINING WALL
SCALE: 3/8" = 1'-0"



ISOLATION JOINT DETAIL
NOT TO SCALE



TYPICAL WALL CONSTRUCTION JOINT
NOT TO SCALE

NOTE: FOOTING CONSTRUCTION JOINT SIMILAR WITHOUT SCORE MARK.

NOTES

1. ALL EXPOSED EDGES OF CONCRETE SHALL BE CHAMFERED 1"x1".
2. CONCRETE SHALL COMPLY WITH CLASS AA, f'c=4000 PSI.
3. REINFORCING BARS SHALL CONFORM TO ASTM A615 GRADE 60. DEFORMED BARS SHALL BE DETAILED, FABRICATED AND ERECTED IN ACCORDANCE WITH ACI 315 LATEST EDITION.
4. COMPLETE SHOP DRAWINGS AND SCHEDULES OF ALL REINFORCING STEEL SHALL BE PREPARED BY THE CONTRACTOR AND SUBMITTED TO THE ENGINEER FOR REVIEW PRIOR TO COMMENCEMENT OF THAT PORTION OF THE WORK. ALL ACCESSORIES MUST BE SHOWN ON THE SHOP DRAWINGS.
5. EXPOSED CONCRETE SHALL BE NEATLY FINISHED-RUBBED.
6. MAXIMUM SPACING OF CONSTRUCTION JOINTS IN WALLS SHALL BE 30'.

Scale:			
No.	Revision	By	Date

Designed by:

Stantec

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

By	Date	By	Date
Designed	MJC 3\20\19	Checked	GAB 3\20\19
Drawn	JTB 3\20\19	In Charge of	GAE 3\20\19

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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

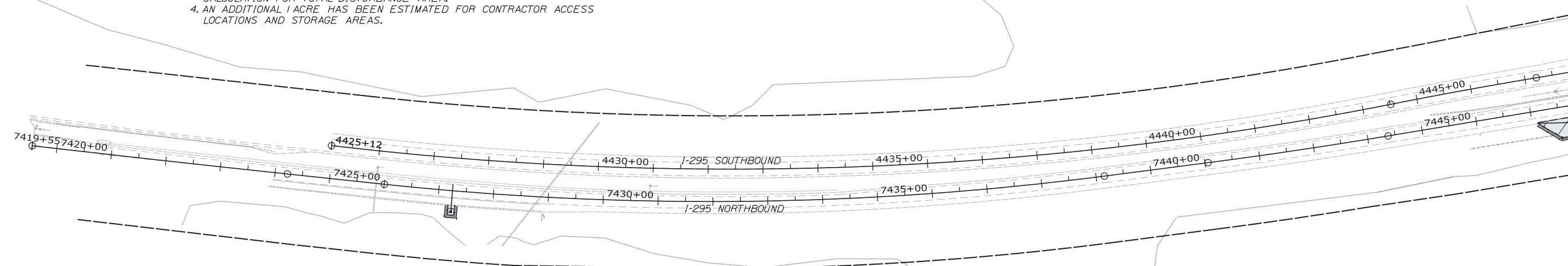
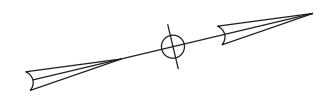
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION
ADMINISTRATION BUILDING
RETAINING WALL DETAILS 2

SHEET NUMBER: RW-02
231 OF 503

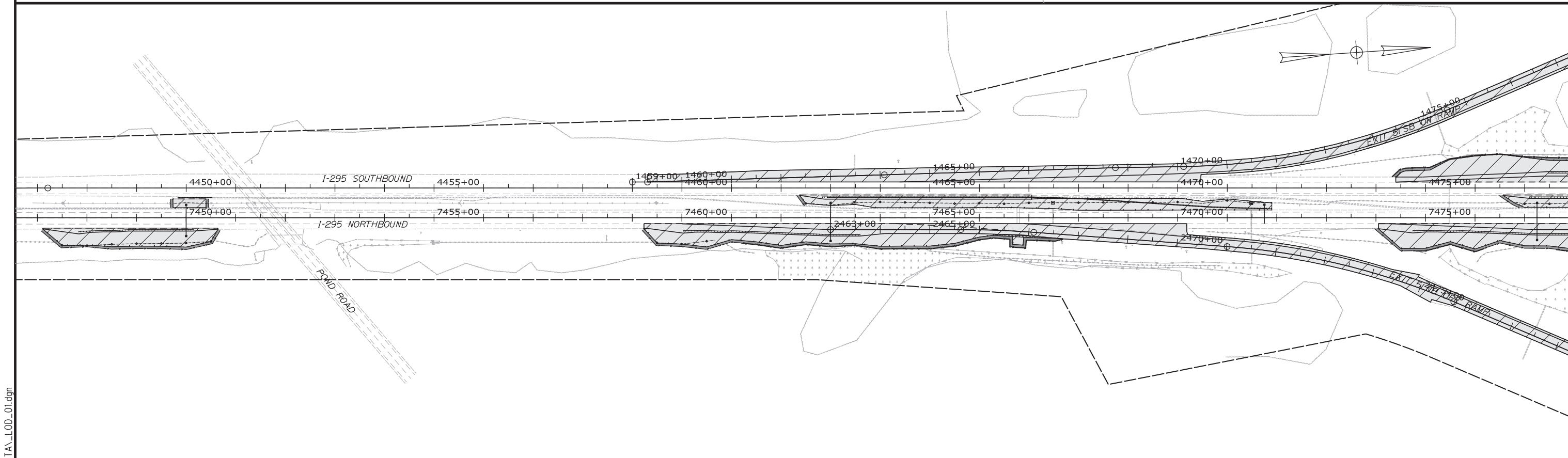
CONTRACT: 2019.04

- NOTES:
1. EROSION AND SEDIMENTATION CONTROL DEVICES REQUIRED FOR THE CONTRACTOR'S ACCESS LOCATIONS AND STORAGE AREAS WILL NOT BE MEASURED FOR PAYMENT.
 2. SEE SPECIFICATIONS FOR CONTRACTOR'S SUBMITTAL REQUIREMENTS IF ADDING ADDITIONAL LIMITS OF DISTURBANCE TO THE PROJECT ESTIMATED QUANTITIES.
 3. AREA OF VARIABLE DEPTH MILL & OVERLAY, IS NOT INCLUDED IN THE CALCULATION FOR TOTAL DISTURBANCE AREA.
 4. AN ADDITIONAL 1 ACRE HAS BEEN ESTIMATED FOR CONTRACTOR ACCESS LOCATIONS AND STORAGE AREAS.



TEMPORARY IMPACTS.....	2.55 ACRES (111,069 SF)
PERMANENT IMPACTS.....	36.13 ACRES (1,573,766 SF)
TOTAL IMPACTS.....	38.68 ACRES (1,678,835 SF)

Date: 3/20/2019



Filename: ...:\00\HIGHWAY\MSTA\..._LOD_01.dgn

Scale: Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date	Checked	By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
ORT CONVERSION

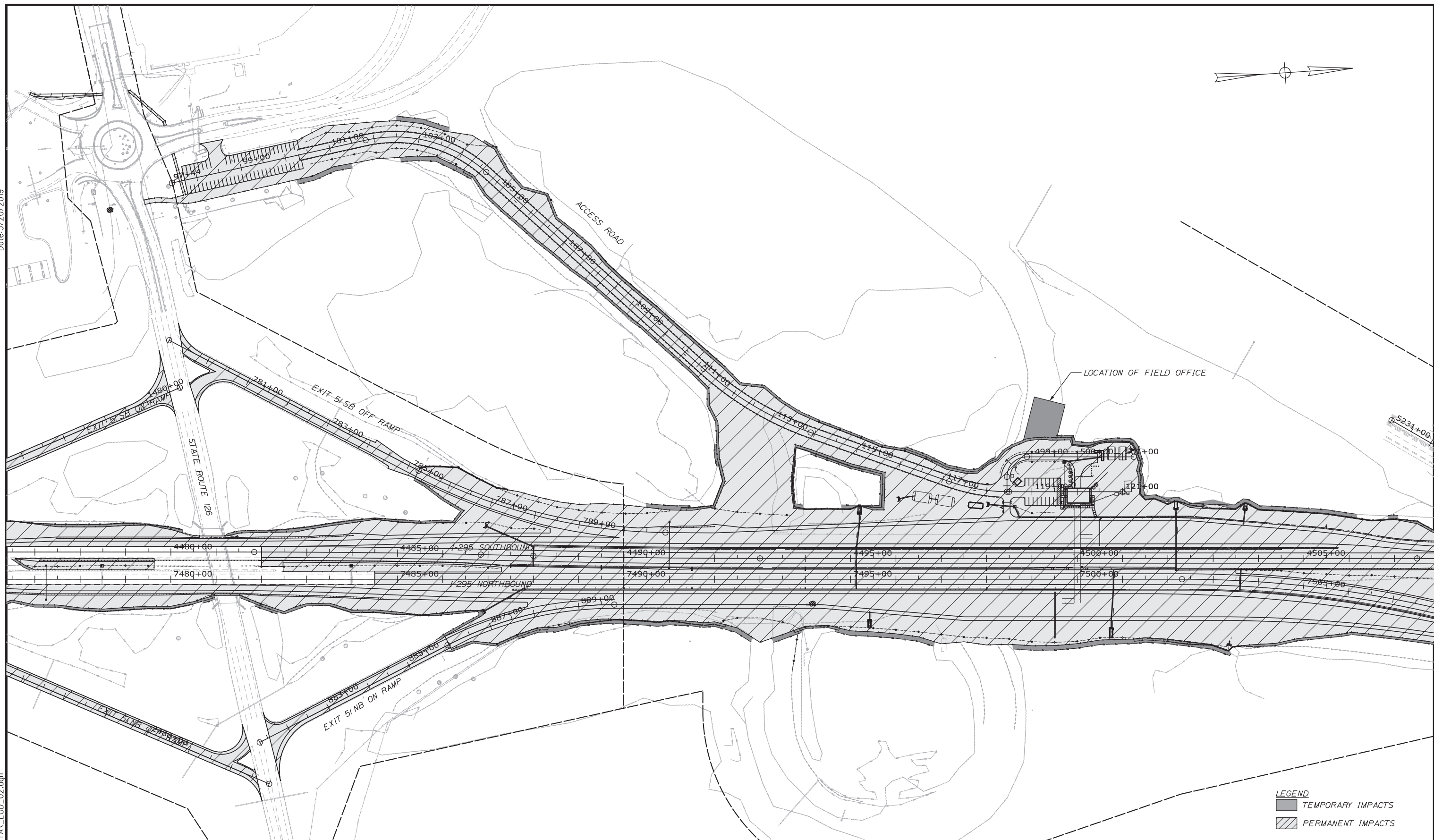
LIMIT OF DISTURBANCE PLAN 1

SHEET NUMBER: LOD-01
232 OF 503

CONTRACT: 2019.04

Date: 3/20/2019

Filename: ...\\00.HIGHWAY\MSTA\...LOD_02.dgn



LEGEND
 [Solid Grey Box] TEMPORARY IMPACTS
 [Hatched Box] PERMANENT IMPACTS

Scale: 100 0 100 200
Scale of Feet

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

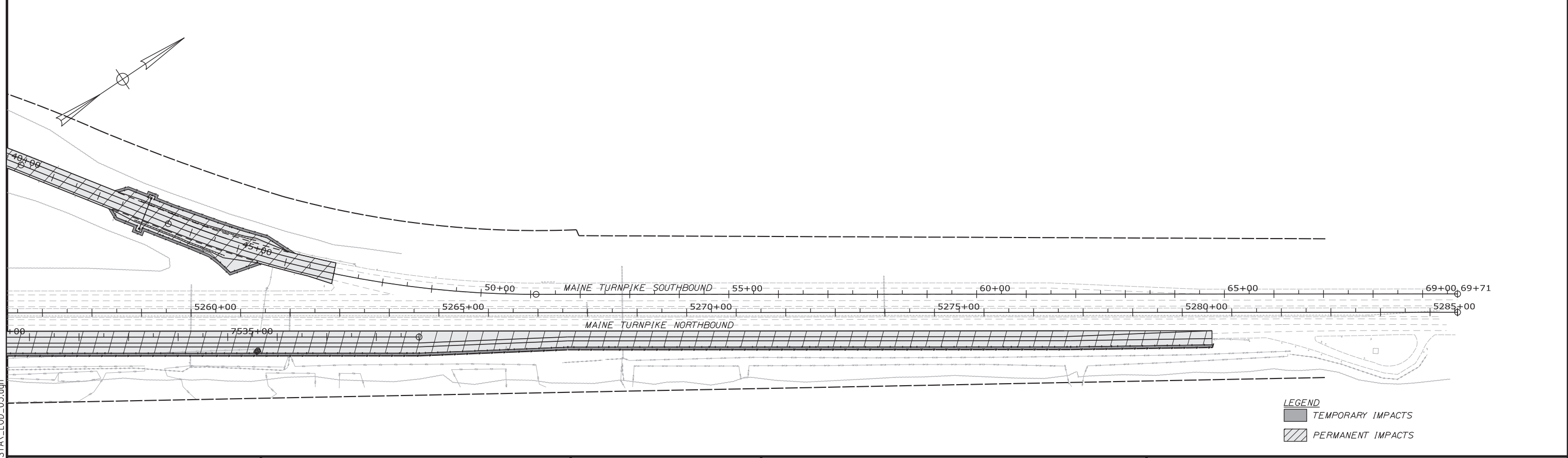
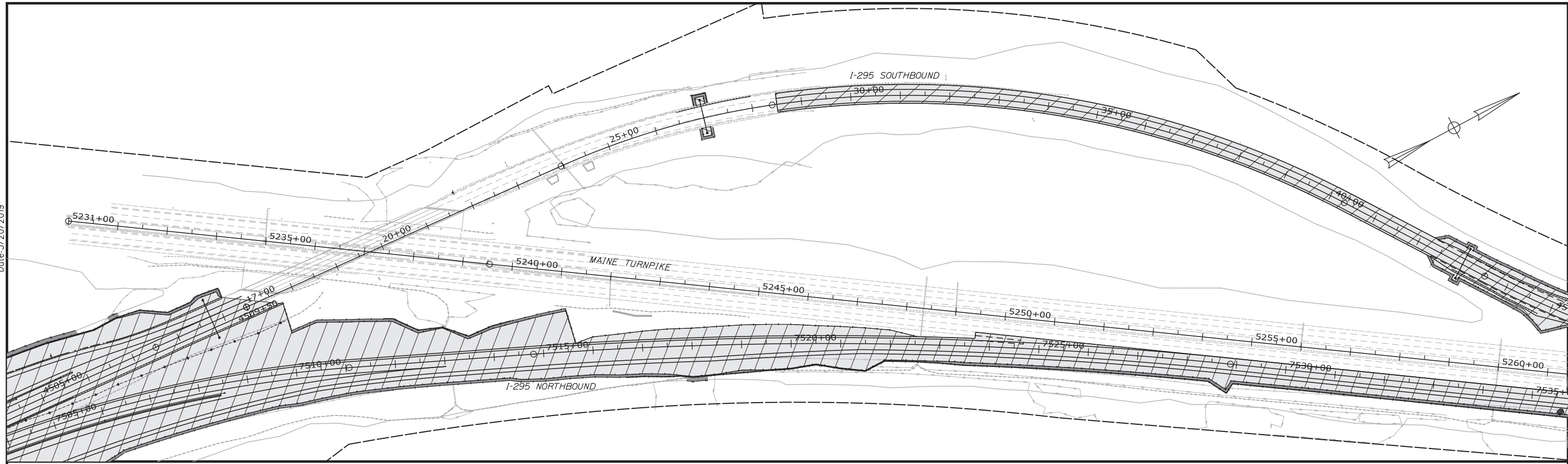
MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 LIMIT OF DISTURBANCE PLAN 2

SHEET NUMBER: LOD-02
 233 OF 503

CONTRACT: 2019.04

Date: 3/20/2019



LEGEND
 TEMPORARY IMPACTS
 PERMANENT IMPACTS

Scale:

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: LAUREN MEEK, PE

	By	Date		By	Date
Designed	PLP	3\20\19	Checked	LEM	3\20\19
Drawn	EJB	3\20\19	In Charge of	GAE	3\20\19

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH NORWOOD, IV, PE, PTOE

INTERCHANGE 103
 ORT CONVERSION
 LIMIT OF DISTURBANCE PLAN 3

SHEET NUMBER: LOD-03
 234 OF 503

CONTRACT: 2019.04

Filename: ...:\00.HIGHWAY\MSTA\...LOD_03.dgn