

REFERENCES

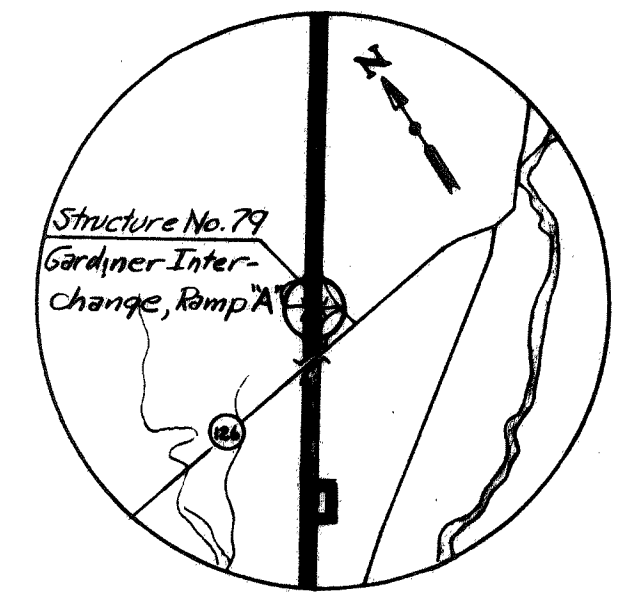
Dwg. No.	TITLE	Substructure Contractor	Superstructure		
			Steel Fabricator	Steel Erector	Floor Contractor
SD1A	Standard Abutment Details	✓	✓	✓	✓
SD2	Standard Pier Details	✓	✓	✓	✓
SD3	Standard Abutment Drainage Details	✓	✓	✓	✓
SD3	Standard Handrail, Bearing Devices, and Miscellaneous Details	✓	✓	✓	✓
SD6	Standard Diaphragm Details	✓	✓	✓	✓
SD8	Type "A" Splices for 30W Beams	✓	✓	✓	✓
SD11A	Type "X" and "Y" Expansion Joints, Expanding Length to 100'	✓	✓	✓	✓
SD12A	Type "Z" Expansion Joint, Expanding Length Over 100'	✓	✓	✓	✓

GENERAL NOTES

Design Specifications: AASHO (1953) with minor modifications.

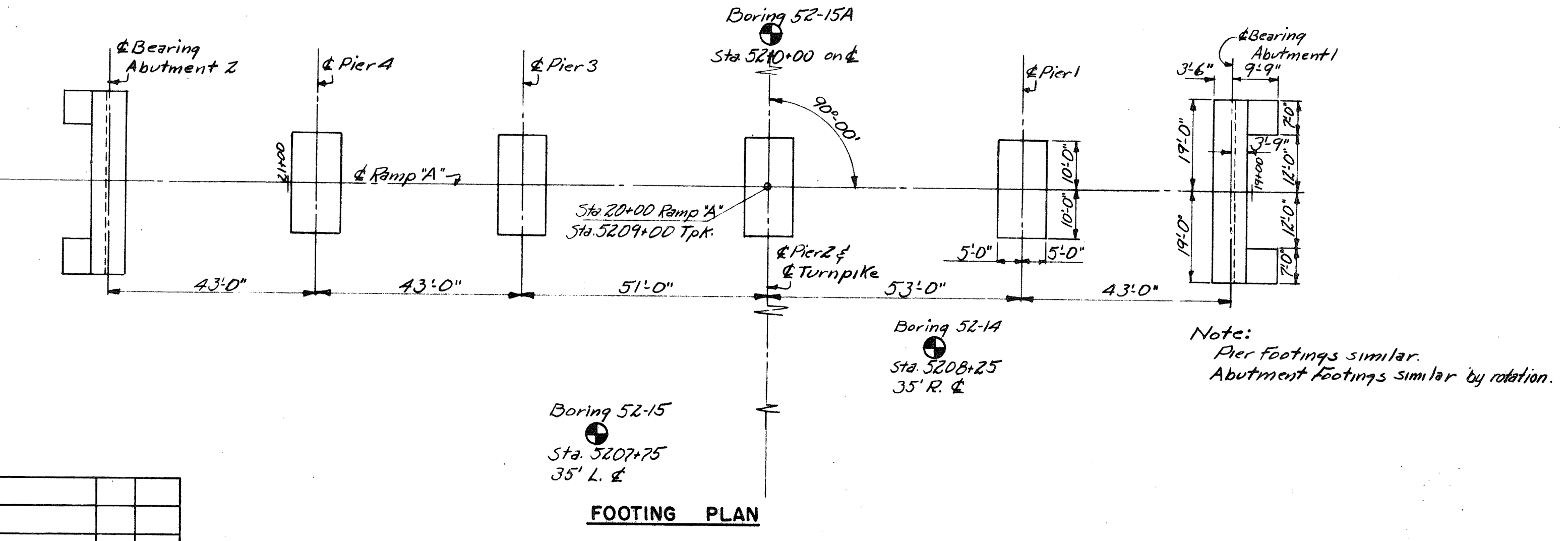
Design Live Load: H20-516

Maximum Base Pressure:
 Abutments - 1.3 Tons per square foot
 Piers - 2.8 Tons per Square foot



DRAWING 79.01.03

BY	DATE	REVISION	BY	DATE
MADE	R.S.G. 4-26-54			
TRACED				
CHECKED	M.J.G. 4-29-54	As-Built	HBH	123.56
IN CHARGE OF	I.D.S.K.	No.		



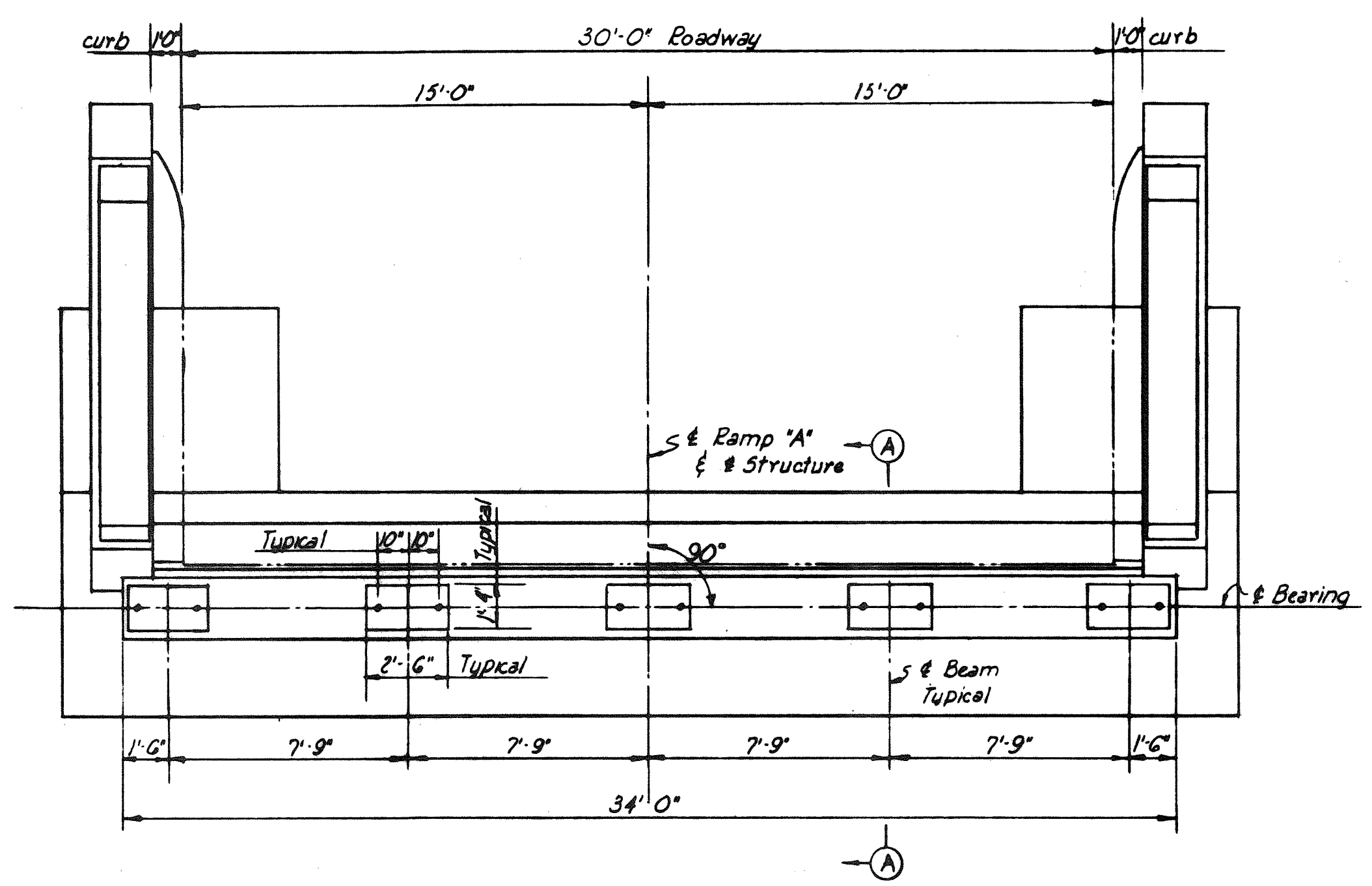
MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
 SECTION 2 - PORTLAND TO AUGUSTA

STRUCTURE NO. 79
GARDINER INTERCHANGE RAMP "A"
 STA. 5209+00

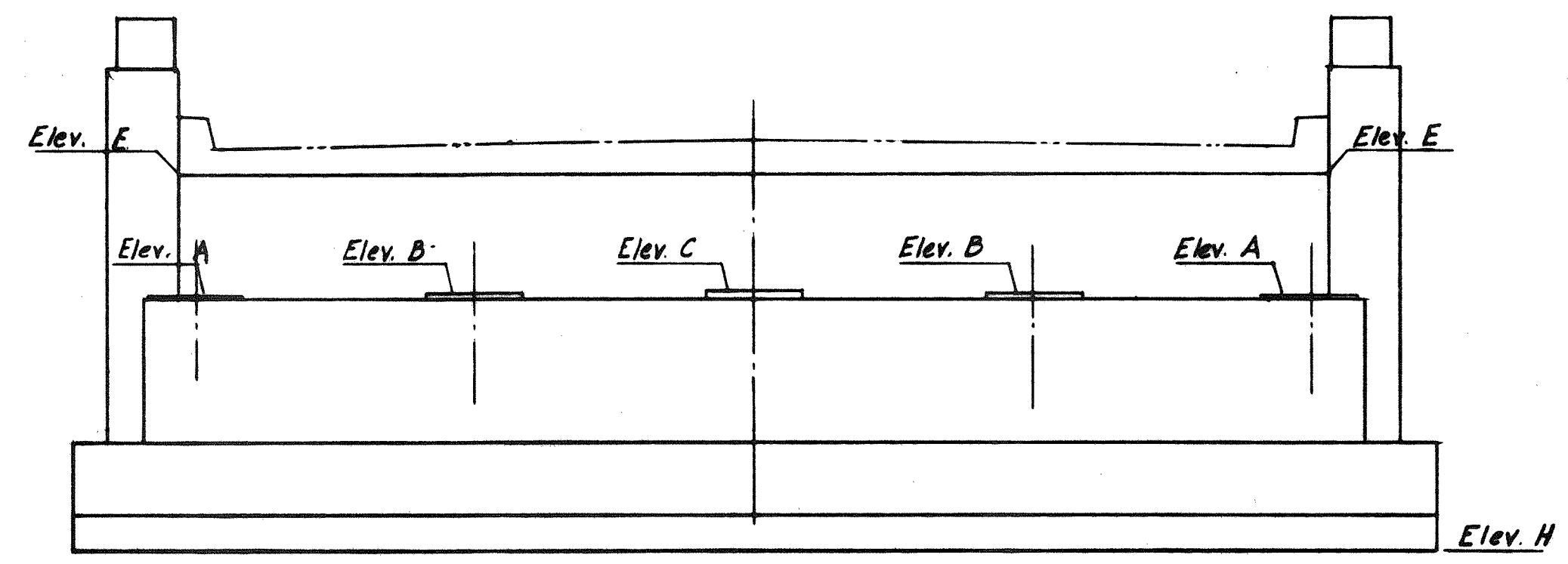
GENERAL PLAN AND ELEVATION

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
 CONSULTING ENGINEERS
 NEW YORK KANSAS CITY

SCALE: 1"=20'-0"
 CONTRACT NO. _____
 SHEET NO. 346 OF 382

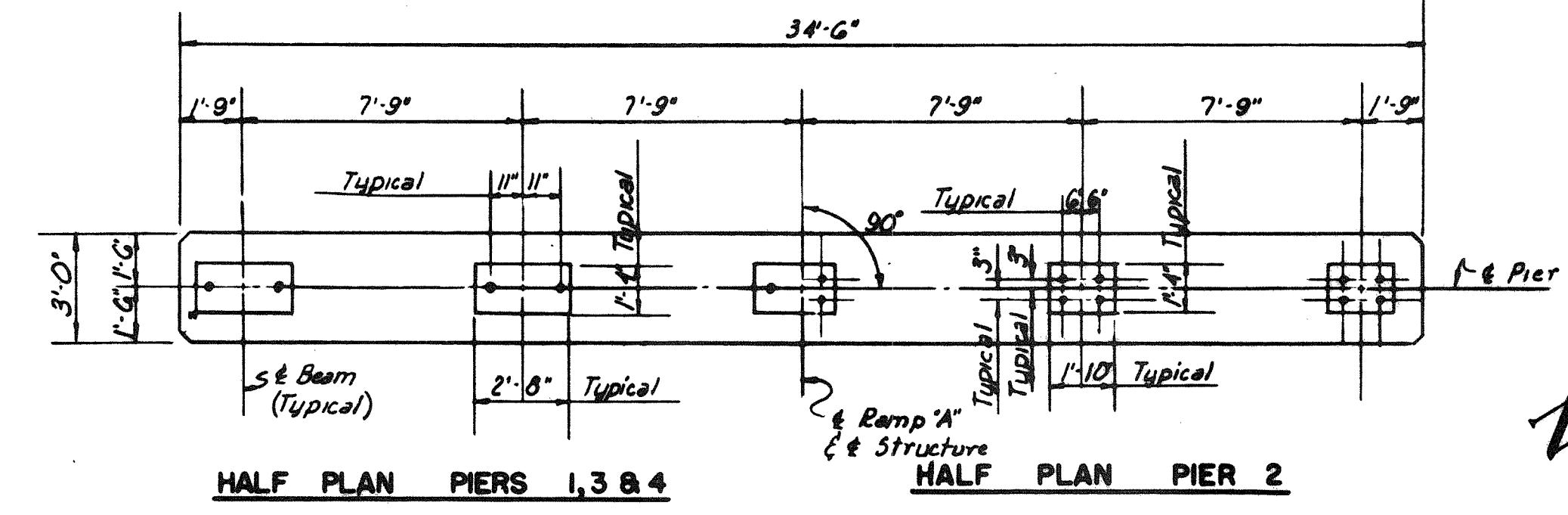


PLAN

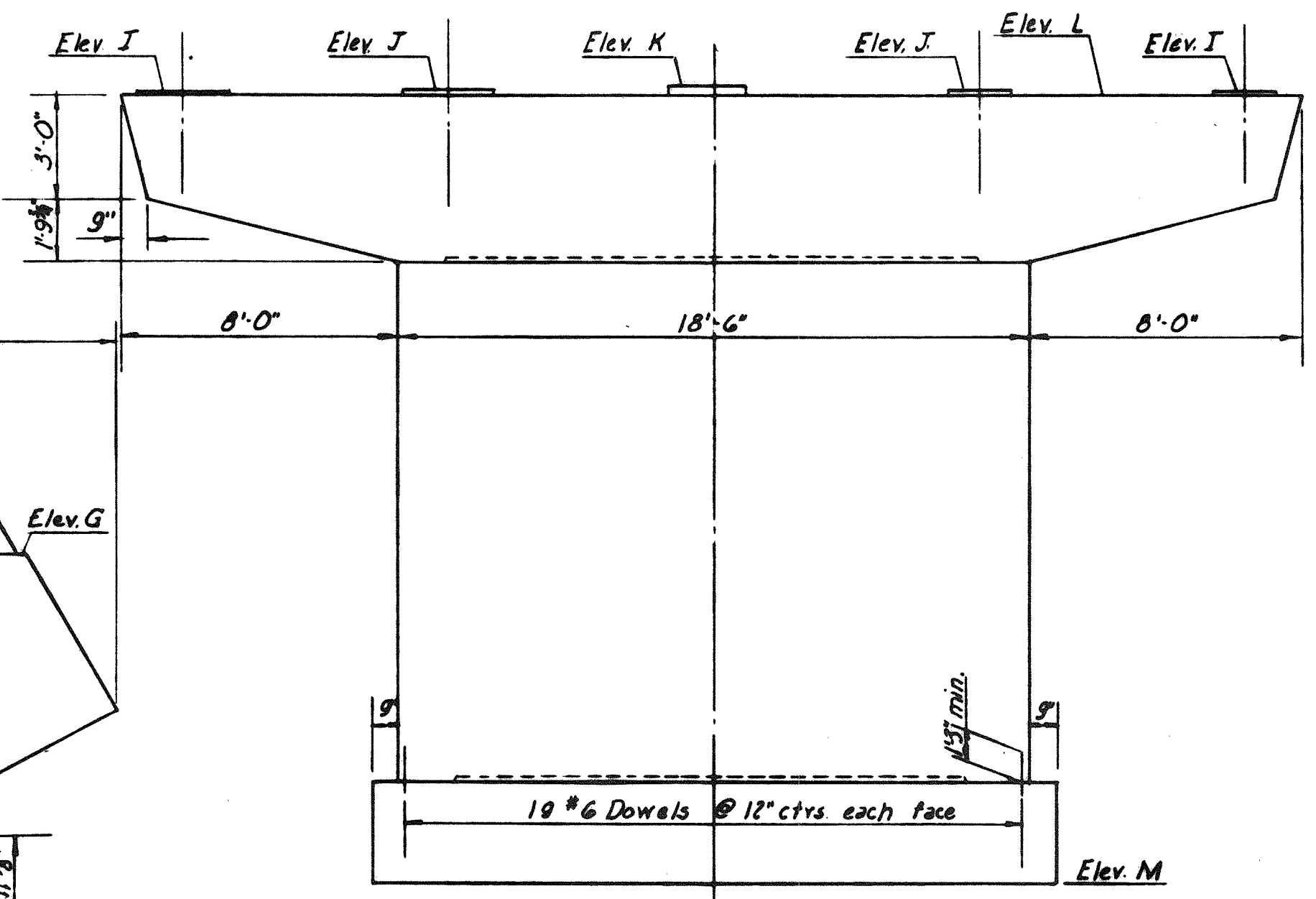


ELEVATION

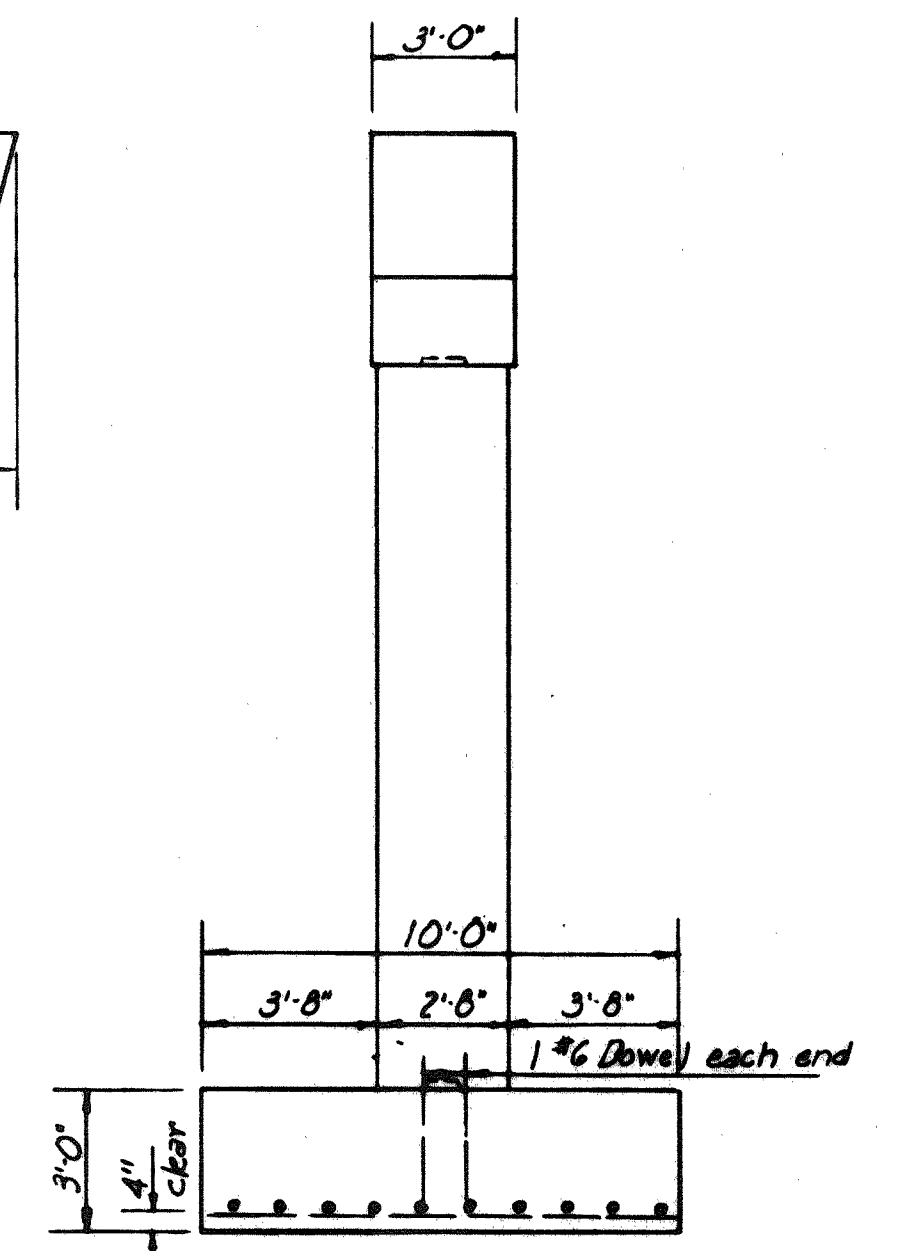
ELEVATIONS	
Abut. 1	Abut. 2
A	225.82
B	225.87
C	225.95
D	225.74
E	229.13
F	232.05
G	231.86
H	218.70



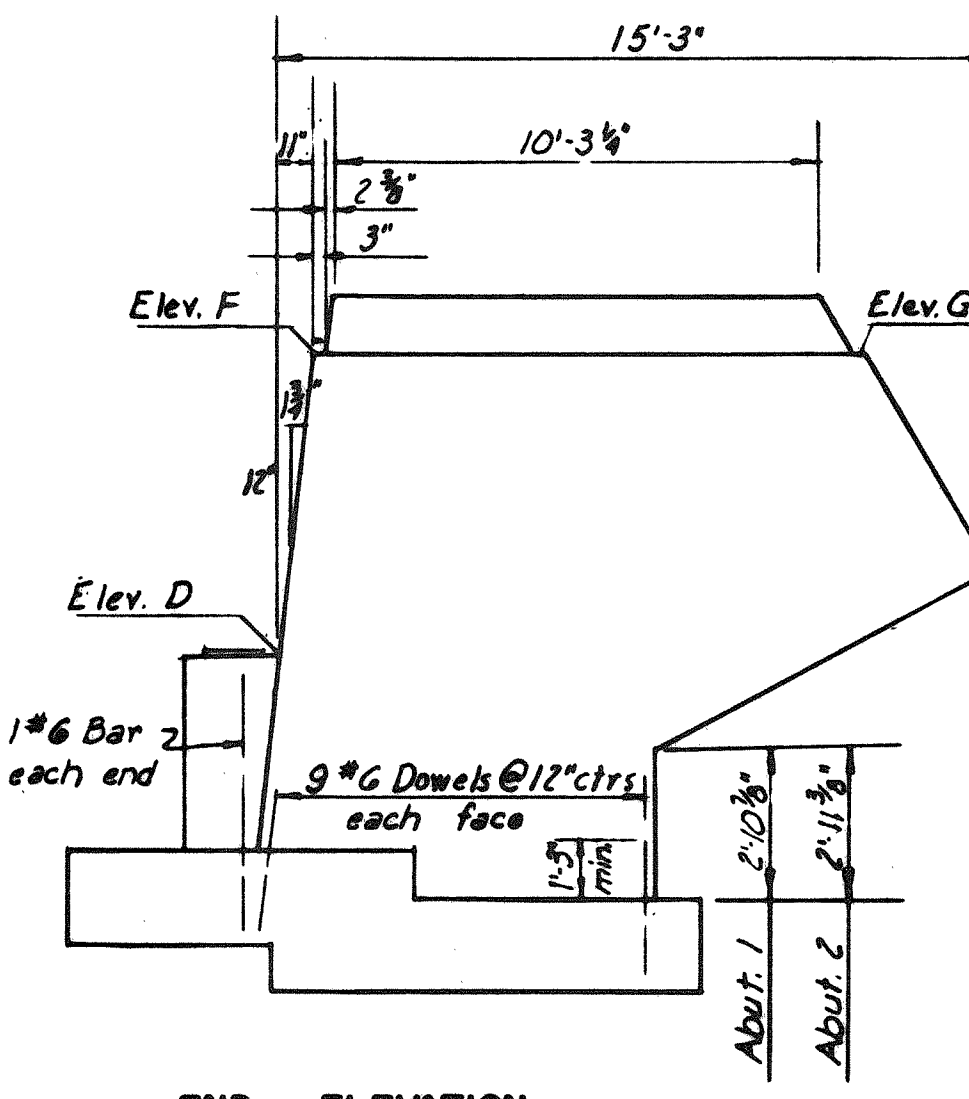
HALF PLAN PIER 2



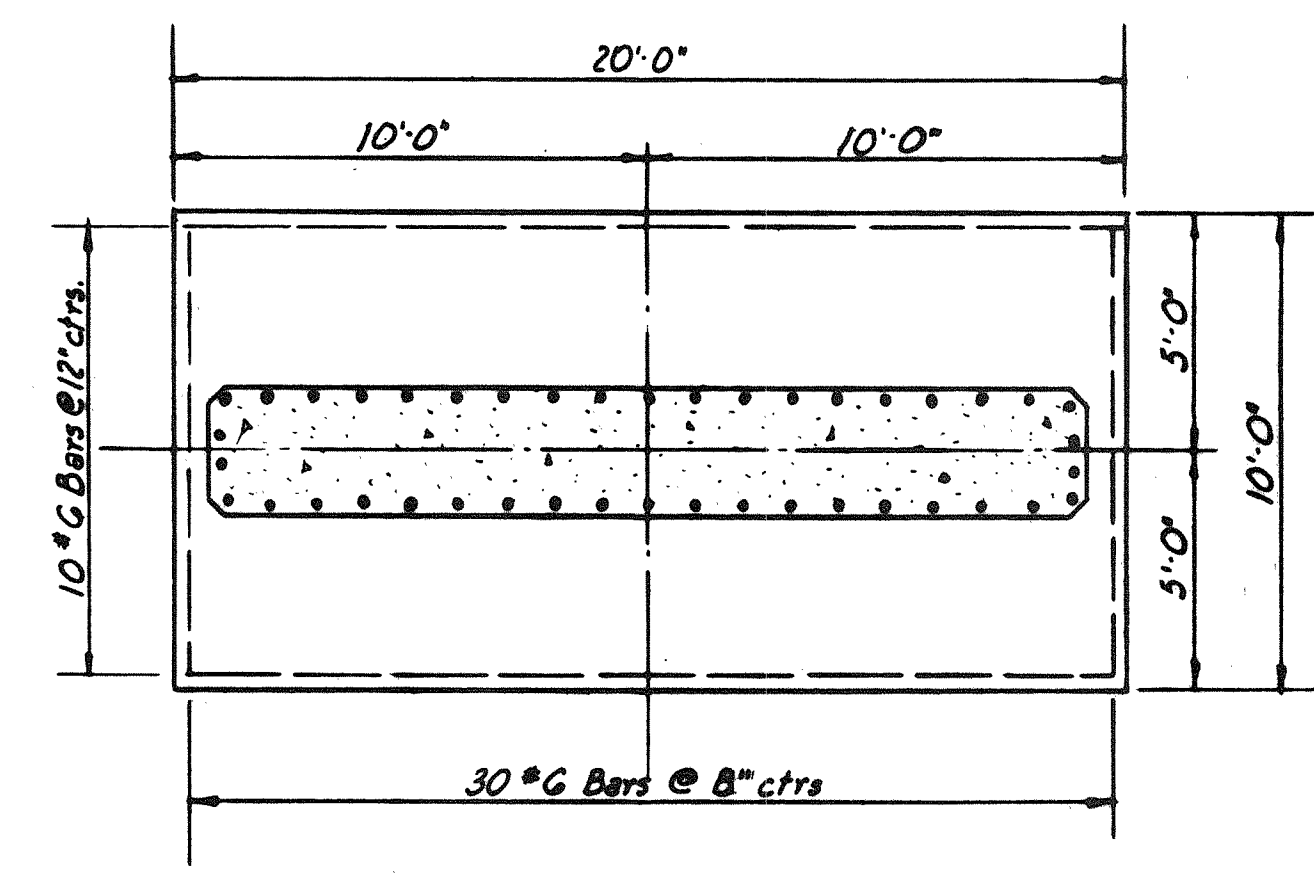
ELEVATION



END ELEVATION

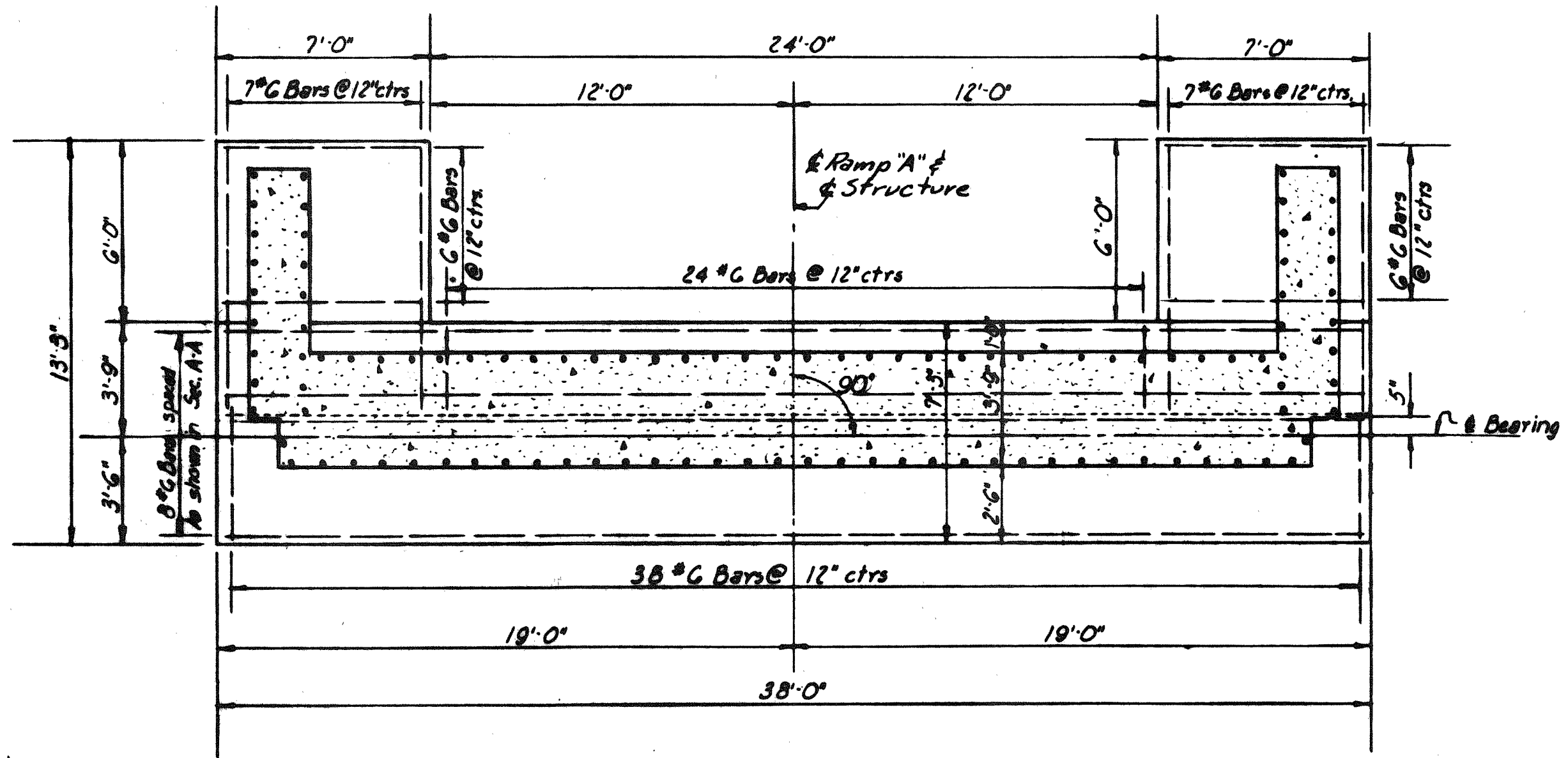


SECTION A-A



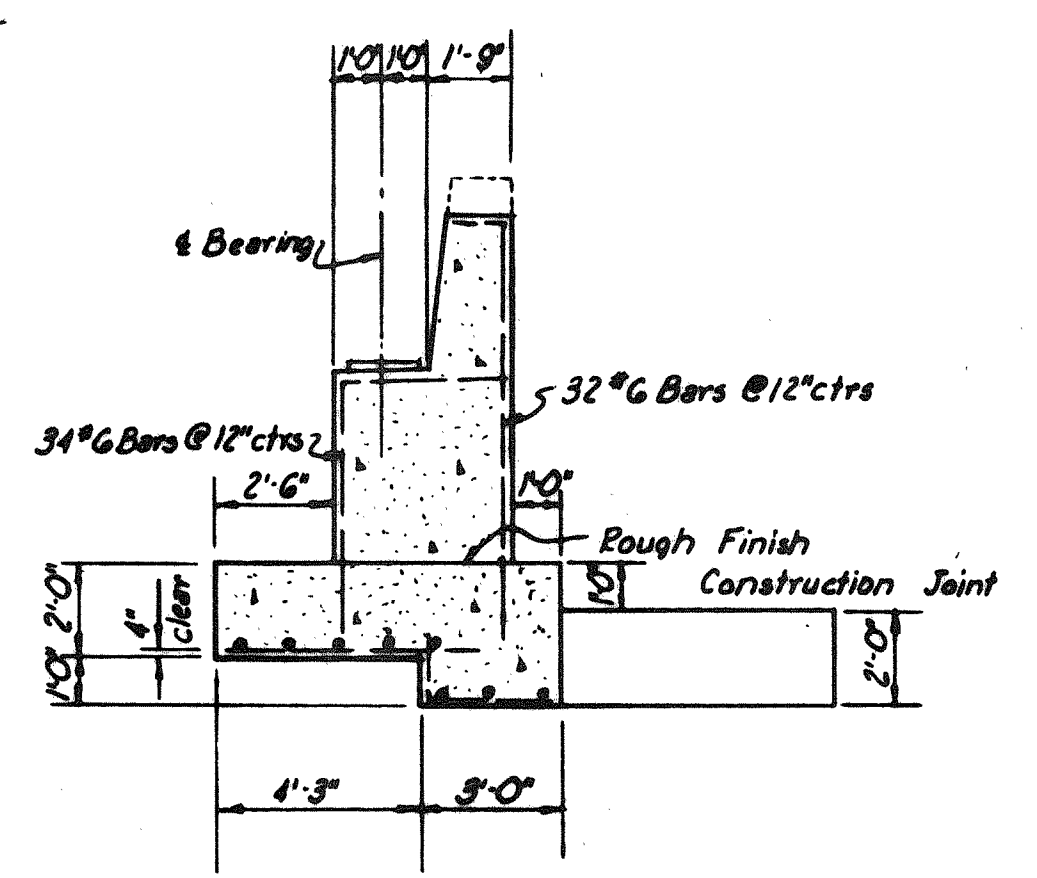
FOOTING PLAN

ELEVATIONS				
	Pier 1	Pier 2	Pier 3	Pier 4
I	226.13	226.63	226.49	226.24
J	226.18	226.68	226.54	226.29
K	226.26	226.76	226.62	226.37
L	226.11	226.61	226.47	226.22
M	202.50	204.00	204.00	202.00



FOOTING PLAN

ABUTMENTS



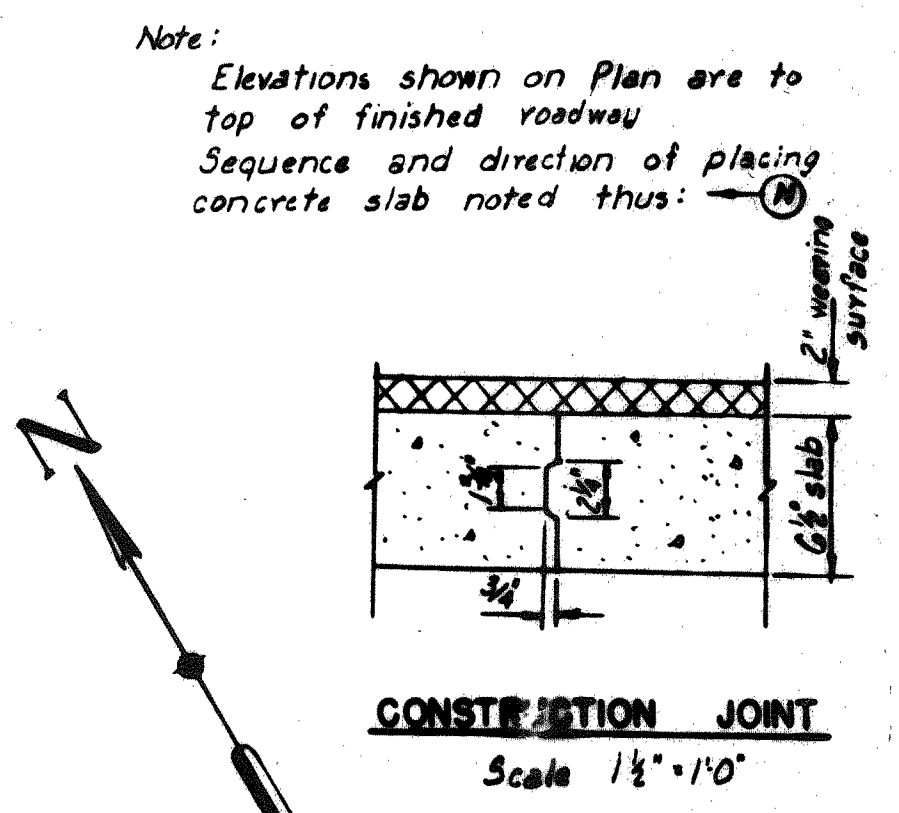
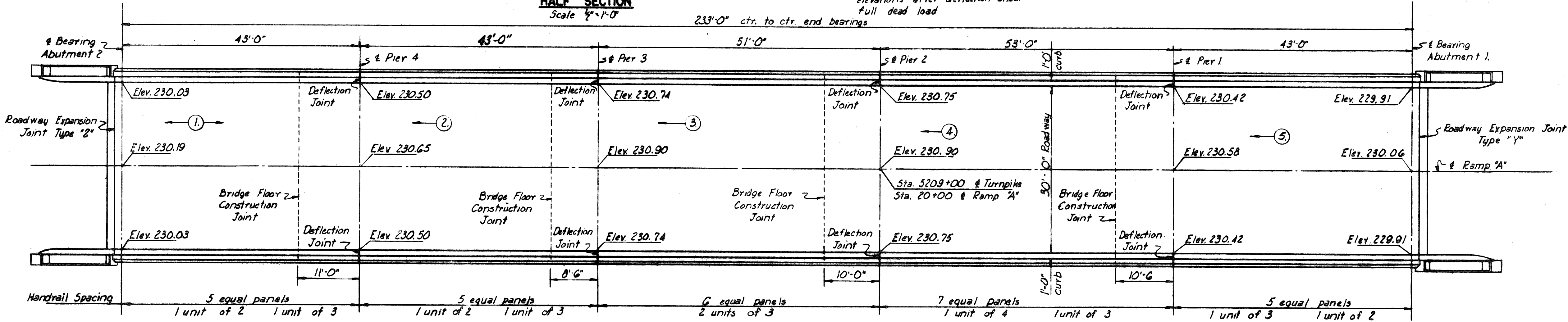
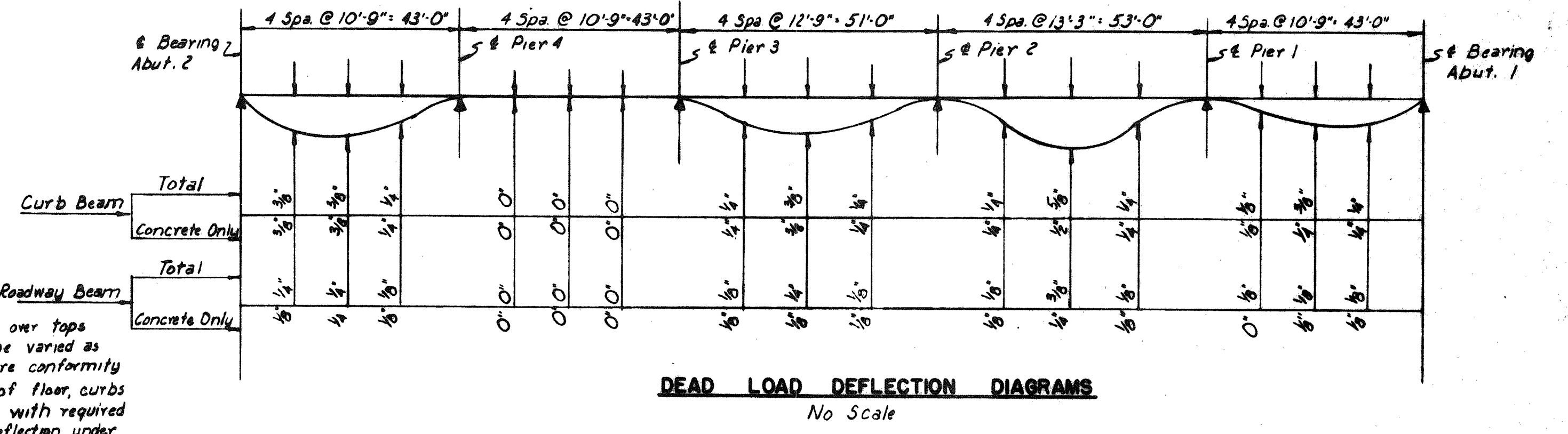
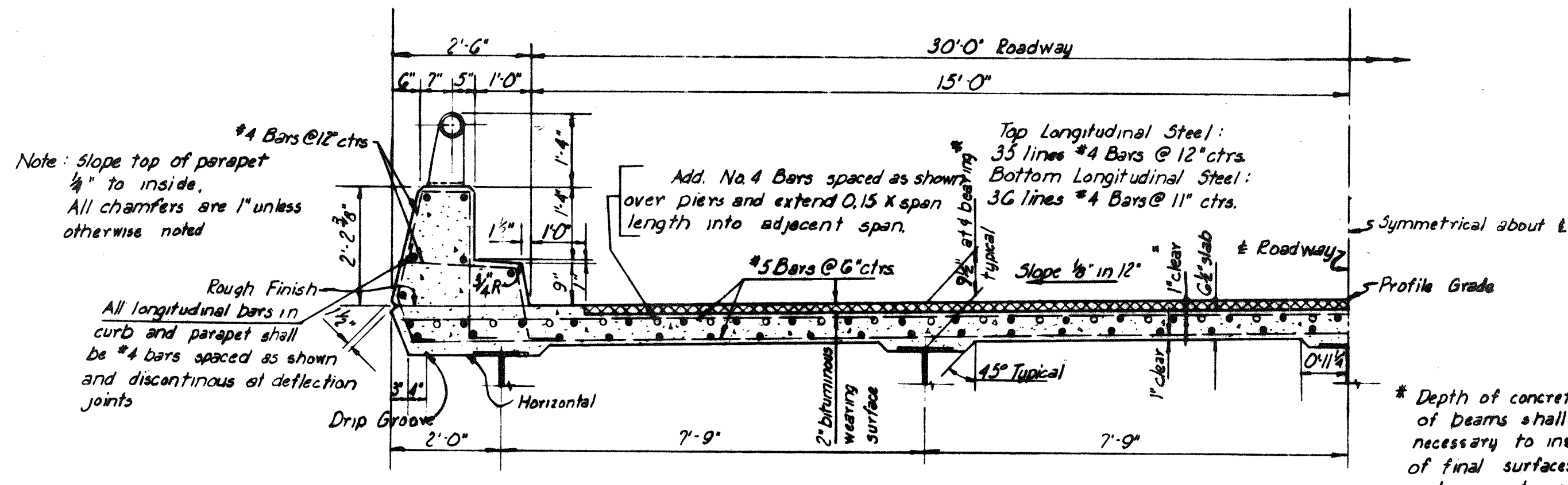
SECTION A-A

PIERS

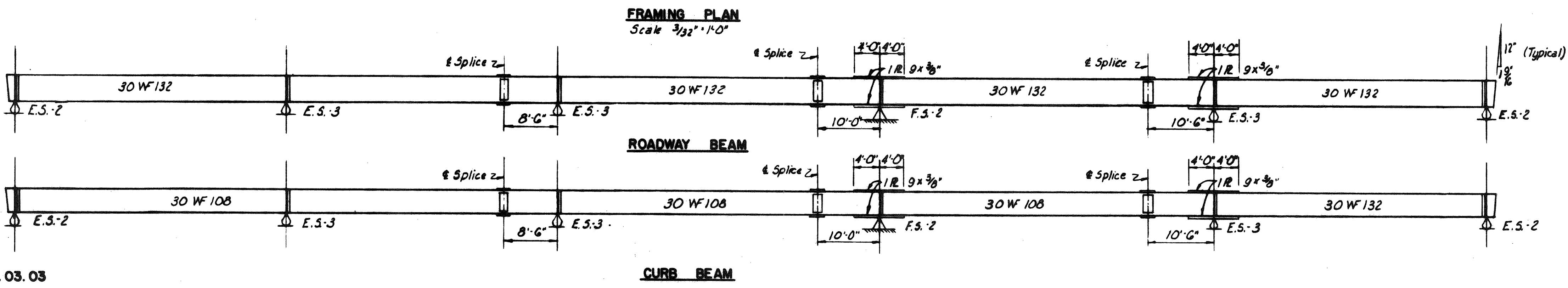
DRAWING 79.02.03.

BY	DATE			
MADE	B.K.	4-26-54		
TRACED				
CHECKED	R.S.G.	4-29-54	As-Built	ADH 4-29-54
IN CHARGE OF	IDSK			

MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
 SECTION 2 - PORTLAND TO AUGUSTA
 STRUCTURE NO. 79. TURNPIKE UNDER
 GARDNER INTERCHANGE RAMP "A"
 STA. 8208 + 00.00
ABUTMENTS AND PIERS
 HOWARD, NEEDLES, TAMMEN & BERGENDORFF SCALE: 1/4" = 1'-0"
 CONSULTING ENGINEERS CONTRACT NO. _____
 NEW YORK KANSAS CITY SHEET NO. 262 OF 388



Diaphragm Spacing	Elev. 229.24	Elev. 229.70	Elev. 229.95	Elev. 229.95	Elev. 229.62	Elev. 229.11
2 Spaces @ 21'-6" = 43'-0"	30 WF 108	30 WF 108	30 WF 108	30 WF 108	30 WF 108	30 WF 108
2 Spaces @ 21'-6" = 43'-0"	30 WF 132	30 WF 132	30 WF 132	30 WF 132	30 WF 132	30 WF 132
8'-6"	30 WF 132	30 WF 132	30 WF 132	30 WF 132	30 WF 132	30 WF 132
3 Spaces @ 17'-0" = 51'-0"	30 WF 132	30 WF 132	30 WF 132	30 WF 132	30 WF 132	30 WF 132
10'-0"	30 WF 132	30 WF 132	30 WF 132	30 WF 132	30 WF 132	30 WF 132
3 Spaces @ 17'-8" = 53'-0"	30 WF 108	30 WF 108	30 WF 108	30 WF 108	30 WF 108	30 WF 108
2 Spaces @ 21'-6" = 43'-0"	30 WF 108	30 WF 108	30 WF 108	30 WF 108	30 WF 108	30 WF 108



Note: Bawl at end of beam is with respect to beam axis. Bearing stiffeners at points of support are 2L 7x4 x 3/8"

DRAWING NO. 79.03.03

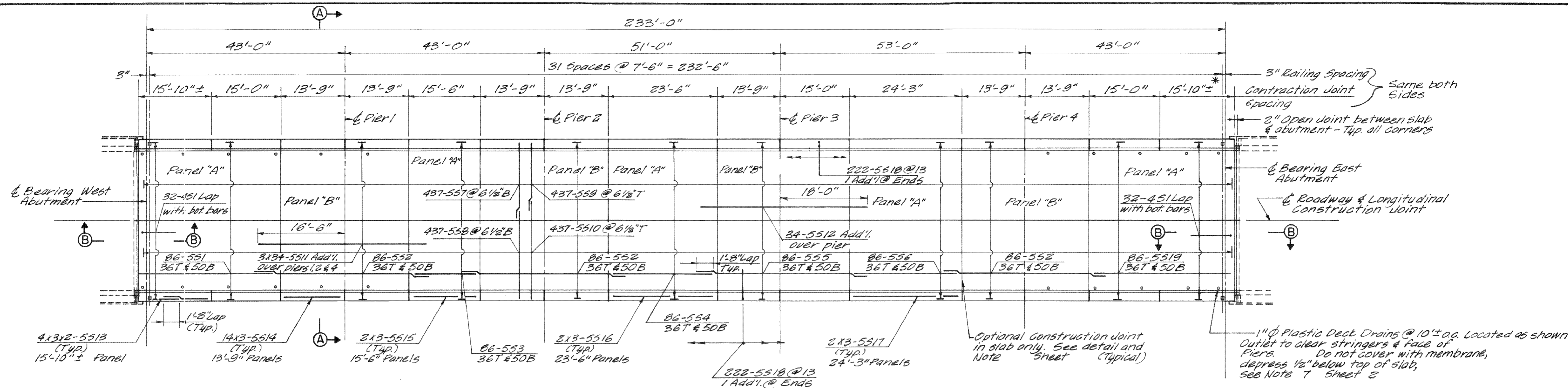
BY	DATE	REVISION	BY	DATE
MADE	B.K.	4-28-54		
TRACED				
CHECKED	H.D.G.	4-29-54	As-Built	MBW 4-29-54
IN CHARGE OF	105 K			

MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
SECTION 2 - PORTLAND TO AUGUSTA

STRUCTURE NO. 79 TURNPIKE UNDER
GARDINER INTERCHANGE RAMP "A"
STA. 8209 + 00.00
SUPERSTRUCTURE

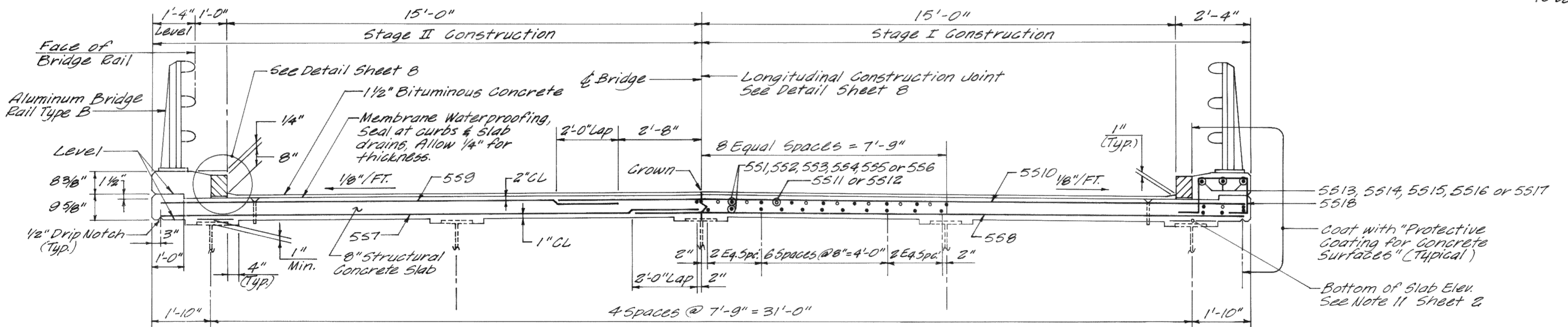
HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS

SCALE: As shown
CONTRACT NO. _____
SHEET NO. 362 OF 382

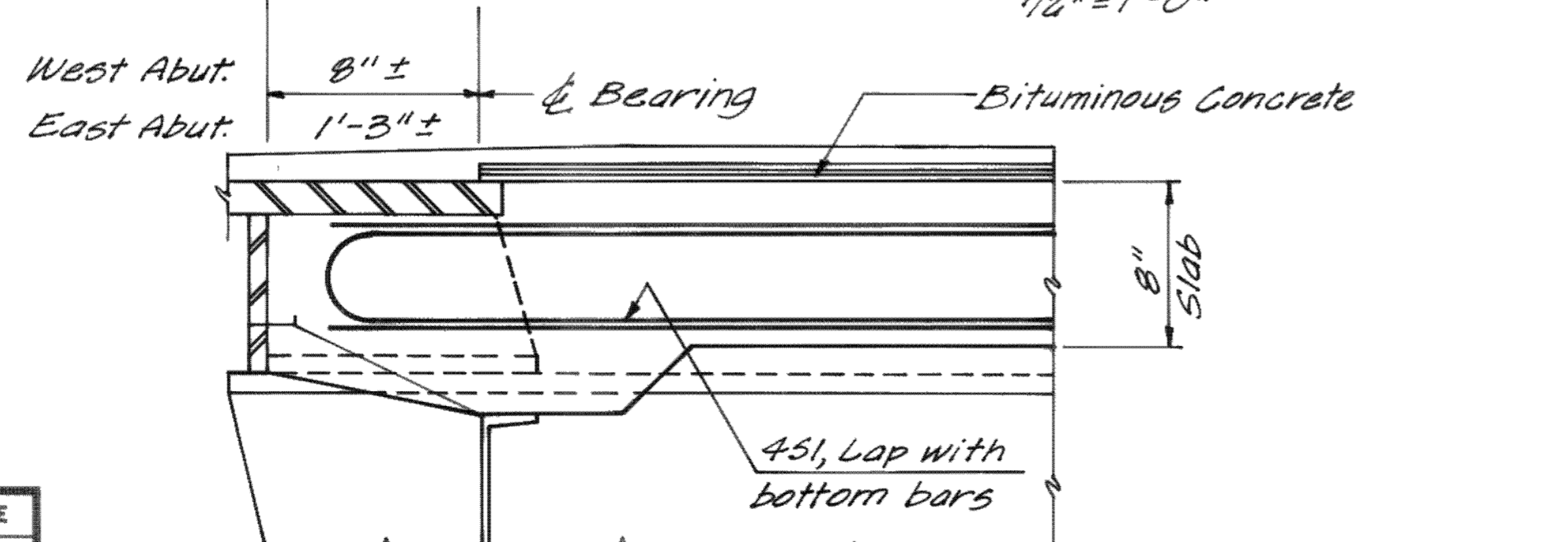


DECK PLAN
3/32" = 1'-0"

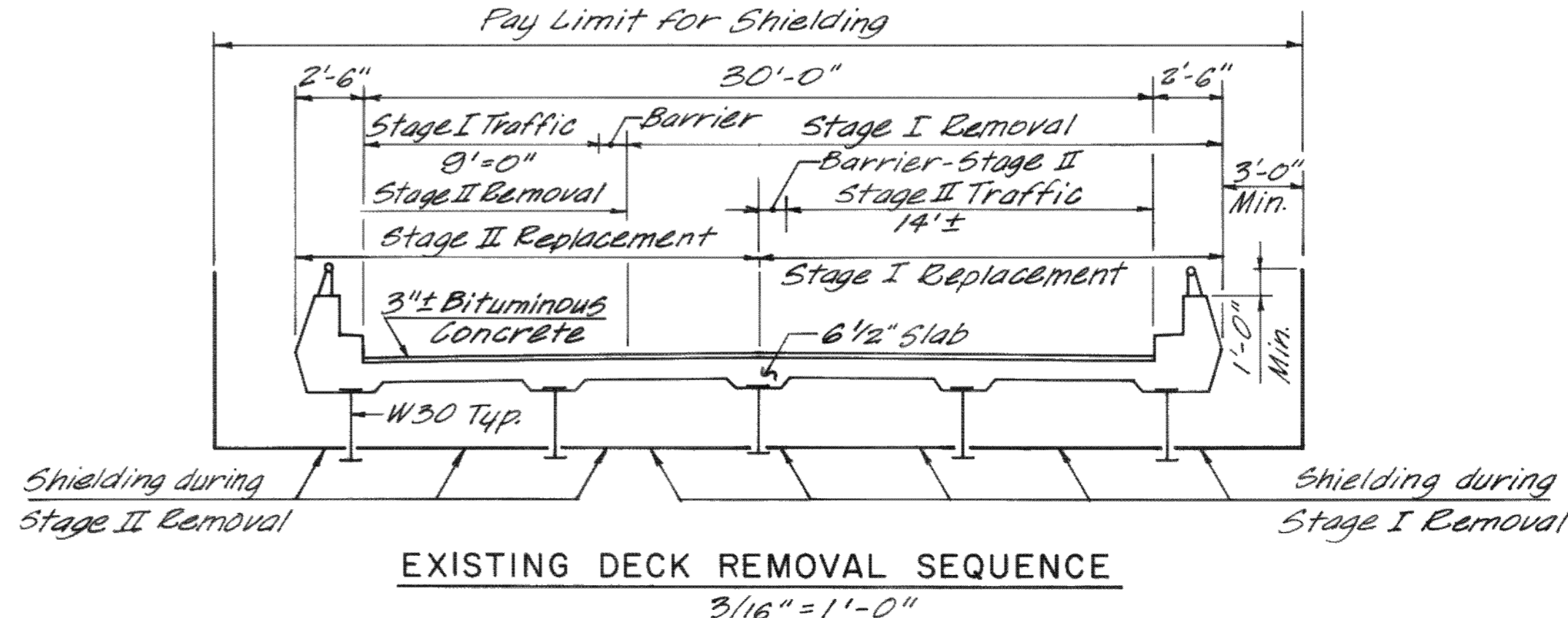
- NOTES:**
1. For General Notes & Quantities, see Sheet 2
 2. For Barrier Details, see Sheet 8
 3. For 1" V-Groove Details, see Sheet 8
 4. For Railing Details, see Sheet 10
 5. * Length of Last Section of Curbing to be field checked.



SECTION A-A
1/8" = 1'-0"



SECTION B-B
1 1/2" = 1'-0"



EXISTING DECK REMOVAL SEQUENCE
3/16" = 1'-0"

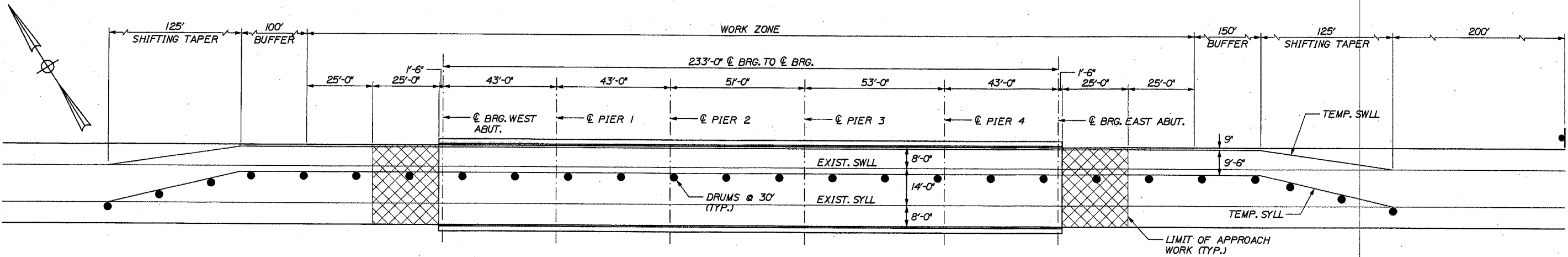
NO.	REVISION	BY	DATE	IN CHARGE OF
		MADE	R.D.	12/01
		TRACED		
		CHECKED	M.A.S.	12/01
		BY		J.P.W.

MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE

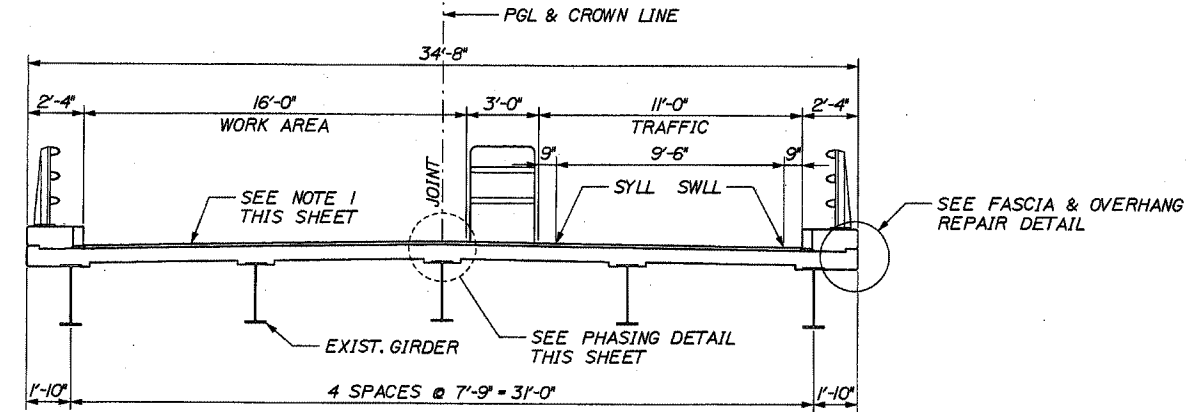
GARDINER INTERCHANGE
DECK REPLACEMENT

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS
BOSTON

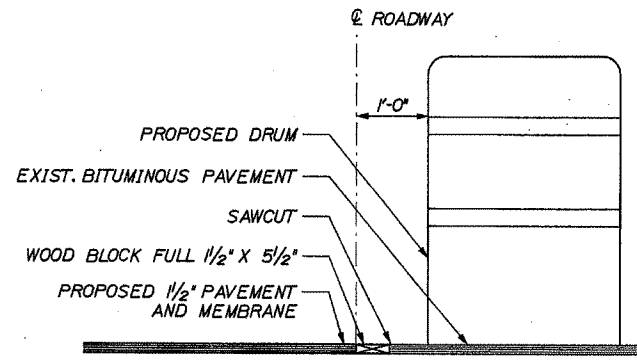
SCALE:
SHEET NO. 7 of 16



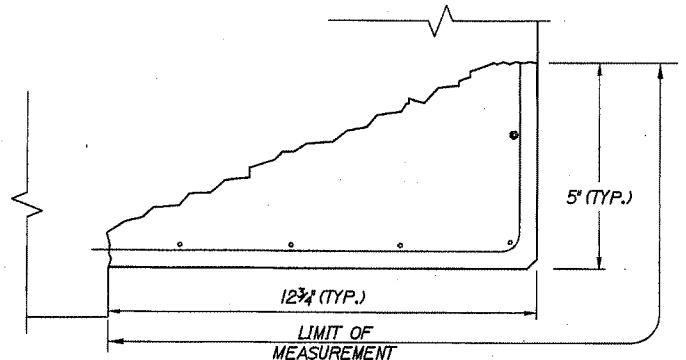
DECK PLAN
1" = 20'



TRANSVERSE SECTION - PHASE I
(PHASE II SIMILAR BUT OPPOSITE HAND)
1/4" = 1'-0"



PHASING DETAIL
N.T.S.



FASCIA & OVERHANG REPAIR DETAIL
N.T.S.

NOTES:

- 1) PRIOR TO IMPLEMENTING THE PHASE I TRAFFIC CONTROL CLOSE THE RIGHT HAND SHOULDER WITH DRUMS, SAW CUT THE DECK 4 FEET FROM THE RIGHT HAND CURB, REMOVE THE BITUMINOUS PAVEMENT AND MEMBRANE AND REPAIR THE DETERIORATED DECK CONCRETE WITHIN 3 FEET OF THE CURB. PAYMENT TO BE MADE UNDER THE APPROPRIATE CONTRACT ITEMS.
- 2) SAW CUT EXISTING PAVEMENT 15'-6" OFF THE CURB (WORK AREA SIDE) TO ALLOW FOR LAPPING OF THE HIGH PERFORMANCE MEMBRANE. SEE PHASING DETAIL THIS SHEET.
- 3) REMOVE AND DISPOSE EXISTING MEMBRANE (1/4") AND PAVEMENT (1-1/2"). INSTALL 1-1/2" HOT MIX ASPHALT, 9.5 mm NOMINAL MAXIMUM SIZE ON HIGH PERFORMANCE MEMBRANE. REMOVAL OF EXISTING SOUND CONCRETE PATCHES IN BITUMINOUS PAVEMENT AND RESURFACING OR LEVELING THE SOUND PATCHES FLUSH WITH THE CONCRETE SURFACE SHALL BE INCIDENTAL TO THE ITEM 202.202.
- 4) REPAIR DECK CONCRETE TO THE LIMITS DELINEATED BY THE RESIDENT. PAYMENT TO BE MADE UNDER 518.80 AND 518.81.
- 5) REMOVE PAVEMENT FULL DEPTH OVER BACKWALLS TO EXPOSE CONCRETE. REPAIR BACKWALL CONCRETE TO THE LIMITS DELINEATED BY THE RESIDENT. PAYMENT TO BE MADE UNDER 518.80.
- 6) TEMPORARY RAISED PAVEMENT MARKINGS SHALL BE USED TO DELINEATE THE TRAFFIC LANE ON NEW PAVEMENT. TEMPORARY PAINT LINES SHALL BE USED TO DELINEATE THE TRAFFIC LANE ON EXISTING PAVEMENT.
- 7) SEAL SHOULDER AREAS WITH TWO APPLICATIONS OF EMULSIFIED BITUMINOUS SEALER PER MDOT STANDARD SPECIFICATION 403.03 AFTER COMPLETING ALL DECK PAVING WORK.
- 8) RETROFIT ALL EXISTING ALUMINUM BRIDGE RAIL SPLICES NOT IN CONFORMANCE WITH THE ALUMINUM BRIDGE RAILING - SPLICE MODIFICATION DETAIL. PAYMENT TO BE MADE UNDER 507.095.
- 9) MODIFY THE EXISTING EXPANSION JOINTS PER THE EXPANSION JOINT MODIFICATION DETAILS AND SPECIFICATIONS. PAYMENT TO BE MADE UNDER 520.2213.
- 10) PLUG OR EXTEND ALL DECK WEEPS WHICH DRAIN ONTO PIERS, ABUTMENTS OR STRUCTURAL STEEL AS DIRECTED BY THE RESIDENT. PAYMENT SHALL BE BY THE VARIOUS LABOR AND EQUIPMENT ITEMS.

NOTE:
BOND BREAKER REQUIRED ON DECK UNDER WOOD BLOCK. DURING PAVING OPERATION, DO NOT PERMIT TRAFFIC TO TRAVEL OVER JOINT WHEN CHANGING CONSTRUCTION PHASING.

Date: 3/26/2008

Filename: M:\jobs\09009\959 Bridge Repair\030 2008 Bridge Repairs\CAD\200805\016_GardinerDeck.dgn

Scale:

No.	Revision	By	Date

Designed by:

HNTB

By	Date	By	Date
Designed	CAH 03/08	Checked	MDD 03/08
Drawn	MPC 03/08	In Charge of	RAL 03/08

HNTB CORPORATION
2 Thomas Drive
Westbrook, ME 04092
TEL (207) 774-5155
FAX (207) 772-7410

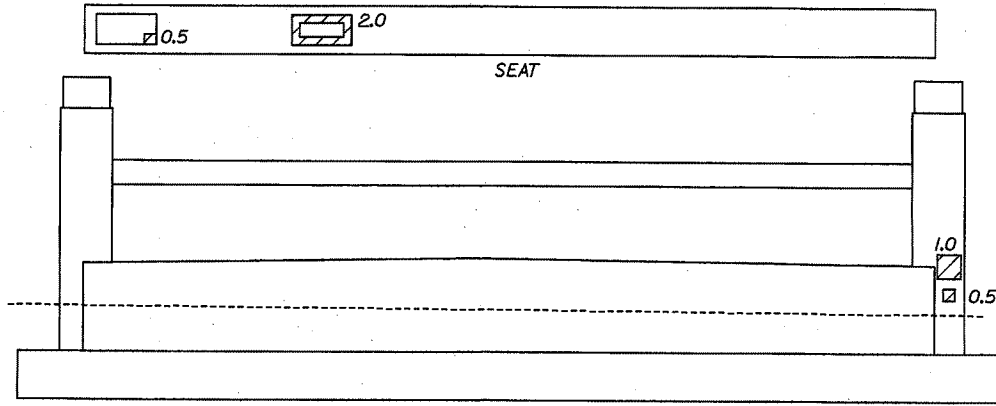


THE GOLD STAR MEMORIAL HIGHWAY

CONCRETE REPAIR, MEMBRANE AND PAVING
MM 59.9, MM 74.0, MM 93.3 AND MM 102.0
GARDINER INTERCHANGE MM 102.0
DECK PLAN, SECTIONS AND DETAILS

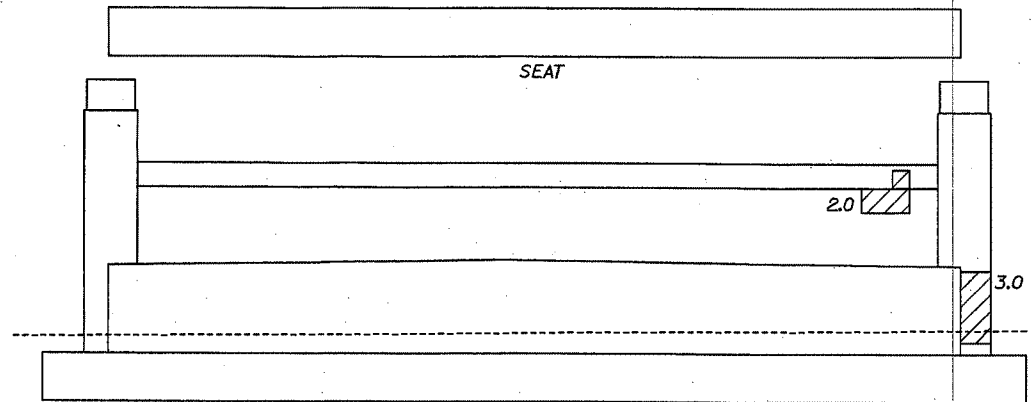
Date: 3/26/2008

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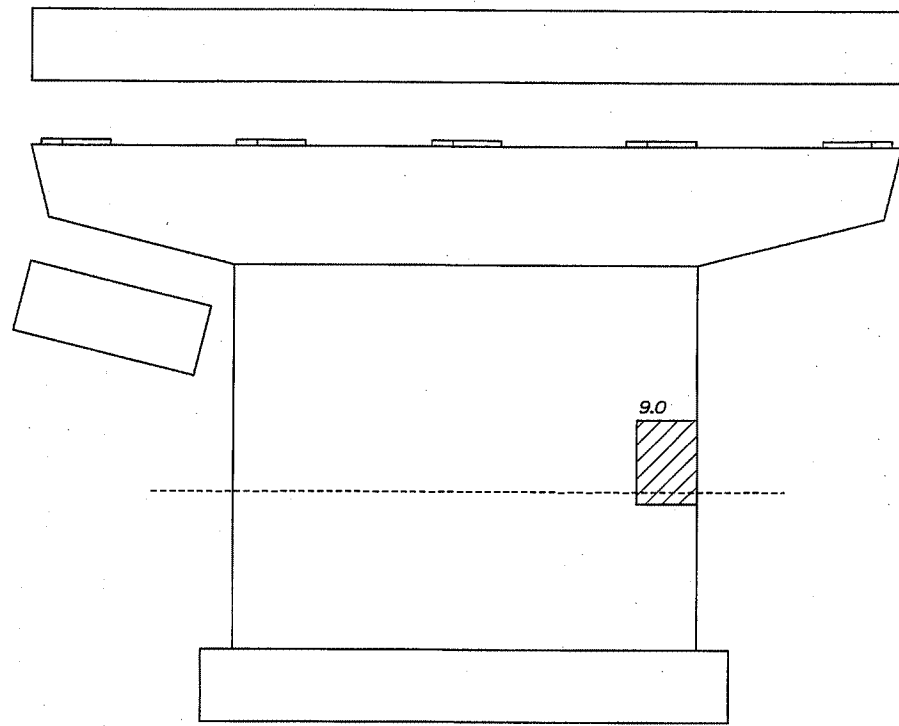
WEST ABUTMENT

ESTIMATED REPAIR QUANTITIES (INCLUDING WINGWALLS AND END POSTS) = 4.0 SF

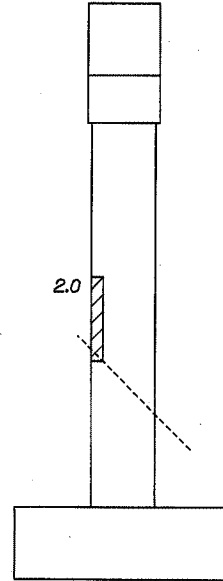


EAST ABUTMENT

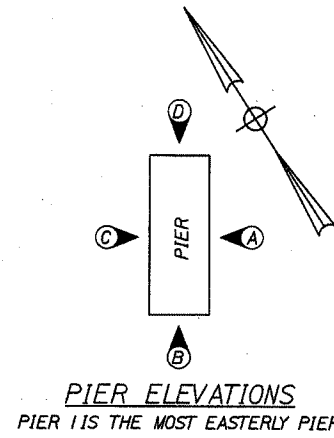
ESTIMATED REPAIR QUANTITIES (INCLUDING WINGWALLS AND END POSTS) = 5.0 SF



ELEVATION C



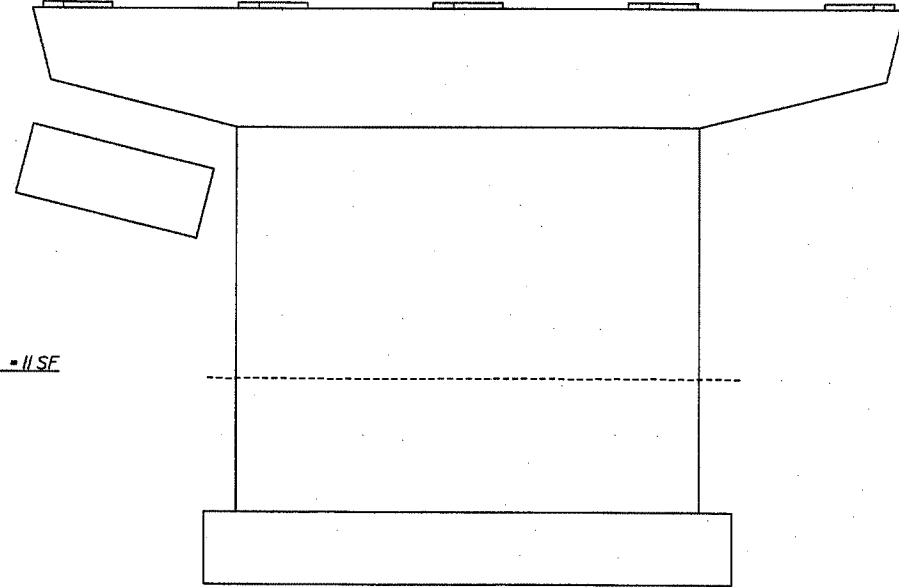
ELEVATION B



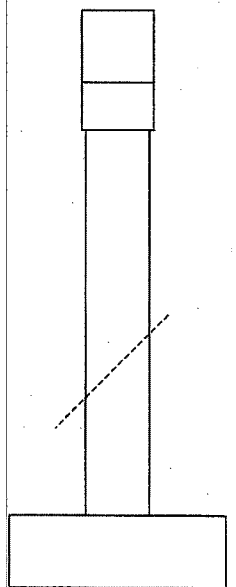
PIER ELEVATIONS
PIER 1 IS THE MOST EASTERLY PIER

ESTIMATED REPAIR QUANTITIES = 11 SF

PIER 1
(MOST EASTERLY)



ELEVATION A



ELEVATION D

NOTE:
NO SUBSTRUCTURE REPAIRS ON PIERS 2, 3 AND 4.

Scale:			
No.	Revision	By	Date

Designed by:						
HNTB						
	By	Date		By	Date	
	CAH	03/08	Checked	MDD	03/08	
	Drawn	MPC	03/08	In Charge of	RAL	03/08

HNTB CORPORATION
2 Thomas Drive
Westbrook, ME 04092
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**THE GOLD STAR
MEMORIAL HIGHWAY**

CONCRETE REPAIR, MEMBRANE AND PAVING
MM 59.9, MM 74.0, MM 93.3 AND MM 102.0
GARDINER INTERCHANGE MM 102.0
SUBSTRUCTURE REPAIRS

CONTRACT: 2008.05

SHEET NUMBER: GI-2
17 OF 21