

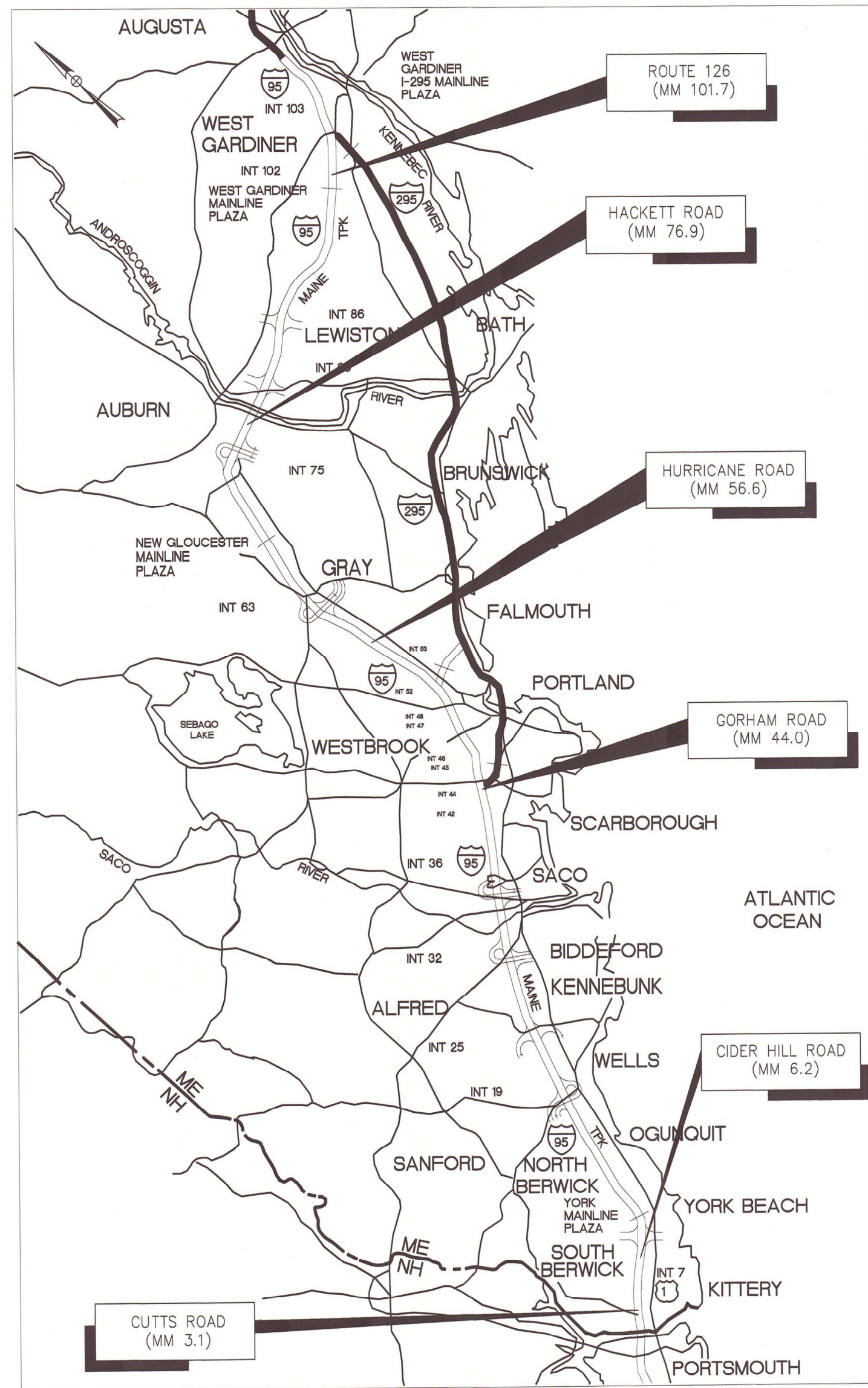


THE GOLD STAR
MEMORIAL HIGHWAY

MAINE TURNPIKE AUTHORITY

DANIEL E. WATHEN, CHAIR
MICHAEL J. CIANCHETTE, MEMBER
JANE L. LINCOLN, MEMBER
ANDREW McLEAN, MEMBER
THOMAS J. ZUKE, MEMBER
BRUCE A. VAN NOTE, EX-OFFICIO

PETER MERFELD, ACTING EXECUTIVE DIRECTOR



LOCATION MAP

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	HAUNCH REMOVAL DETAILS
3-8	MAINTENANCE OF TRAFFIC DETAILS
9-20	AS-BUILT REFERENCE PLANS

CONTRACT 2025.05
WIDENED CONCRETE HAUNCH REMOVAL
CUTTS ROAD (MM 3.1)
CIDER HILL (MM 6.2)
GORHAM ROAD (MM 44.0)
HURRICANE ROAD (MM 56.6)
HACKETT ROAD (MM 76.9)
ROUTE 126 (MM 101.7)

CONTRACT 2025.05

APPROVED: MAINE TURNPIKE AUTHORITY



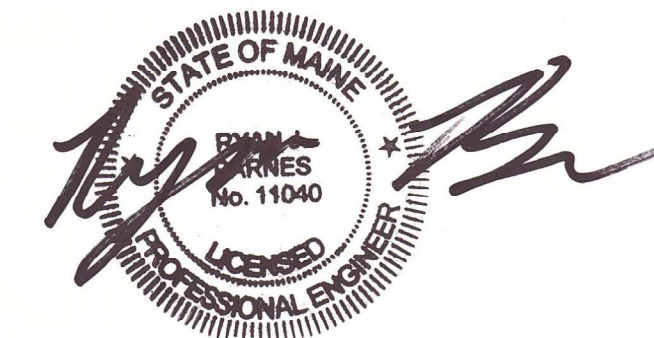
Peter S. Merfeld
PETER S. MERFELD, P.E. - CHIEF OPERATIONS OFFICER

9/20/24
DATE



Stephen R. Bartre
STEPHEN R. BARTRE, P.E., - DIRECTOR OF ENGINEERING

9/20/24
DATE

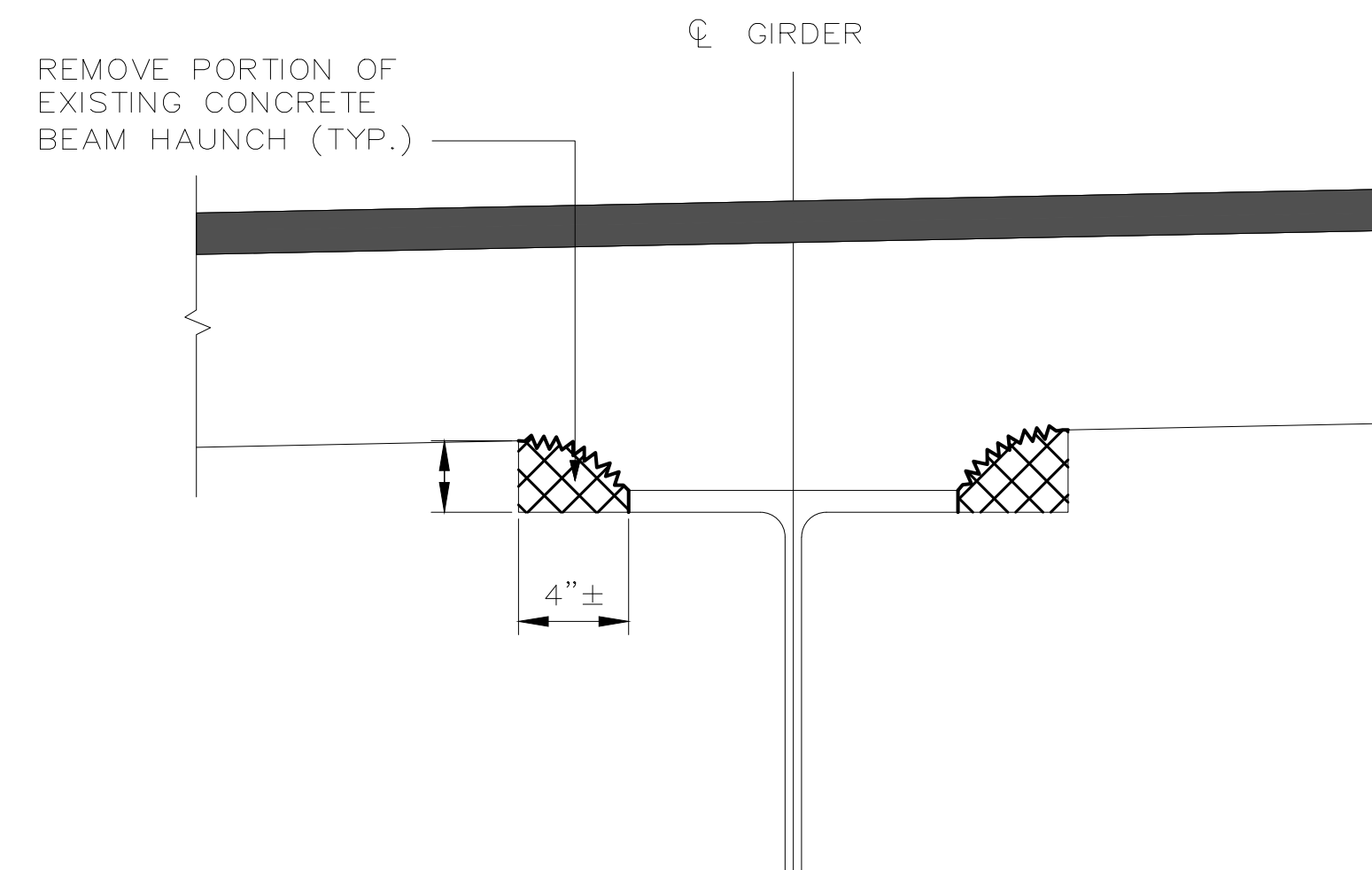


RYAN BARNES, P.E.
PROJECT MANAGER

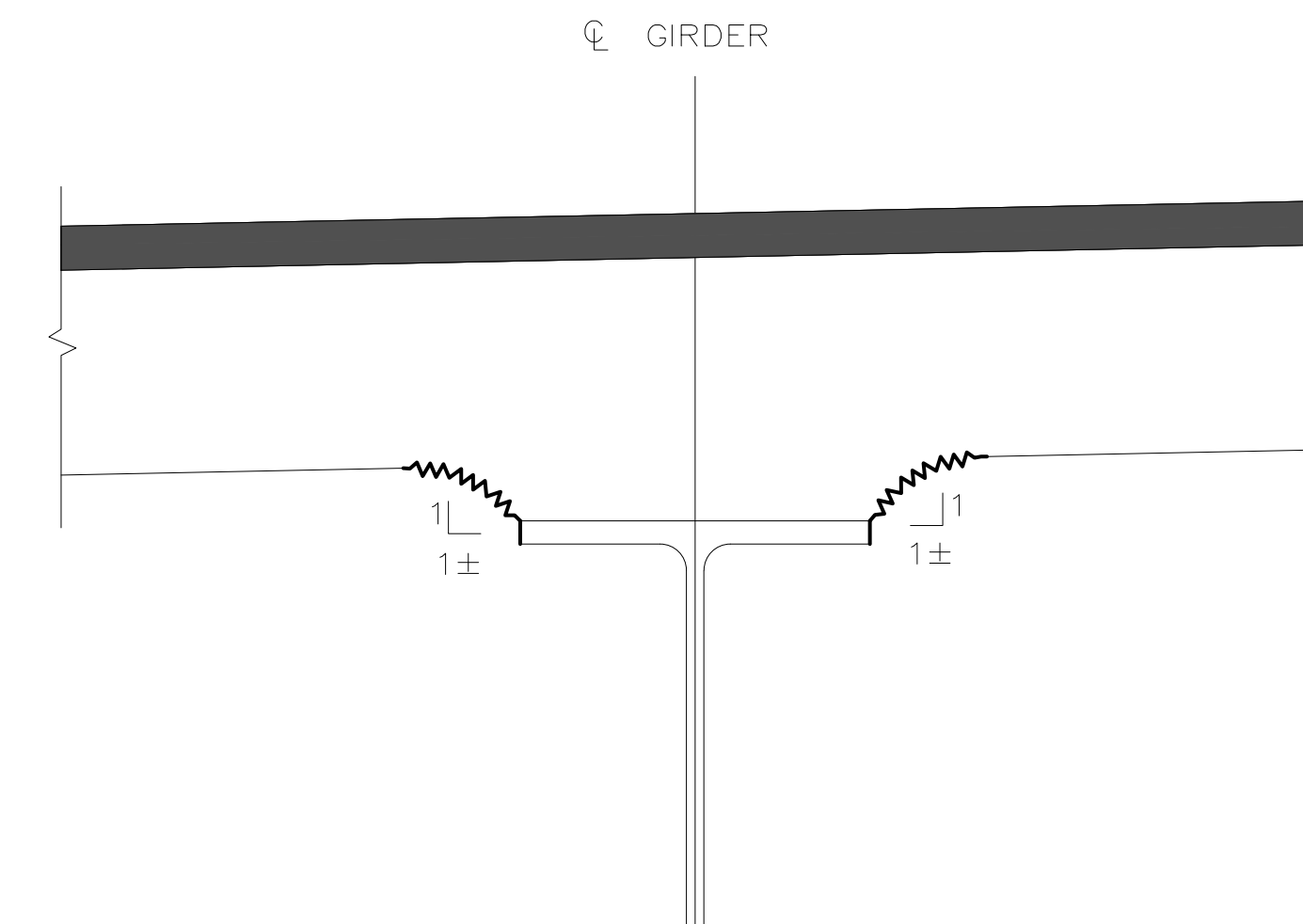
9.20.24
DATE

GENERAL NOTES

1. ALL DETAILS SHALL BE IN CONFORMANCE WITH MAINEDOT DEPARTMENT OF TRANSPORTATION (MAINEDOT) 2020 STANDARD DETAILS FOR HIGHWAYS AND BRIDGES WITH ALL UPDATES AND MAINEDOT BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL, LATEST REVISION, UNLESS OTHERWISE NOTED IN THESE PLANS.
2. ALL WORK MUST BE COMPLETED WITHIN THE RIGHT OF WAY.
3. EXISTING HAUNCHES TO BE REMOVED OVER EXISTING PAVEMENT AREA, OR AS DIRECTED BY THE RESIDENT. ANY PORTIONS OF THE EXISTING BRIDGES REMOVED BY THE CONTRACTOR SHALL BE COME THE PROPERTY OF THE CONTRACTOR.
4. CONTRACTOR SHALL SUBMIT PROPOSED STAGING AREA(S) AND FIELD TRAILER LOCATION TO THE RESIDENT FOR APPROVAL PRIOR TO STARTING WORK.
5. EXISTING UTILITIES WERE NOT LOCATED FOR THESE PLANS. EXISTING UTILITIES ARE ATTACHED TO THE CIDER HILL BRIDGE. IMPACTS TO UTILITIES ARE NOT ANTICIPATED TO PERFORM THIS WORK. NO WORK SHALL BE STARTED UNTIL THE OWNERS OF THE VARIOUS UTILITIES ARE NOTIFIED BY THE CONTRACTOR OF THE PROPOSED CONSTRUCTION. THE CONTRACTOR IS ALSO REQUIRED TO CALL DIG SAFE AT 1-888-344-7233 AT LEAST 72 HOURS PRIOR TO THE START OF WORK.
6. THE CONTRACTOR SHALL ALSO NOTIFY ALL NON-MEMBERS THROUGH WWW.OKTODIG.COM OR AS OTHERWISE REQUIRED BY THE MAINE PUBLIC UTILITIES COMMISSION. ALL PROPOSED SIGN LOCATIONS AND EXCAVATION LOCATIONS SHALL BE MARKED AT THE NOTIFICATION TIME. NO EXCAVATION SHALL BE PERMITTED UNTIL THE AUTHORITY HAS LOCATED AND MARKED ITS ELECTRONIC COPY OF ALL DIG SAFE TICKETS WITHIN 24 HOURS OF THEIR RELEASE FOR PROJECT NOTIFICATIONS AND 3RD PARTY UTILITY LOCATER COORDINATION.
7. THE CONTRACTOR SHALL NOTIFY THE RESIDENT 10 CALENDAR DAYS PRIOR TO SUBMITTING A UTILITY LOCATE REQUEST THROUGH DIG SAFE SO THAT THE RESIDENT CAN ARRANGE FOR MAINE TURNPIKE UNDERGROUND UTILITY LOCATION. ALL PROPOSED SIGN LOCATIONS AND EXCAVATION LOCATIONS SHALL BE MARKED AT THE NOTIFICATION TIME.



EXISTING CONDITION
NOT TO SCALE




FINAL CONDITION
NOT TO SCALE

CONCRETE HAUNCH REMOVAL DETAILS

Scale: NOT TO SCALE


No.	Revision	By	Date

Designed by:



	By	Date		By	Date
Designed	RJB	9/17/24	Checked	KVO	9/17/24
Drawn	BMB	9/17/24	In Charge of	SRT	9/17/24

MAINE TURNPIKE AUTHORITY
 2360 Congress Street
 Portland, ME 04102
 TEL (207) 871-7771
 FAX (207) 879-5567



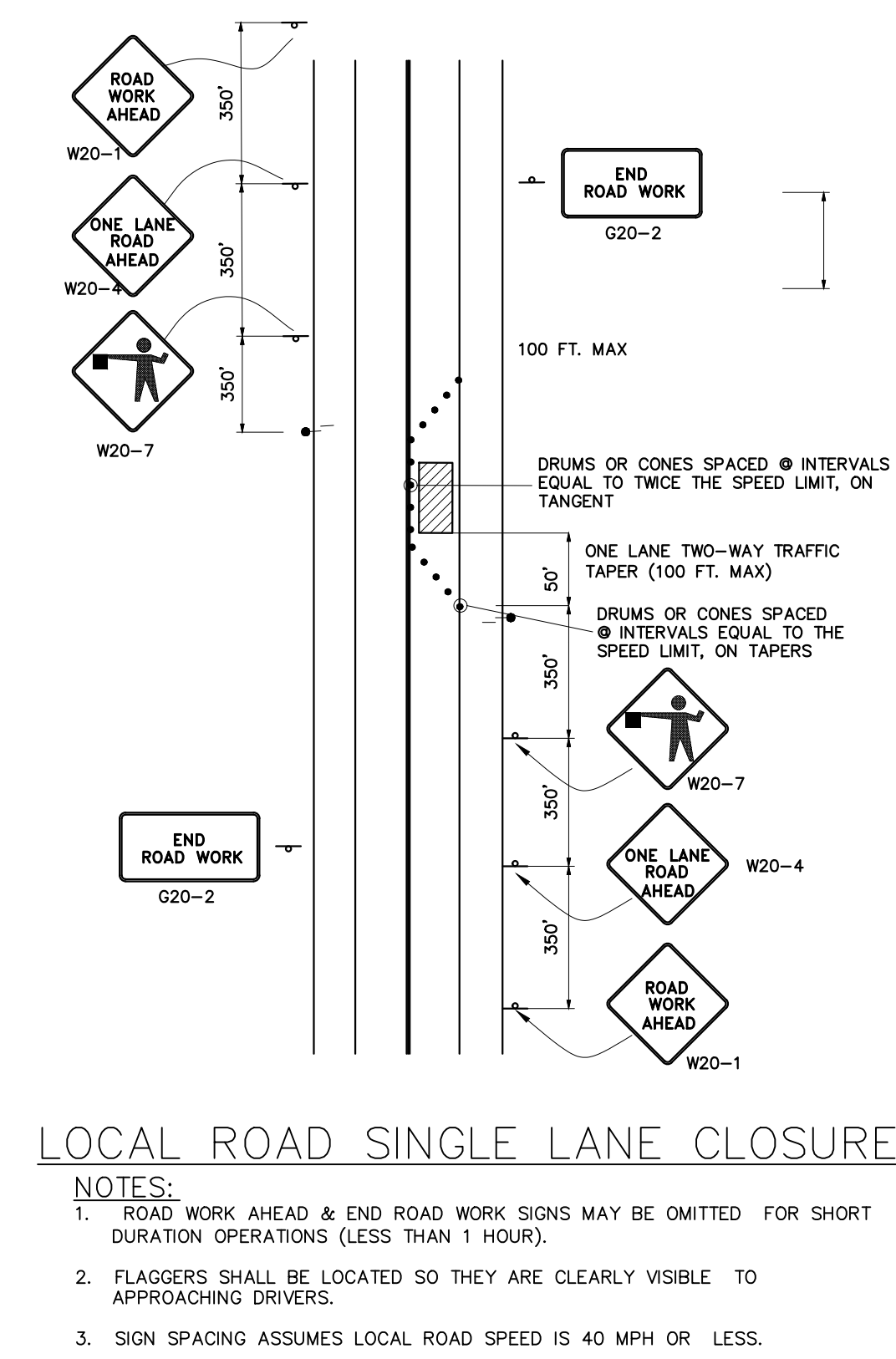
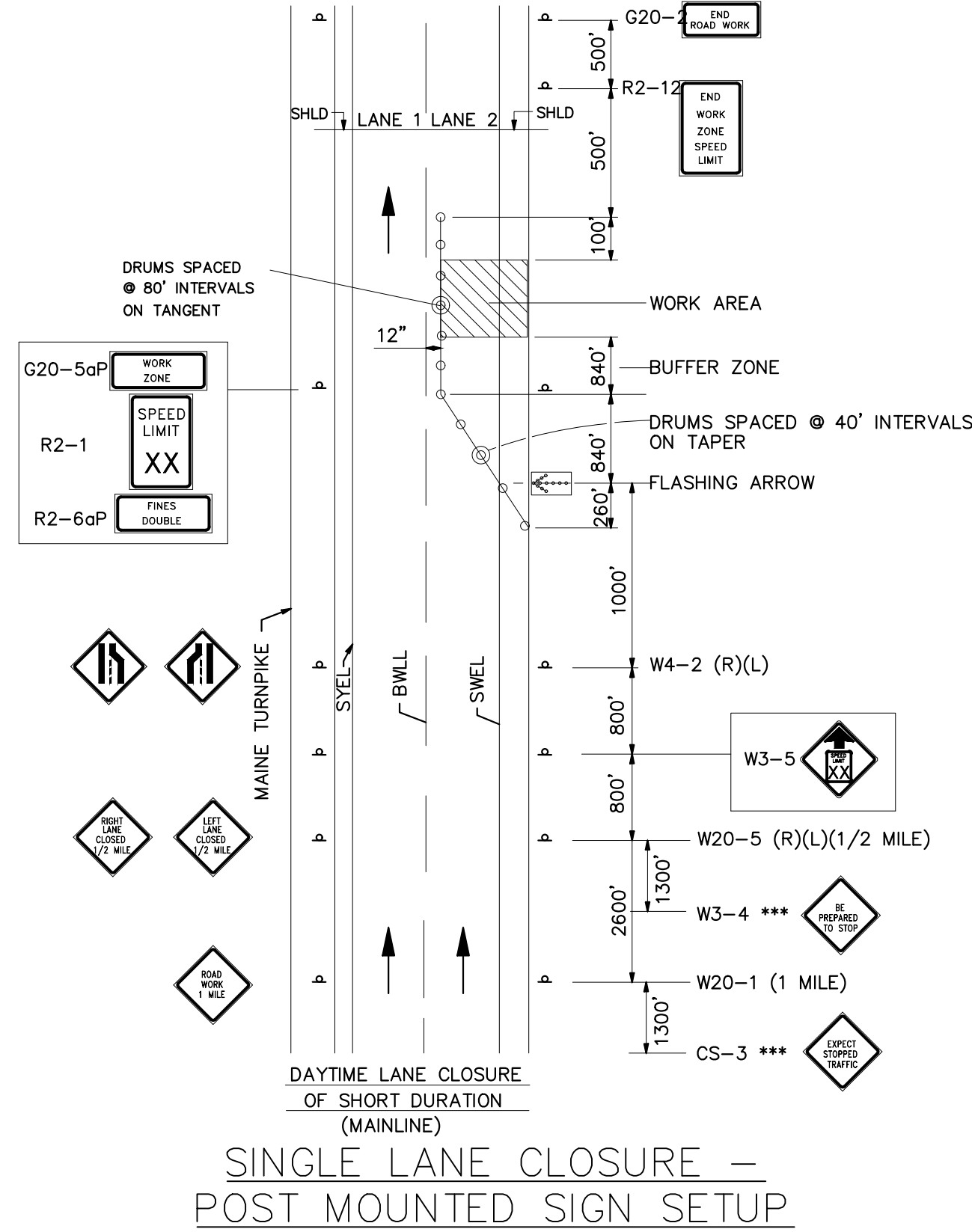
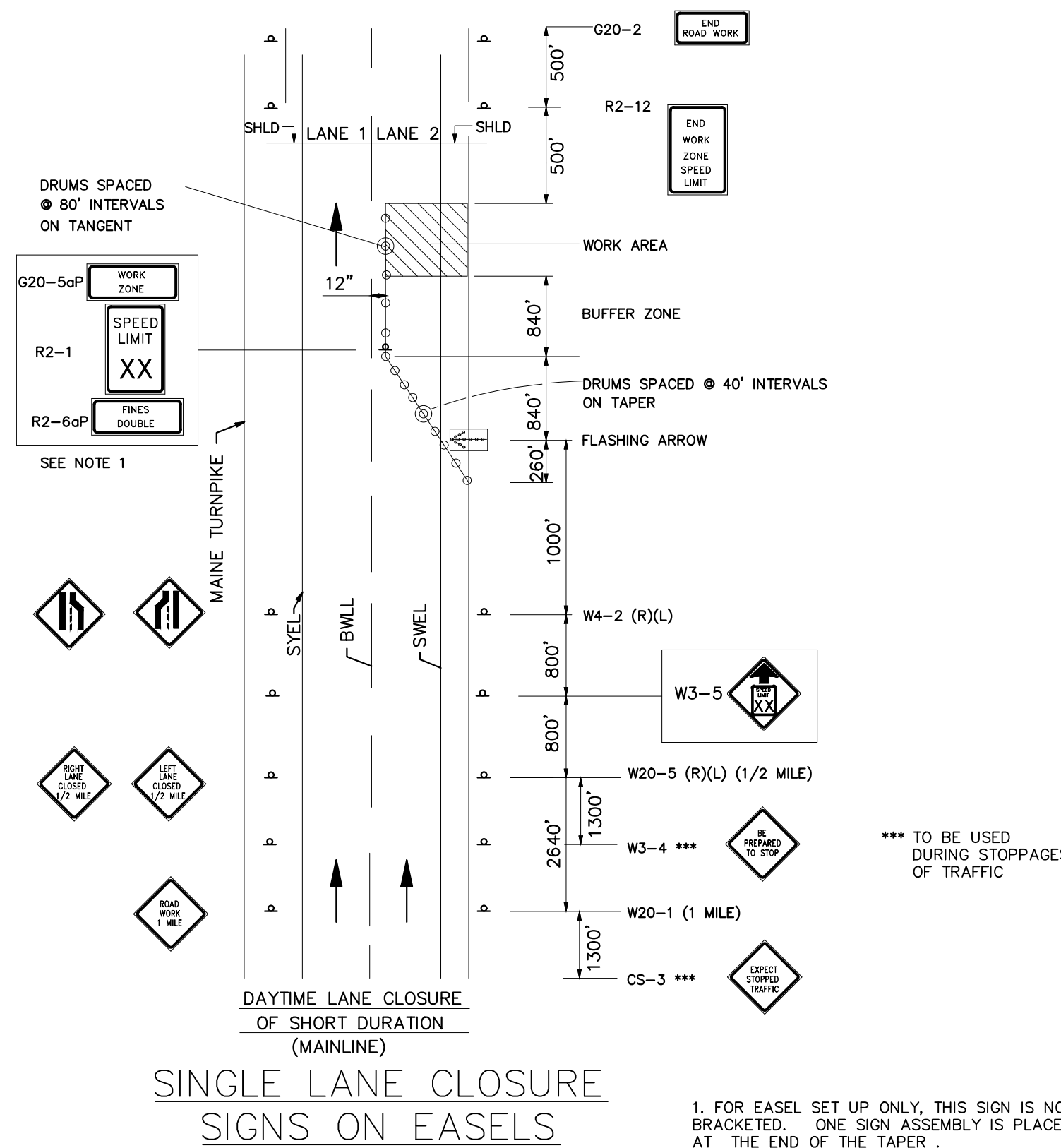
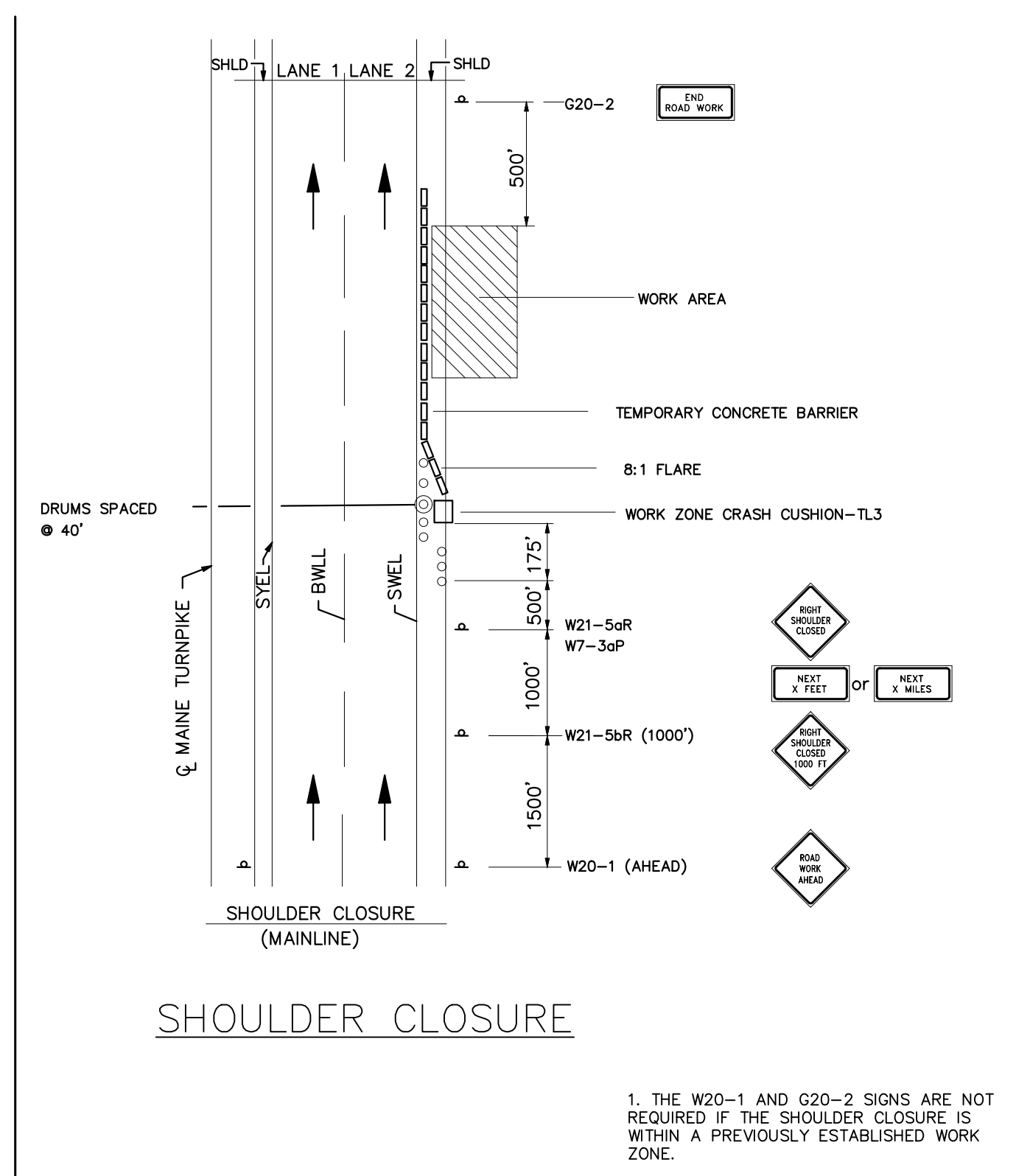
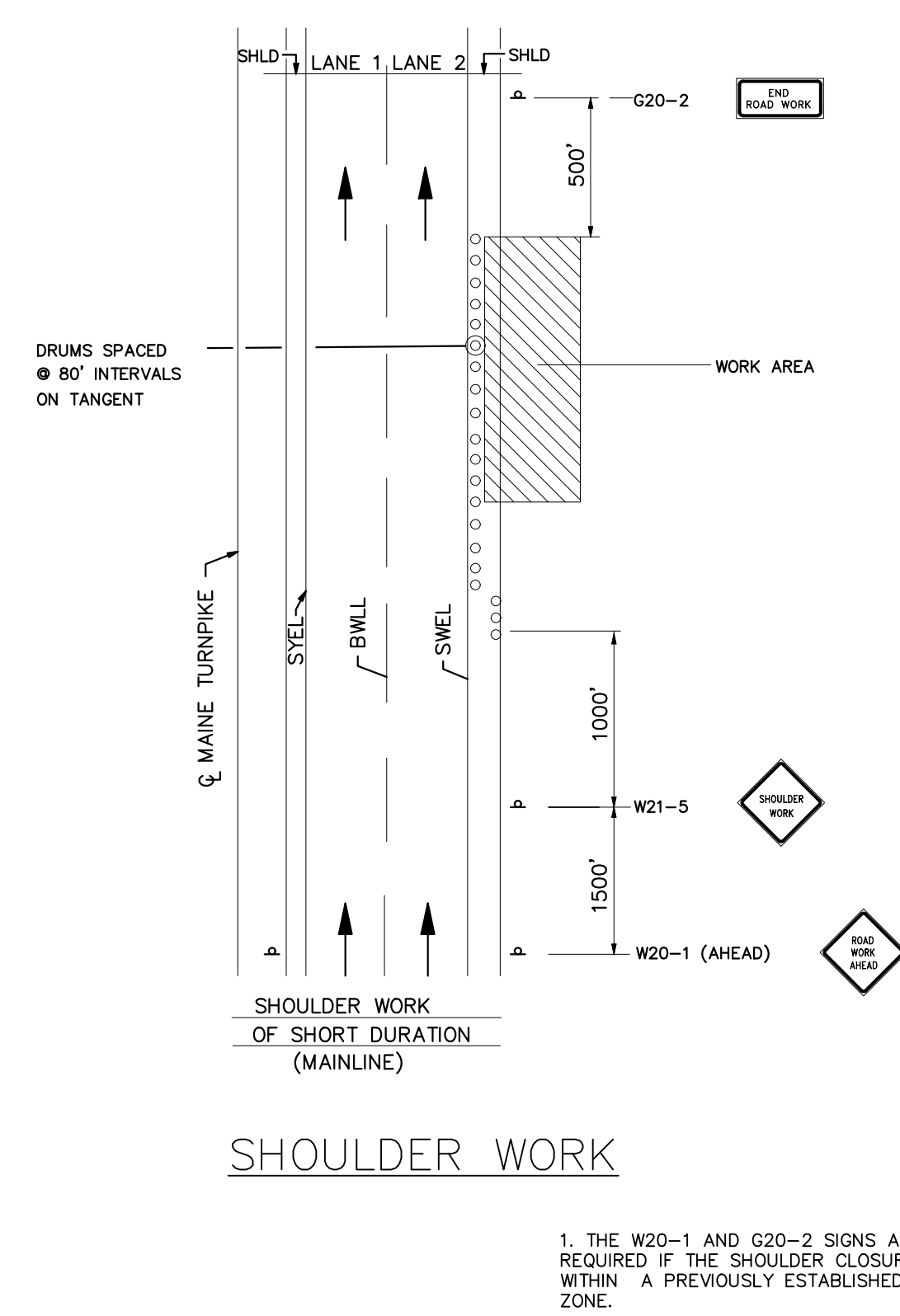
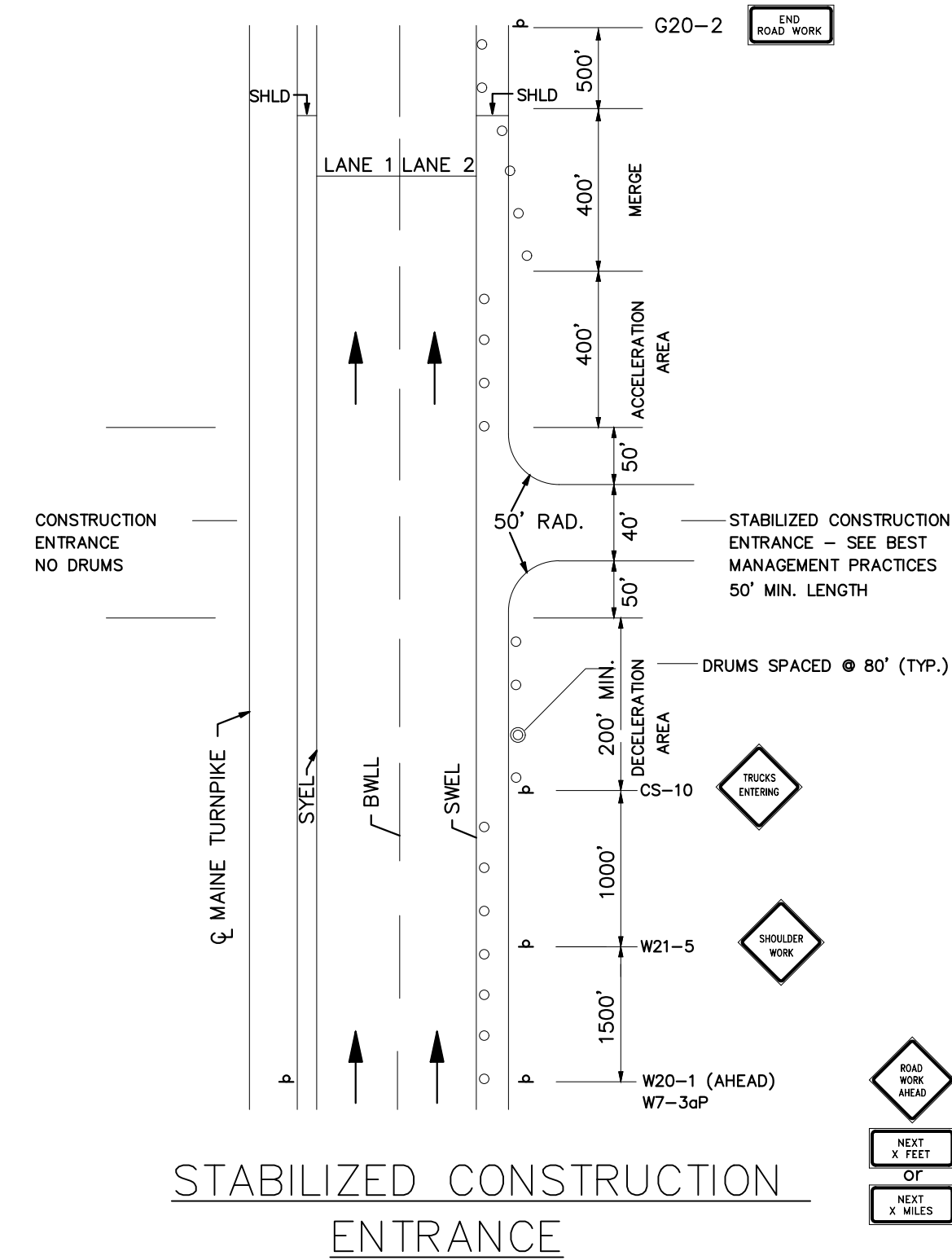
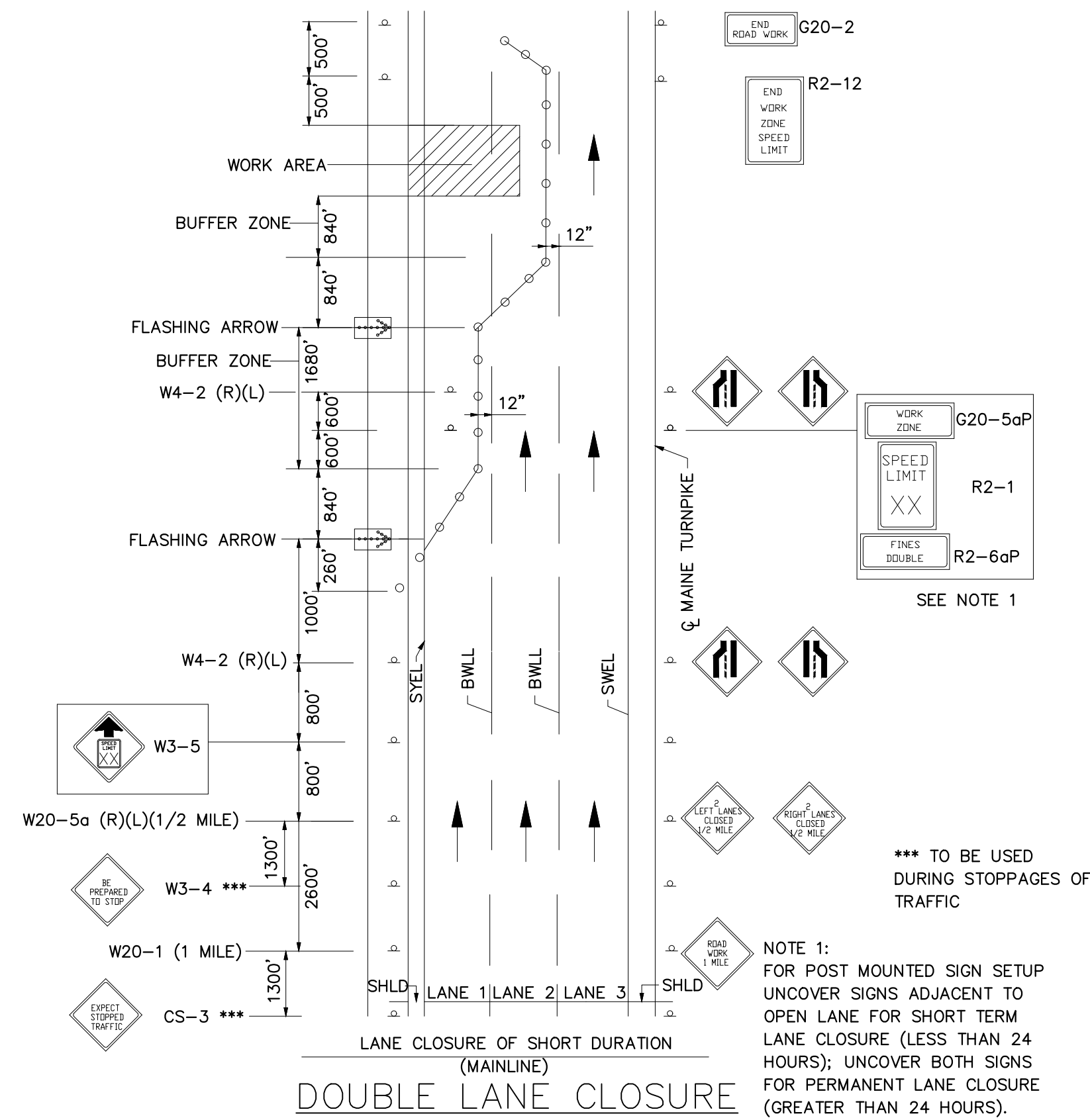
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RYAN BARNES, P.E.

EXISTING GIRDER HAUNCH REMOVAL
CONCRETE REMOVAL DETAILS

SHEET NUMBER: 2 of 13

CONTRACT: 2025.05



GENERAL MAINTENANCE OF TRAFFIC NOTES:

- ALL WORK TO CONFORM TO MAINE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGES 2002, EXCEPT AS MODIFIED BY THE MAINE TURNPIKE AUTHORITY'S GENERAL AND SPECIAL PROVISIONS.
- ALL PAVEMENT STRIPING & SIGNING SHALL BE IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", U.S.D.O.T., F.H.W.A., LATEST EDITION. NOTE THERE IS A 2009 EDITION OF MUTCD.
- THESE PLANS SHOW THE GENERAL CONDITION FOR TURNPIKE MAINLINE TRAFFIC CONTROL DURING CONSTRUCTION, SLIGHT MODIFICATIONS IN CONSTRUCTION PROCEDURE MAY OCCUR AND MAY REQUIRE SOME MINOR ADJUSTMENTS TO BE MADE IN THE FIELD, ALL PROCEDURES MUST BE APPROVED BY THE RESIDENT.
- THE CONTRACTOR SHALL REMOVE ALL PAVEMENT MARKINGS THAT CONFLICT WITH PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH THE SPECIFICATIONS AND MUTCD. PAYMENT SHALL BE MADE UNDER ITEM 627.77 - REMOVING PAVEMENT MARKINGS.
- EXPOSED BARRIER ENDS SHALL BE PROTECTED BY A WORK ZONE CRASH CUSHION. PAYMENT WILL BE UNDER ITEM 527.341 - WORK ZONE CRASH CUSHION - TL-3.
- ALL GEOMETRIC INFORMATION FOR ALL PHASES OF TRAFFIC CONTROL WILL BE SUPPLIED TO THE CONTRACTOR AFTER AWARD.

ABBREVIATIONS FOR ALL M.O.T. PLANS

- BWLL = BROKEN WHITE LANE LINE
- SWEL = SOLID WHITE EDGE LINE
- SYTEL = SOLID YELLOW EDGE LINE
- TBWL = TEMPORARY BROKEN WHITE LANE LINE
- TSWEL = TEMPORARY SOLID WHITE EDGE LINE
- TSYEL = TEMPORARY SOLID YELLOW EDGE LINE

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

MAINE TURNPIKE

	By	Date	By	Date	
Designed	RJB	9/17/24	Checked	KVO	9/17/24
Drawn	BMB	9/17/24	In Charge of	SRT	9/17/24

MAINE TURNPIKE AUTHORITY
2360 Congress Street
Portland, ME 04102
TEL (207) 871-7771
FAX (207) 879-5567

MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RYAN BARNES, P.E.

EXISTING GIRDER HAUNCH REMOVAL
MAINTENANCE OF TRAFFIC DETAILS

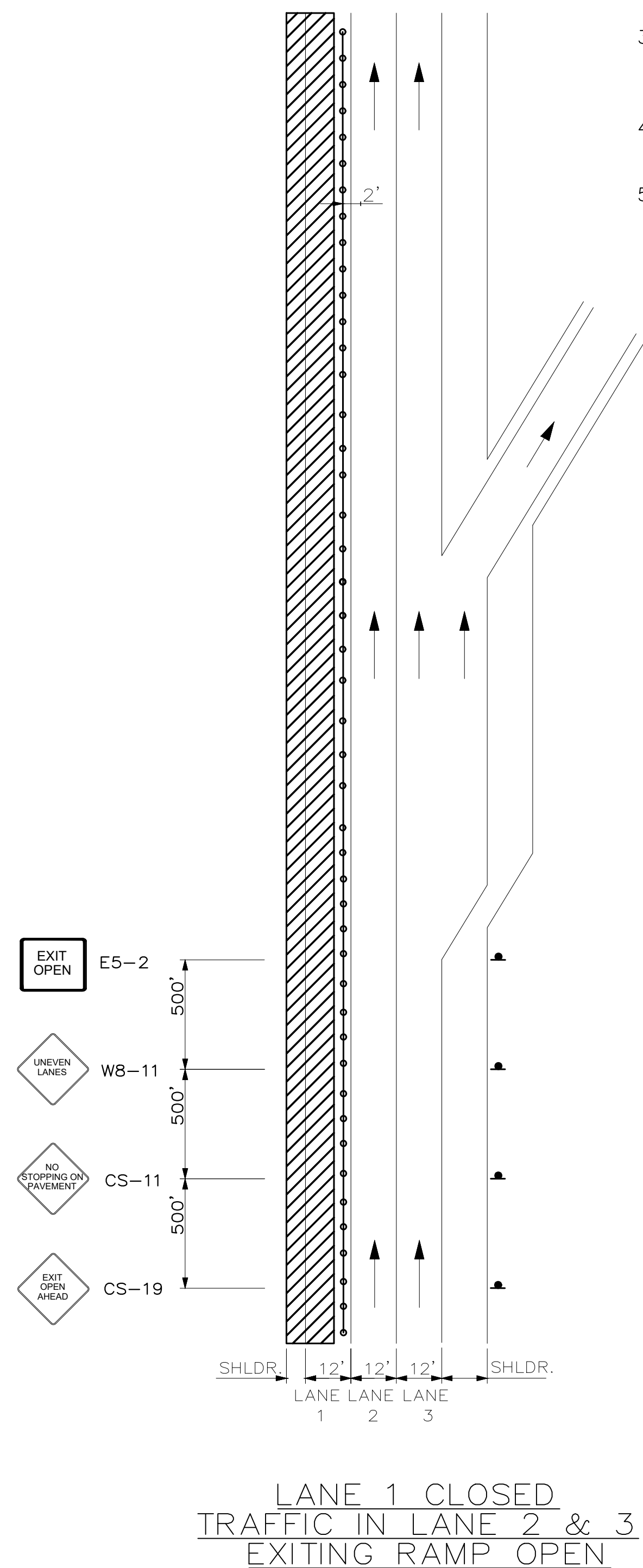
SHEET NUMBER: 3 of 13

CONTRACT: 2025.05

K:\Contracts\2024 Construction Project Files\2024.51 - Haunch Removal - Beech Ridge\4.8 Mountain\10.6 Bennett\68.6\Project Development\60% CAD print date 9.17.2024

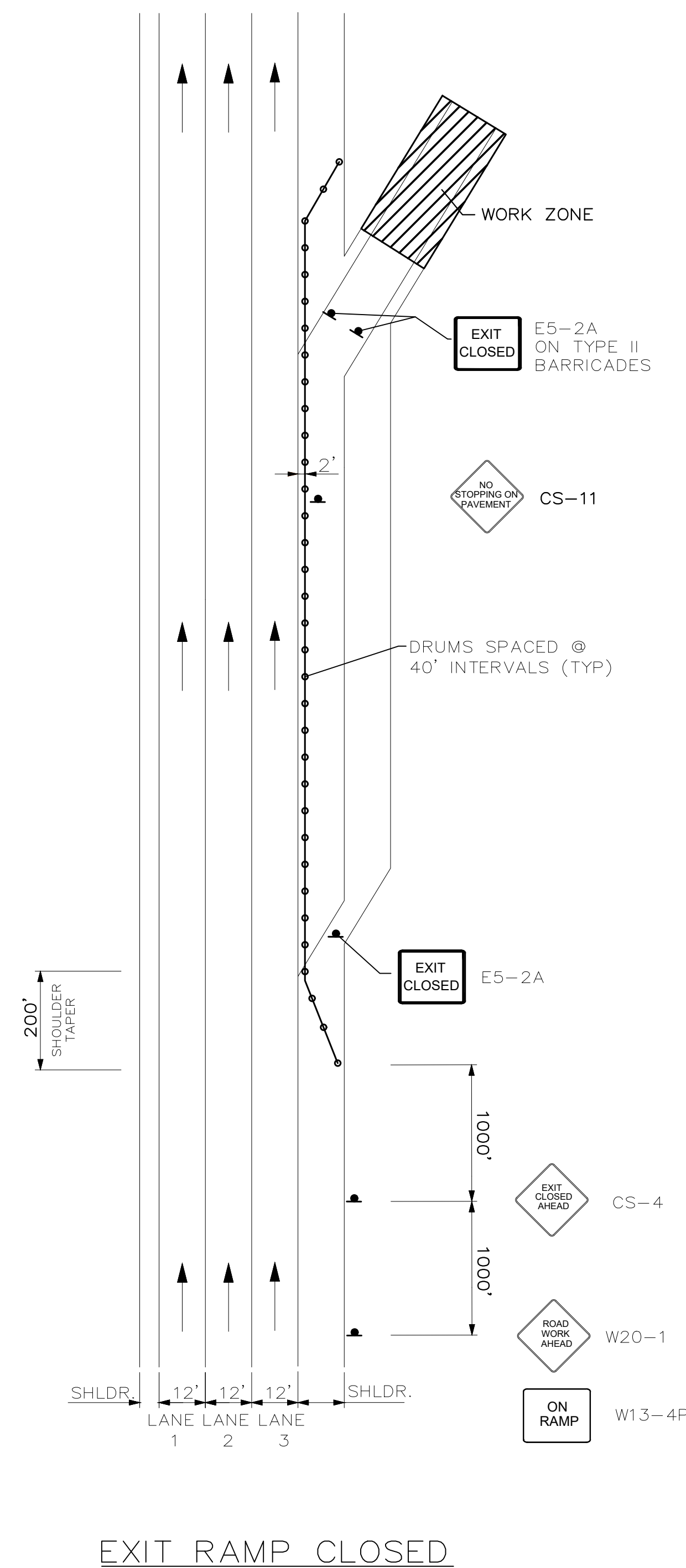
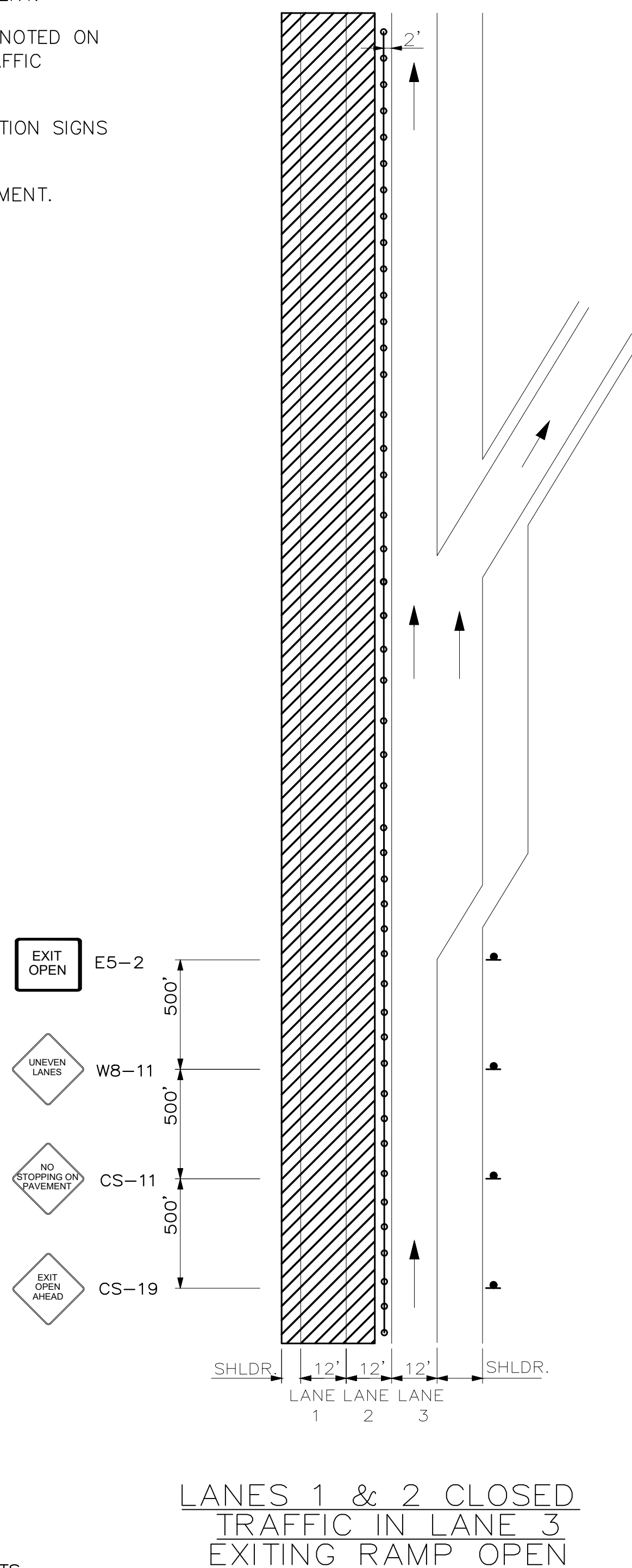
NOTES:

1. FOR SIGN DETAILS, SEE SIGN SUMMARY SHEETS.
2. LANE CLOSURES AT ENTRANCE AND EXIT RAMP MAY BE COMBINED IF APPROVED BY THE RESIDENT.
3. ALL M.O.T. DEVICES AND LOCATIONS NOT NOTED ON THIS SHEET SHALL BE AS NOTED ON "TRAFFIC CONTROL MAINLINE CLOSURE" SHEETS..
4. CONTRACTOR SHALL RELOCATE CONSTRUCTION SIGNS AS DIRECTED BY THE RESIDENT.
5. EXIT SIGNS MUST BE 5' ABOVE THE PAVEMENT.



LEGEND

- = FLASHING ARROW BOARD
- = CONSTRUCTION SIGN
- = PORTABLE - CHANGEABLE MESSAGE SIGN
- = DRUM SPACED AT 80' INTERVALS ON TANGENTS 40' INTERVALS ON TAPERS
- = WORK AREA



PCMS (1 WEEK PRIOR TO CLOSURE)



PCMS (DURING CLOSURE)



NOTES:

1. STANDARD MESSAGES PROVIDED FOR INFORMATION. USE OF MESSAGES SHALL BE DETERMINED PER CLOSURE AND SETUP ON THE APPROPRIATE PCMS'S ACCORDINGLY.
2. EXCEPT FOR EMERGENCIES, RAMP CLOSURES SHALL BE COORDINATED WITH DEPARTMENT DIRECTOR OR ASSIGNED MTA ENGINEER.
3. A PORTABLE CHANGEABLE MESSAGE SIGN (PCMS) SHALL BE SET UP APPROXIMATELY ONE(1) MILE BEFORE THE RAMP TO BE CLOSED.

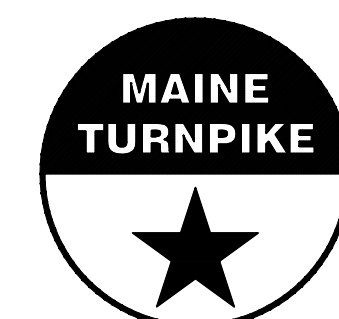
Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

	By	Date		By	Date
Designed	RJB	9/17/24	Checked	KVO	9/17/24
Drawn	BMB	9/17/24	In Charge of	SRT	9/17/24

MAINE TURNPIKE AUTHORITY
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THE GOLD STAR
MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RYAN BARNES, P.E.

EXISTING GIRDER HAUNCH REMOVAL
TRAFFIC CONTROL AT EXIT RAMP
(1 OF 2)

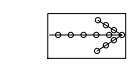




SHEET NUMBER: 4 of 13

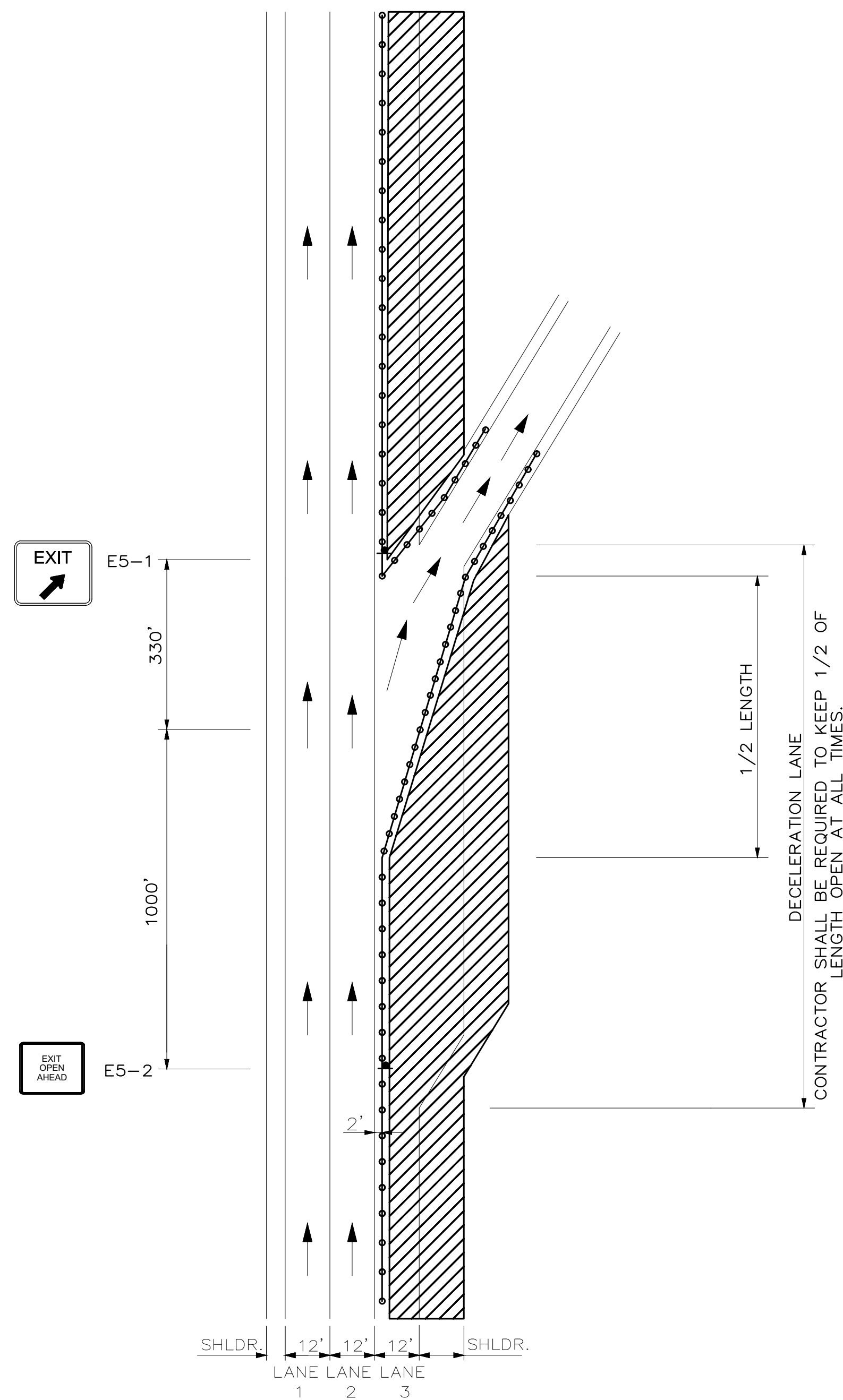
CONTRACT: 2025.05

NOTES:

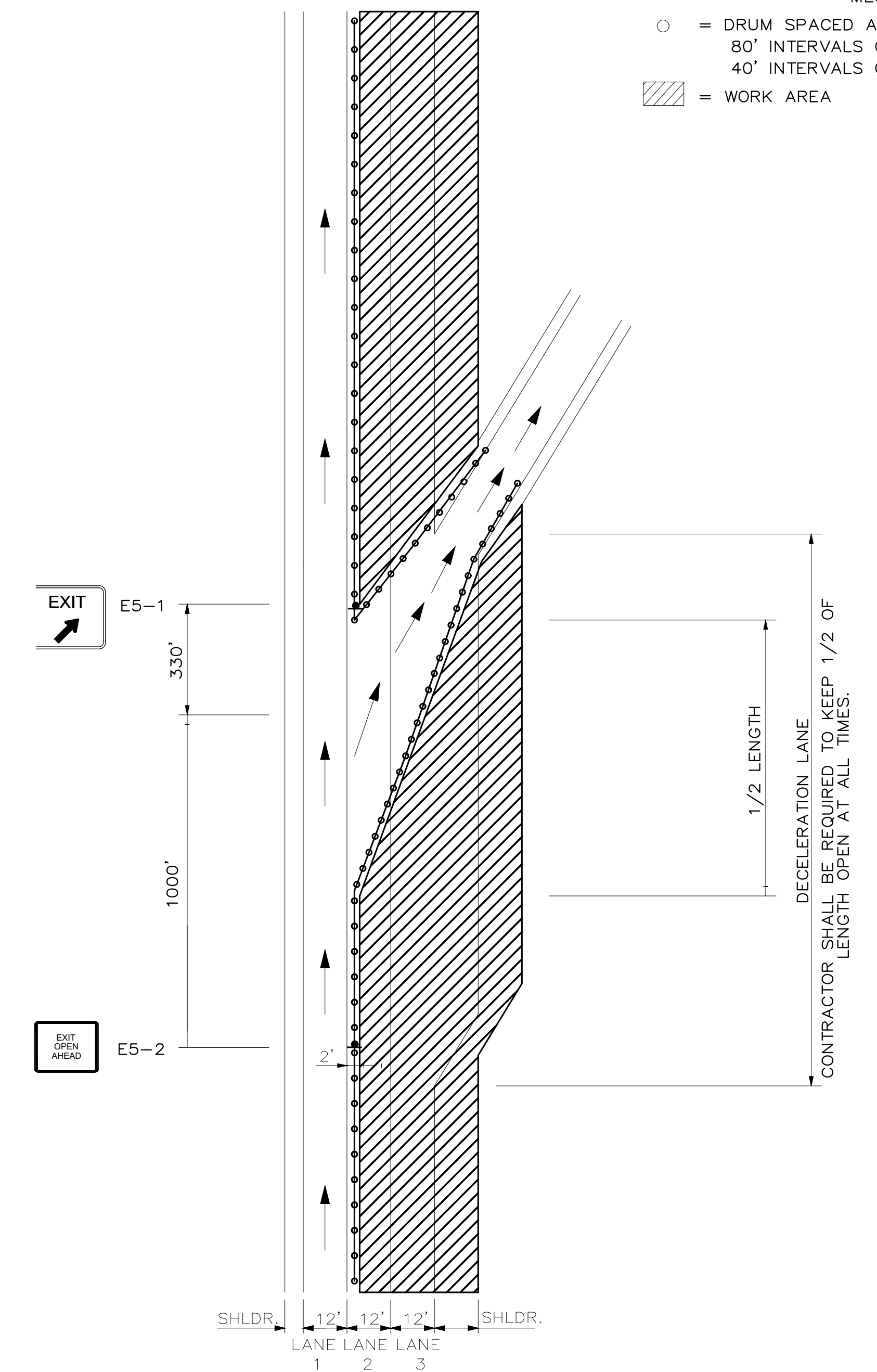
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3. ALL M.O.T. DEVICES AND LOCATIONS NOT NOTED ON THIS SHEET SHALL BE AS NOTED ON "TRAFFIC CONTROL MAINLINE CLOSURE" SHEETS..
4. CONTRACTOR SHALL RELOCATE CONSTRUCTION SIGNS AS DIRECTED BY THE RESIDENT.
5. EXIT SIGNS MUST BE 5' ABOVE THE PAVEMENT.

LEGEND

-  = FLASHING ARROW BOARD
-  = CONSTRUCTION SIGN
-  = PORTABLE - CHANGEABLE MESSAGE SIGN
-  = DRUM SPACED AT 80' INTERVALS ON TANGENTS 40' INTERVALS ON TAPERS
-  = WORK AREA



LANE 3 CLOSED
TRAFFIC IN LANE 1 & 2
EXITING RAMP OPEN




LANE 2 & 3 CLOSED
TRAFFIC IN LANE 1
EXITING RAMP OPEN

Scale: NOT TO SCALE


No.	Revision	By	Date

Designed by:



	By	Date	By	Date
Designed	RJB	9/17/24	Checked	KVO 9/17/24
Drawn	BMB	9/17/24	In Charge of	SRT 9/17/24

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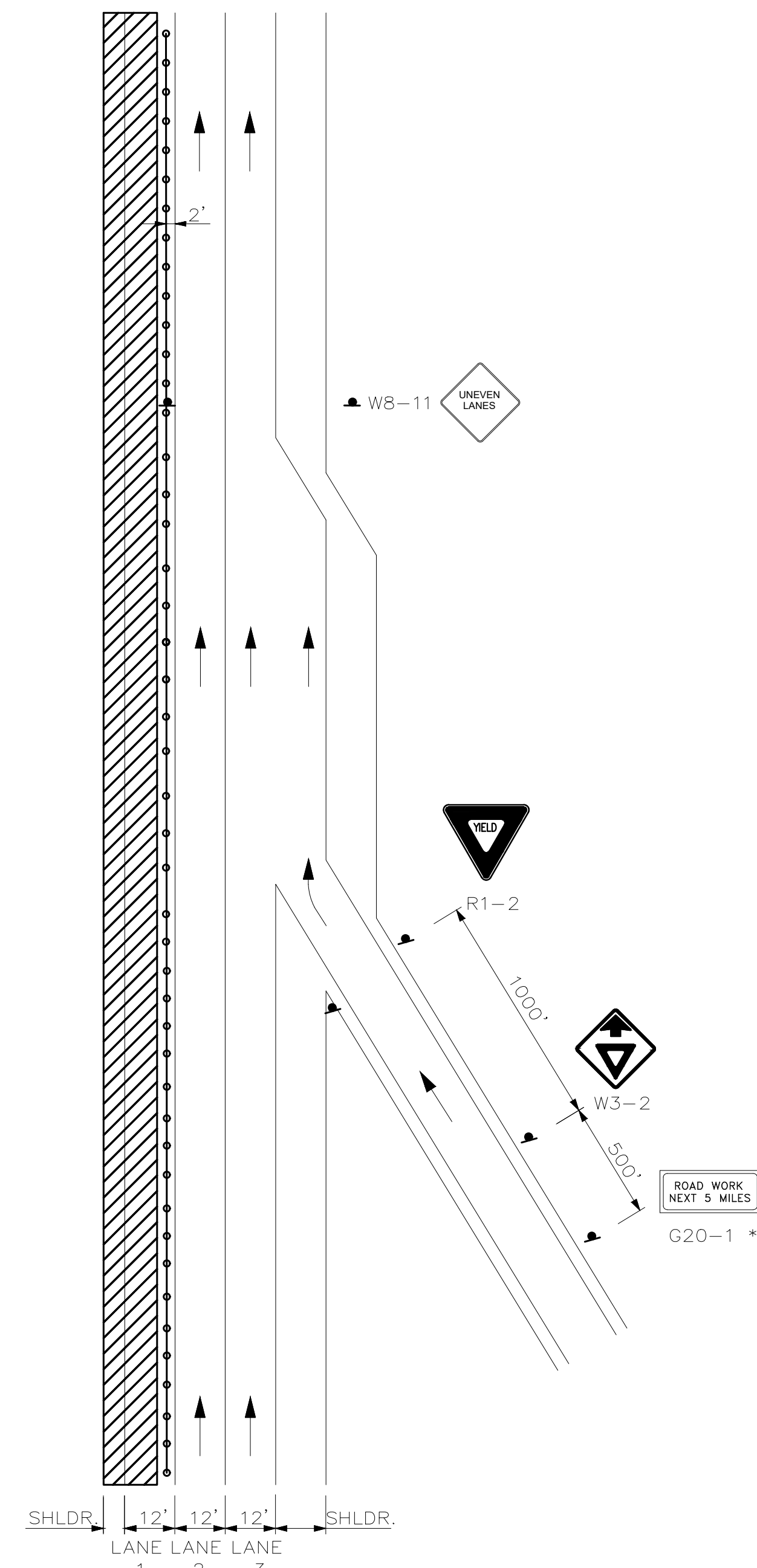
MTA PROJECT MANAGER: RYAN BARNES, P.E.

EXISTING GIRDER HAUNCH REMOVAL
TRAFFIC CONTROL AT EXIT RAMP
(2 OF 2)

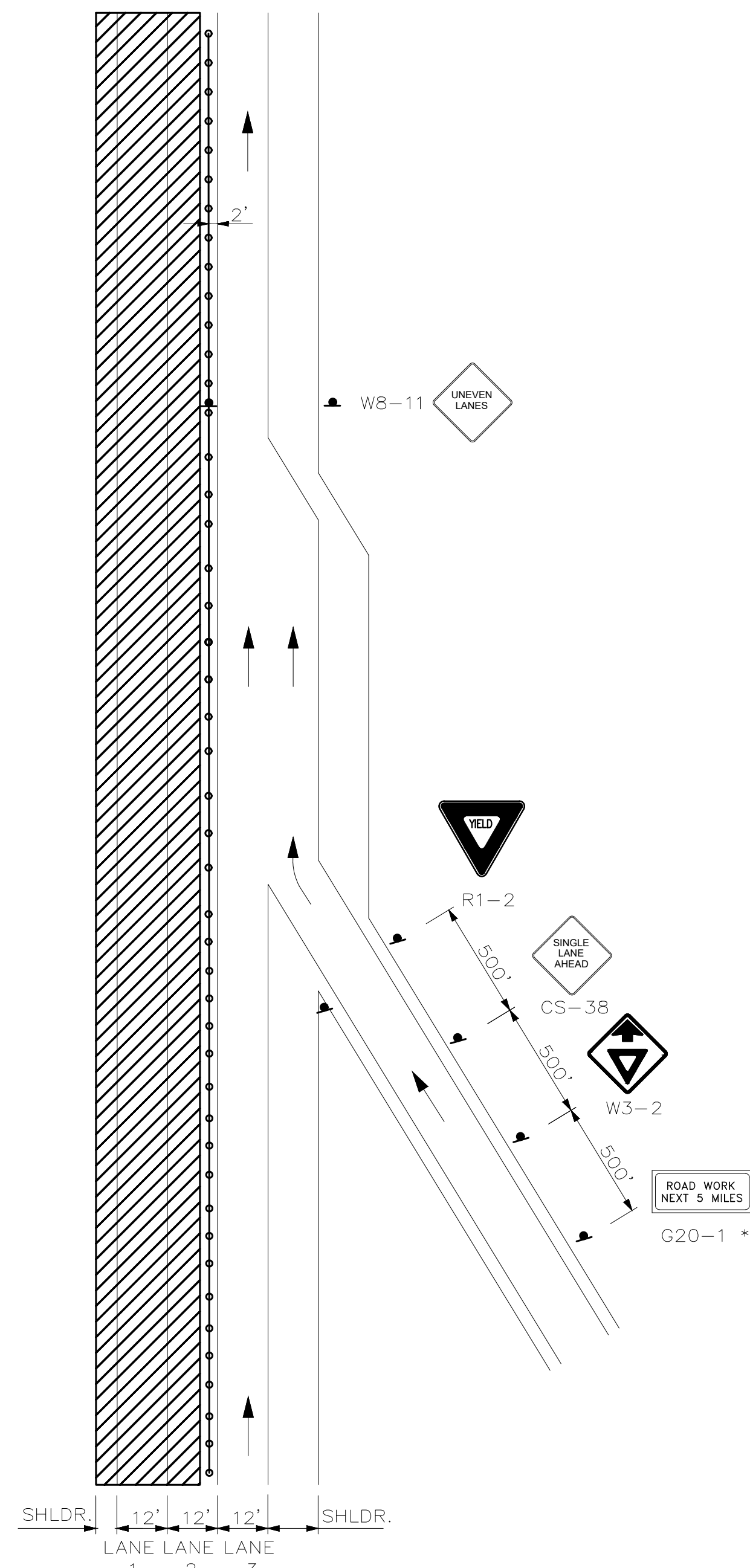
SHEET NUMBER: 5 of 13

CONTRACT: 2025.05

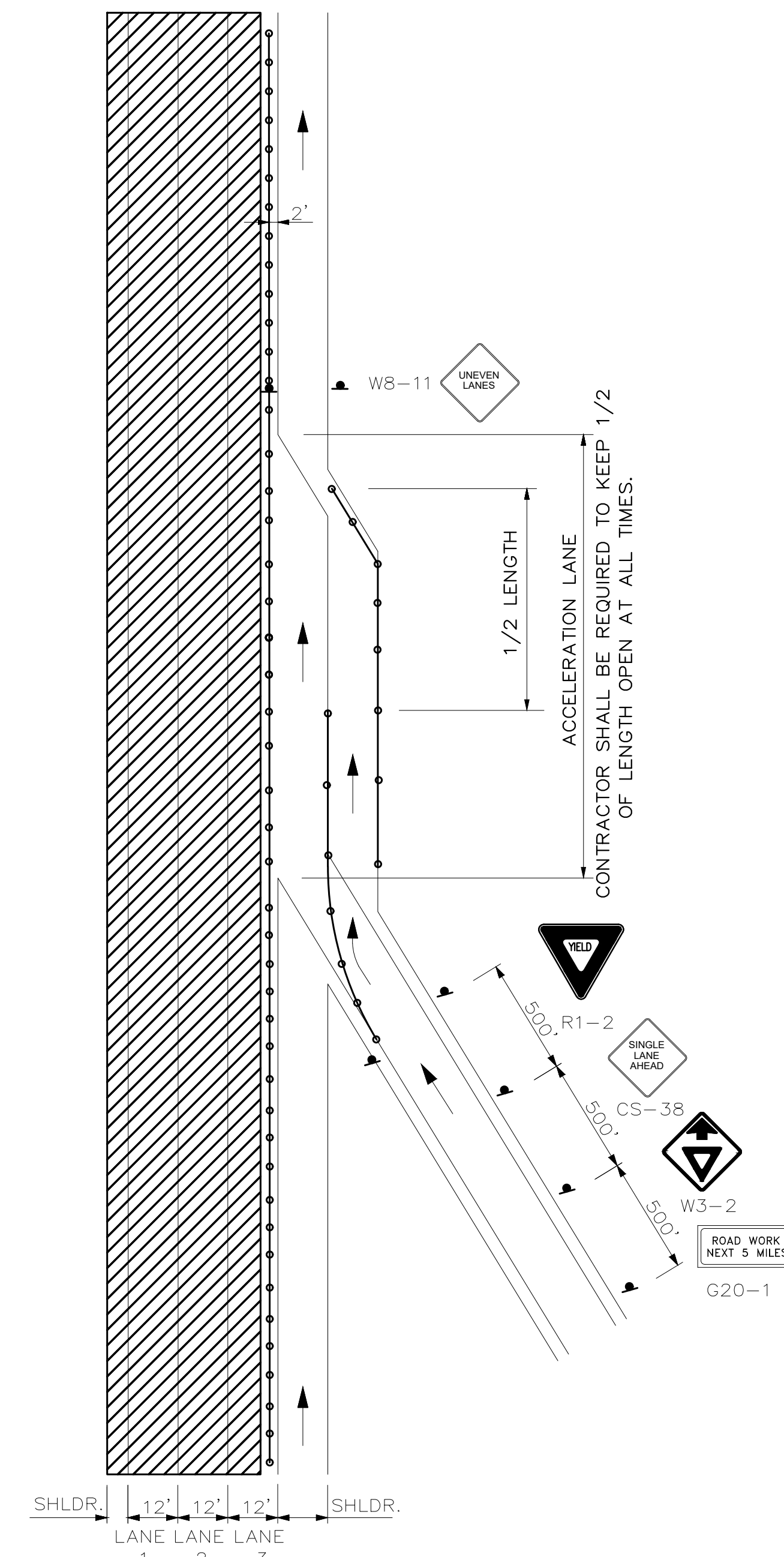
* POST MOUNTED



LANE 1 CLOSED
TRAFFIC IN LANE 2 & 3
ENTERING RAMP OPEN



LANE 1 & 2 CLOSED
TRAFFIC IN LANE 3
ENTERING RAMP OPEN



LANE 1, 2, & 3 CLOSED
TRAFFIC IN SHOULDER
ENTERING RAMP OPEN

NOTES:

1. LANE CLOSURES AT ENTRANCE AND EXIT RAMPs AY BE COMBINED IF APPROVED BY THE RESIDENT.
2. ALL M.O.T. DEVICES AND LOCATIONS NOT NOTED ON THIS SHEET SHALL BE AS NOTED ON "TRAFFIC CONTROL MAINLINE CLOSURE" SHEETS M-1 THROUGH M-3.
3. CONTRACTOR SHALL RELOCATE CONSTRUCTION SIGNS AA DIRECTED BY THE RESIDENT.
4. ALL STOP AND YIELD SIGNS SHALL BE MOUNTED 5' ABOVE THE PAVEMENT.

LEGEND

- = FLASHING ARROW BOARD
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- = PORTABLE - CHANGEABLE MESSAGE SIGN
- = DRUM SPACED AT
80' INTERVALS ON TANGENTS
40' INTERVALS ON TAPERS
- = WORK AREA

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No.	Revision	By	Date

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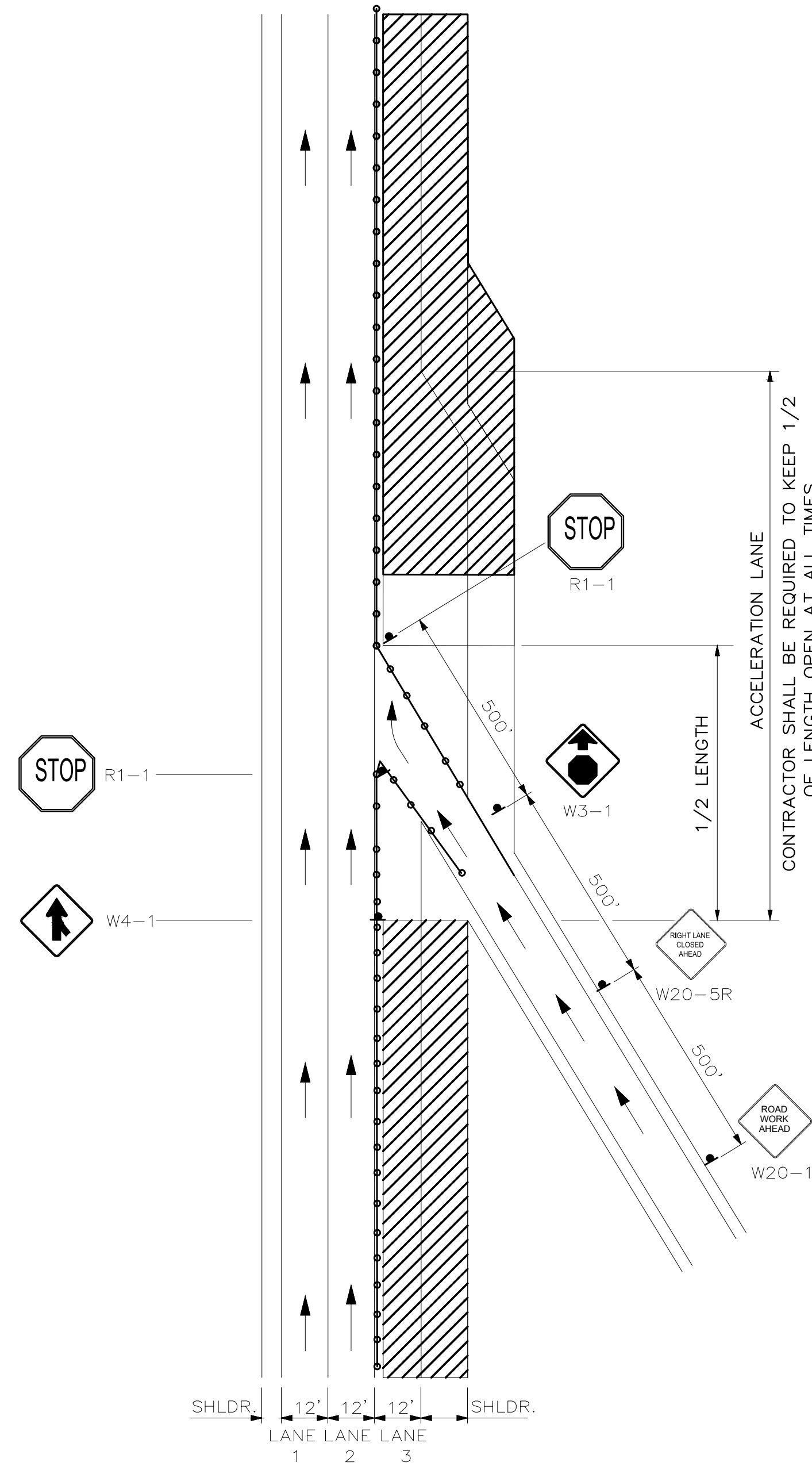
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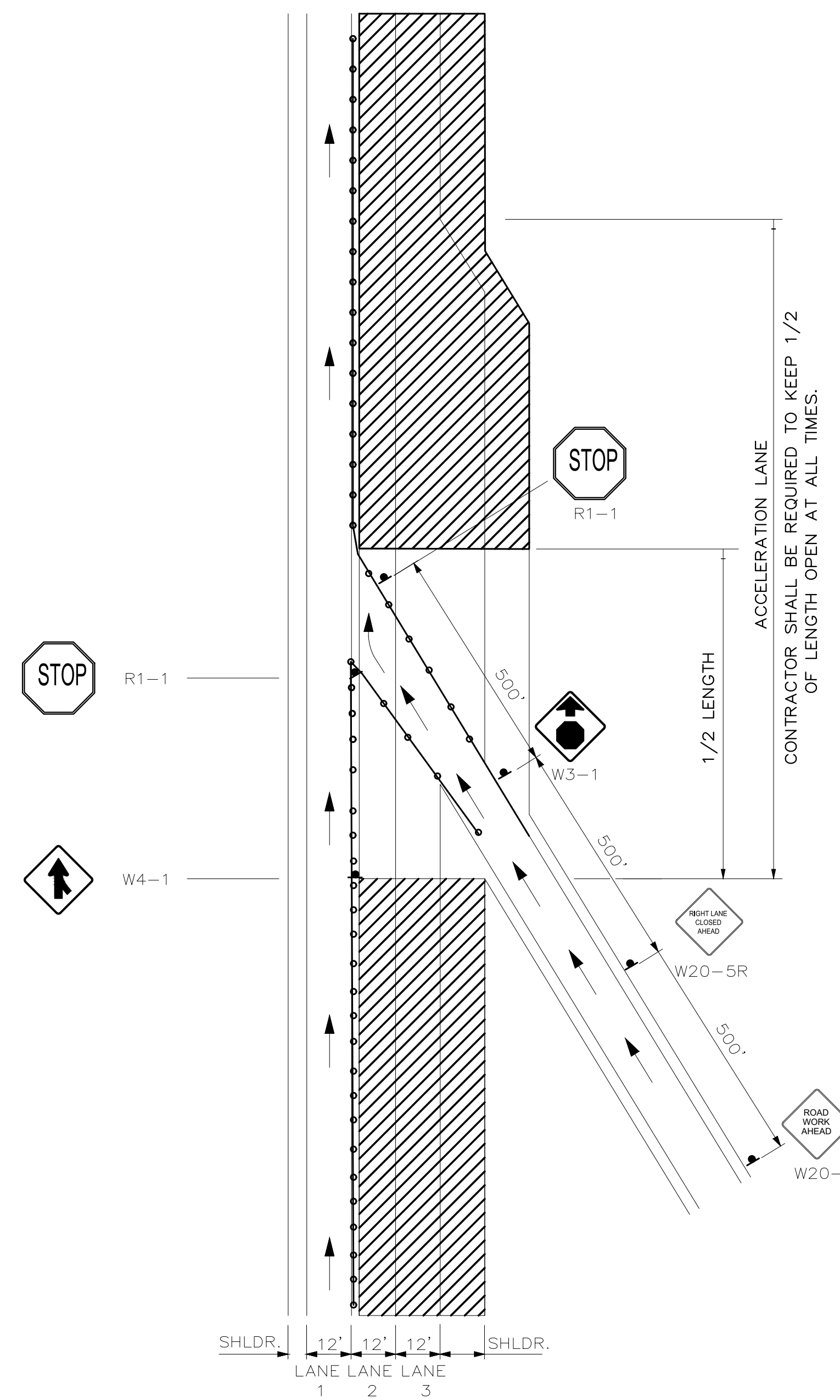
MTA PROJECT MANAGER: RYAN BARNES, P.E.

EXISTING GIRDER HAUNCH REMOVAL
TRAFFIC CONTROL AT ENTRANCE RAMPS
(1 OF 2)

SHEET NUMBER: 6 of 13
CONTRACT: 2025.05



LANE 3 CLOSED
TRAFFIC IN LANE 1 & 2
ENTERING RAMP OPEN



LANE 2 & 3 CLOSED
TRAFFIC IN LANE 1
ENTERING RAMP OPEN

NOTES:

1. CONTRACTOR SHALL USE YIELD CONTROL UNLESS APPROVED BY THE AUTHORITY. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN AN ACCELERATION LANE IF STOP CONTROL IS APPROVED.
2. LANE CLOSURES AT ENTRANCE AND EXIT RAMP MAY BE COMBINED IF APPROVED BY THE RESIDENT.
3. ALL M.O.T. DEVICES AND LOCATIONS NOT NOTED ON THIS SHEET SHALL BE AS NOTED ON "TRAFFIC CONTROL MAINLINE CLOSURE" SHEETS M-1 THROUGH M-3.
4. CONTRACTOR SHALL RELOCATE CONSTRUCTION SIGNS AA DIRECTED BY THE RESIDENT.
5. ALL STOP AND YIELD SIGNS SHALL BE MOUNTED 5' ABOVE THE PAVEMENT.

LEGEND

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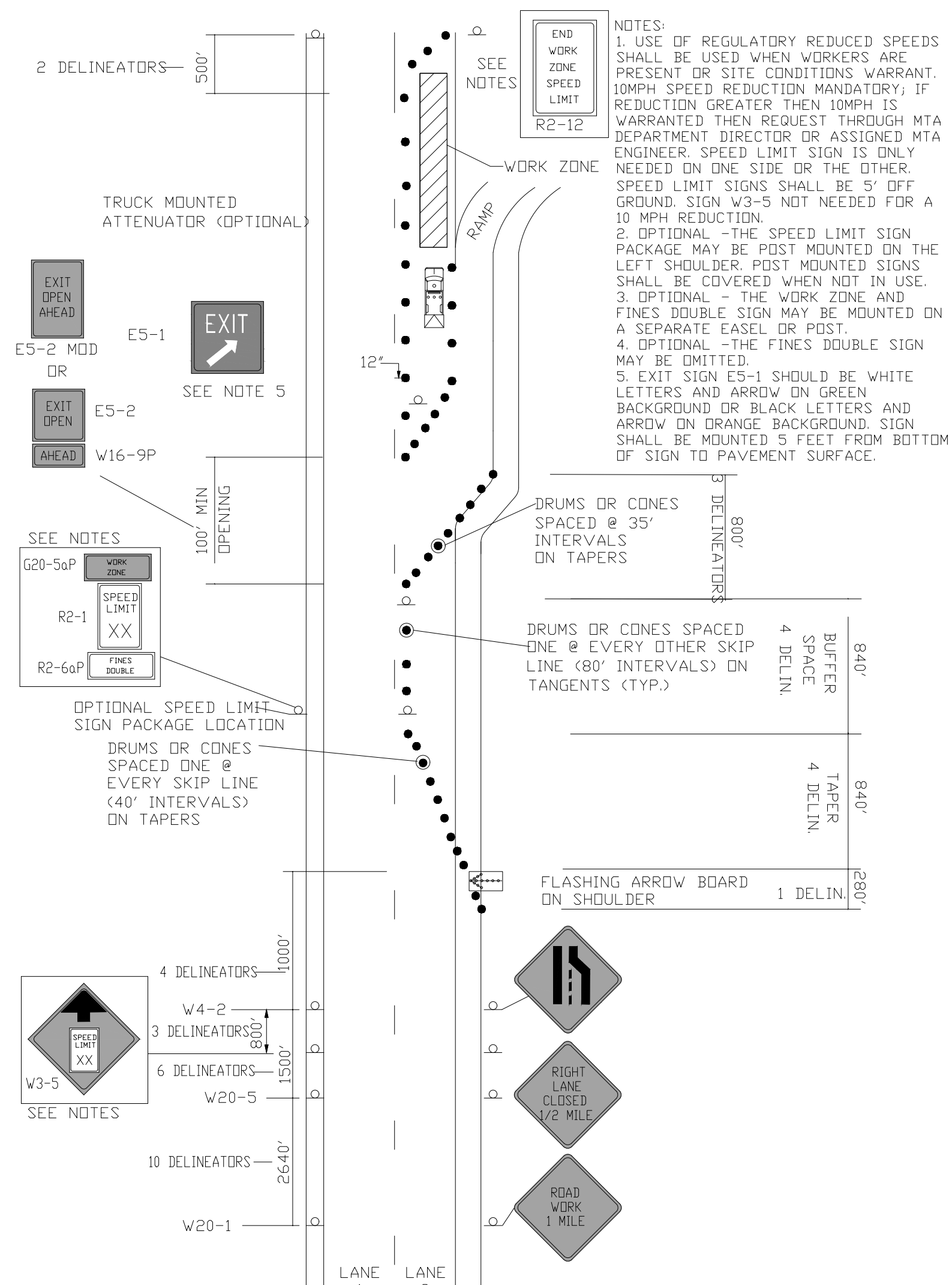
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RYAN BARNES, P.E.

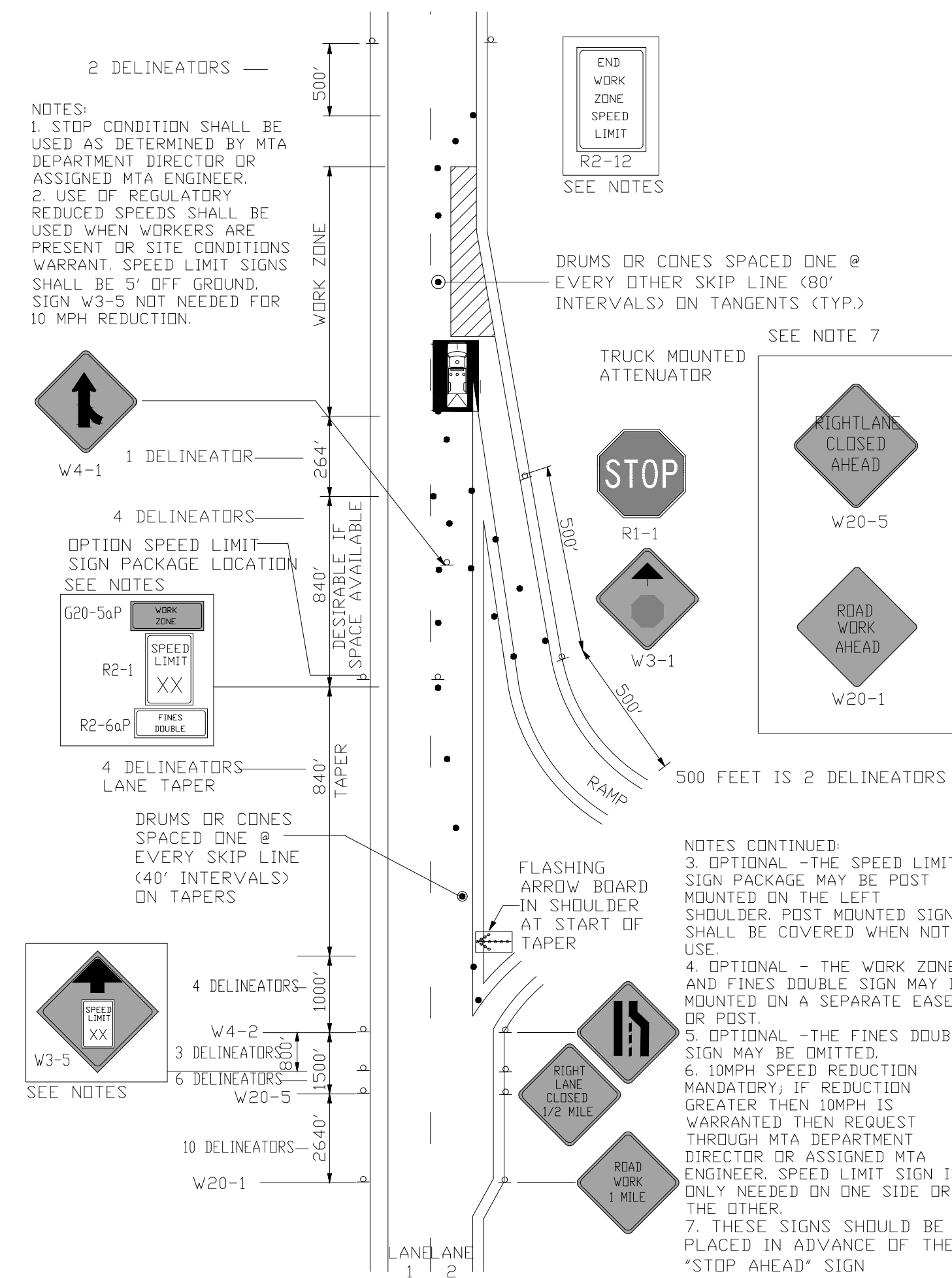
EXISTING GIRDER HAUNCH REMOVAL
TRAFFIC CONTROL AT ENTRANCE RAMPS
(2 OF 2)

SHEET NUMBER: 7 of 13

CONTRACT: 2025.05



DETAIL 42B
TRAVEL LANE CLOSURE AT AN OFF RAMP



DETAIL 44B
TRAVEL LANE CLOSURE AT ENTRANCE RAMP
WITHOUT ACCELERATION LANE

NOTES:
1. USE OF REGULATORY REDUCED SPEEDS SHALL BE USED WHEN WORKERS ARE PRESENT OR SITE CONDITIONS WARRANT. 10MPH SPEED REDUCTION MANDATORY; IF REDUCTION GREATER THEN 10MPH IS WARRANTED THEN REQUEST THROUGH MTA DEPARTMENT DIRECTOR OR ASSIGNED MTA ENGINEER. SPEED LIMIT SIGN IS ONLY NEEDED ON ONE SIDE OR THE OTHER. SPEED LIMIT SIGNS SHALL BE 5' OFF GROUND. SIGN W3-5 NOT NEEDED FOR A 10 MPH REDUCTION.
2. OPTIONAL -THE SPEED LIMIT SIGN PACKAGE MAY BE POST MOUNTED ON THE LEFT SHOULDER. POST MOUNTED SIGNS SHALL BE COVERED WHEN NOT IN USE.
3. OPTIONAL - THE WORK ZONE AND FINES DOUBLE SIGN MAY BE MOUNTED ON A SEPARATE EASEL OR POST.
4. OPTIONAL -THE FINES DOUBLE SIGN MAY BE OMITTED.
5. EXIT SIGN E5-1 SHOULD BE WHITE LETTERS AND ARROW ON GREEN BACKGROUND OR BLACK LETTERS AND ARROW ON ORANGE BACKGROUND. SIGN SHALL BE MOUNTED 5 FEET FROM BOTTOM OF SIGN TO PAVEMENT SURFACE.

NOTES:
1. STOP CONDITION SHALL BE USED AS DETERMINED BY MTA DEPARTMENT DIRECTOR OR ASSIGNED MTA ENGINEER.
2. USE OF REGULATORY REDUCED SPEEDS SHALL BE USED WHEN WORKERS ARE PRESENT OR SITE CONDITIONS WARRANT. SPEED LIMIT SIGNS SHALL BE 5' OFF GROUND. SIGN W3-5 NOT NEEDED FOR 10 MPH REDUCTION.

NOTES CONTINUED:
3. OPTIONAL -THE SPEED LIMIT SIGN PACKAGE MAY BE POST MOUNTED ON THE LEFT SHOULDER. POST MOUNTED SIGNS SHALL BE COVERED WHEN NOT IN USE.
4. OPTIONAL - THE WORK ZONE AND FINES DOUBLE SIGN MAY BE MOUNTED ON A SEPARATE EASEL OR POST.
5. OPTIONAL -THE FINES DOUBLE SIGN MAY BE OMITTED.
6. 10MPH SPEED REDUCTION MANDATORY; IF REDUCTION GREATER THEN 10MPH IS WARRANTED THEN REQUEST THROUGH MTA DEPARTMENT DIRECTOR OR ASSIGNED MTA ENGINEER. SPEED LIMIT SIGN IS ONLY NEEDED ON ONE SIDE OR THE OTHER.
7. THESE SIGNS SHOULD BE PLACED IN ADVANCE OF THE 'STOP AHEAD' SIGN

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

	By	Date	By	Date
Designed	RJB	9/17/24	Checked	KVO 9/17/24
Drawn	BMB	9/17/24	In Charge of	SRT 9/17/24

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RYAN BARNES, P.E.

EXISTING GIRDER HAUNCH REMOVAL
TRAFFIC CONTROL AT RAMPS

SHEET NUMBER: 8 of 13

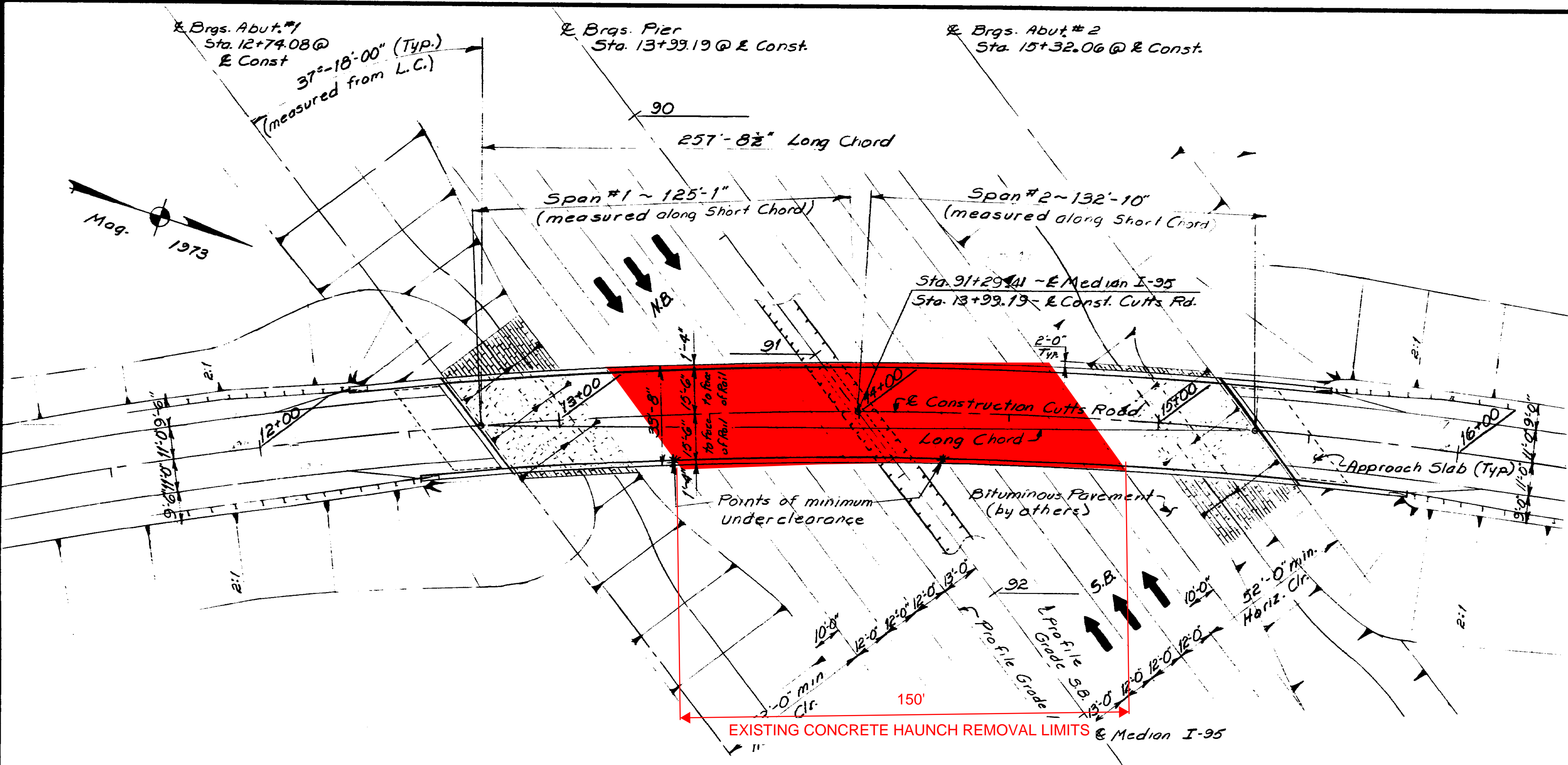
CONTRACT: 2025.05

INDEX TO STRUCTURAL STEEL PLANS CUTTS ROAD

Description	Sheet Number
General Plan	1
Profile	2
Framing Plan	3
Lateral Bracing and Splice Details	4
Haunch Detail and Camber Diagram	5
Blocking	6
Sign Support Details	7

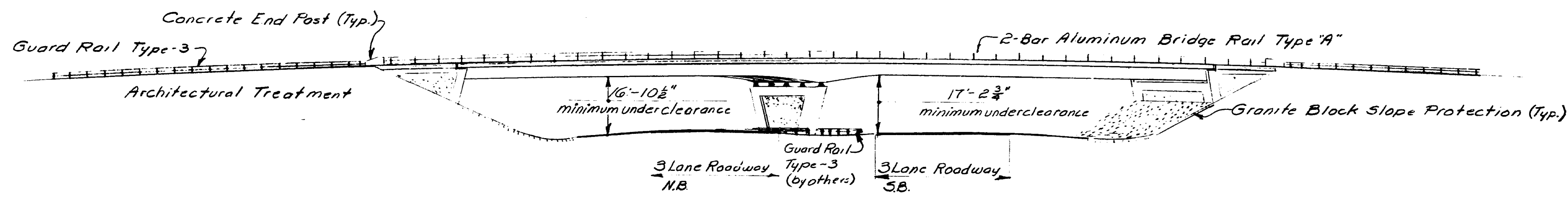
ESTIMATED QUANTITIES

Item No.	Description	Quantity	Unit
504.7002	Structural Steel, Fabrication and Delivered		

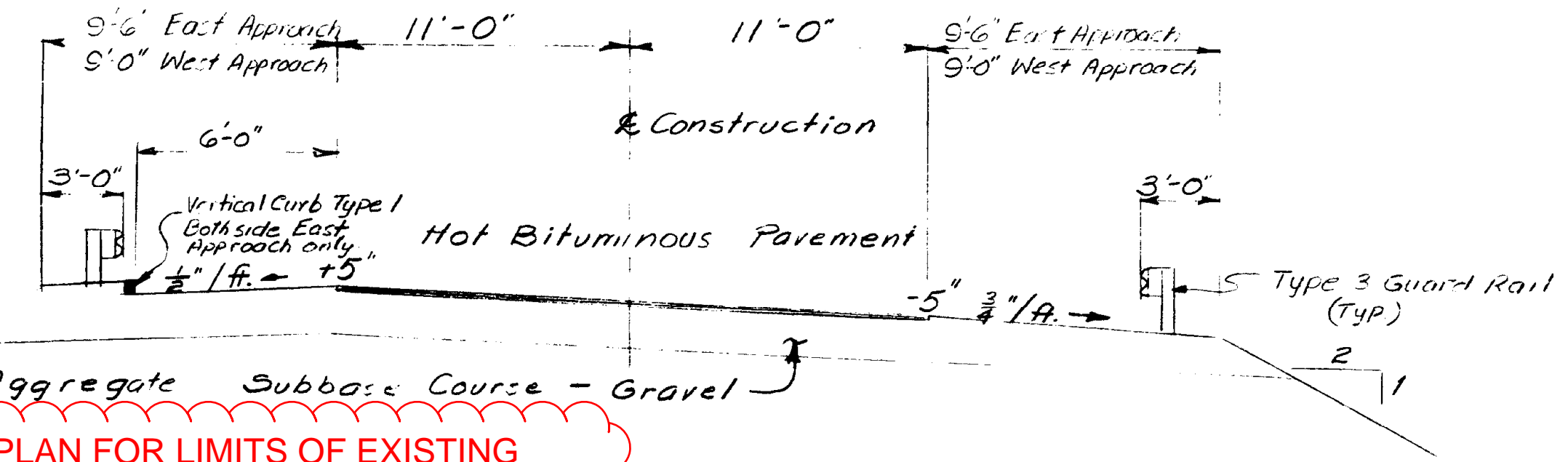


PLAN
Scale in Feet

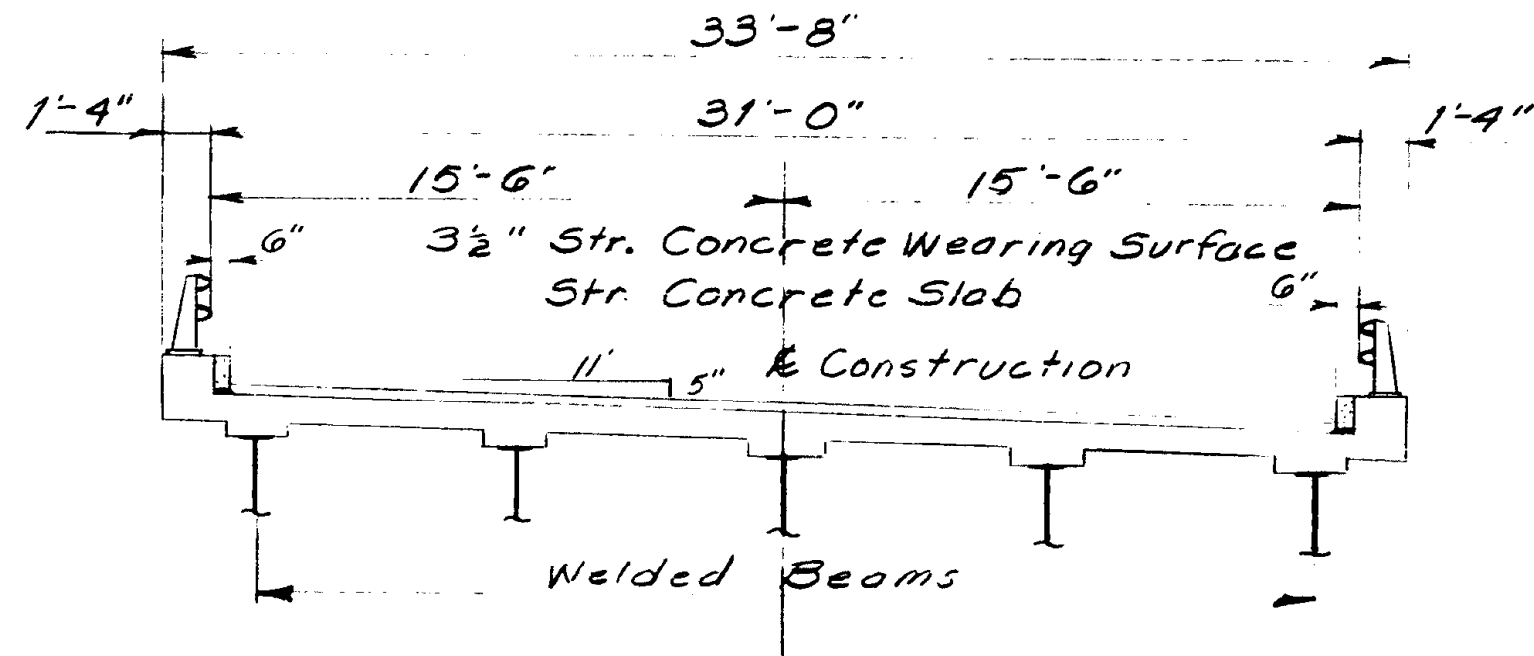
NOTE: The contract for Cutts Road is only for Item 504.7002 Structural Steel, Fabrication and Delivered. The construction of the structure is to be by others.



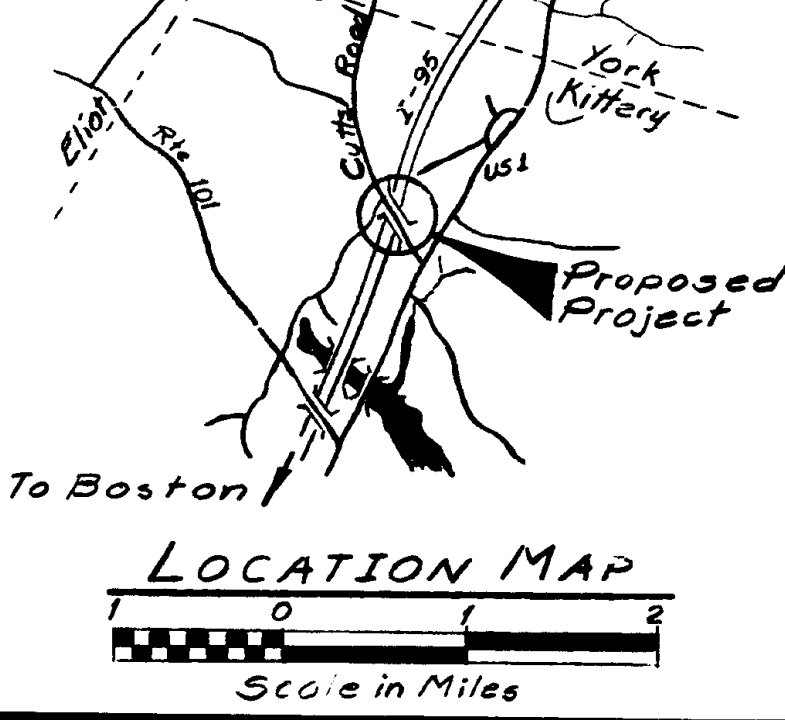
ELEVATION
Scale in Feet



APPROACH SECTION



TYPICAL BRIDGE SECTION

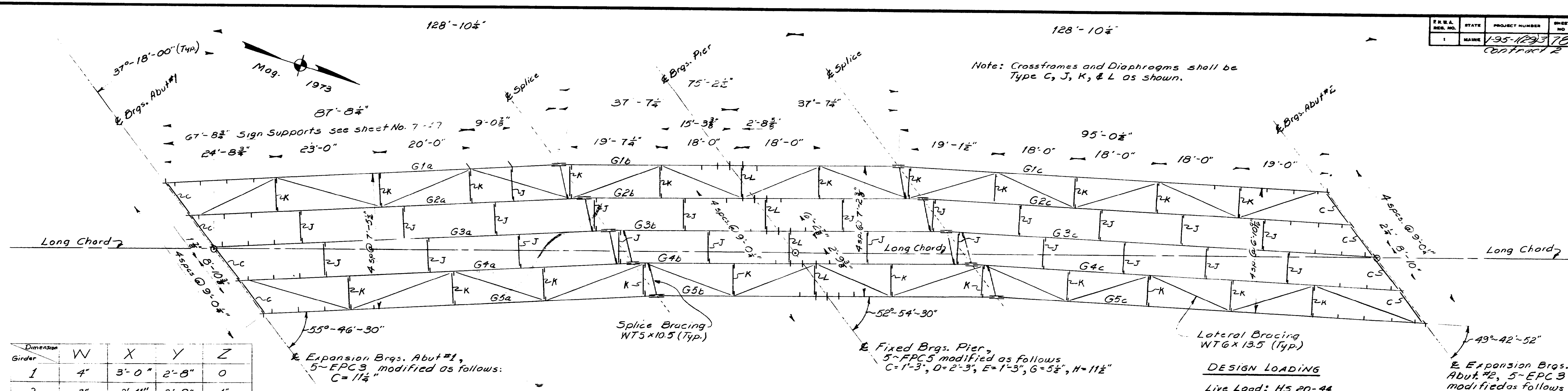


STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

**CUTTS ROAD
OVER
INTERSTATE 95
IN THE TOWN OF
KITTERY
YORK COUNTY
GENERAL PLAN**

REFERENCE PLAN FOR LIMITS OF EXISTING HAUNCH REMOVAL. NO ADDITIONAL BRIDGE WORK SHOWN ON THESE PLANS REQUIRED. PLANS NOT TO SCALE.

PROJECT DESIGN ENGINEER	DATE
DESIGN - DETAILED	9-77
CHECKED	11-77
REVISIONS	
FIELD CHANGES	



Dimension	W	X	Y	Z
Girder 1	4'	3'-0"	2'-8"	0
Girder 2	3'	2'-11"	2'-9"	1"
Girder 3	2'	2'-10"	2'-10"	2"
Girder 4	1'	2'-9"	2'-11"	3"
Girder 5	0	2'-8"	3'-0"	4"

Stud Layout Table

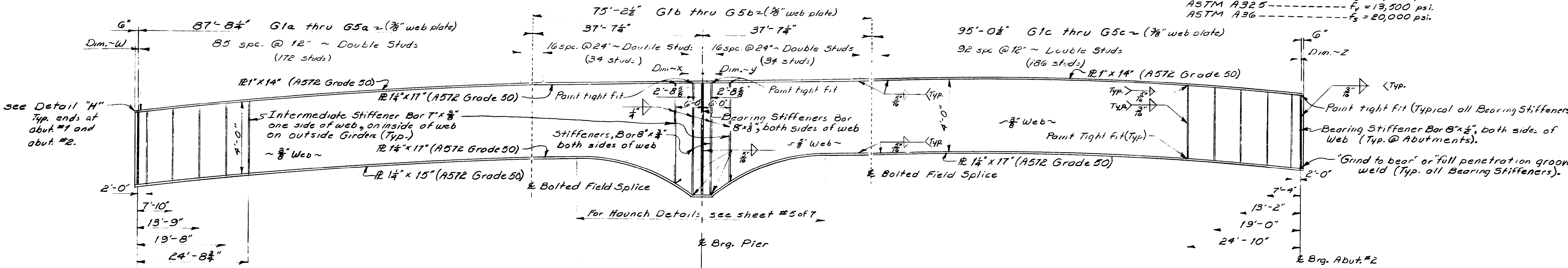
FRAMING PLAN

Structural Steel:

Girder Flanges ----- ASTM A572, Grade 50
 High Strength Bolts ----- ASTM A325, Type 1
 All other ----- ASTM A36

Basic Allowable Stresses

Structural Steel: ASTM A572, Grade 50 --- $f_y = 27,000$ psi.
 ASTM A325 --- $f_y = 13,500$ psi.
 ASTM A36 --- $f_y = 20,000$ psi.



GIRDER ELEVATION

Total number of Studs required ~ 2,130 ~ By others
 Weight of Studs = 2,087 lbs.

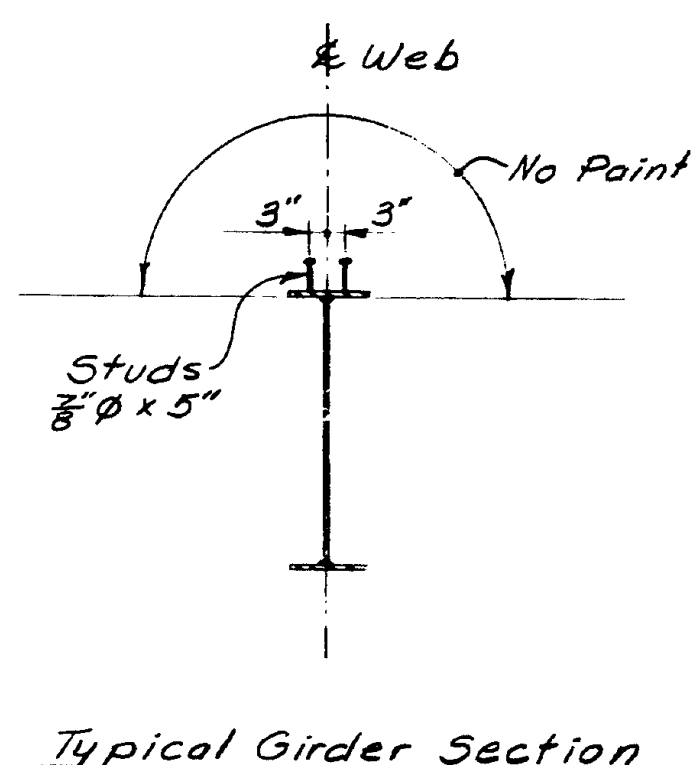
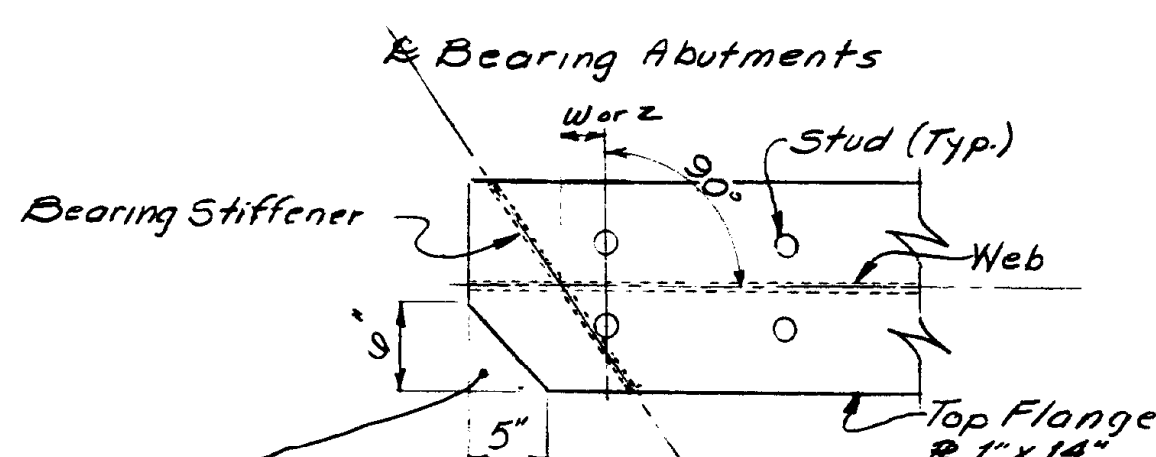
STRUCTURAL STEEL NOTES

1. Camber ordinates, sh#5 of 7, are computed to compensate for all dead load deflections and for the curvature of the finish grade profile.
2. No transverse butt weld splices in the flange plates or web plates will be allowed within 10 feet of the points of maximum negative moment or maximum positive moment.
3. Sections of flange plates or web plates shall be not less than 20 feet in length unless otherwise shown on the plans.
4. Butt weld splices in flanges shall be not less than 1 foot from transverse welds in the web plates.
5. Bearing stiffeners and ends of girders at abutments shall be plumb after erection and dead loading of the structure. Intermediate web stiffeners may be either plumb or normal to the top flange.
6. Crossframe and diaphragm connection plates may be either plumb or normal to the top flange.
7. Mill tests for filler plate material will not be required.
8. Connection Bars for Sign Supports shall be detailed on the shop drawings to be plumb after erection and dead load deflection of the structure.
9. All structural steel shall be ASTM-A36 unless otherwise noted.
10. All bolts shall be $\frac{3}{4}$ " ϕ H.S. ASTM A325, Type 1. Bolt holes shall be $\frac{1}{16}$ " ϕ .

References:

Shear Connectors ----- Standard Detail, BD104-77
 Bearing Pedestals ----- Standard Detail, BD101-74
 Crossframes & Diaphragms ----- Standard Detail, BD113-72

Camber Detail ----- Sheet # 5 of 7
 Bolted field Splice ----- Sheet # 4 of 7
 Lateral Bracing Details ----- Sheet # 4 of 7



Typical Girder Section

REFERENCE PLAN FOR LIMITS OF EXISTING HAUNCH REMOVAL. NO ADDITIONAL BRIDGE WORK SHOWN ON THESE PLANS REQUIRED. PLANS NOT TO SCALE.

STATE OF MAINE
 DEPARTMENT OF TRANSPORTATION

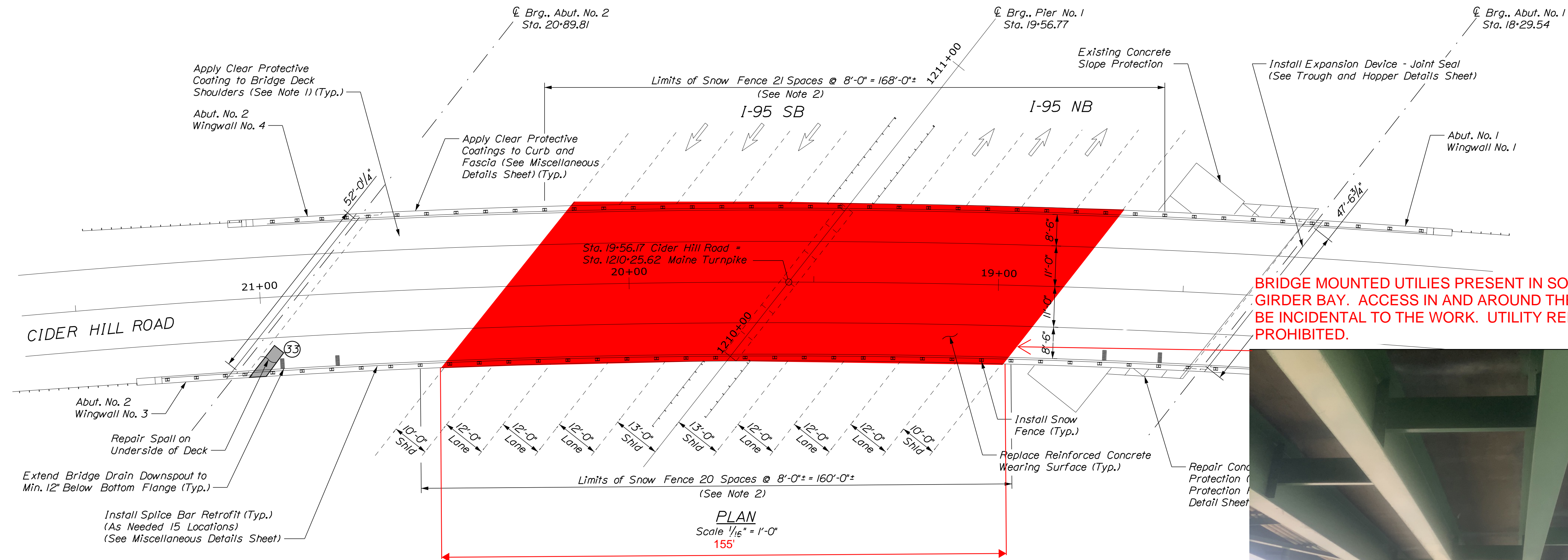
CUTTS ROAD
 OVER
 INTERSTATE 95
 IN THE TOWN OF
 KITTEERY
 YORK COUNTY
 FRAMING PLAN

PROJECT DESIGN ENGINEER
 DESIGN - DETAILED
 CHECKED
 REVISIONS
 FIELD CHANGES

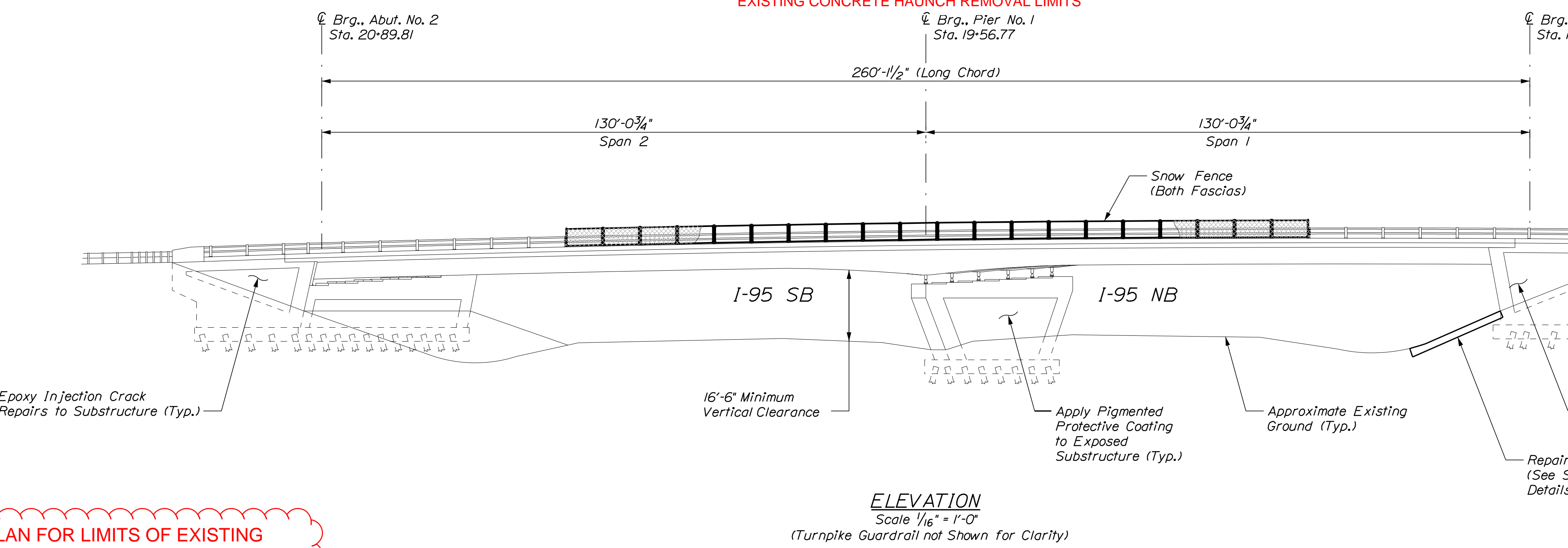
DATE
 11-77
 11-77
 11-77
 11-77

PLANS

Date: 2/17/2017



BRIDGE MOUNTED UTILITIES PRESENT IN SOUTHERN MOST GIRDER BAY. ACCESS IN AND AROUND THE UTILITIES SHALL BE INCIDENTAL TO THE WORK. UTILITY RELOCATION PROHIBITED.



REFERENCE PLAN FOR LIMITS OF EXISTING HAUNCH REMOVAL. NO ADDITIONAL BRIDGE WORK SHOWN ON THESE PLANS REQUIRED. PLANS NOT TO SCALE.

Filename: ...MSTA021_plan&elev_01.dgn

Scale: 1/16" = 1'-0"

No.	Revision	By	Date

Designed by:

CONSULTANT PROJECT MANAGER: T. Bryant

	By	Date		By	Date
Designed	MED	2/17/17	Checked	RSB	2/17/17
Drawn	KDW	2/17/17	In Charge of	TSB	2/17/17

VANASSE HANGEN BRUSTLIN, INC.
 500 Southborough Dr.
 Suite 105B
 South Portland, ME 04106
 TEL (207) 889-3150
 FAX (207) 253-5596

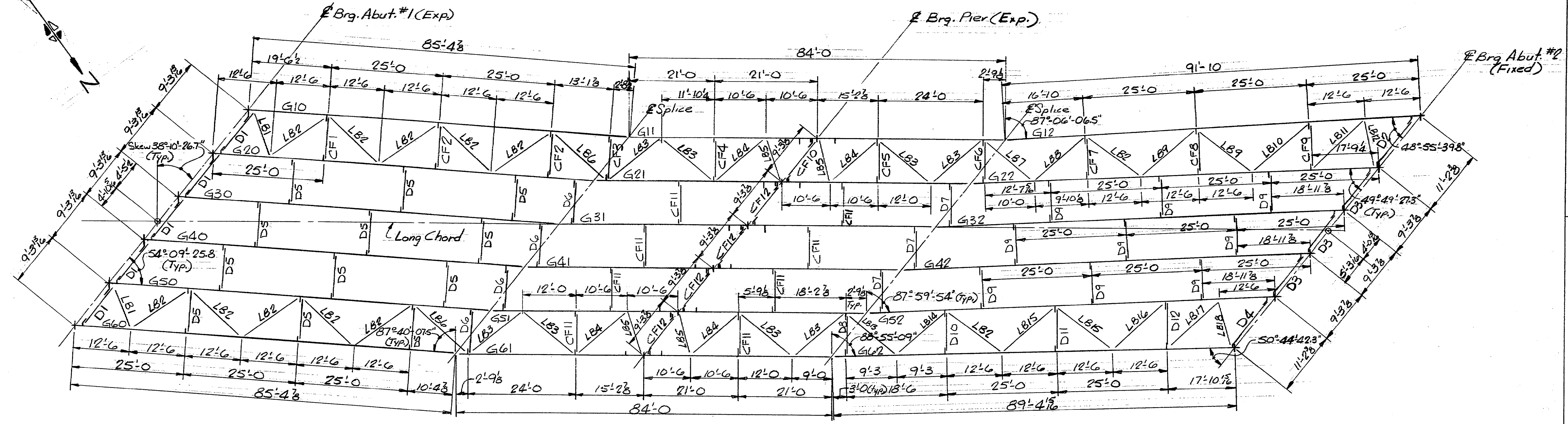
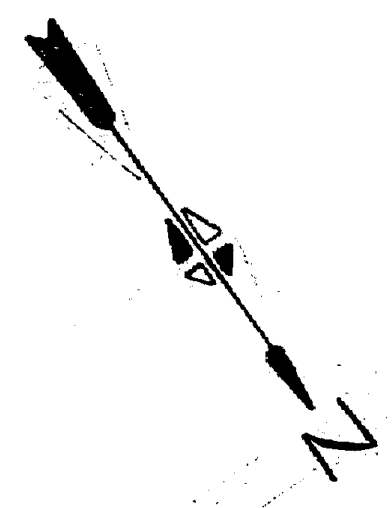
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: RALPH C. NORWOOD, IV

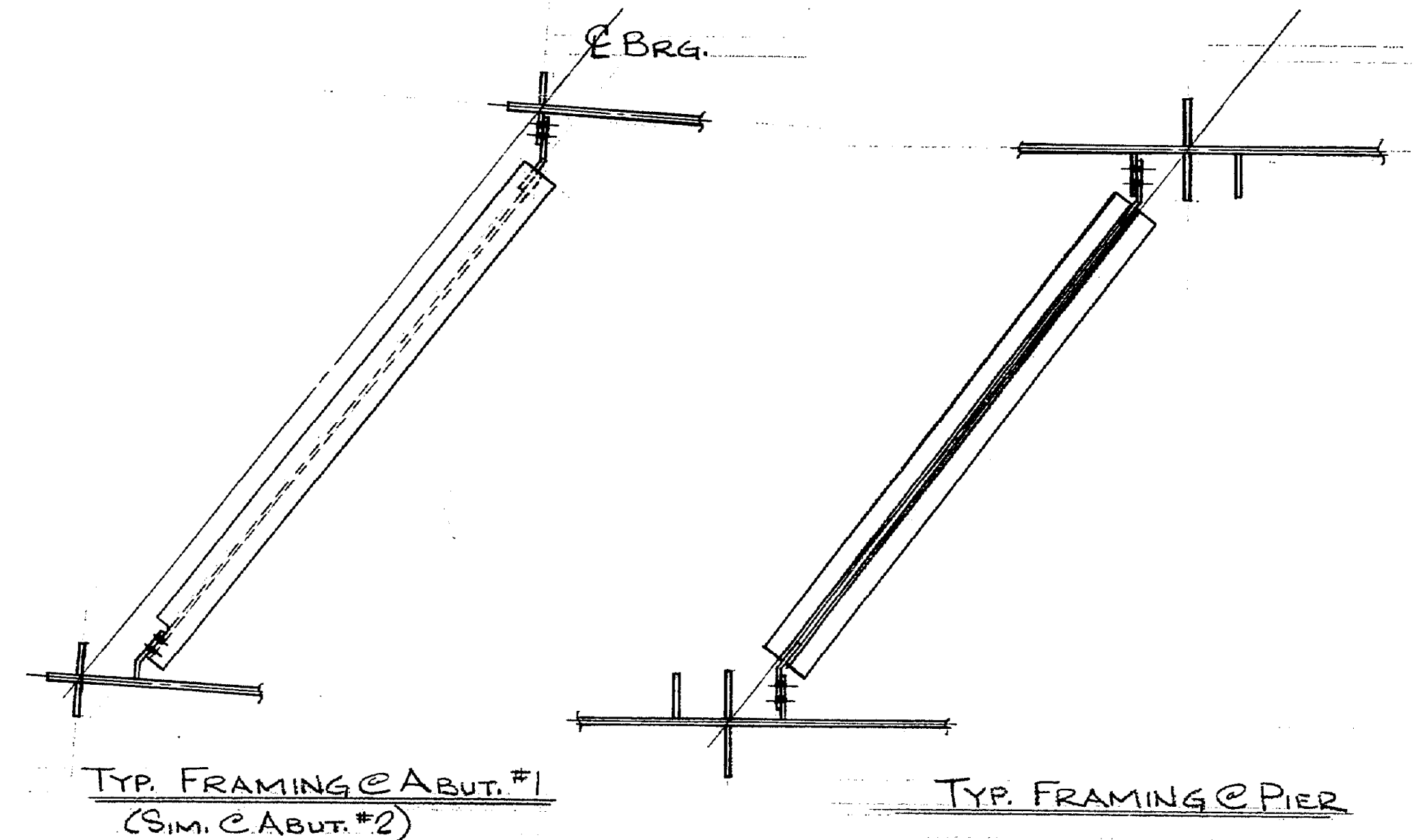
MTA PROJECT REHABILITATION OF
 CIDER HILL ROAD OVER INTERSTATE 95
 AT MM 6.20 IN YORK, ME
 GENERAL PLAN AND ELEVATION

VHB: 55108.00
 CONTRACT: 2017.05

SHEET NUMBER: 21
 21 OF 36



FRAMING PLAN



NOTE:
ALL DIMENSIONS ARE HORIZONTAL.
BOLTS ARE TO BE 3/4" - A325 TYPE 1.

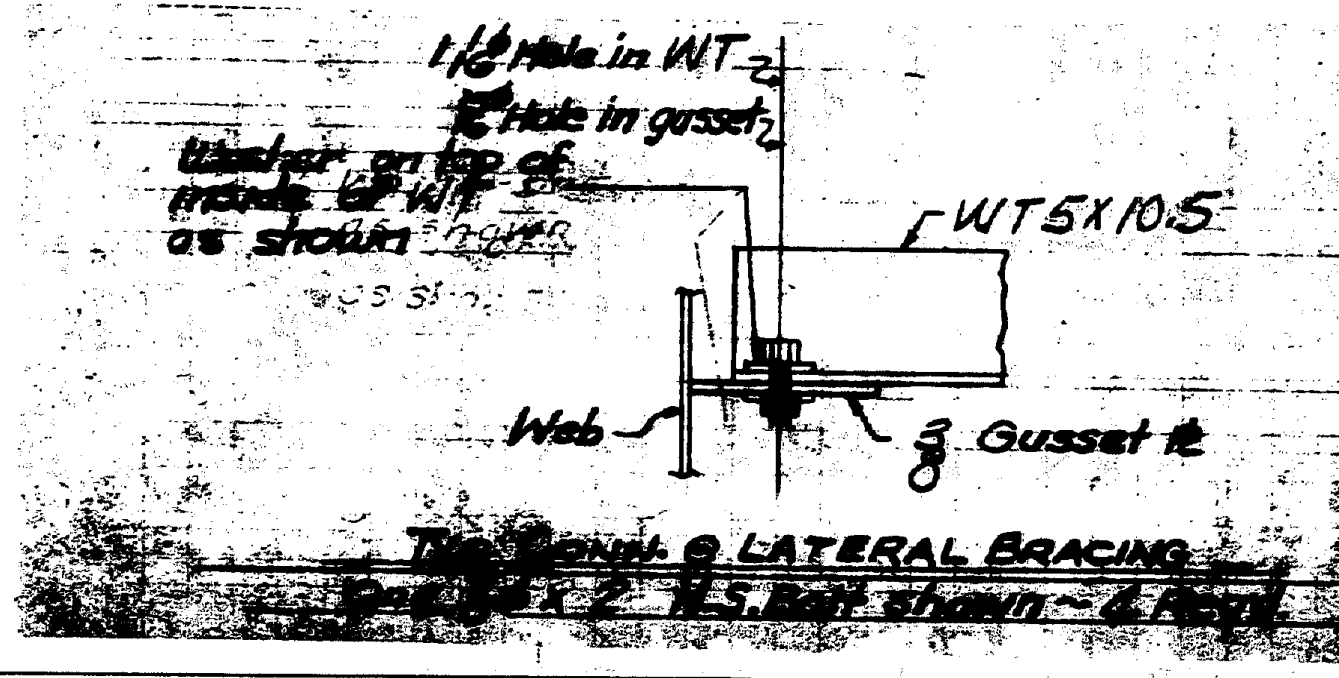
SPLICE NOTE

In order to maintain the 0-1/4" gap shown, the webs and flanges are to be cut (with the use of mechanical guide) at the time of shop assembly of the field splices. Holes are to be drilled full size from the solid with all material in exact relative field position.

No welding on filler plates or splice plates is allowed; except that web splice plates and fillers may be tack welded for drilling as follows: no tack weld closer to flange than 1/6 of web width, or longer than 3", or spaced less than approximately 12" on center. All tack welds will be removed and web surface ground flush. Bolt for shipment

ITEM No. 504.7001
PROJECT No. 1-95-1(29) 3

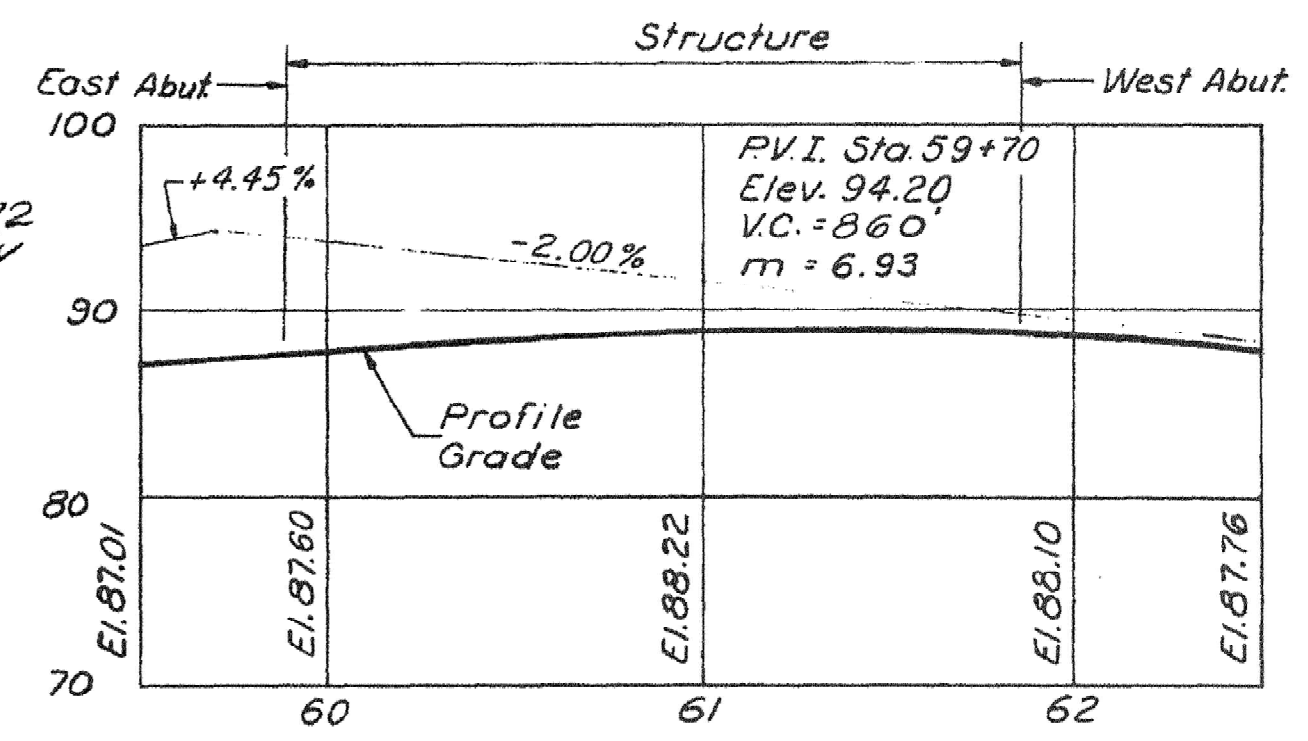
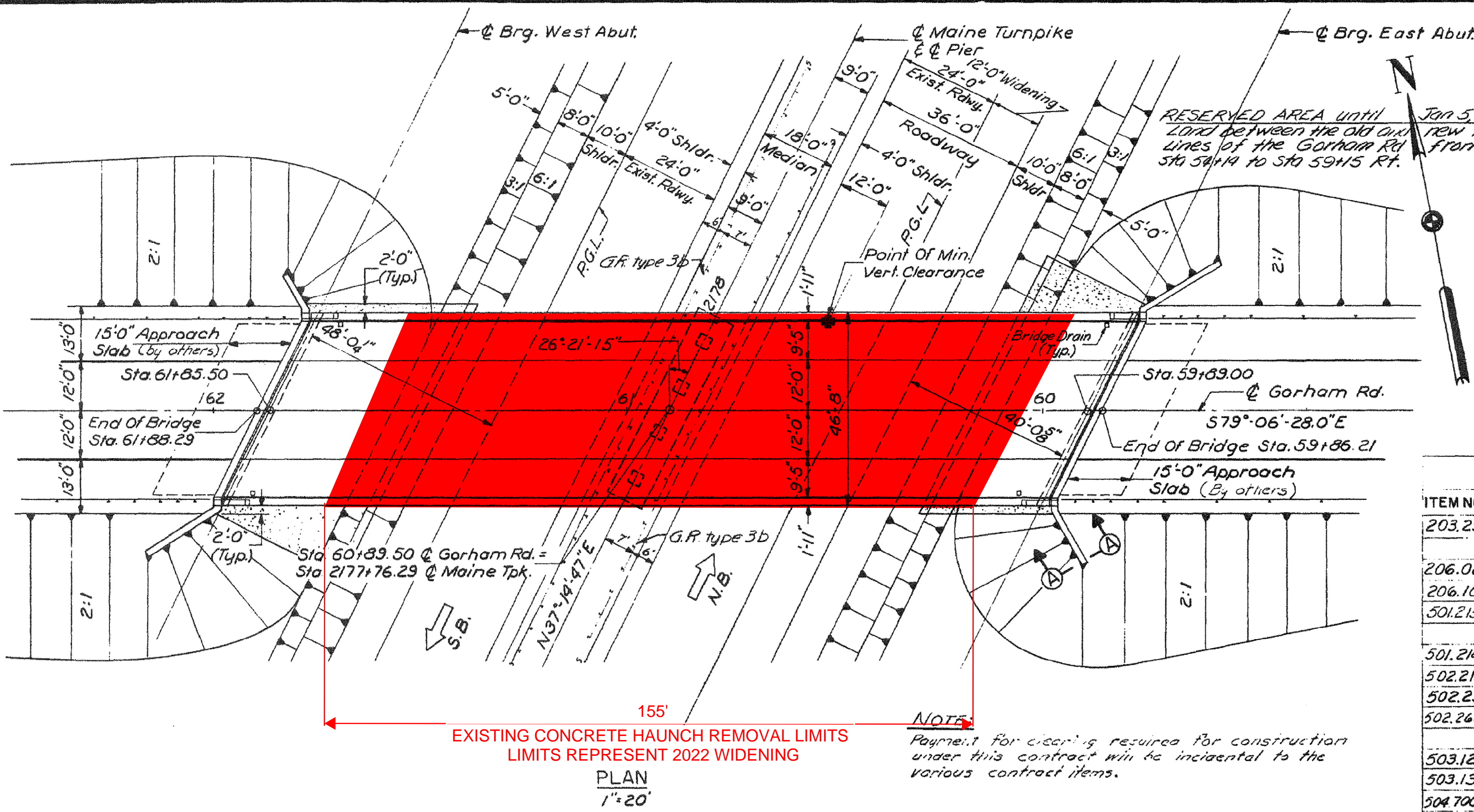
REFERENCE PLAN FOR LIMITS OF EXISTING HAUNCH REMOVAL. NO ADDITIONAL BRIDGE WORK SHOWN ON THESE PLANS REQUIRED. PLANS NOT TO SCALE.



APPROVED: 4-6-78

FRAMING PLAN			Bancroft & Martin Inc. South Portland, Maine 04106		
PRINT DIST.			JOB: CIDER HILL ROAD OVER I-95 YORK, MAINE		
1p	3/17/78	Shops	CUSTOMER: SHOALS INC.		
2p	3/17/78	Stake	DESIGNER: MAINE DEPT. of TRANS.		
1s	4-7-78	By	ORDER NO.	JOB NO.	DRAWING NO.
1s	4-7-78	Stake			
3p	5/15/78	Check			
REV.					
CHECKED					
DRAWN	2-22-78	BB		BS78-8	E-1

160-197



SPECIFICATIONS

DESIGN
A.A.S.H.O. Standard Specifications for Highway Bridges 1963 with interim specifications.

CONTRACT
State of Maine, State Highway Commission Standard Specifications, Highways and Bridges, Revision of June 1968.

LIVE LOADING

HS 20-44

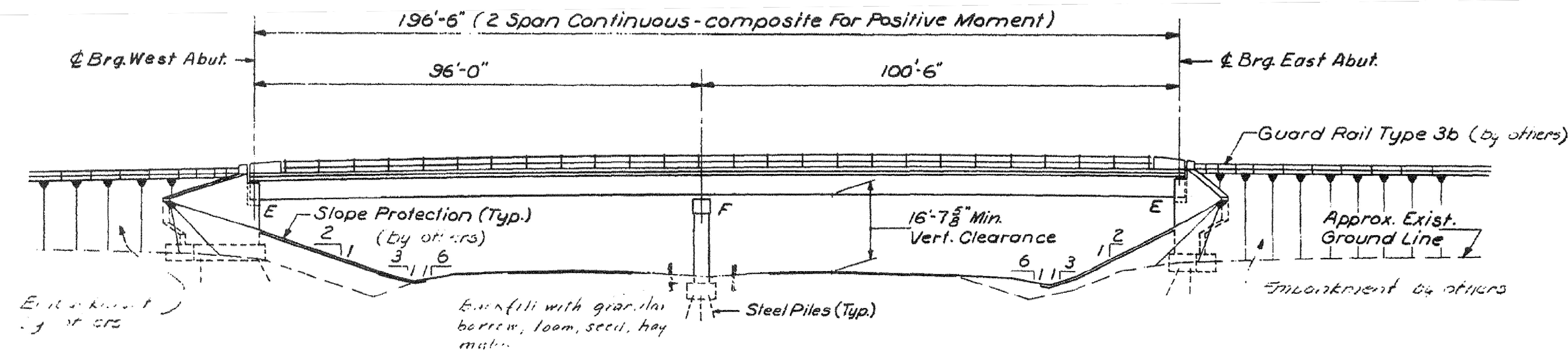
FOUNDATIONS

Abutments-10 BP57 Steel Bearing Piles - total capacity 75.5T design capacity 52.5 tons - allowance of 23 tons for Negative Skin Friction

BRIDGE QUANTITIES

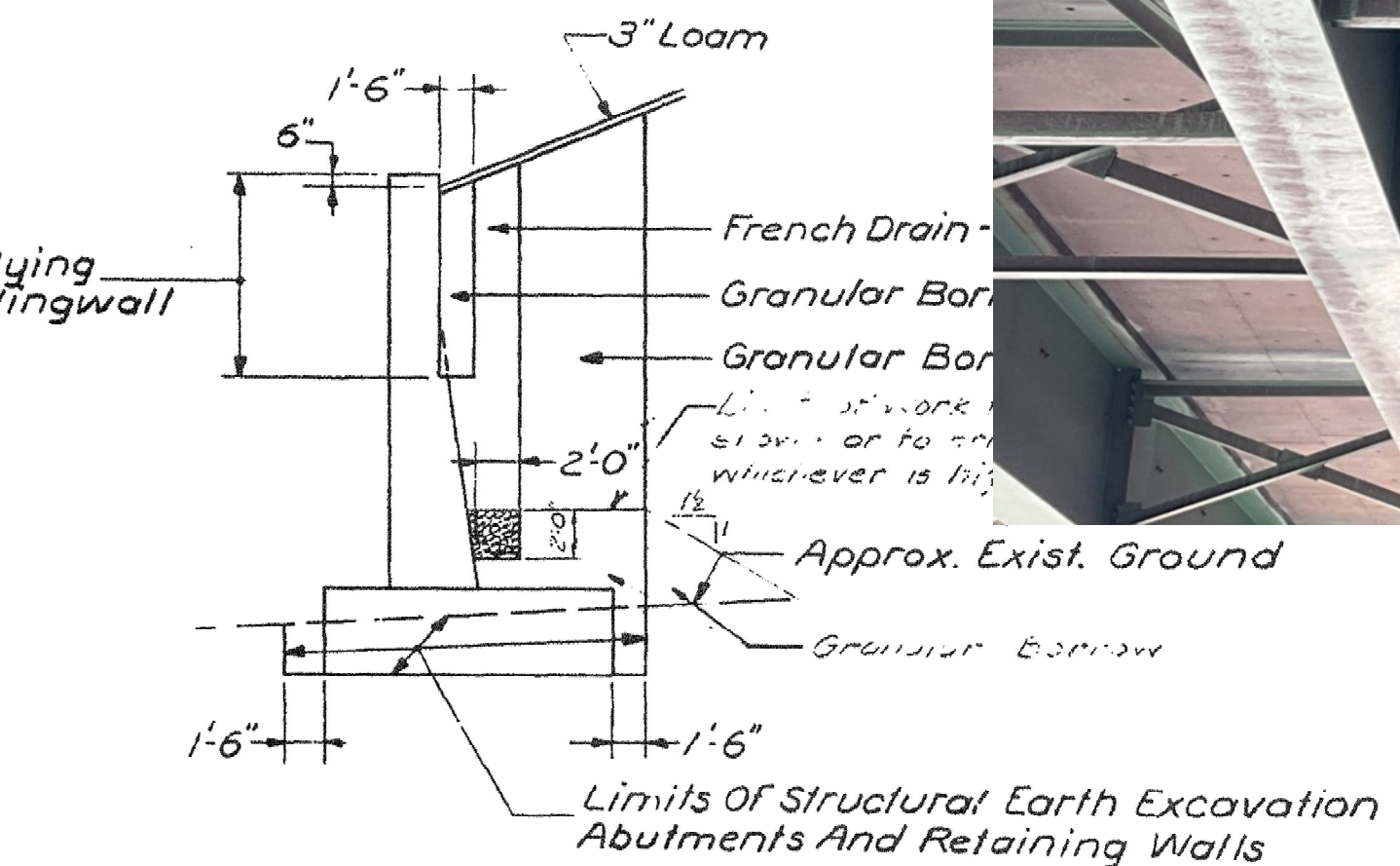
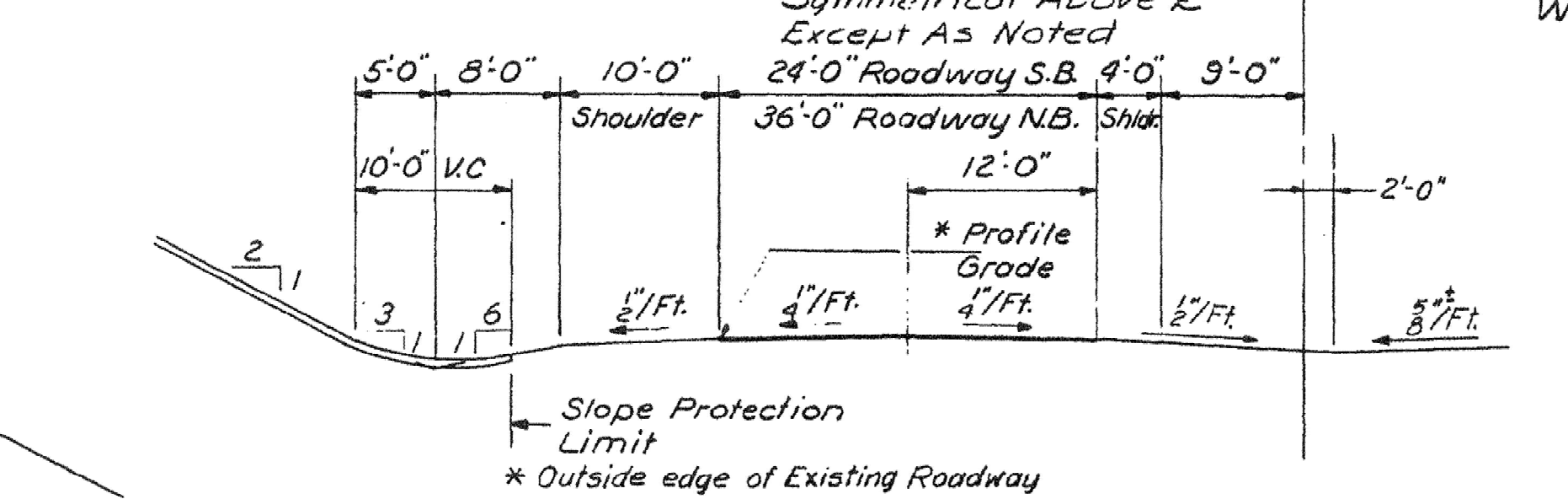
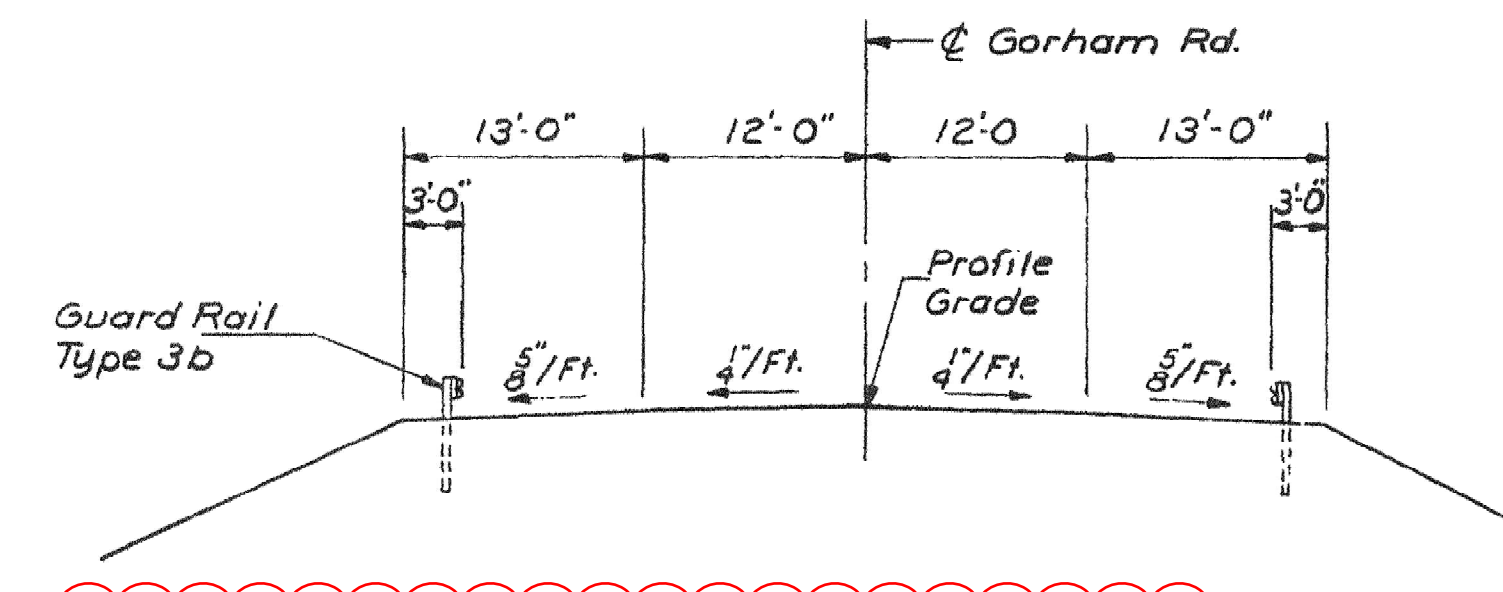
ITEM NO.	DESCRIPTION
203.25	Granular Borrow
206.08	Structural Earth Excavation - Abut. & Retaining Walls
206.10	Structural Earth Excavation - Piers
501.213	Steel H-Beam Piles 57 lbs./ft.
501.214	Steel H-beam Piles 53 lbs./ft.
502.21	Structural Concrete, Abutts. & Retaining Walls
502.23	Structural Concrete, Piers
502.260	Structural Concrete, Roadway & Sidewalk Slabs on Steel Br
503.12	Reinforcing Steel, Fabricated & Delivered
503.13	Reinforcing Steel, Placing
504.7001	Structural Steel, Fabricated & Delivered
504.7101	Structural Steel, Erection
505.0801	Shear Connectors
506.1401	Field Painting, Structural Steel
507.08	Bridge Railing
512.06	French Drains
515.20	Protective Coating For Concrete Surfaces
609.13	Vertical Bridge Curb - Type 1

NOTE:
Estimated Quantity of Structural Steel, Fab., Del., Erected & Painted
Estimated Quantity of Shear Connectors - 2,028 Ea. (1,990 Lbs.)
Estimated Quantity of Concrete Item 502.2601 - 270 C.Y.



Sta 2176+50 to Sta 2179+0, T/Lt of E. Med Me Turnpike
Install G.R. type 3b, last 50' on 20:1 taper
Install twisted end section Sta. 2179+0 to Sta 2179+25, on taper
Install terminal end section Sta 2176+50
Install G.R. Delineator Posts Sta. 2176+50 and Sta 2179+25

Sta 2176+0 to Sta 2178+50, T/Lt of E. Med. Me. Turnpike
Install G.R. type 3b, first 50' on 20:1 taper
Install twisted end section Sta. 2175+75 to Sta 2176+0, on taper
Install terminal end section Sta 2178+50
Install G.R. Delineator Post Sta 2176+25 and Sta. 2178+50



NOTE:
For payment limits at abutments see Sheet 13.

REFERENCE PLAN FOR LIMITS OF EXISTING HAUNCH REMOVAL. NO ADDITIONAL BRIDGE WORK SHOWN ON THESE PLANS REQUIRED. PLANS NOT TO SCALE.

DESIGN - TRACE - CHECK -

BRIDGE NO. SURVEY - PLOT -

STATE HIGHWAY COMMISSION
BRIDGE DIVISION

GORHAM ROAD
OVER
MAINE TURNPIKE

IN THE TOWN OF
SCARBOROUGH
CUMBERLAND COUNTY

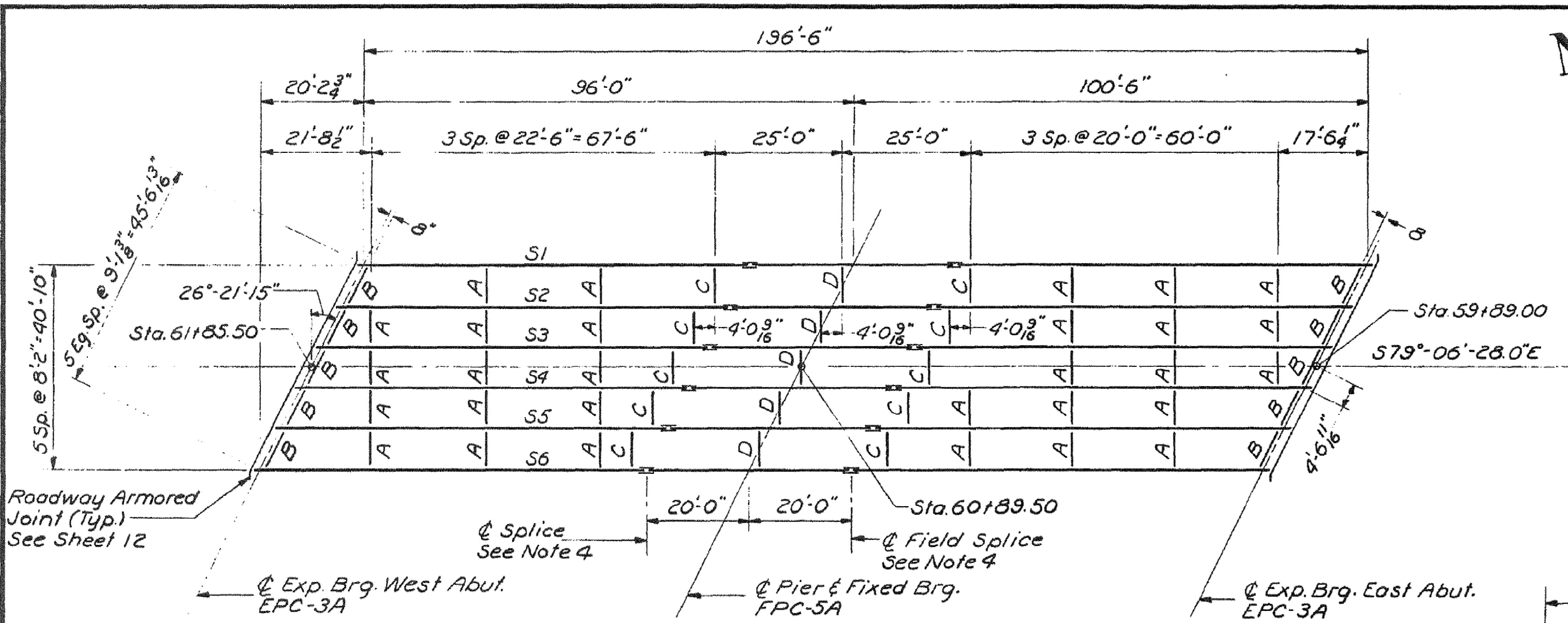
GENERAL PLAN

SHEET 1 OF 15 AUGUSTA, MAINE SEPTEMBER, 1971

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS

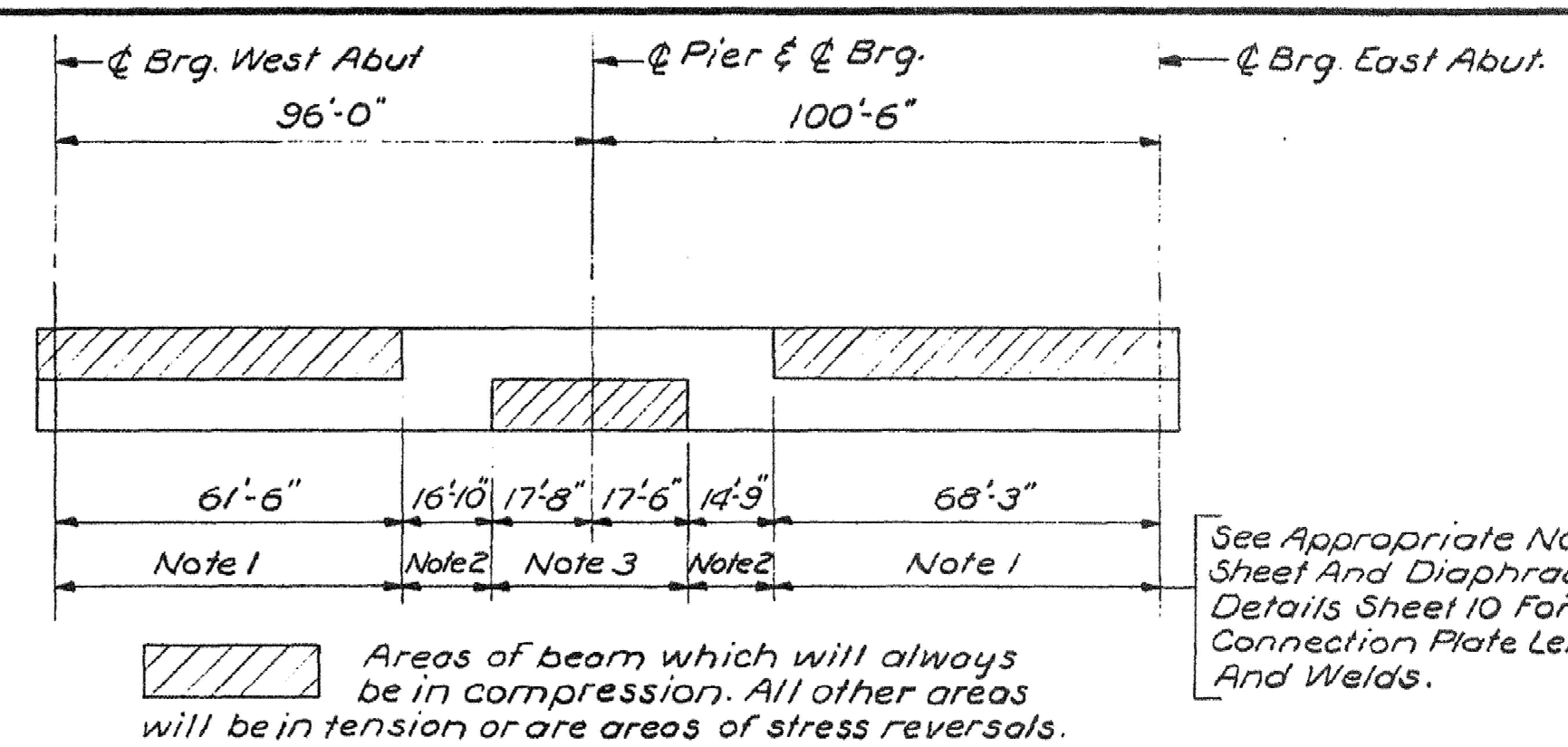
SCARBOROUGH

1295 (56) 150-49



PEDESTALS
EPC-3A 12 Required
FPC-5A 6 Required

ERECTION DIAGRAM
1" = 20'



BEAM STRESS TYPE DIAGRAM
No Scale

REFERENCE
Blocking Detail - see Sheet 10.
Diaphragms - see Sheet 10.
Pedestals - see Standard Details BD101-70 and Sheet 10.
Armored Joint - see Standard Details BD104-66 and Sheet 12.
Shear Connectors - see Standard Details BD104-66 and Sheet 10.
For additional steel details, see Sheet 10.

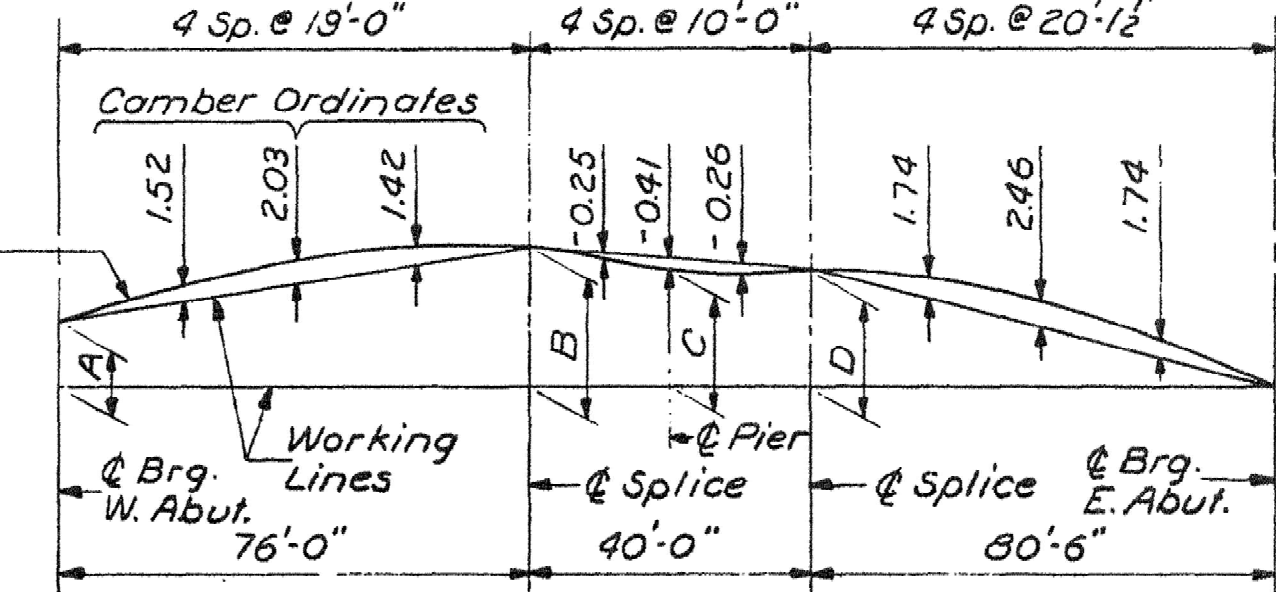
SPECIFICATIONS
Fabrication and Erection: State of Maine Standard Specifications, Highways and Bridges Revision of June 1968.
Design and Details: A.A.S.H.O. Standard Specifications for Highway Bridges 1969 with Interim Specifications 1970.
Materials: Stringer flanges, (except as noted on Typical Stringer Elevation) webs and splices shall conform to A.S.T.M. Designation A572-50. All other structural steel shall conform to A.S.T.M. Designation A36 unless otherwise noted on Standard Details.
Welding: Specifications for Welded Highway and Railway Bridges, American Welding Society, A.W.S.-D2.0-69 as modified by the appropriate special provisions.

- NOTES:**
1. Weld to web, point tight fit at top flange, step 2" clear at bottom flange.
 2. Weld to web, step 2" clear at top and bottom flanges.
 3. Weld to web and bottom flange, point tight fit at top flange.
 4. At the Contractor's option a shop bolted or field bolted splice may be used. *Work Splice was shop welded.*
 5. Spacing may be varied on S1 & S6 as required to clear bridge drains providing total number of shear connectors indicated remains unchanged.
 6. A maximum of one shop welded web splice will be allowed in the positive moment section of the girders. No shop welded flange splices will be allowed. Location of shop web splices is subject to the approval of the Engineer.

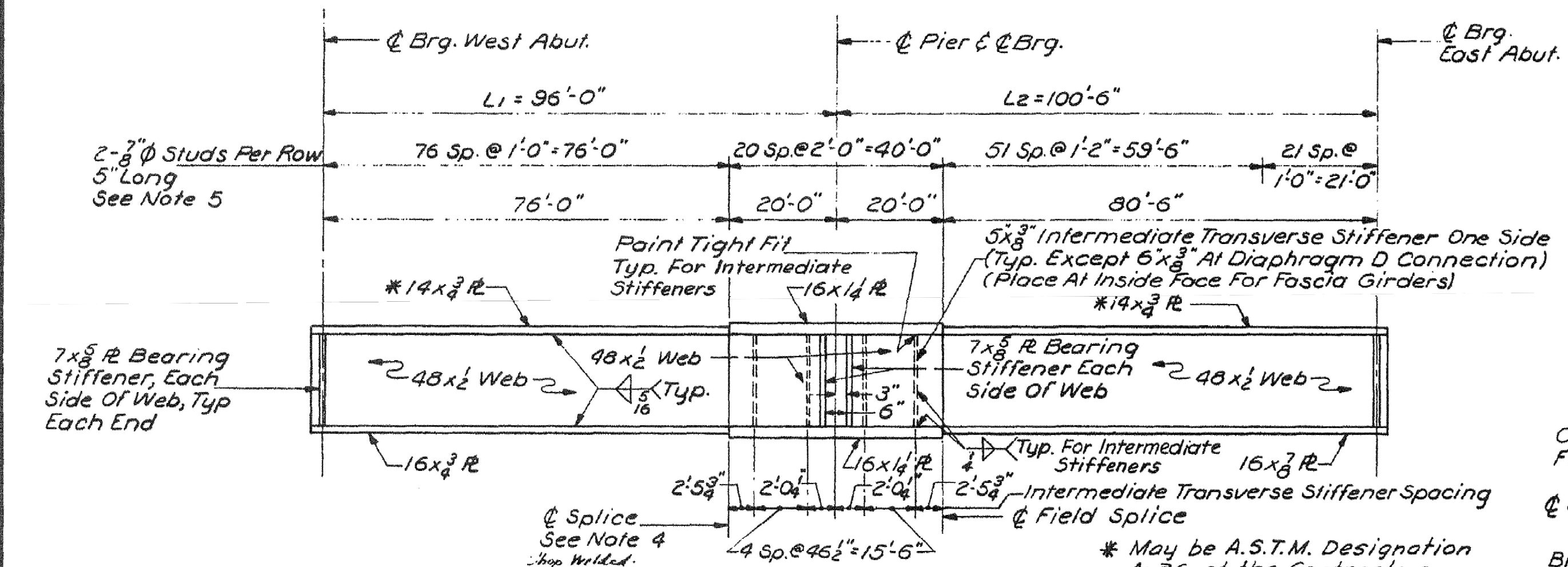
DIMENSION SCHEDULE

	A	B	C	D
S1	0.83	0.89	0.79	0.74
S2	0.77	0.85	0.76	0.72
S3	0.71	0.82	0.72	0.70
S4	0.65	0.78	0.69	0.67
S5	0.59	0.75	0.66	0.65
S6	0.53	0.71	0.63	0.62

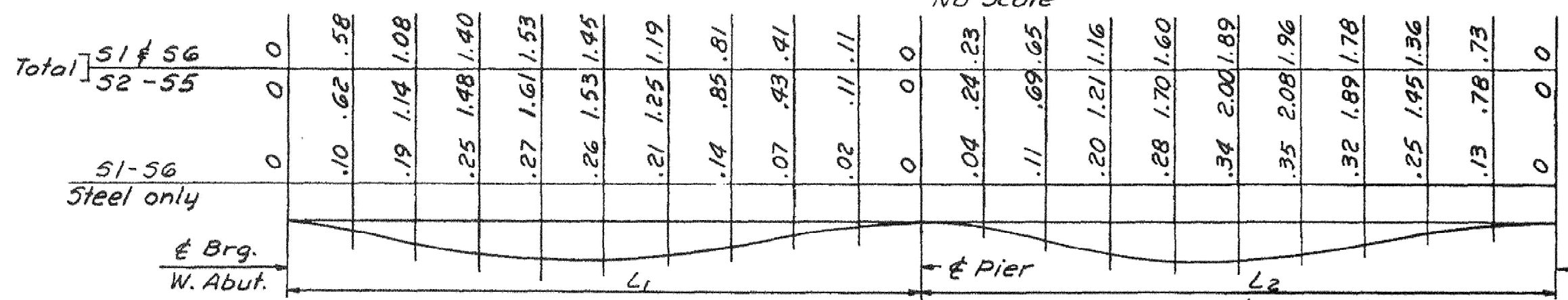
NOTE:
Camber ordinates shown include total dead load deflection. All dimensions are horizontal or vertical. Camber ordinates are in inches. Dimensions A, B, C & D are in feet.



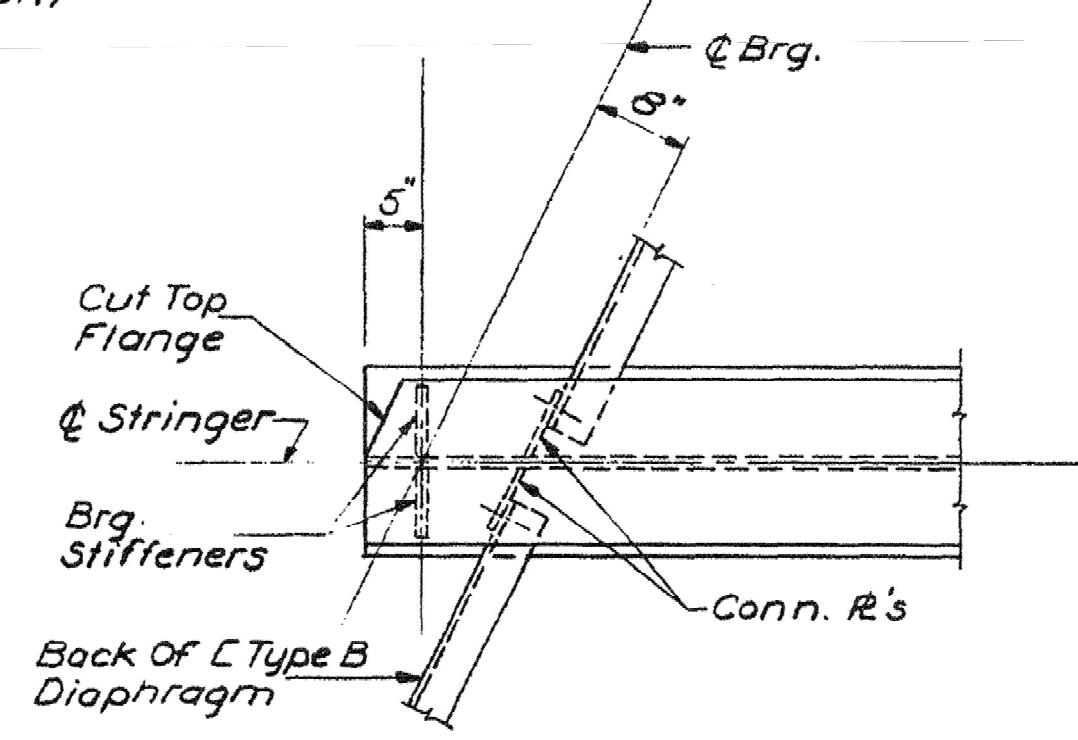
CAMBER DIAGRAM
No Scale



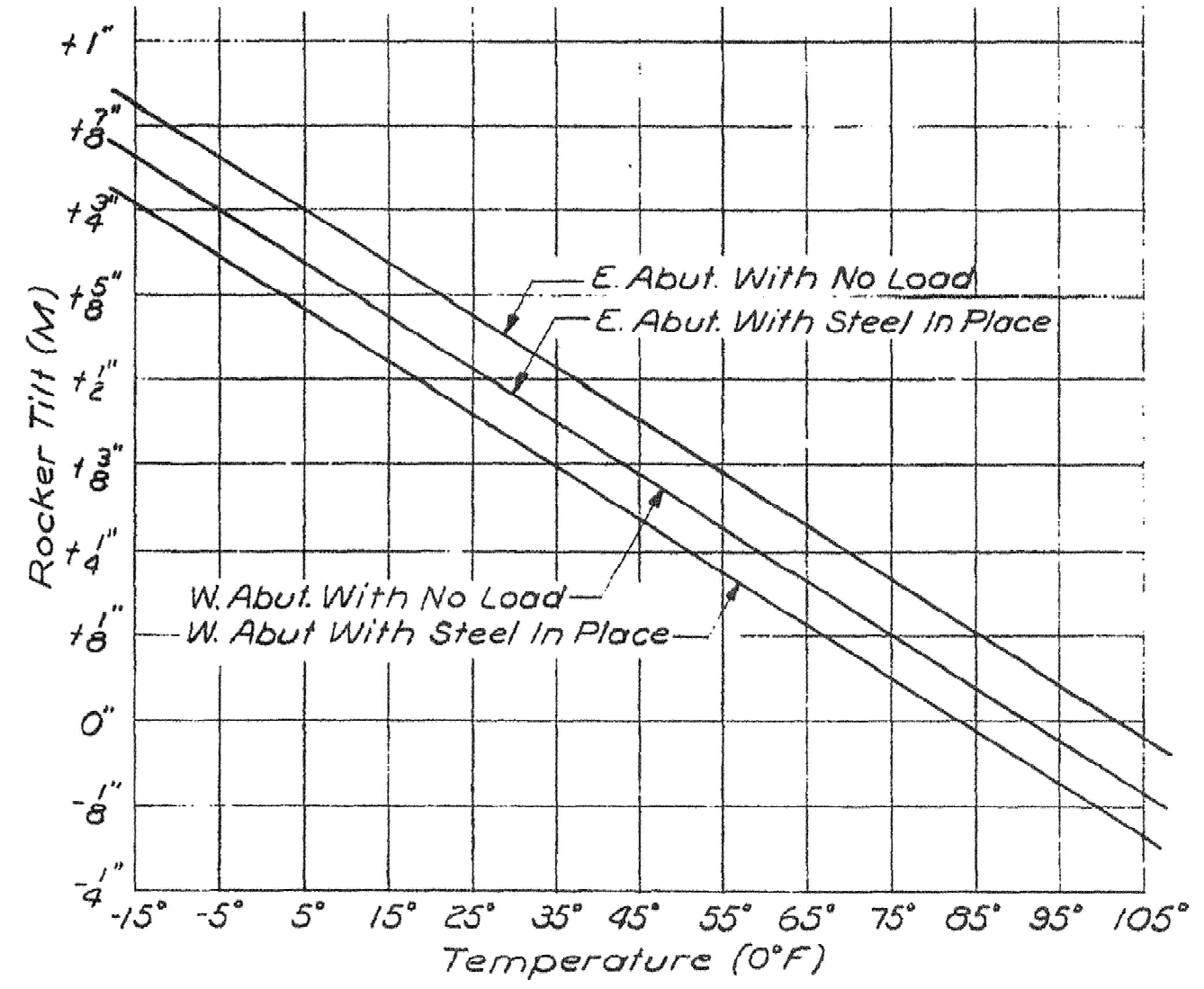
TYPICAL STRINGER ELEVATION
No Scale



DEAD LOAD DEFLECTION DIAGRAM
No Scale
Note: All figures given in inches at tenth point of spans.



END OF STRINGER DETAIL
3/4" = 1'-0"



EXPANSION ROCKER SETTINGS

NOTE:
Fabricator shall verify all rocker setting.

REFERENCE PLAN FOR LIMITS OF EXISTING HAUNCH REMOVAL. NO ADDITIONAL BRIDGE WORK SHOWN ON THESE PLANS REQUIRED. PLANS NOT TO SCALE.

BOTTOM OF SLAB ELEVATIONS AT BLOCKING POINTS

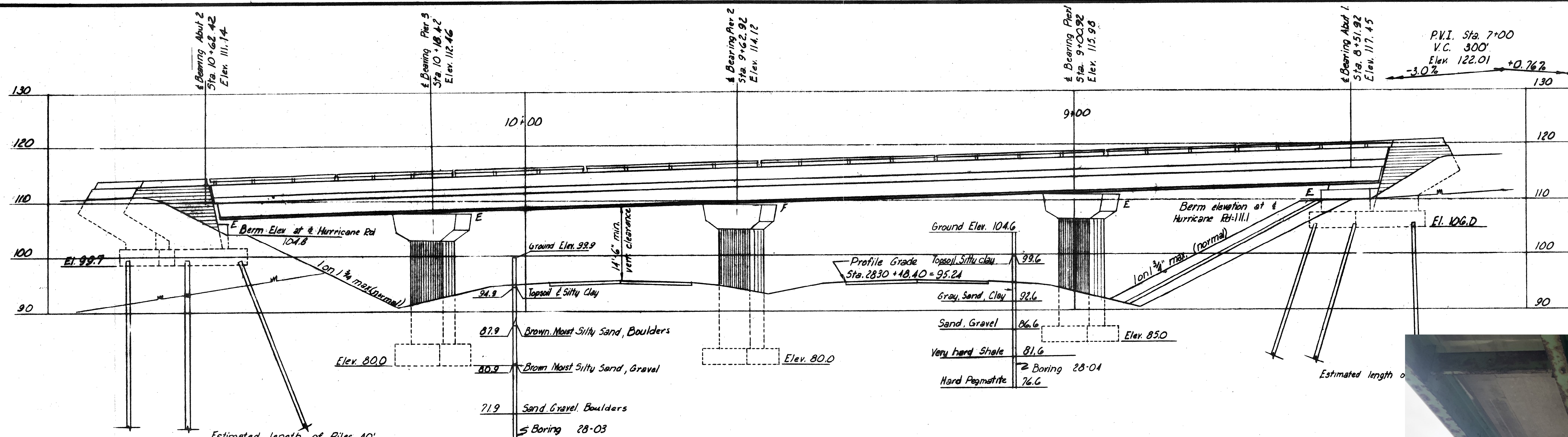
Stringer No.	Brq. W. Abut.	SPAN 1										Pier	SPAN 2										Brq. E. Abut.
		.1L1	.2L1	.3L1	.4L1	.5L1	.6L1	.7L1	.8L1	.9L1	.1L2		.2L2	.3L2	.4L2	.5L2	.6L2	.7L2	.8L2	.9L2			
S1	86.92	86.99	87.04	87.08	87.09	87.06	87.02	86.97	86.92	86.88	86.85	86.83	86.80	86.77	86.71	86.63	86.53	86.40	86.26	86.09			
S2	87.08	87.15	87.21	87.25	87.27	87.26	87.24	87.20	87.15	87.11	87.07	87.04	87.02	87.00	86.97	86.92	86.85	86.75	86.62	86.47	86.31		
S3	87.23	87.31	87.37	87.41	87.44	87.43	87.41	87.38	87.33	87.29	87.25	87.23	87.21	87.20	87.17	87.12	87.05	86.95	86.83	86.69	86.53		
S4	87.22	87.30	87.36	87.41	87.43	87.43	87.41	87.38	87.34	87.30	87.26	87.24	87.23	87.22	87.19	87.15	87.08	86.99	86.87	86.73	86.57		
S5	87.03	87.11	87.18	87.23	87.26	87.26	87.24	87.21	87.18	87.14	87.10	87.09	87.08	87.07	87.05	87.01	86.94	86.85	86.73	86.60	86.44		
S6	86.84	86.92	86.99	87.04	87.07	87.08	87.07	87.04	87.01	86.97	86.95	86.93	86.92	86.92	86.89	86.85	86.79	86.70	86.59	86.46	86.31		

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS

BOSTON

DESIGN - E.F.K. DETAIL - R.D.F.
TRACE - I.S.
BRIDGE NO. SURVEY - PLOT -

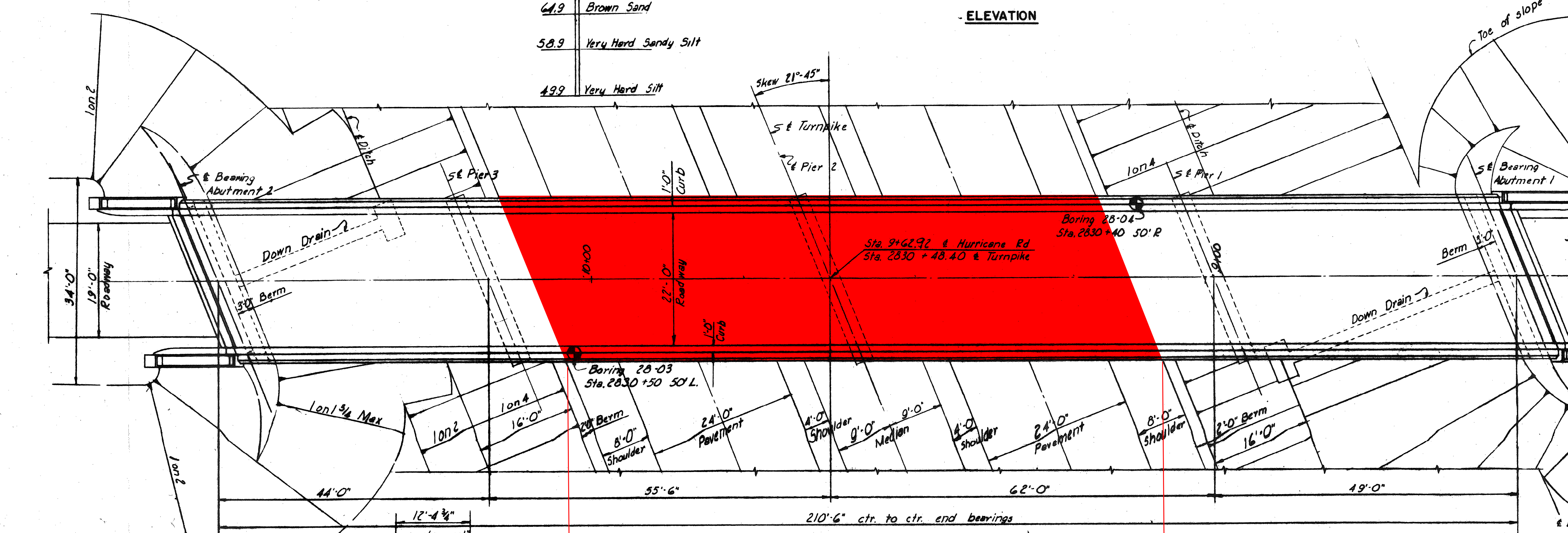
STATE HIGHWAY COMMISSION
BRIDGE DIVISION
GORHAM ROAD
OVER
MAINE TURNPIKE
IN THE TOWN OF
SCARBOROUGH
CUMBERLAND COUNTY
STRUCTURAL STEEL & BLOCKING POINTS
SHEET 9 OF 15 AUGUSTA, MAINE SEPTEMBER, 1971



GENERAL NOTES
 Design Specifications: AASHTO (1953) with minor modifications
 Design Loading: H15
 Maximum Soil Pressure:
 Pier 3 2.7 Tons/sq.ft.
 Pier 2 3.9 Tons/sq.ft.
 Pier 1 3.6 Tons/sq.ft.
 Maximum Pile Load Abutment 1 & 2 36.2 Tons/Pile

REFERENCES

Sub	Superstructure
Steel Floor	✓
Erector	✓
Concrete	✓
Abutment	✓
Pier	✓
Foundation	✓



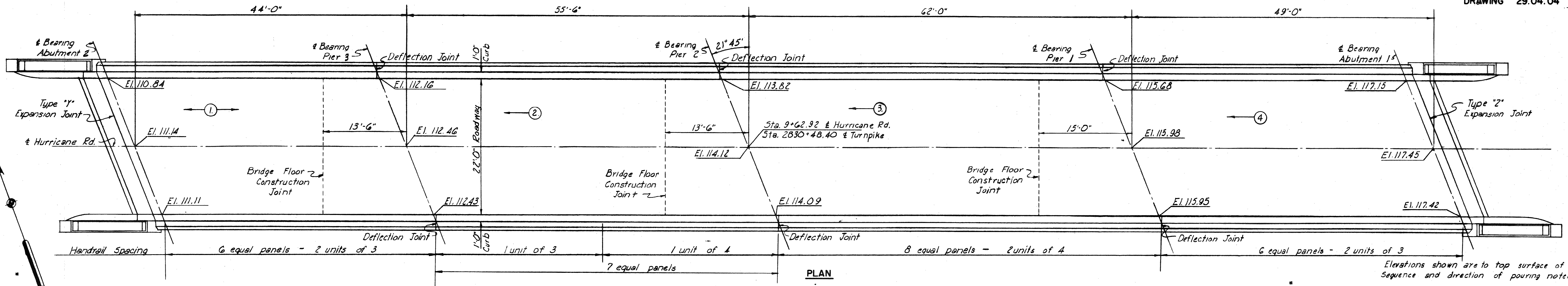
REFERENCE PLAN FOR LIMITS OF EXISTING HAUNCH REMOVAL. NO ADDITIONAL BRIDGE WORK SHOWN ON THESE PLANS REQUIRED. PLANS NOT TO SCALE.

DRAWING 29.01.04

BY	DATE	NO.	REVISION	BY	DATE
MADE	BA	12-24-53			
TRACED					
CHECKED	F.S. Jr.	1-12-53	As-Built	HBN	1-19-56
IN CHARGE OF					

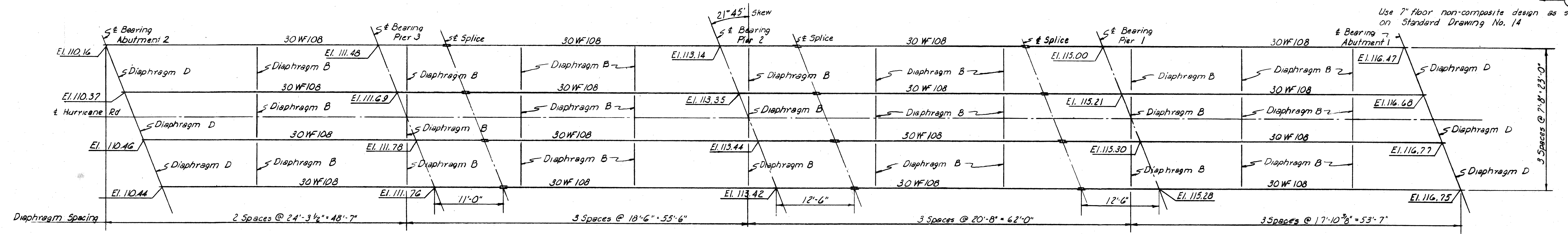
MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
 SECTION 2— PORTLAND TO AUGUSTA
 STRUCTURE NO. 29. TURNPIKE UNDER
 HURRICANE ROAD
 STA. 2830 + 48.40
GENERAL PLAN AND ELEVATION
 HOWARD, NEEDLES, TAMMEN & BERGENDOFF
 CONSULTING ENGINEERS
 NEW YORK KANSAS CITY
 CONTRACT NO. _____
 SCALE: 3/8" = 1'-0"
 SHEET NO. 165 OF 382

FOOTING PLAN

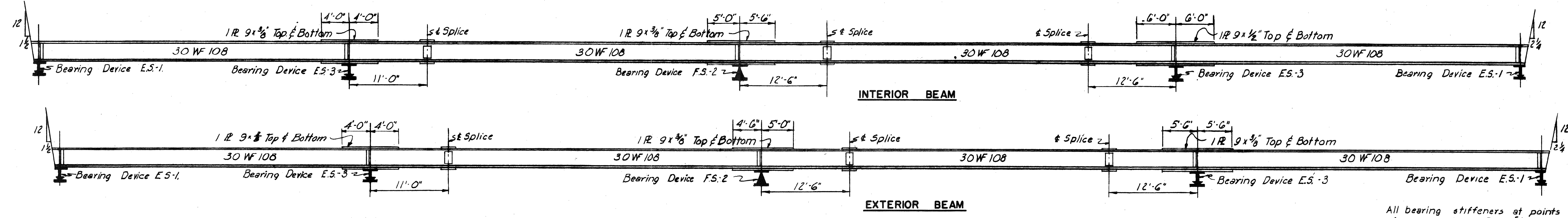


Elevations shown are to top surface of concrete. Sequence and direction of pouring noted thus: \rightarrow (2)

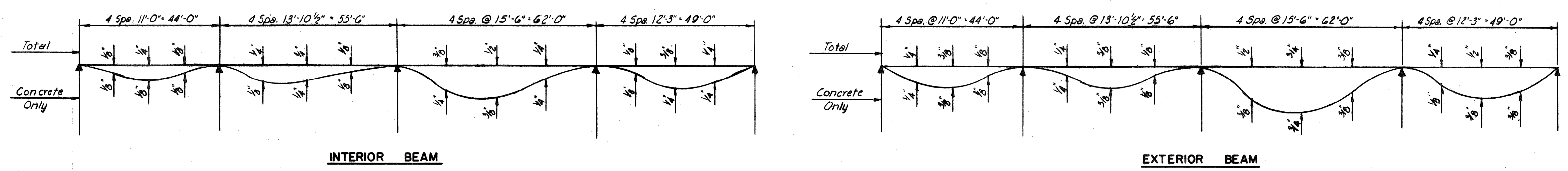
Use 7" floor non-composite design as shown on Standard Drawing No. 14



Elevations shown are to top flange of beams.



All bearing stiffeners at points of support shall be 7x4x3/8" angles. Bevel shown at end of beams is true with respect to axis of beam only.



DRAWING 29.04.04.

BY	DATE			
MADE	DM	12-21-53		
TRACED				
CHECKED	F.S.Jr	1-11-53	As-Built	HBN 149-58
IN CHARGE OF	IDSK			

REFERENCE PLAN FOR LIMITS OF EXISTING HAUNCH REMOVAL. NO ADDITIONAL BRIDGE WORK SHOWN ON THESE PLANS REQUIRED. PLANS NOT TO SCALE.

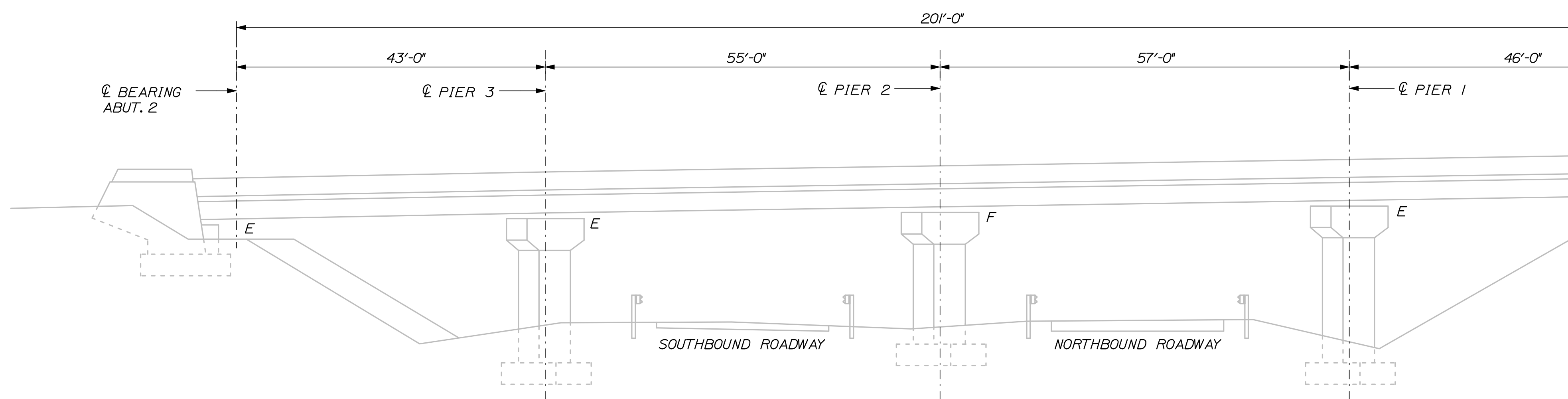
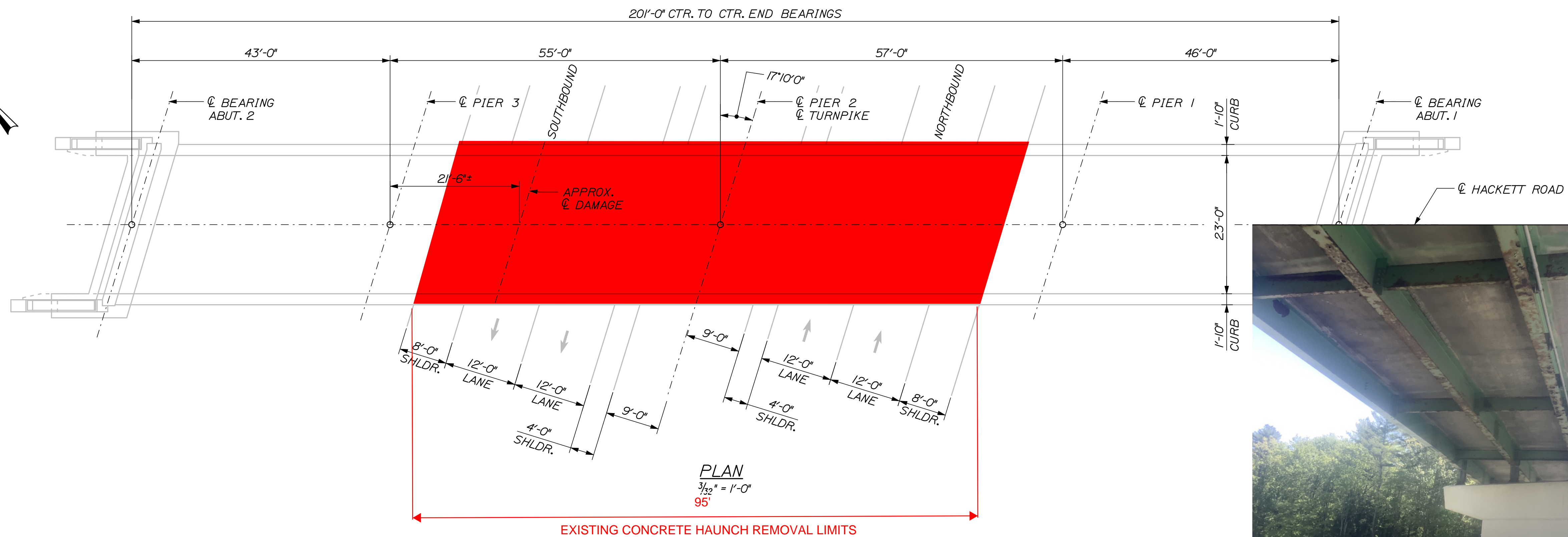
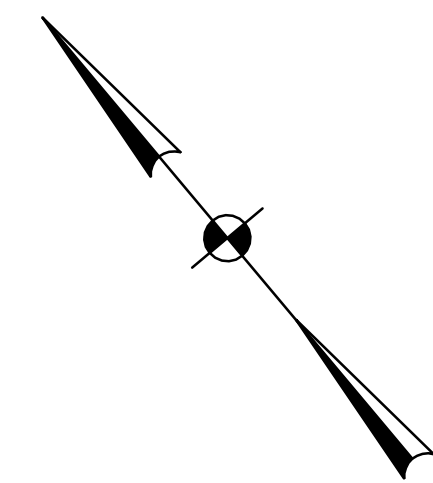
MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
 SECTION 2— PORTLAND TO AUGUSTA

STRUCTURE NO. 29 TURNPIKE UNDER
 HURRICANE ROAD
 STA. 2830+48.40
 SUPERSTRUCTURE

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
 CONSULTING ENGINEERS
 NEW YORK KANSAS CITY

SCALE: 1/8" = 1'-0"
 CONTRACT NO. _____
 SHEET NO. 148 OF 382

Date: 2/18/2011



ELEVATION
(LOOKING NORTHBOUND)
3/32" = 1'-0"

REFERENCE PLAN FOR LIMITS OF EXISTING HAUNCH REMOVAL. NO ADDITIONAL BRIDGE WORK SHOWN ON THESE PLANS REQUIRED. PLANS NOT TO SCALE.



Filename: 009_GeneralPlan and TypicalSection.dgn

Scale: AS NOTED			
No.	Revision	By	Date

Designed by:					
HNTB					
	By	Date		By	Date
	JF	01/11	Checked	JLB	01/11
	SPL	01/11	In Charge of	RAL	01/11

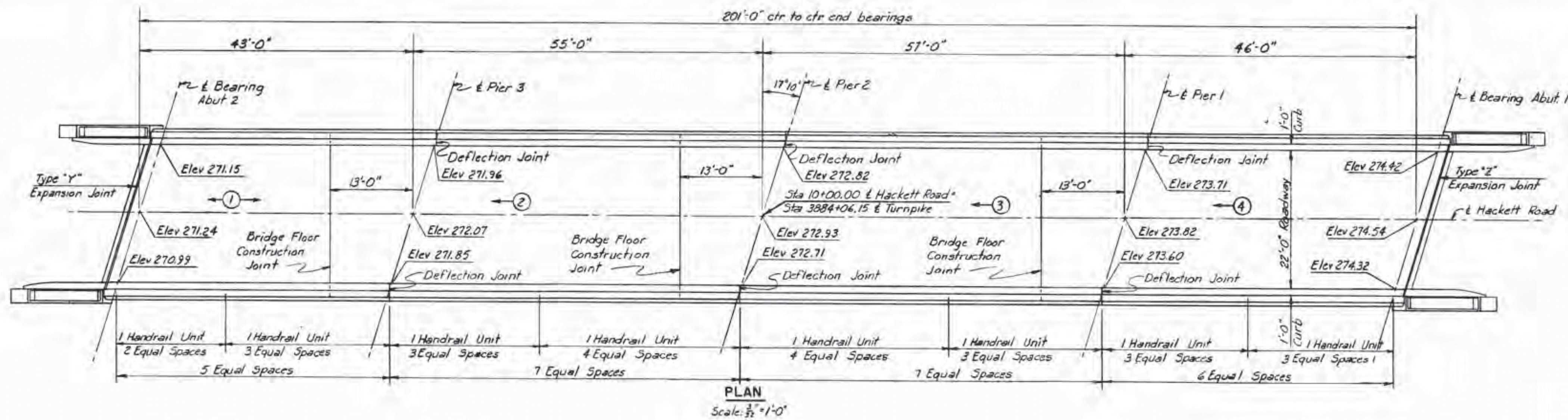
HNTB CORPORATION
340 County Road, Suite 6-C
Westbrook, ME 04092
TEL (207) 774-5155
FAX (207) 228-0909



**THE GOLD STAR
MEMORIAL HIGHWAY**

HACKETT ROAD EMERGENCY REPAIR
GENERAL PLAN AND ELEVATION

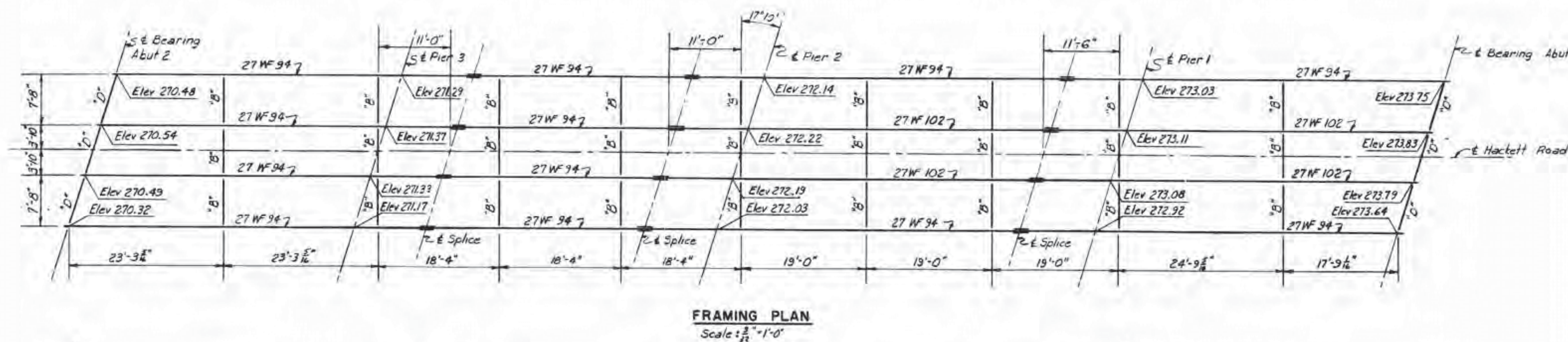
SHEET NUMBER: S-1
CONTRACT: S2011.51
9 OF 15



Note: Sequence and direction of pouring are noted thus: \rightarrow (N)

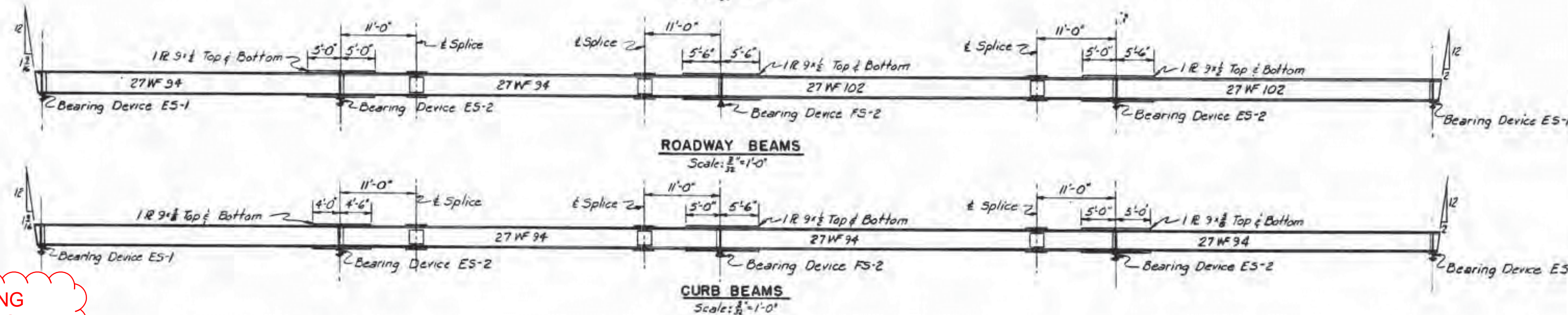
Elevations shown on Plan are to top surface of concrete

Use 7" bridge floor (non-composite) as shown on Standard Drawing 14.



Note: Elevations shown on Framing Plan are to top of beam flange.

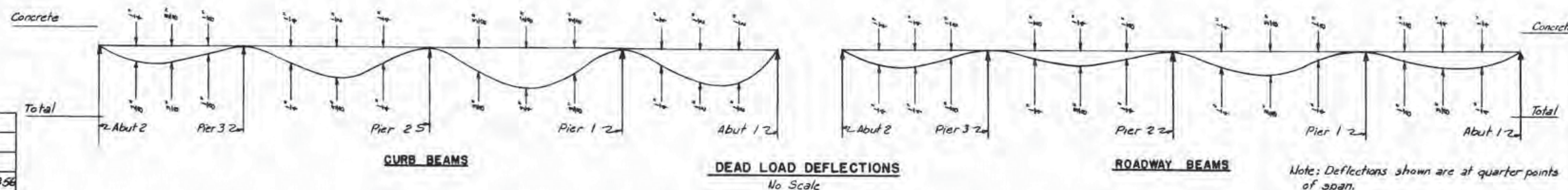
Type of diaphragm is noted thus: "B" & "D"



Note: Bevel of beam ends is true with respect to the beam axis only.

All bearing stiffeners at points of support are $L 7 \times 4 \times \frac{1}{2}$

REFERENCE PLAN FOR LIMITS OF EXISTING HAUNCH REMOVAL. NO ADDITIONAL BRIDGE WORK SHOWN ON THESE PLANS REQUIRED. PLANS NOT TO SCALE.



DRAWING 53.03.03

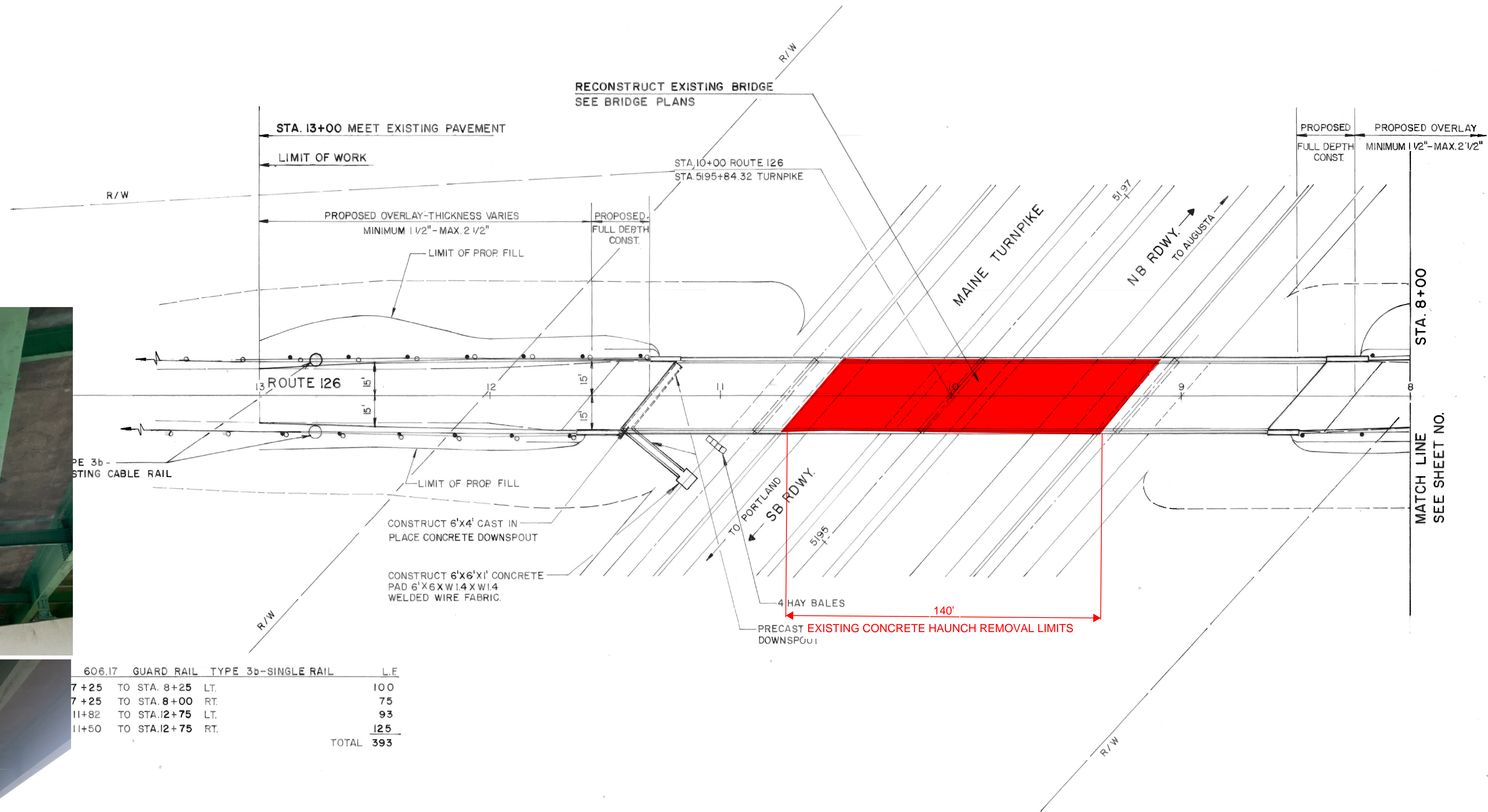
BY	DATE				
MADE	WCM	3-17-54			
TRACED					
CHECKED	F.S.Ur.	3-17-54	1	As-Built	HHH/MB/56
IN CHARGE OF	I.D.S.K.		No.	REVISION	BY DATE

MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
 SECTION 2 — PORTLAND TO AUGUSTA

STRUCTURE NO. 53 TURNPIKE UNDER
 HACKETT ROAD
 STA 3884+06.15
 SUPERSTRUCTURE

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
 CONSULTING ENGINEERS
 NEW YORK KANSAS CITY

SCALE: As noted
 CONTRACT NO. _____
 SHEET NO. 292 OF 382



606.17	GUARD RAIL TYPE 3b-SINGLE RAIL	L.F.
7+25	TO STA. 8+25 LT.	100
7+25	TO STA. 8+00 RT.	75
11+82	TO STA. 12+75 LT.	93
11+50	TO STA. 12+75 RT.	125
TOTAL		393

606.174	GUARD RAIL ATTACHMENT TYPE "A"	E.A.
8+25	TO STA. 8+50 LT.	1
8+00	TO STA. 8+25 RT.	1
11+57	TO STA. 11+82 LT.	1
11+25	TO STA. 11+50 RT.	1
TOTAL		4

LEGEND

- ○ ○ EXISTING CABLE GUARDRAIL WOOL POST/ STEEL POST.
- ○ ○ EXISTING DOUBLE RAIL GUARDRAIL STEEL POST.
- ● ● PROPOSED SINGLE RAIL GUARDRAIL STEEL POST.
- — — WOVEN WIRE FENCE

REFERENCE PLAN FOR LIMITS OF EXISTING HAUNCH REMOVAL. NO ADDITIONAL BRIDGE WORK SHOWN ON THESE PLANS REQUIRED. PLANS NOT TO SCALE.



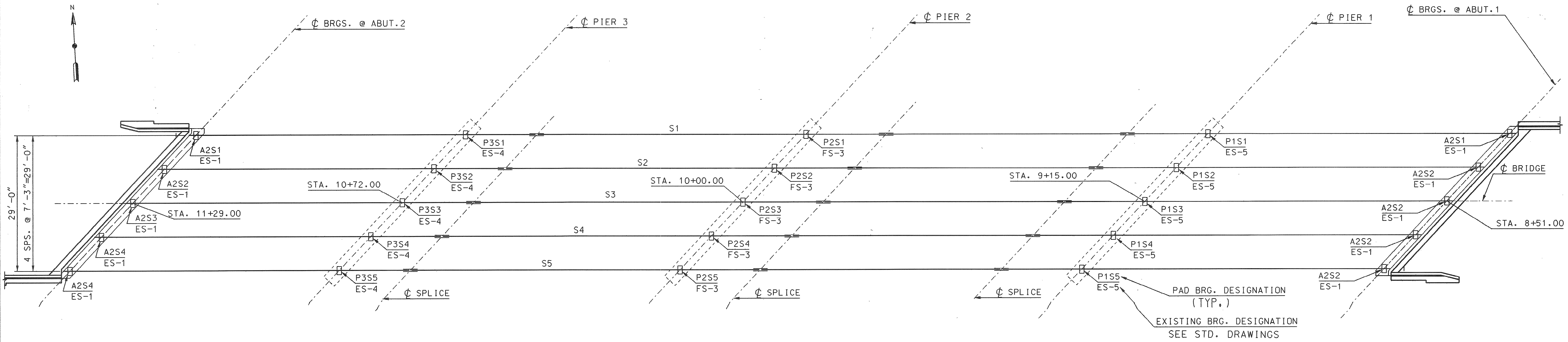
No.	Revision	By:	Date:
		Designed	MN 4/94
		Drawn	CW 4/94
		Checked	RJD 4/94
		In charge of:	RAL

Maine Turnpike Authority
Maine Turnpike

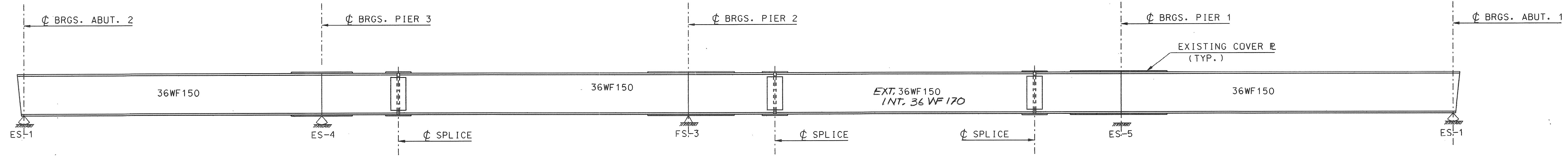
BRIDGE DECK REPLACEMENT
ROUTE 126
PLAN SITE

HNTB HOWARD NEEDLES TAMMEN & BERGENDOFF
ARCHITECTS ENGINEERS PLANNERS

Contract **94.3** Sheet No. 14 of 60



FRAMING PLAN
1" = 10'-0"



TYPICAL BEAM ELEVATION
N.T.S.

PAD TABLE				
LOCATION	Δ (FT.)	MASONRY #	PAD SIZE	PAD REINFORCEMENT
A1S1	0.0000			
A1S2	0.0000			
A1S3	0.0000			
A1S4	0.0000			
A1S5	0.0000			
P1S1	0.0000			
P1S2	0.0000			
P1S3	0.0000			
P1S4	0.0000			
P1S5	0.0000			
P2S1	0.0000			
P2S2	0.0000			
P2S3	0.0000			
P2S4	0.0000			
P2S5	0.0000			
P3S1	0.0000			
P3S2	0.0000			
P3S3	0.0000			
P3S4	0.0000			
P3S5	0.0000			
A2S1	0.0000			
A2S2	0.0000			
A2S3	0.0000			
A2S4	0.0000			
A2S5	0.0000			

FOR PAD DETAIL, SEE SHEET S-27
 FOR REINFORCING SCHEDULE, SEE SHEETS S27 & S28

EXISTING TO REMAIN

EXISTING TO REMAIN

REFERENCE PLAN FOR LIMITS OF EXISTING HAUNCH REMOVAL. NO ADDITIONAL BRIDGE WORK SHOWN ON THESE PLANS REQUIRED. PLANS NOT TO SCALE.

NOTES:

1. SHEAR STUDS ARE NOT REQUIRED UNLESS SHOWN.
2. THERE IS NO PROFILE CHANGE FOR THIS BRIDGE.
3. EXISTING BEARINGS. TO REMAIN.

Maine Turnpike Authority
Maine Turnpike

**RT. 126
OVER
MAINE TURNPIKE
FRAMING PLAN**

HOWARD NEEDLES TAMMEN & BERGENDOFF
ARCHITECTS ENGINEERS PLANNERS

Contract 94.3	Sheet No. S-25
	55 of 60

No.	Revision	By	Date	In Charge Of:

Design Filename: B:\17050503\17050503\17050503.dwg
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 Generated by: H.H.