

MAINE TURNPIKE AUTHORITY

ADDENDUM NO. 1

CONTRACT 2024.07

BRIDGE REPAIRS

ST. LAWRENCE & ATLANTIC OVERPASS (MM 74.50)

BLACKSTRAP ROAD OVERPASS (MM FS0.40)

MCRR OVERPASS (EB/WB) (MM FS0.50/FS0.51)

GRAY ROAD (ROUTE 26 & 100) UNDERPASS (MM FS0.70)

A pre-bid meeting was held on April 9, 2024. The pre-bid meeting agenda, presentation and sign-in sheet are attached to this addendum. **All questions must be submitted by 5:00pm on Tuesday April 16, 2024 to be considered.**

The bid opening date has been moved to April 23, 2024. See below.

Make the following changes to the bid documents:

In the Contract Documents, Notice to Contractors, in the first paragraph **CHANGE** April 18, 2024 to April 23, 2024.

Note: The above items shall be considered as part of the bid submittal.

Attachments

- Pre-Bid Agenda (5 pages)
- Pre-Bid Presentation (25 pages)
- Pre-Bid Sign-In Sheet (1 page)

The total number of pages included with this addendum is thirty three (33).

All bidders are requested to acknowledge the receipt of the Addendum No. 1 by signing below and faxing this sheet to Nate Carll, Purchasing Department, (207) 871-7739. Bidders are also required to acknowledge receipt of this Addendum No. 1 on Page P-6 of the bid package.

Business Name

Print Name and Title

Signature

Date

April 10, 2024

Very truly yours,

MAINE TURNPIKE AUTHORITY

Purchasing Manager
Maine Turnpike Authority

MAINE TURNPIKE AUTHORITY

Pre-Bid Conference

CONTRACT 2024.07

BRIDGE REPAIRS

ST. LAWRENCE & ATLANTIC OVERPASS (MM 74.50)

BLACKSTRAP ROAD OVERPASS (MM FS0.40)

MCRR OVERPASS (EB/WB) (MM FS0.50/FS0.51)

GRAY ROAD (ROUTE 26 & 100) UNDERPASS (MM FS0.70)

April 9, 2024 1:00 PM

1) Location:

The general limits of work are as shown in the Contract Plans. The St. Lawrence & Atlantic Overpass is located at Mile 74.50 of the Maine Turnpike. The Blackstrap Road Overpass is located at MM FS0.40 of the Falmouth Spur. The MCRR Overpass is located at MM FS0.50/FS0.51 of the Falmouth Spur. The Gray Road (Route 26 & 100) Underpass is located at MM FS 0.70 of the Falmouth Spur.

2) General Description:

The work consists of bridge repairs at four locations. At the St. Lawrence & Atlantic Overpass in the City of Auburn, the work generally includes pavement and high performance membrane removal and replacement, approach paving, repairs to impact damage on the bridge rail, bridge joint repairs and miscellaneous superstructure repairs. At the Blackstrap Road Overpass in the Town of Falmouth, the work generally consists of joint repairs and header installations, pavement mill and overlay, and miscellaneous superstructure repairs. At the MCRR Overpass (EB/WB) in the Town of Falmouth, the work generally consists of joint repairs and header installations, pavement mill and overlay, and miscellaneous superstructure repairs. At the Gray Road (Route 26 & 100) Underpass in the Town of Falmouth, the work generally consists of joint modifications and header installations, adjacent pavement replacement, removing existing girder haunches, abutment patch repairs, and miscellaneous superstructure and substructure repairs. The work also includes maintenance of traffic and all other work incidental thereto in accordance with the Plans and Specifications.

3) Bid: April 23, 2024

- a) Bid opening is ~~April 18, 2024~~ at 11:00 A.M. at MTA headquarters 2360 Congress Street, Portland.
- b) All bid and contractual questions shall be directed to Purchasing Department, Phone No. (207) 482-8115.
- c) All questions on plans and specifications shall be in writing and shall be directed to Nate Carll, Purchasing Manager, at (207) 871-7739 (fax) or email ncarll@maineturnpike.com.
- d) All questions must be submitted by 5:00pm on ~~Monday April 15, 2024~~ to be considered.

Tuesday April 16, 2024

4) Notification:

- a) Contractor shall notify and obtain approval from the Authority prior to visiting the Project site for field inspection. The contact person is Mr. Steve Tartre at (207) 482-8144 or startre@maineturnpike.com.

5) Construction Schedule/Prosecution of Work:

- a) MTA Board is scheduled to consider the Contract Award on April 25, 2024.
- b) At the St. Lawrence & Atlantic Overpass and Gray Road (Route 26 & 100) Underpass:
 - i) All work shall be substantially complete by November 1, 2024.
 - ii) All work shall be completed on or before November 22, 2024.
- c) At the Blackstrap Road Overpass and MCRR Overpass:
 - i) All work shall be substantially complete by October 31, 2025.
 - ii) All work shall be completed on or before November 21, 2025.
- d) Substantially complete shall be defined as:
 - i) All bridge repair work at all locations required by the Contract that requires shoulder and/or lane closures.
 - ii) All roads fully opened to two-way traffic including shoulders, surface pavement and pavement markings.
 - iii) Supplemental Liquidated damages on a calendar day basis in accordance with Subsection 107.8 shall be assessed for each calendar day that substantial completion is not achieved.
- e) The following restrictions shall be applied to the work at the St. Lawrence & Atlantic Overpass
 - i) The Authority has submitted a Right-of-Entry Application to G&W. The Contractor is responsible for reviewing the Right-of-Entry Application and complying with all requirements, including all insurance requirements. The Contractor shall submit an updated Right-of-Entry Application to initiate coordination with the Railroad.
 - ii) The Contractor shall coordinate all work at the St. Lawrence & Atlantic Overpass with the Genesee & Wyoming Railroad Services, Inc. (G&W) prior to beginning any work. The Contractor shall not enter the railroad right-of-way without permission of the railroad.
 - iii) All track access and track protection shall be scheduled by the Contractor. The Contractor shall provide invoices for track protection to the Authority and the Authority will make payment for the track protection directly to the Railroad.
 - iv) The Contractor shall be allowed to close each lane for a maximum of twenty-eight (28) consecutive calendar days. Supplemental liquidated damages on a calendar day basis in accordance with Subsection 107.8 shall be assessed for each calendar day that each lane is closed in excess of twenty-eight (28) consecutive calendar days.
- f) The following restrictions shall be applied to the work at the Blackstrap Road Overpass:
 - i) The Contractor shall be allowed to close each lane for a maximum of twenty-eight (28) consecutive calendar days. Supplemental liquidated damages on a calendar day basis in accordance with Subsection 107.8 shall be assessed for each calendar day that each lane is closed in excess of twenty-eight (28) consecutive calendar days.
- g) The following restrictions shall be applied to the work at the MCRR Overpass:
 - i) The Contractor shall be allowed to close each lane for a maximum of twenty-eight (28) consecutive calendar days. Supplemental liquidated damages on a calendar day basis in accordance with Subsection 107.8 shall be assessed for each calendar day that each lane is closed in excess of twenty-eight (28) consecutive calendar days.

- ii) The Contractor shall complete all work from the topside of the bridge and the approaches. The Contractor shall not do any work or access the bridge from below.
- h) The following restrictions shall be applied to the work at the Gray Road (Route 26 & 100) Underpass:
 - i) The Contractor shall not do any work requiring lane closures of the Falmouth Spur while the Maintenance of Traffic at the Blackstrap Road Overpass or MCRR Overpass is in place.
 - ii) Lanes on Gray Road (Route 26 & 100) shall not be closed until 8:00 p.m. and shall be reopened the following day by 6:00 a.m. Supplemental liquidated damages of \$500 per hour shall be assessed for each hour that any lane is closed past 6:00 a.m. each day.
 - iii) The Contractor shall close each lane on Gray Road (Route 26 & 100) for a maximum of five nights per lane. The maximum five nights of closures per lane are not required to be consecutive nights.
- 6) Maine Department of Labor – Fair Hourly Wages (Special Provision 104.3.8)
 - a) Heavy and Bridge wages for Cumberland County and Androscoggin County are included in the Contrack Book.
- 7) Utility Coordination (Special Provision 104.4.6)
 - a) Existing utilities were not located for this project.
 - b) The Contractor shall contact Dig Safe and any non-member utility operators through OK-TO-DIG prior to any work.
 - c) Utility adjustments are not anticipated as part of this project. If any unexpected utility relocations become necessary, they will be scheduled in compliance with Section 104 of the Standard Specifications and will be done by the utilities in conjunction with the work by the Contractor.
- 8) Cooperation With Other Contractors (Special Provision 104.4.7)
 - a) MTA Contract 2023.03 – Route 122 Underpass Superstructure Replacement, MM 74.00
 - i) The lane closures for the St. Lawrence & Atlantic Overpass crosses under the Poland Spring Road Underpass. MTA Contract 2023.03 utilizes this road for the detour.
 - b) MTA Contract 2024.04 – Exit 75 Interchange Rehabilitation
 - i) The wide load detour for the St. Lawrence & Atlantic Overpass uses Exit 75 Southbound. MTA Contract 2024.04 will have up to 4 nighttime closures for milling/paving.
- 9) Lead Paint (Special Provision 105.2.4.2)
 - a) The Contractor shall note that the existing bridge structures may contain lead based paint. The Contractor shall institute every precaution when working with materials coated with lead based paints.
- 10) Permit Requirements (Special Provisions 105.8.2 and 105.8.3)
 - a) The Project is subject to the Stormwater Memorandum of Agreement for Stormwater Management Between the Maine Department of Transportation, Maine Turnpike Authority, and Maine Department of Environmental Protection (Stormwater MOA).
 - b) This Project is also subject to the requirements of the Maine Pollutant Discharge and Elimination System (MPDES) General Permit for the Discharge of Stormwater from MTA’s Municipal Separate Storm Sewer Systems (MS4).

- c) Compliance with the erosion and sedimentation control requirements outlined in this Contract is required by the Contractor.

11) General Requirements

- a) U-Turns at toll plazas and median openings not allowed. (Supplemental Specification 105.5.1)
- b) Contractor access to and from the mainline shall not negatively impact mainline traffic flow. The Contractor may be required to establish lane closures to provide for safe access. Refer to Special Provision 652, Specific Project Maintenance of Traffic Requirements, for lane closure requirements and restrictions.
- c) All vehicles used on the Project, including concrete delivery trucks, shall be equipped with amber flashing beacons in accordance with Supplemental Specification 652.3.4.
- d) Class III safety vests must be worn at all times.

12) Traffic Control (Special Provision Section 652):

- a) Special Provision Section 652 replaces the MaineDOT Standard Specification 2014 Edition and MTA 2016 Supplemental Specification Section 652.
- b) Substantive revisions have been bolded in the 652 SP.
- c) Contractor is responsible for supplying all traffic control devices.
- d) Contractor is responsible for placement, relocation, removal and maintenance of traffic control devices. Maintenance of traffic control devices is a 24-hour a day, seven days per week responsibility. Contractor shall inspect devices as required.
- e) Temporary detours, lane closures and construction phasing shall be established and maintained at all times in accordance with the details shown on the Plans and the requirements of the Special Provisions.
 - i) A truck mounted attenuator shall be utilized for all lane closures on the Turnpike mainline, shall be utilized for all temporary shoulder closures (i.e. closures that do not include temporary concrete barrier) on the Turnpike mainline, and other construction operations where workers are exposed to traffic and not protected by positive means.
 - ii) A truck mounted attenuator shall be utilized for all work completed behind guardrail if that work is being completed within the deflection zone of the guardrail (within four feet behind the guardrail post).
 - iii) Automated trailer mounted speed limit signs shall only be used when a work zone speed limit is in place and shall be required when the work zone speed limit is active.
 - iv) Truck mounted attenuators and automated trailer mounted speed limit signs are incidental to Item 652.361, Maintenance of Traffic Control Devices.
- f) Turnpike/Falmouth Spur Lane closures
 - i) One lane of traffic flowing in each direction at all times. Minimum traveling width of 14' required unless otherwise shown in the Plans. Minimum traveling width of 13' is permitted at the St. Lawrence & Atlantic Overpass.
 - ii) All lane closures require the approval of the Resident. The Resident is required to submit a request to MTA for lane closures by noon on Thursday for lane closures needed for the following week. The Contractor shall plan the work and requests for lane closures accordingly. Requests are subject to approval by MTA.
 - iii) Supplemental liquidated damages shall be assessed at \$1,000 per minute for every minute that a temporary lane closure is in place outside of the allowable times.

- g) All signs, which do not apply to current construction activity, shall be 100% covered or removed in accordance with the plans. This includes any speed limit signs when work zone speed is in operation.
- h) All temporary striping, including covering existing striping with blackout tape, shall be incidental to Item 652.361, Maintenance of Traffic Control Devices.
- i) Traffic control devices shall be NCHRP 350 compliant. All traffic control devices shall meet MASH guidelines if the date of manufacture was after December 31, 2019.

13) Specific Contract Items:

- a) The existing unreinforced concrete girder haunches at the Gray Road (Route 26 & 100) Underpass shall be removed using pneumatic chipping hammers of a size approved by the Resident, or other methods approved by the Resident. The Contractor shall remove the haunches to an approximate 45° angle, as shown in the Plans. The Contractor is not required to grind the removal line to a smooth finish.
- b) Elastomeric Concrete Headers shall be paid under Item 518.86 Elastomeric Concrete Header Repair. Prior to placing the elastomeric concrete, the Contractor shall remove any areas of deteriorated concrete in accordance with Supplemental Specification Section 518 and as directed by the Resident. If repairs to the top of the backwall or end of the deck are needed, the Contractor shall repair with elastomeric concrete and repairs shall be paid under Item 518.864 Elastomeric Concrete Header Repair. If repair depth is greater than 3” below the header, the repairs may be done with AAA concrete at the direction of the Resident and paid under Item 518.80, Partial Depth Concrete Deck Repairs. The Elastomeric Concrete Header Repair quantity in the plans is based on an average depth of 4” which includes 3” for the header and additional depth for repairs.

14) Questions:



**Maine
Turnpike
Authority**

MTA Contract 2024.07



Bridge Repairs
St. Lawrence & Atlantic Overpass
Blackstrap Road Overpass
MCRR Overpass
Gray Road (Route 26 & 100) Underpass

Pre-Bid Conference

April 9, 2024

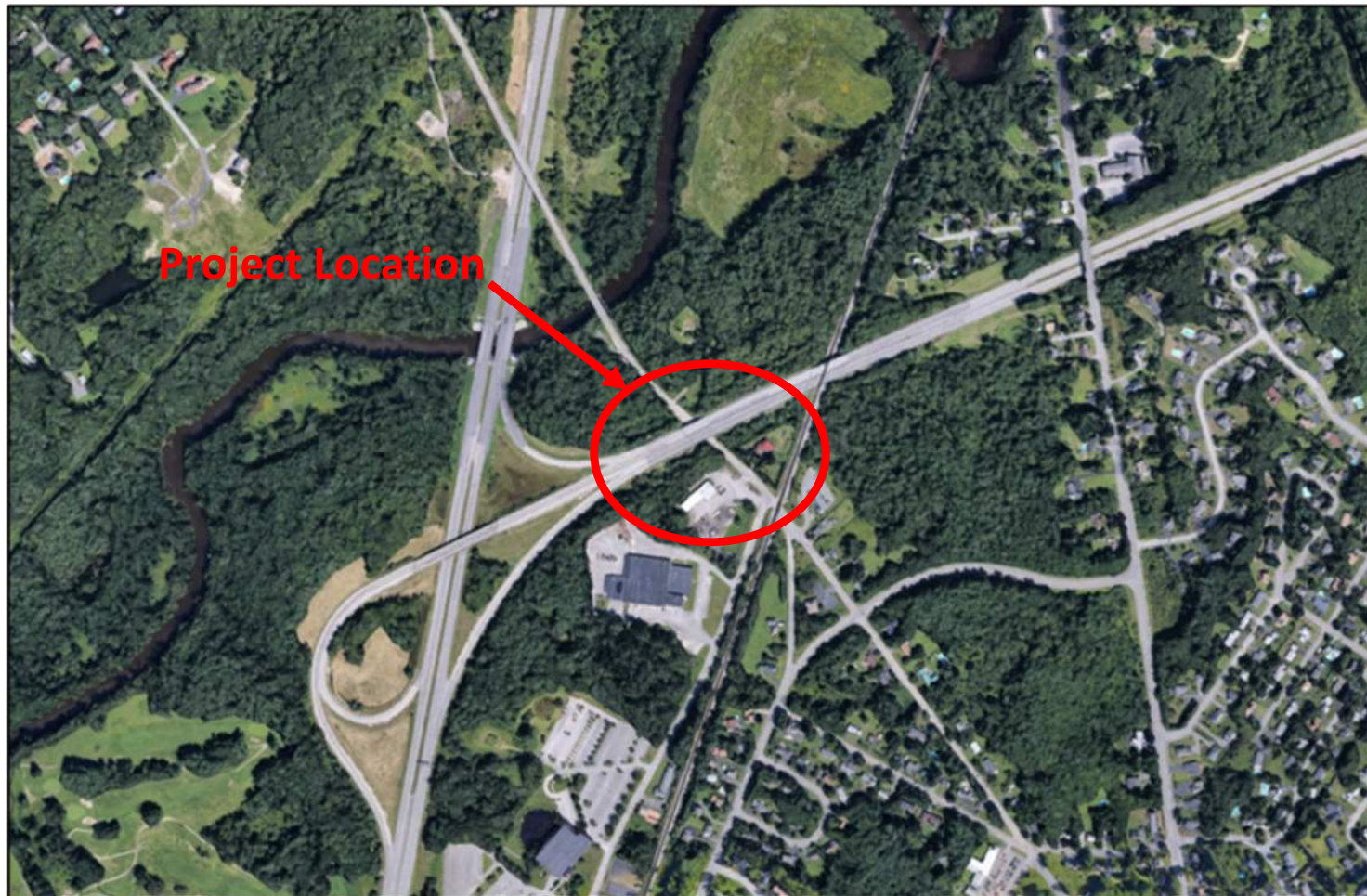


1) Location – St. Lawrence & Atlantic Overpass (74.50)



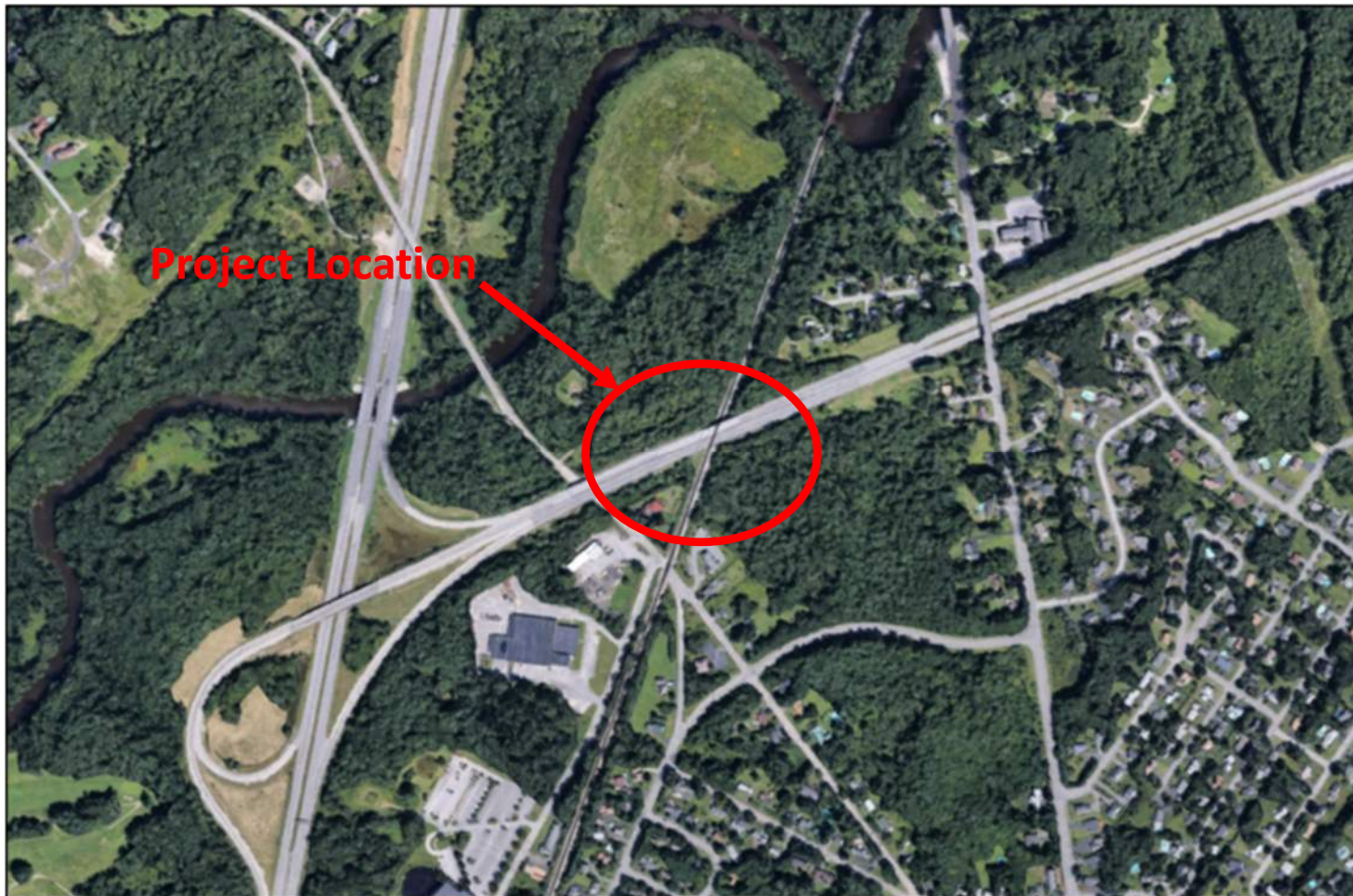


1) Location – Blackstrap Road Overpass (FS0.40)



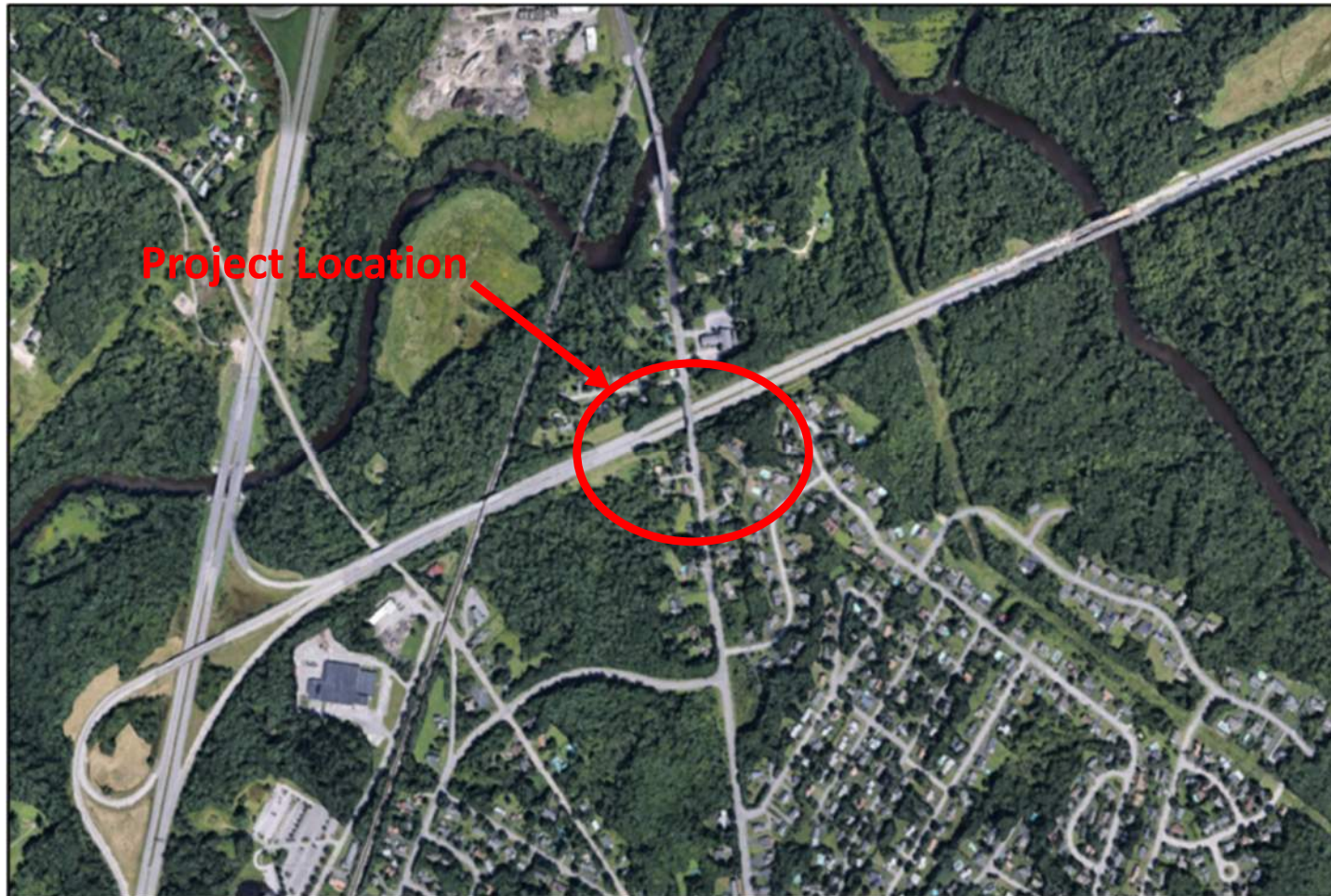


1) Location – MCRR Overpass (FS0.50/FS0.51)



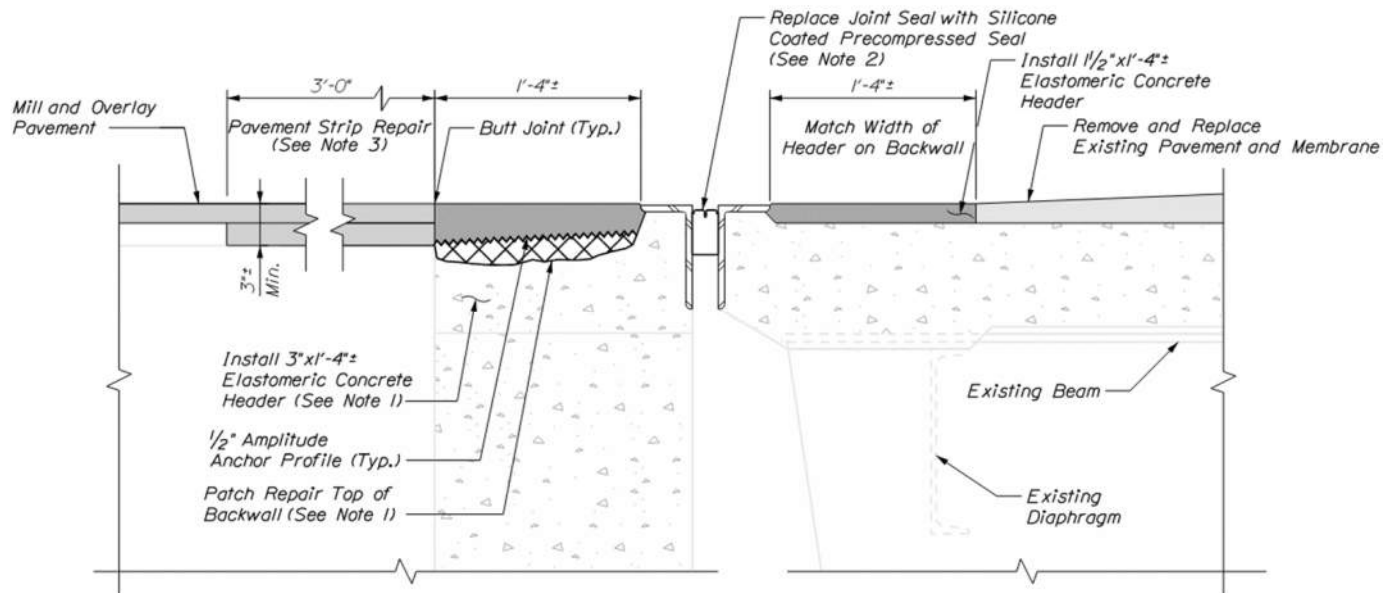


1) Location – Gray Road (Route 26 & 100) Underpass (FS0.70)



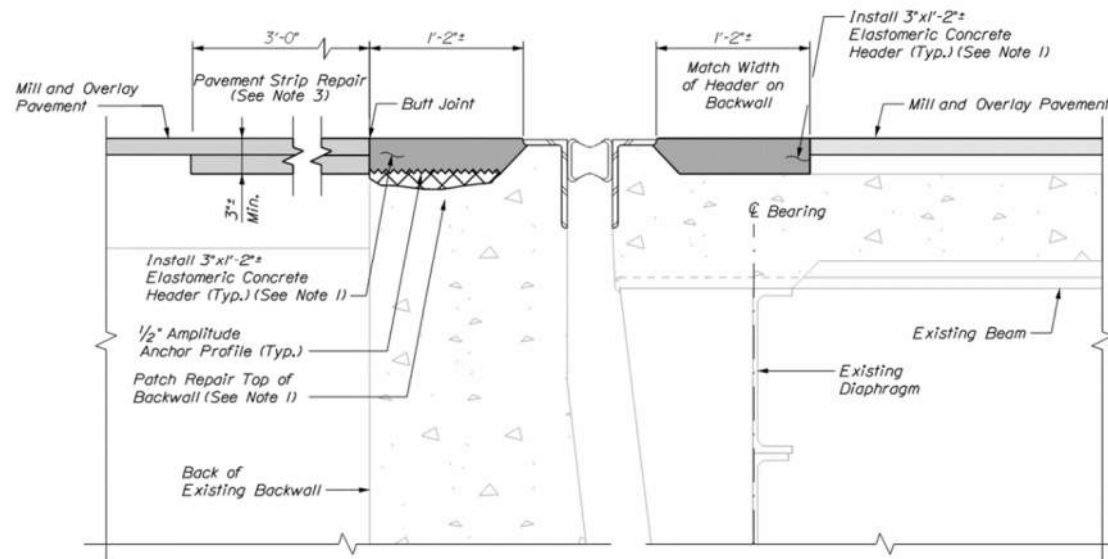
2) General Description

- At the St. Lawrence & Atlantic Overpass, the work includes:
 - Pavement and high performance membrane removal and replacement
 - Repairs to impact damage on the bridge rail
 - Bridge joint repairs
 - Miscellaneous superstructure repairs



2) General Description (Cont'd)

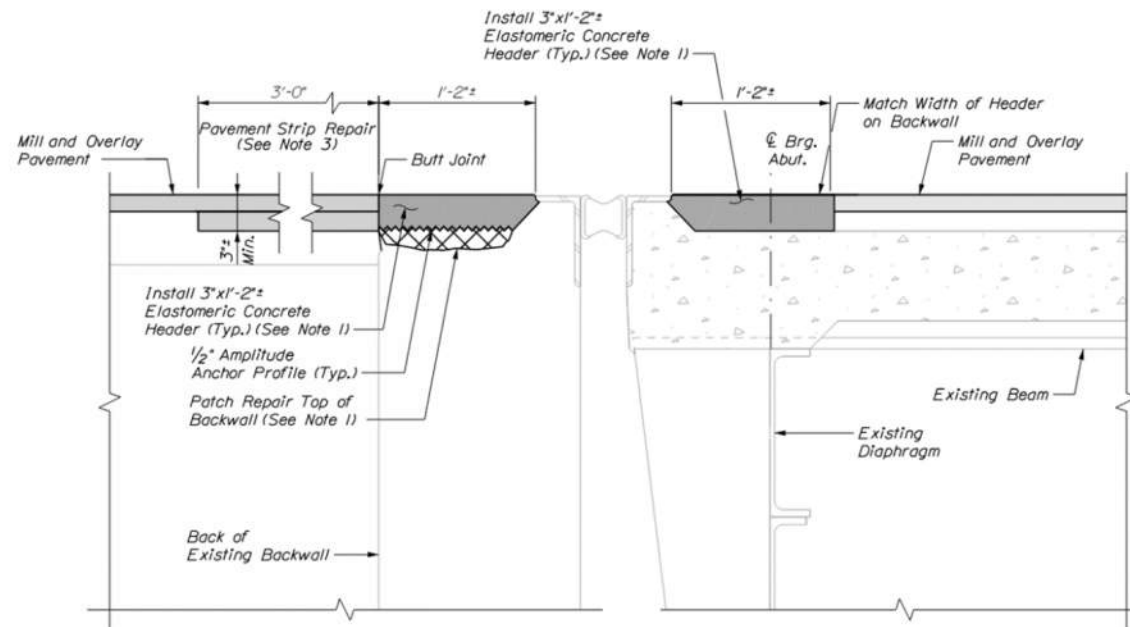
- At the Blackstrap Road Overpass, the work includes:
 - Pavement mill and overlay
 - Joint repairs and header installations
 - Miscellaneous superstructure repairs





2) General Description (Cont'd)

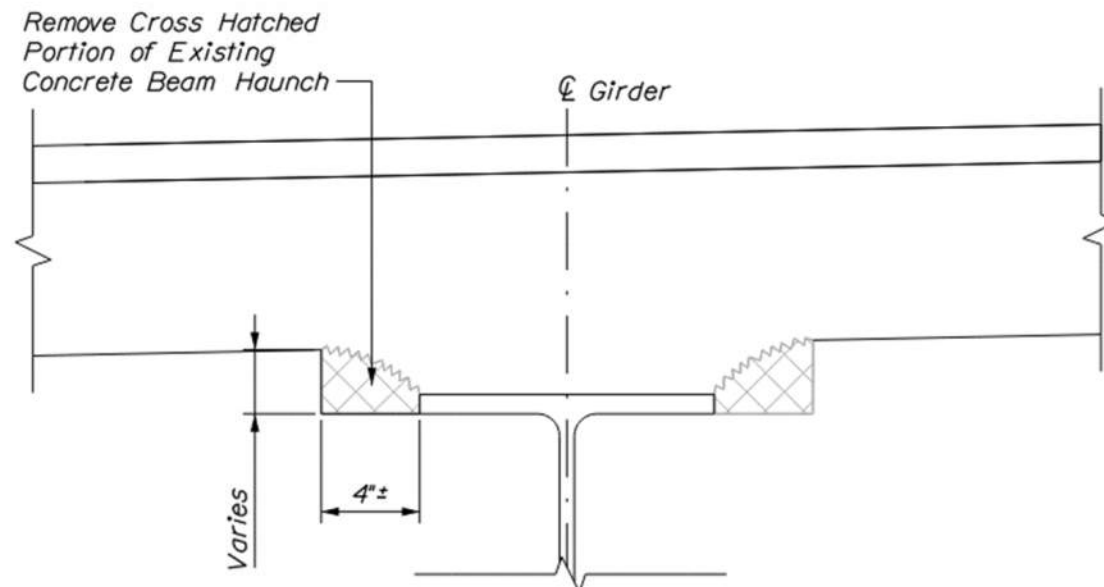
- At the MCRR Overpass, the work includes:
 - Pavement mill and overlay
 - Joint repairs and header installations
 - Miscellaneous superstructure repairs





2) General Description (Cont'd)

- At the Gray Road (Route 26 & 100) Underpass, the work includes:
 - Joint modifications and header installations
 - Adjacent pavement replacement
 - Removing existing girder haunches
 - Abutment patch repairs
 - Miscellaneous superstructure and substructure repairs





3) Bid

April 23, 2024

- **Bid opening:** ~~April 18, 2024~~, at 11:00 A.M.
- All **bid & contractual questions** direct to Purchasing Dept.
 - Ph#: (207) 482-8115
- All **plan & spec questions** submit in writing directed to Nate Carll, Purchasing Manager
 - Fax: (207) 871-7739
 - Email: ncarll@maineturnpike.com
- All questions **must be submitted** by 5:00 pm on ~~Monday April 15, 2024~~
Tuesday April 16, 2024



4) Notification

- Contractor shall **notify and obtain approval** from the Authority (Steve Tartre) **prior to visiting the Project site** for field inspection.
 - Ph#: (207) 482-8144
 - email: startre@maineturnpike.com



5) Construction Schedule/Prosecution of Work

- MTA Board is scheduled to consider the **Contract Award on April 25, 2024.**

- At the St. Lawrence & Atlantic Overpass and Gray Road (Route 26 & 100) Underpass:
 - Substantially complete: **November 1, 2024**
 - All work complete: **November 22, 2024**

- At the Blackstrap Road Overpass and MCRR Overpass:
 - Substantially complete: **October 31, 2025**
 - All work complete: **November 21, 2025**



5) Construction Schedule/Prosecution of Work (Cont'd)

- At the St. Lawrence & Atlantic Overpass:
 - The Authority has submitted a Right-of-Entry Application to G&W. The Contractor is responsible for reviewing the Right-of-Entry Application and complying with all requirements, including all insurance requirements. The Contractor shall submit an updated Right-of-Entry Application to initiate coordination with the Railroad.
 - The Contractor shall coordinate all work at the St. Lawrence & Atlantic Overpass with the Genesee & Wyoming Railroad Services, Inc. (G&W) prior to beginning any work.
 - All track access and track protection shall be scheduled by the Contractor. The Contractor shall provide invoices for track protection to the Authority and the Authority will make payment for the track protection directly to the Railroad.
 - The Contractor shall be allowed to close each lane for a maximum of twenty-eight (28) consecutive calendar days.



5) Construction Schedule/Prosecution of Work (Cont'd)

- At the Blackstrap Road Overpass :
 - The Contractor shall be allowed to close each lane for a maximum of twenty-eight (28) consecutive calendar days.

- At the MCRR Overpass:
 - The Contractor shall be allowed to close each lane for a maximum of twenty-eight (28) consecutive calendar days.
 - The Contractor shall complete all work from the topside of the bridge and the approaches. The Contractor shall not do any work or access the bridge from below.



5) Construction Schedule/Prosecution of Work (Cont'd)

- At the Gray Road (Route 26 & 100) Underpass:
 - The Contractor shall not do any work requiring lane closures of the Falmouth Spur while the Maintenance of Traffic at the Blackstrap Road Overpass or MCRR Overpass is in place.
 - Lanes on Gray Road (Route 26 & 100) shall not be closed until 8:00 p.m. and shall be reopened the following day by 6:00 a.m.
 - The Contractor shall close each lane on Gray Road (Route 26 & 100) for a maximum of five nights per lane. The maximum five nights of closures per lane are not required to be consecutive nights.



6) Maine Department of Labor – Fair Hourly Wages

- Included Wage Rates:
 - Cumberland County
 - Androscoggin County

7) Utility Coordination

- Existing utilities were not located for this project.
- The Contractor shall **contact Dig Safe** and any non-member utility operators through **OK-TO-DIG prior to any work.**
- If any **unexpected utility relocations** become necessary, they will be scheduled **in compliance with Section 104** of the Standard Specifications and will be **done by the utilities** in conjunction with the work by the Contractor.



8) Cooperation With Other Contractors

- **MTA Contract 2023.03** – Route 122 Underpass Superstructure Replacement, MM 74.00
 - The lane closures for the St. Lawerance & Atlantic Overpass crosses under the Poland Spring Road Underpass. MTA Contract 2023.03 utilizes this road for the detour.
- **MTA Contract 2024.04** – Exit 75 Interchange Rehabilitation
 - The wide load detour for the St. Lawerance & Atlantic Overpass uses Exit 75 Southbound. MTA Contract 2024.04 will have up to 4 nighttime closures for milling/paving

9) Lead Paint

- The Contractor shall note that the **existing bridge structures may contain lead-based paint**. The Contractor shall **institute every precaution** when working with materials coated with lead-based paints.



10) Permit Requirements

- The Project is subject to the **Stormwater Memorandum of Agreement for Stormwater Management** Between the Maine Department of Transportation, Maine Turnpike Authority, and Maine Department of Environmental Protection.
- This Project is also subject to the requirements of the **Maine Pollutant Discharge and Elimination System (MPDES)** General Permit for the Discharge of Stormwater from MTA's Municipal Separate Storm Sewer Systems (MS4).
- **Compliance with the erosion and sedimentation control** requirements outlined in this Contract is required by the Contractor.



11) General Requirements

- **U-Turns** at toll plazas and median openings **not allowed**.
- **Contractor access** to and from the mainline **shall not negatively impact** mainline traffic flow. The Contractor may be required to establish lane closures to provide for safe access. Refer to Special Provision 652.
- **All vehicles** used on the Project, including concrete delivery trucks, shall be **equipped with amber flashing beacons** in accordance with Supplemental Specification 652.3.4.
- **Class III safety vests** must be worn at all times.



12) Traffic Control (Special Provision Section 652)

- SP 652 **replaces** the MaineDOT Standard Specs 2014 Edition and MTA 2016 Supplemental Specs.
- Substantive **revisions have been bolded** in the 652 SP.
- Contractor is responsible for supplying all traffic control devices.
- Contractor is responsible for placement, relocation, removal and maintenance of traffic control devices. Maintenance of traffic control devices is a **24-hour a day, seven days per week** responsibility. Contractor shall inspect devices as required.



12) Traffic Control (Special Provision Section 652)

- Temporary detours, lane closures and construction phasing shall be established and **maintained at all times**.
 - **A truck mounted attenuator** shall be utilized for **all lane closures** on the Turnpike mainline, shall be utilized for **all temporary shoulder closures** (i.e. closures that do not include temporary concrete barrier) on the Turnpike mainline, and other construction operations where workers are exposed to traffic and not protected by positive means.
 - **A truck mounted attenuator** shall be utilized for **all work completed behind guardrail** if that work is being completed **within the deflection zone** of the guardrail (within four feet behind the guardrail post).
 - **Automated trailer mounted speed limit signs** shall only be used when a work zone speed limit is in place and shall be required when the work zone speed limit is active.
 - Truck mounted attenuators and automated trailer mounted speed limit signs are incidental to Item 652.361, Maintenance of Traffic Control Devices.



12) Traffic Control (Special Provision Section 652)

- Turnpike Lane closures
 - **One lane of traffic flowing** in each direction at all times. **Minimum traveling width of 14'** required unless otherwise shown in the Plans. **Minimum traveling width of 13'** is permitted at the St. Lawrence & Atlantic Overpass.
 - All lane **closures require the approval of the Resident**. The Resident is required to submit a request to MTA for lane closures by **noon on Thursday** for lane closures needed for the following week. The Contractor shall plan the work and requests for lane closures accordingly. Requests are subject to approval by MTA.
 - **Supplemental liquidated damages** shall be assessed at **\$1,000 per minute** for every minute that a temporary lane closure is in place outside of the allowable times.



12) Traffic Control (Special Provision Section 652)

- All signs, which do not apply to current construction activity, **shall be 100% covered or removed** in accordance with the plans. This includes any speed limit signs when work zone speed is in operation.
- All **temporary striping, including covering existing striping with blackout tape**, shall be incidental to Item 652.361, Maintenance of Traffic Control Devices.
- Traffic control devices **shall be NCHRP 350 compliant**. All traffic control devices **shall meet MASH guidelines** if the date of manufacture was **after December 31, 2019**.



13) Specific Contract Items

- The existing unreinforced concrete girder haunches at the Gray Road (Route 26 & 100) Underpass shall be removed. The Contractor shall remove the haunches to an approximate 45° angle. The Contractor is not required to grind the removal line to a smooth finish.
- Elastomeric Concrete Headers shall be paid under Item 518.86 Elastomeric Concrete Header Repair. Prior to placing the elastomeric concrete, the Contractor shall remove any areas of deteriorated concrete. If repairs to the top of the backwall or end of the deck are needed, the Contractor shall repair with elastomeric concrete and repairs shall be paid under Item 518.864 Elastomeric Concrete Header Repair. If repair depth is greater than 3" below the header, the repairs may be done with AAA concrete at the direction of the Resident and paid under Item 518.80, Partial Depth Concrete Deck Repairs. The Elastomeric Concrete Header Repair quantity in the plans is based on an average depth of 4" which includes 3" for the header and additional depth for repairs



**Maine
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Questions?

