

**MAINE TURNPIKE AUTHORITY**

**ADDENDUM NO. 2**

**CONTRACT 2018.20**

**YORK TOLL PLAZA**  
**MILE 8.8**

The bid opening date is October 11, 2018 at 11am.

The following changes are made to the Proposal, Plans and Specifications.

**PLANS**

Plan Sheet 47 of 489, "Construction Staging Plan 3 - Phase 3" is deleted and replaced in its entirety with attached revised sheet 47. Note was added, "Prior to shifting traffic from phase 2 to phase 3, highway lighting mounted on barrier will be functioning".

**SPECIFICATIONS**

Page SP-19, Special Provision 107.4.6 - Prosecution of Work: added:

"The winter traffic configuration, where noted, is from November 15 through April 15 except for Phases A and B".

Page SP-21, Special Provision 107.4.6 - Prosecution of Work, Phase C, first sentence: revised, "Phase C provides for the long term configuration, including winter shutdown, of Phase 2." to "Phase C provides for the long term configuration, including winter traffic configuration from November 15 to April 15, of Phase 2.".

Page SP-22, Special Provision 107.4.6 - Prosecution of Work, third paragraph, second sentence: added "without prior approval from the Resident and coordination with system Integrator."

Page SP-23, Special Provision 107.4.6 - Prosecution of Work, Incentive/Disincentive Payments, second paragraph, added "April 1 to May 3, 2019 or"

Special Provision SP-19 to SP-26 deleted and replaced SP-19 to SP-26.

Specification Section 10200 - Miscellaneous, paragraphs 2.09 and 3.05 has been deleted. 10200-1 to 10200-4 deleted and replaced 10200-1 to 10200-4.

**QUESTIONS**

The following are questions submitted to the Maine Turnpike Authority in writing. Answers to the questions are noted. Bidders shall utilize this information in preparing their bid.

1. What type or types of mechanical couplers are acceptable in the drilled shafts?

**Answer: Tension couplers conforming to Subsection 503.07 of the Specifications are acceptable.**

2. Are the bars, spirals, & couplers in the ORT space frame drilled shafts to be included in BI#503.14 or BI#626.3321?

**Answer: Bid Item 626.3321.**

3. Is the reinforcing steel for toll lane #3 structural slab to be the same as in toll lanes #2, #4, #7, & #8?

**Answer: Yes.**

4. Are the bars & mesh in the dumpster pad, sidewalk, & site concrete slabs epoxy coated or uncoated?

**Answer: Epoxy coated rebar and mesh is not required for the dumpster pad, sidewalk or site concrete slabs.**

5. Are the bars in BI#626.341 24" light standard foundations epoxy coated or uncoated?

**Answer: Epoxy-coated.**

6. Will the precast concrete supplier be responsible for the threaded bars that attach to the mechanical couplers in the precast stairway structure endwall & tunnel sidewalls (sheet S-27)?

**Answer: This would be a coordination issue between the contractor and his precast concrete manufacturer.**

7. Can we get a clear set of hardware requirements for all the aluminum storefront doors? Hardware set #1 in section 8700 calls for the doors to have electric strikes. This contradicts the use of electrified exit devices called out on the drawings for the tunnel doors. Also conflicting information as to the hinging requirements, aluminum door specification calls for Rixson pivots, drawing calls out butt hinges.

**Answer:**

**See sheet 472 of 489 for Plaza storefront doors:**

**HW-1 for Hardware Group No. 01**

**(Exterior single with electric mortise panic hardware x card reader)**

**Card Reader (Specified Elsewhere)**

**Power Supply**

**Door Contact**

**Drip Cap**

**Threshold**

**Door Sweep - Brush**

**Seals**

**Kick Plate**

**Surface Closer**

**Overhead Stop**

**Mortise Cylinder**

**Elec. Panic Hardware**

**Power Transfer**

**Hinge**

See sheet 483 of 489 for the Administration Building doors:  
HW-1 for Doors 101, 102 & 104

Pivots  
Card Reader (provided by the Authority)  
Electric Strike  
Power Supply  
Closer  
Exit Device with external Pull & Deadbolt  
Weatherstripping at Head & Jambs  
Door Bottom  
Threshold

HW-3 for Door 105

Pivots  
Exit Device with external Pull  
Closer  
Weatherstripping at Head and Jambs  
Door Bottom  
Door Stop

8. For the aluminum storefront door panels, it calls for slate blue smooth panels but it is missing the finish of the aluminum. For the Admin. Building calls out dark green panels. Please clarify the color of the Stair enclosure panels as well.  
**Answer: Color for aluminum finish of stair enclosures, stair enclosure doors, and pre-fabricated toll booths shall be clear anodized. Finish for all Stair Enclosure Door Panels shall be coordinated with aluminum finish of stair enclosure storefront system and pre-fabricated toll booths.**
9. Will all items supplied by MTA be installed by MTA with the exception of the Cash Drop units? Specifications are specific with the cash drop units but there is no other mention of the other MTA supplied items under Part 3 of the Execution.  
**Answer: MTA will install MTA-provided items, noted as "N.I.C."**
10. The Equipment List shown on sheets 475 & 476 have items that are not part of our contract. Are these Owner supplied and installed?  
**Answer: Yes, the items indicated as "N.I.C." will be provided and installed by the MTA.**
11. Will we need to provide in-wall blocking for item #19 of the Equipment List on sheet 475 & 476? If so, can we get a detail of what these shelves look like?  
**Answer: Yes. Provide one row of continuous 2 x 8 wood blocking at all shelving locations (#19) within the metal stud framing at 68" AFF to the center of the blocking.**
12. Is the intent for the spec section 10200 Part 2.10 aluminum grating to cover Areaway #1 & #2? The structural drawings call for hot dip galvanized steel grating in those areas. Please advise.  
**Answer: The grating at Administration Building areaway #1 and #2 is hot dip galvanized steel grating. See structural drawing S-56. Aluminum Grating in Specification Section 10200, paragraphs 2.09 and 3.05 has been deleted.**

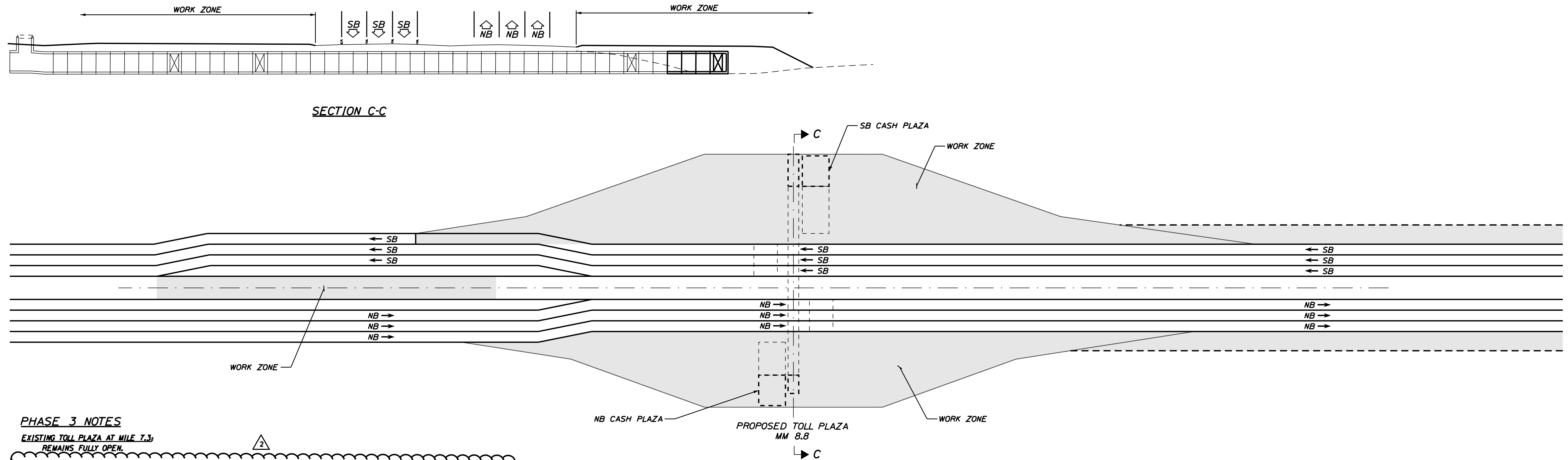
13. Should we include a 4" base should with the lockers?  
**Answer: Yes, provide a 4" high base consisting of 2x4 wood framing at 16" o.c. and ½" thick plywood sheathing at all locker locations. This base will receive VCB finish.**
14. Confirm the size to be 12" W x 18" deep for all lockers- both 2-tier and 6-tier.  
**Answer: 2-tier lockers shall be 12"W x 18"D x 72"H. 6-tier lockers shall be 12"W x 12"D x 72"H.**
15. Special provisions SP-22 States that the System integrator needs 70 Calendar days to complete testing of the southbound toll plaza. With the contract completion date set at June 21, 2021. That would require that the toll plazas be turned over for testing no later than April 12, 2021. This does not allow for any time in the spring, due to temperature requirements, for any final paving and striping. These two requirements combined create a required completion for the plaza work in the fall of 2020, which is unrealistic for the amount of work that needs to be completed.  
**Answer: We believe there is enough time in the schedule to complete the project within the dates specified. SP-22 has been revised to read that "No work shall be performed at the toll plazas or on the pavement from the cash lane barrier to the edge of the pavement without prior approval from the Resident and coordination with system Integrator.**
16. As you know, TUF-BAR GFRP bars have been approved for the Auburn Ext 75 Toll. The bars were supplied and used in the AVC slabs. Now, Contract 2018.20 for YORK TOLL PLAZA-MILE 8.8 is out for tender but the SP 503 for GFRP (please see attached) does not include my product and company. I am wondering if Turnpike Authority can issue an addendum to this section to include my product and company (TUF-BAR by TUF-BAR INC.) so that I can bid on this project.  
**Answer: Tuf-Bar by Tuf-Bar, Inc. is an acceptable product for the GFRP reinforcement.**
17. Addendum 1 states that all three wage rates will be used on the project depending on the activity. Can you please clarify which activities apply to which wage determination. Since there are no bridges or heavy civil structures on the job it would appear that all work on the project with the exception of the Administration building proper would fall under the Highway and Earth wage determination.  
**Answer: The building rates should be used for the building construction and associated work, the heavy highway & bridge rates should be used for Toll Plaza Construction including the tunnel, the Highway Rates should be used for the Roadway construction including the median barrier.**

## ATTACHMENTS

Plan Sheet	(1 page)
Special Provisions	(8 pages)
Division 800, Specifications	(4 pages)

Date: 9/28/2018

Filename: ...0259637\047\_Typical\_03\_CS.dgn



**PHASE 3 NOTES**

EXISTING TOLL PLAZA AT MILE 7.3;  
REMAINS FULLY OPEN.

PROPOSED LIGHTING ON CONCRETE BARRIER SHALL BE FUNCTIONING PRIOR TO SHIFTING TRAFFIC FROM PHASE 2 TO PHASE 3.  
(POLE #S: B13A, B18, B22, B23, B26, B27, B28, B29, B32, B33, B36, B38, B41)

PROPOSED TOLL PLAZA AT MILE 8.8;  
MAINTENANCE OF TRAFFIC:

- NORTHBOUND: SHIFT 3 LANES OF NORTHBOUND TRAFFIC ONTO THE COMPLETE NORTHBOUND ORT LANES. INCREASE THE NORTHBOUND SPEED LIMIT TO 60 MPH CONSTRUCTION SPEED LIMIT. NORTHBOUND CASH PLAZA IS NOT OPEN.
- SOUTHBOUND: SHIFT 3 LANES OF SOUTHBOUND TRAFFIC ONTO THE COMPLETE SOUTHBOUND ORT LANES. INCREASE THE SOUTHBOUND SPEED LIMIT TO 60 MPH CONSTRUCTION SPEED LIMIT. SOUTHBOUND CASH PLAZA IS NOT OPEN.
- 1. CONSTRUCT 13 FEET OF PRECAST TUNNEL AND 15 FOOT CAST-IN-PLACE TUNNEL AND STAIRCASE STRUCTURE (CASH LANE 9) AT NORTHBOUND CASH PLAZA. TUNNEL CONSTRUCTION IS COMPLETE.
- 2. COMPLETE MECHANICAL, ELECTRICAL, PLUMBING AND ITS SYSTEMS IN THE TUNNEL.
- 3. CONSTRUCT NORTHBOUND CASH BOOTHS FOR CASH LANES 8 AND 9; AND SOUTHBOUND CASH BOOTHS FOR CASH LANES 1 AND 2; INCLUDING CANOPY FOOTINGS. CONSTRUCT CONCRETE PAVEMENT FOR NORTHBOUND AND SOUTHBOUND CASH LANES.
- 4. CONSTRUCT PERMANENT PAVEMENT IN AREAS OF TEMPORARY PAVEMENT WIDENING FROM PHASE 2. CONSTRUCT PERMANENT PAVEMENT ADJACENT TO NORTHBOUND AND SOUTHBOUND TOLL PLAZAS.
- 5. COMMISSION THE NORTHBOUND AND SOUTHBOUND CASH LANES.
- 6. REMOVE THE NORTHBOUND AND SOUTHBOUND TEMPORARY PAVEMENT WIDENINGS.
- 7. MILL AND PAVE SHOULDERS FROM 2% CROSS SLOPE USED IN PHASE 2 TO 4% CROSS SLOPE FINAL GRADE.
- 8. CONSTRUCT LIGHT POLE FOUNDATIONS, CONDUIT, AND POLES AND LIGHT FIXTURES.
- 9. CONSTRUCT SOUTHBOUND CASH BARRIER FROM STA. 2335+50 (BEGIN) TO STA. 2338+00.
- 10. CONSTRUCT NORTHBOUND CASH BARRIER FROM STA. 1337+00 (BEGIN) TO STA. 1339+50.
- 11. CONSTRUCT MEDIAN BARRIER FROM STA. 306+00 (BEGIN) TO STA. 332+00. SHIFT THREE NORTHBOUND AND THREE SOUTHBOUND LANES TO THE OUTSIDE LANES TO PROVIDE A WORK AREA FOR THE CONSTRUCTION OF THE MEDIAN AND MEDIAN BARRIER.
- 12. CONSTRUCT MILL AND PAVE PAVEMENT FROM SOUTHBOUND STA. 2290+00 TO STA. 2306+00, AND FROM NORTHBOUND STA. 1287+00 TO STA. 1306+00.
- 13. COMPLETE FINAL MILL AND OVERLAY PAVING OPERATIONS WITH DAILY LANE CLOSURES AND/OR NIGHT WORK.

PHASE 3  
NOT TO SCALE

**LEGEND**

- REQUIRED CONSTRUCTION
- PRIORITY CONSTRUCTION
- WORK ZONE OPEN TO CONSTRUCTION

Scale: NOT TO SCALE

Designed by:



JACOBS ENGINEERING GROUP  
120 ST. JAMES AVENUE  
BOSTON, MA 02116  
TEL (617) 242-9222  
FAX (617) 242-9824



THE GOLD STAR  
MEMORIAL HIGHWAY

YORK TOLL PLAZA  
CONSTRUCTION STAGING PLAN 3  
PHASE 3

No.	Revision	By	Date
1	Lighting note added	CSM	9/18

CONSULTANT PROJECT MANAGER: T. MORIN					
	By	Date		By	Date
Designed	RRP	7-18	Checked	CSM	7-18
Drawn	AMS	7-18	In Charge of	TWM	7-18

MTA PROJECT MANAGER: R. NORWOOD

CONTRACT: 2018.20

SHEET NUMBER: CS-03

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107.1 Contract Time and Contract Completion Date

This Subsection is amended by the addition of the following:

All work shall be completed on or before June 21, 2021. The contract completion shall include completion of all contract work and all punch list items.

107.4.2 Schedule of Work Required

This Subsection is amended by the addition of the following:

The work shall be completed in logical timely increments. The Contractor shall submit a schedule for review that shows large segments of work scheduled for continuous blocks of time. Work in a segment shall be completed over a period of continuous work days. Work shall not be started in an area until the Contractor has scheduled the labor and equipment necessary to complete all work in the segment. The Contractor will not be permitted to “stretch” the Project over the entire Contract period, doing a day or two work per week.

A schedule that shows sporadic work activities through the duration of the Contract will not be approved. Actual work activities that are sporadic will not be allowed.

A two (2) week schedule shall be submitted by the Contractor weekly, the first week shall be detailed. The weekly detailed schedule shall show all lane closures that are anticipated for the following week. Lane closures that are not shown on this schedule will only be allowed if they are deemed emergency lane closures by the Resident.

107.4.6 Prosecution of Work

The following is a summary of the Construction Stages and key dates:

The work at the new toll plaza including the administration building shall be accomplished in three (3) major phases of construction, Phase 1, Phase 2 and Phase 3. Phase A, Phase B and Phase C shall be performed within Phases 1 and 2.

The winter traffic configuration, where noted, is from November 15 through April 15 except for Phases A and B.

Phase 1 and Phase 2: Project Start to end of Phase 2.

Phase 1:

With traffic maintained in the existing northbound and southbound lanes and reduced outside shoulder widths, the work includes the construction of the northbound and southbound widening at the proposed toll plaza and along the mainline. Work includes clearing; rock excavation; excavation and embankment; granular subbase and base pavement materials; HMA pavement to binder grade; proposed drainage improvements; precast and cast-in-place tunnel construction; utility construction including water and electric; temporary pavement markings and signs; earthwork and pavement for temporary widening; and temporary traffic control.

The Contractor shall provide three (3) 12 foot lanes with existing shoulders along the median with four foot outside shoulders and temporary barriers.

Also included is the construction and completion of the new administration building, access road and parking area.

#### Phase A:

Included is the full depth pavement construction and pavement shimming from northbound Sta. 1371+50 to Sta. 1379+00 and from southbound Sta. 2370+00 to Sta. 2379+25.

During Phase A, the Contractor shall provide two (2) 12 foot travel lanes with 2' – 4' reduced shoulder widths and temporary barrier in both northbound and southbound directions in order to construct the pavement widening required to support the travel lanes and shoulders required for Phase B.

This work also includes the construction of the drainage culvert at Sta. 374+77 which will require work during off peak hours using daily lane closure(s) to facilitate the construction and maintenance of traffic.

#### Phase B:

Included is the full depth pavement construction and pavement shimming from northbound Sta. 1371+50 to Sta. 1379+00 and from southbound Sta. 2370+00 to Sta. 2379+25.

The transition from Phase A to Phase B will shift the two mainline travel lanes in each direction onto two (2) 11 foot lanes with 2' minimum shoulders constructed in Phase A. The Contractor shall immediately begin the pavement construction so that the third lane (inside lane) can be opened to traffic, providing three (3) 11 foot travel lanes with 2' - 4' reduced shoulder widths in both directions (2' minimum shoulders).

This work also includes the construction of the drainage culvert at Sta. 374+77 which will require work during off peak hours to provide daily lane closure(s) to facilitate the construction and maintenance of traffic.

The period of time in Phase A and Phase B, during which the northbound and southbound travel lanes are reduced to two lanes with shoulders in each direction, shall be no longer than 24 calendar days with incentive and disincentive payments.

#### Phase 2:

Upon completion of the permanent and temporary widening necessary in Phase 1, traffic will be shifted from the existing lanes to the temporary lanes shown in Phase 2. Work includes the rock excavation; excavation and embankment; granular subbase and base pavement materials; permanent HMA pavement to binder grade; proposed drainage improvements; precast and cast-in-place tunnel construction; utility construction including water and electric, mechanical, electrical, plumbing and ITS systems in the tunnel; major portions of the northbound

and southbound median and cash lane barrier; ORT space frame, footings and concrete slab; canopy footings for inside cash lanes; and temporary traffic control.

The northbound and southbound ORT lanes will be turned over to the Authority for commissioning. No work shall be performed on the ORT lanes and shoulder, including the cash lane barriers, for a distance of 1,400 feet north and south of the ORT facilities.

The system Integrator shall commence ORT lane commissioning testing after both the northbound and southbound ORT facilities are complete and will complete the commissioning no later than 126 calendar days from the Authority's acceptance that both the northbound and southbound ORT facilities are complete. It is preferred that both northbound and southbound ORT are turned over to the Authority for commissioning testing as the same time, but in no case shall one barrel be turned over to the Authority later than 30 calendar days after the first. The 126 days of commissioning for each bound begins from the Authority's acceptance of that bound.

The Contractor shall provide and maintain traffic control associated with the testing of ORT equipment including providing sufficient roadway approaches to ORT facilities to properly test the operation at highway speeds. The Contractor shall coordinate traffic control requirements with the tolling engineer and system integrator including any adjustments to the temporary traffic control measures needed for ORT testing.

The Contractor may continue working in Phase 2, Phase A and Phase B work area during commissioning, except for the area required for the ORT commissioning described above. It is the Contractor's responsibility to maintain long-term lane closures of the ORT zones and cash lanes and maintain all proposed ORT and cash lanes signage covered until the ORT and cash lanes are operational.

#### Phase C:

Phase C provides for the long term configuration, including winter traffic configuration from November 15 to April 15, of Phase 2. The work involves shifting the three lanes of northbound and southbound traffic, north of the toll plaza, toward the median by one full travel lane to provide widened shoulders (8 foot minimum) on both sides. The temporary barrier along the outside shoulder from Phase 2 shall remain in place, and the outside lane shall become the outside shoulder. The Contractor shall remove/relocate the temporary barrier on the median side and provide temporary pavement markings and signage to accommodate the lane shifts and widened shoulders. The travel lanes and shoulders, including the pavement supporting the temporary barrels or barrier, shall be at the same elevation – there shall be no longitudinal pavement drop-off.

#### Phase 3: From Phase 3

#### Phase 3:

Upon completion of the work on Phase 2, Phase A, Phase B, and the ORT northbound and southbound commissioning, the three travel lanes will be shifted from the outside temporary



alignments to the three (3) 12 foot travel lanes with 12 foot shoulders (4 foot minimum) along the ORT lanes in each direction.

The ORT's will be commissioned at this time, but not operational to collect tolls. Work includes completion of the northbound and southbound cash plazas; tunnel construction and mechanical, electrical, plumbing and ITS systems; completion of the median barrier; removal of temporary pavement and pavement widening; HMA pavement and final surface course; completing side slopes and open drainage to final grades; signing and striping; and commissioning of the northbound and southbound cash plazas, and traffic control.

The Contractor shall maintain a minimum of three (3) 12 foot travel lanes, 2 foot minimum left shoulder and 4 foot minimum right shoulder during construction of the median and median barrier from station 306+00 to 332+00.

The northbound cash lanes and southbound cash lanes will be turned over to the Authority for commissioning. No work shall be performed at the toll plazas or on the pavement from the cash lane barrier to the edge of pavement without prior approval from the Resident and coordination with system Integrator. The commissioning and testing on the northbound cash lanes may be done separately from the southbound cash lanes to facilitate the construction schedule.

The system Integrator shall commence cash lane commissioning and testing after both the northbound cash plaza and southbound cash plaza are complete, and will complete the southbound plaza commissioning no later than 70 calendar days, and the northbound plaza commissioning no later than 56 days, from the Authority's acceptance that both the northbound plaza facility and southbound plaza facility are complete for commissioning.

The Contractor shall provide and maintain traffic control associated with the testing of cash plaza equipment including providing sufficient roadway approaches to facilities to properly test the operation at highway speeds. The Contractor shall coordinate traffic control requirements with the tolling engineer and system integrator including any adjustments to the temporary traffic control measures needed for plaza testing.

The Contractor may continue working in the Phase 3 work area during commissioning, except for the area required for the plaza commissioning described above. It is the Contractor's responsibility to maintain long-term lane closures of the cash plazas and maintain all proposed signage, and covering of signs, until the cash plazas are operational.

The end of Phase 3 marks the completion of the new toll plaza at MM 8.8. This work shall be completed by **June 21, 2021**.

#### Supplemental Liquidated Damages

Supplemental liquidated damages of Five Thousand (\$5,000.00) Dollars per calendar day per the following dates or phase durations shall be assessed for each calendar day that activities listed above are not completed:

Interim Completion Date	Supplemental Liquidated Damages Date	Supplemental Liquidated Damages per calendar day
Waterline Completion Date	March 1, 2019	\$5,000

The “day” begins at 12:01 a.m. and ends at 12:00-midnight.

#### Incentive/Disincentive Payments

Incentive/Disincentive payment for Phase A –Phase B (north end full depth area):

From April 1 to May 3, 2019 or October 15 to November 16, 2019 or from April 1 to May 6, 2020, a permanent single lane closure will be allowed for Phase A and Phase B.

The Incentive/Disincentive activity is for the period of time during which the northbound or southbound travel lanes are reduced to the two lanes during Phase A and Phase B.

The total duration period for this activity for northbound or southbound is 24 calendar days. The duration begins on the day when the traffic is restricted to 2 lanes in Phase A. The duration ends when the travel lanes are open to three (3) travel lanes and shoulders in Phase B. Traffic shall be traveling on binder pavement at the end of this activity, and work may continue in Phase 2 / Phase B to complete the full depth construction.

An Incentive payment for early completion of Five Thousand (\$5,000.00) Dollars per calendar day shall be paid for each calendar day (up to a maximum of 7 days) that activity listed above are completed prior to a 24 calendar day total duration, for northbound and southbound separately. This is separate and distinct from the Liquidated Damages and Supplemental Liquidated Damages.

A Disincentive penalty of negative Five Thousand (\$-5,000.00) Dollars per calendar day shall apply for each calendar day that the activity is not completed beyond the 24 calendar day duration, for northbound and southbound separately. There is no limit on the disincentive.

#### 107.4.7 Limitations of Operations

Construction dates and restrictions for the construction and testing of the water main shall be in accordance with the requirement of the York Water District per Special Provision 822 Water Supply Main.

Lane and shoulder closures required to perform daily and short term operations, as well as overhead operations and equipment moves, shall be allowed in accordance with the tables provided in Special Provision 652 – Maintenance of Traffic - Specific Project Maintenance of Traffic Requirements. The Contractor shall provide strict adherence to lane and shoulder closures in accordance with these tables unless authorized by Authority.

Wide loads are restricted from moving on the turnpike from a half hour after sunset until a half hour before sunrise.

Wide loads must be able to safely pass all daytime work areas.

The Contractor shall submit the proposed staging and storage areas for approval by the Resident. All equipment and material storage must be located no closer than 30 feet from the edge of travel way, unless protected by temporary barrier. Proposed material and equipment storage locations shall be selected based on (1) proximity to UIS/Protected Natural Resources; (2) minimizing rutting or other actions that may hinder sheet flow from roadway; and (3) spill control and prevention, in the event of a fluid release from the equipment.

Material and equipment and vehicles stored behind temporary barrier must be located beyond the maximum barrier system deflection to allow for proper barrier deflection.

For paving operations under active traffic, the Contractor will be allowed to mill the entire work area prior to beginning the paving operation in that work area provided the milled pavement areas can be surfaced within one week. A maximum mill depth of 4" is required. Longitudinal joints adjacent to active traffic or where traffic drives over joint shall use a safety edge.

Care shall be taken when working near catch basins to ensure foreign material and contaminants do not enter the stormdrain systems. If foreign material and/or contaminants enter a catch basin(s), such material shall be removed prior to the material exiting the catch basin and into the stormdrain system and waterway. The Contractor shall remove and properly dispose of this material to the satisfaction of the Resident. Payment shall be incidental to the Contract.

The Contractor shall not install sheet piling or other driven or hammered systems for temporary excavation support or permanent installations during non-daylight hours or within 10 feet of an active traffic lane.

The Contractor vehicle and equipment access to and from the mainline shall provide the least interference with mainline traffic flow at any time. The Contractor shall locate access locations to the work area(s) which provide adequate acceleration/deceleration length and sight distance to and from the mainline, including appropriate warning signs. The Contractor may use the existing shoulders if available for acceleration/deceleration length, but at no time shall active shoulders be used for queuing construction vehicles. When existing shoulders are not available, the Contractor shall establish, construct and maintain facilities within the work area for acceleration/deceleration and merging with the mainline traffic. When access or egress points are not active, all warning signs shall be covered or removed, and appropriate traffic control devices shall close the access locations. When access or egress points are no longer necessary, signage shall be removed and appropriate traffic control devices shall close the access locations.

The Contractor shall use the I-95 mainline for the hauling and delivery of materials and equipment for the construction of this project. Chases Pond Road shall not be used by the Contractor for hauling and delivery of materials and equipment for construction of this contract, except for the construction of the proposed access road, parking lot and administration building. Chases Pond Road shall be the responsibility of the Contractor to properly repair to original condition, as determined by the Resident.

The length of temporary barrier installed during each Phase of work shall be limited to the Contractor's active work area, unless specifically required. The Contractor shall sequence the

work within each Phase in a logical manner that minimizes the length temporary barrier along one or both sides of the active mainline traffic including temporary alignments. When construction or operations in a work area is complete and matches existing surface, the temporary barrier shall be removed or moved away from the active lane providing that a minimum 8 foot paved area can be used as an appropriate shoulder. If the temporary barrier is removed, appropriate traffic control devices will be employed to delineate the mainline edge of shoulder.

The Contractor shall not begin any portion of work that cannot be completed in the construction season prior to winter suspension of work. The intent is to not leave an element of construction incomplete during the winter suspension that will impede mainline traffic flow, maintenance of traffic, winter maintenance operations, or otherwise require the removal and replacement of the work.

The Contractor shall not schedule or begin any work that would require the shift of traffic to the median through the winter months. The Contractor shall not shift traffic toward the median for any portion of work that would remain during the winter months, either as a planned construction event or unforeseen circumstance that would extend this work through the winter months. Should an unforeseen circumstance not allow for the adequate completion of this work, the Contractor shall perform the work necessary to return the traffic to the previous configuration prior to winter suspension of work.

From October 15 to November 16, 2019 or from April 1 to May 6, 2020, a permanent single lane closure will be allowed for Phase A and Phase B.

Temporary transitional pavement ramps will be constructed at the ends of the pavement work transverse to direction of traffic. The pavement ramps shall be constructed prior to opening the lane(s) to traffic. This work shall not be measured separately, but shall be incidental to Item 403.

Permanent signs shall not be installed more than two weeks in advance of the lane or toll facilities for which the sign legend is for being open to traffic. Permanent signs covered until the lanes or toll facilities for which the sign legend is for being open to traffic shall not be covered for more than two weeks.

The commissioning of the northbound and southbound ORT lanes require that the complete electrical and communications services and devices are installed on the ORT pavement and space frame, in the tunnel, and connected to the server room in the administration building. The server room shall be complete to allow for complete operation of the ORT lanes so that commissioning can be completed.

#### 110.1 Indemnification

This Subsection is amended by the addition of the following paragraph:

For any work associated with the water line the Contractor agrees to indemnify , defend , and hold harmless the York Water District and its officers, directors, employees, agents and consultants from and against all claims, actions, torts, costs, losses, and damages for bodily injury (including sickness, disease, or death) and property damage arising out of or relating to

this Contract or the performance of Work by the Contractor, its Subcontractors, subconsultants, Engineers, suppliers, any individuals or entities directly or indirectly employed by any of them, or anyone for whose acts any of them may be liable, excepting only claims directly and solely caused by the negligence of the York Water District. Damages covered include, but are not limited to, all Dispute resolution costs including court costs, attorney's fees, and the fees of Engineers and consultants, arbitrators, and other professionals related to Dispute defense and preparation.

110.3.9 Administrative & General Provisions

This Subsection is amended by the addition of the following:

Under Paragraph A, Additional Insured, in addition to the Authority, The York Water District shall also be named as an additional insured.

SECTION 10200MISCELLANEOUS

## PART 1 - DESCRIPTION

## 1.1 RELATED DOCUMENTS

- A. Examine Drawings, Contract Conditions all other Sections of the Specifications for requirements which affect work of this Section whether or not such work is specifically mentioned in this Section.
- B. Coordinate work with that of all other trades affecting or affected by work of this Section. Cooperate with such trades to assure the steady progress of all Project work.

## 1.2 DESCRIPTION

The work in this section consists of furnishing all materials, equipment, labor and supervision, and performing all operations required to furnish and install all miscellaneous items as shown on the Drawings, as specified herein, and as is additionally necessary to properly complete the work.

## 1.3 SHOP DRAWINGS

Submit eight (8) copies of shop drawings for louvers, lockers, flagpole, mop hanger, shelving & supports. Submit eight (8) copies of catalog cuts for all items and materials supplied.

## PART 2 - MATERIALS

## 2.01 MOP HANGER

Mop hanger shall be 24 inches long, 3 inches wide stainless steel with three (3) rubber tool grips equal to Catalog No. 889-CC as manufactured by Crane Plumbing and Fiat Products, or equal by Florestone or E.L. Mustee & Sons, Inc.

## 2.02 LOUVERS

- A. Where indicated on the Drawings, install Airolite type K6774 Horizontal drainable louvers with 4 inch deep blades. Blades and frame shall be extruded aluminum 12 gauge alloy 6063-T52, with "Kawneer 500" finish, custom color to match the windows and frames. Louvers shall be fitted with 16 gauge aluminum bird screen in extruded aluminum frames. Louvers shall bear AMCA certified ratings seals for air performance and water penetration. Similar louvers by Construction Specialties, Inc. or Rusken Mfg. Div., Phillip Industries Inc. will be considered for use. Where indicated, detail and

fabricate louvers so as to be readily removable from the secure side (interior). Hollow metal frames shall be supplied under Section 08110.

- B. Where indicated on the Drawings, install Airolite type K611 horizontal blade, sightproof louvers (inverted Y) with 4 inch deep blades. Blades and frame shall be extruded aluminum 12 gauge alloy 6063-T52, with "Kawneer 500" finish, custom color to match the windows and frames. Louvers shall be fitted with 16 gauge aluminum bird screen in extruded aluminum frames. The Contractor shall submit the manufacturer's data derived in accordance with AMCA Standard 500 on a 4 foot by 4 foot unit demonstrating that it provided a minimum of 4.11 square feet of free area and shall intake 600 fpm free area at a static pressure drop not exceeding 0.15 inch H<sub>2</sub>O. Similar louvers by Construction Specialties, Inc. or Rusken Mfg. Div., Phillip Industries Inc. will be considered for use. Where indicated, detail and fabricate louvers so as to be readily removable from the secure side (interior). Hollow metal frames shall be supplied under Section 08110.
- C. See Mechanical Specifications for automatic dampers.

#### 2.03 CASH DROP UNIT (SAFE)

Cash drop unit (safe) will be supplied by the Authority.

#### 2.04 CURRENCY SCANNER & COIN SORTER UNITS

Currency Scanner and coin sorter units will be supplied by the Authority.

#### 2.05 LOCKERS

- A. Metal Lockers shall be similar to Global Industries "Infinity" Locker. Lockers shall be Double Tier 12" x 18" x 36" in 6-Door assembled profile. Each door shall have a stainless steel recessed door latch suitable for padlock. Each locker shall have 3 clothes hooks (one per side/rear).
- B. Factory applied paint color shall be selected by the Architect.

#### 2.06 SHELVING AND SUPPORTS

- A. Shelving shall be ¾" plywood or particleboard core melamine-clad units in sizes indicated. All faces and edges shall be melamine clad. Color shall be white.
- B. Supports shall be similar to Knappe & Vogt steel heavy-duty #85 Series vertical standards and #185 Series brackets. Bracket shall be full depth of shelf, unless indicated otherwise.

#### 2.07 TRASH RECEPTACLES

- A. Trash Receptacles will be supplied by the Authority.

#### 2.08 FILE CABINETS

- A. File cabinets will be supplied by the Authority.

## 2.09 FLAGPOLES

Flagpoles shall be seamless cone tapered aluminum 6063-T6 alloy, 30' (exposed) with a mechanical Class I clear anodized finish for two flags as manufactured by American Flagpole. All fittings, such as ball finial, double revolving truck, two halyard and four snaphooks, two cleats, and pole mounting assembly shall be as manufactured by or recommended by the flagpole manufacturer. Similar flagpoles and appurtenances manufactured by Concord Industries Inc., or EMC, a Division of Eder Manufacturing Corp. may also be acceptable.

## PART 3 - EXECUTION

## 3.1 MOP HANGER

Install mop hanger in accordance with the manufacturer's instructions.

## 3.2 LOUVERS

Install louvers as shown on the approved shop drawings and in strict accordance with the manufacturers' instructions.

## 3.3 CASH DROP UNIT (SAFE), CURRENCY SCANNER AND COIN SORTER

Place cash drop unit, currency scanners and coin sorters in position after all surrounding work has been completed and approved. Safe, currency scanners and coin sorters supplied by the Authority.

## 3.4 FLAGPOLES

- A. Before installation, when flagpoles are to be stored on site for an extended period, remove all wrapping material and store poles in a dry place, off the ground.
- B. Flagpole shall be installed by a manufacturer trained and approved erection crew experienced in handling, assembling and erecting poles, in strict accordance with the manufacturer's instructions and as shown on the approved Shop Drawings.

## 3.5 SHELVES AND SUPPORTS

- A. Install shelves and supports in accordance with the manufacturer's written installation instructions and as indicated.

## 3.6 LOCKERS

- A. Install lockers in location indicated and in accordance with manufacturer's installation instructions. Anchor lockers to wall framing.



END OF SECTION