

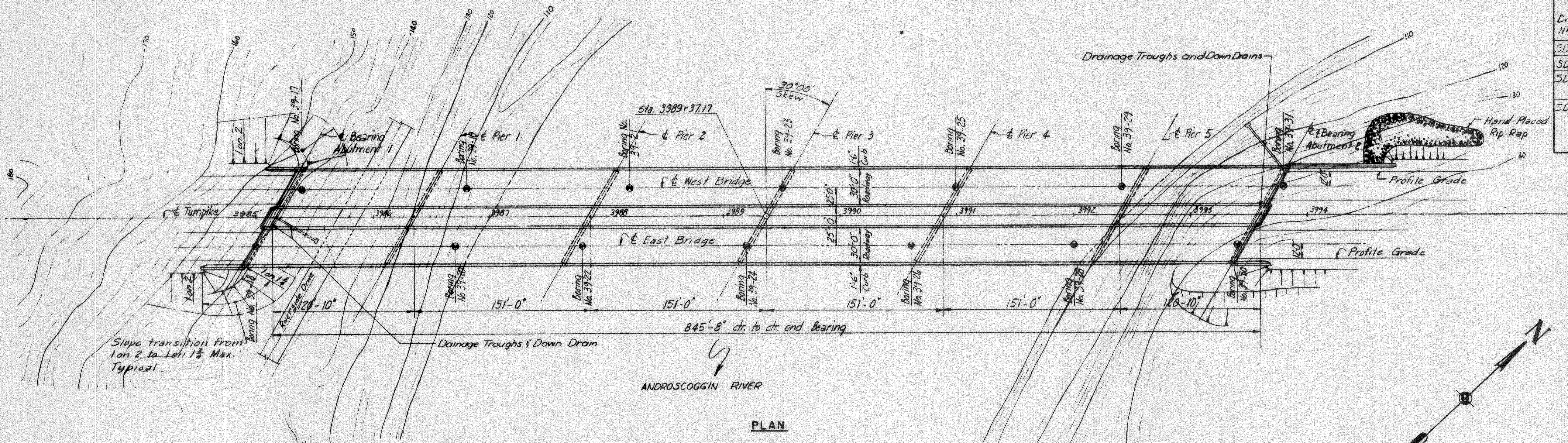
ELEVATION

GENERAL NOTES
 Design Specifications: AASHTO (1953) with minor modifications
 Design Live Load - H20 S16
 Maximum Base Pressure:
 Abutments 1 and 2 - 4.8 Tons/sq. ft.
 Pier 1 - 5.0 Tons/sq. ft.
 Pier 2 - 3.9 Tons/sq. ft.
 Pier 3 - 5.6 Tons/sq. ft.
 Pier 4 - 4.4 Tons/sq. ft.
 Pier 5 - 4.5 Tons/sq. ft.

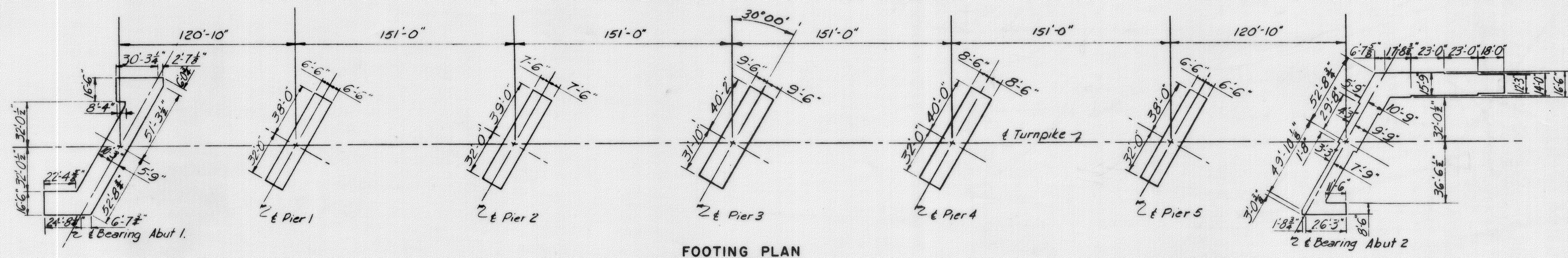
REFERENCES

Dwg No	Title	Substructure Contractor	Superstructure		
			Steel Fabricator	Steel Erector	Floor Contractor
SD1A	Standard Abutment Details	✓			
SD-3	Standard Abut. Drainage Details	✓			
SD-5	Standard Handrail, Bearing Devices and Misc. Details				✓
SD20	Standard Bridge Floor Cross Section, Steel Curb, Handrail and Diaphragms. 30'-0" Roadway		✓	✓	

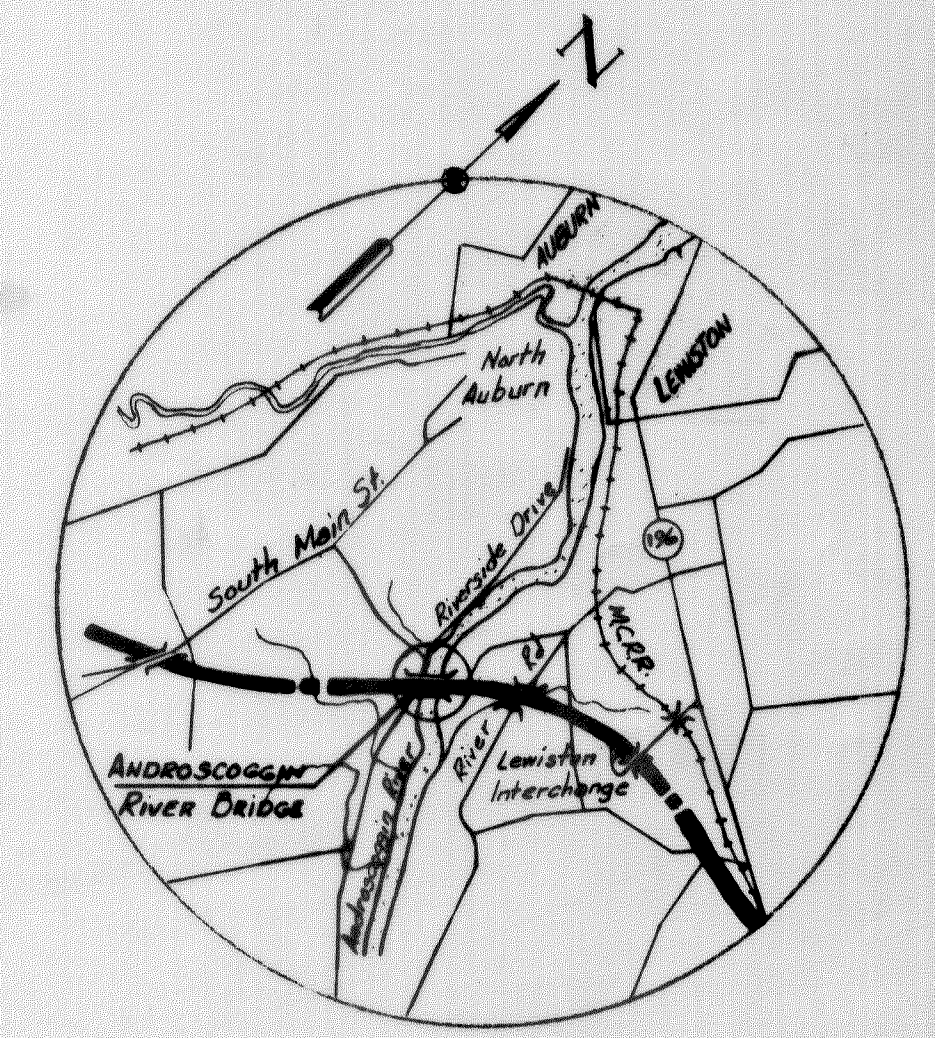
Note: Standard Drawings listed above are applicable only where specifically referred to in the plans.



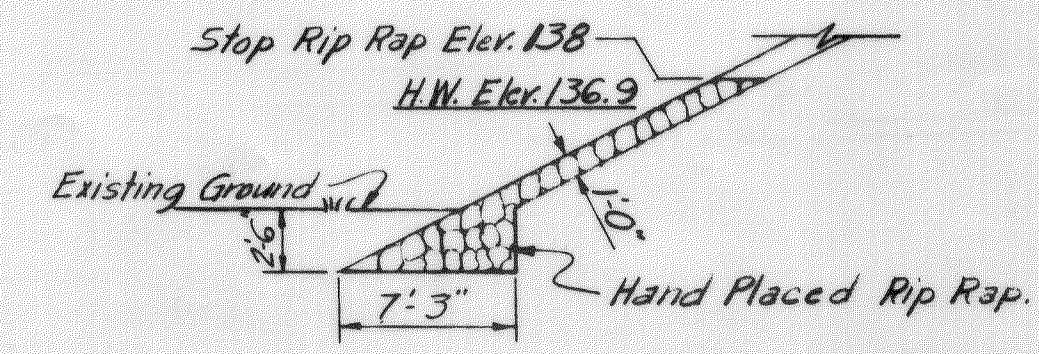
PLAN



FOOTING PLAN



VICINITY MAP
 Scale: 1" = 1 Mi

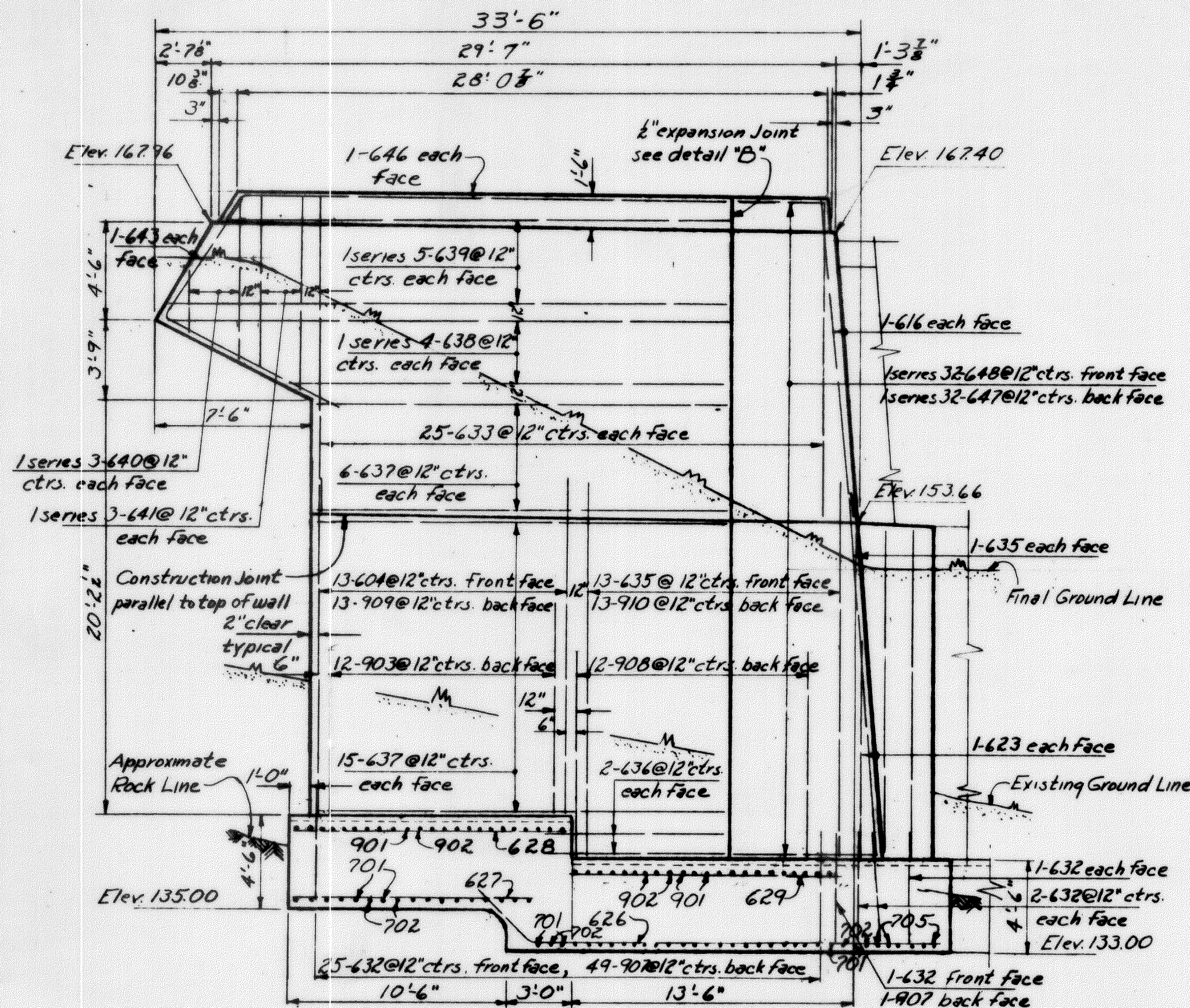


SECTION THROUGH RIPRAP
 NOT IN CONTRACT

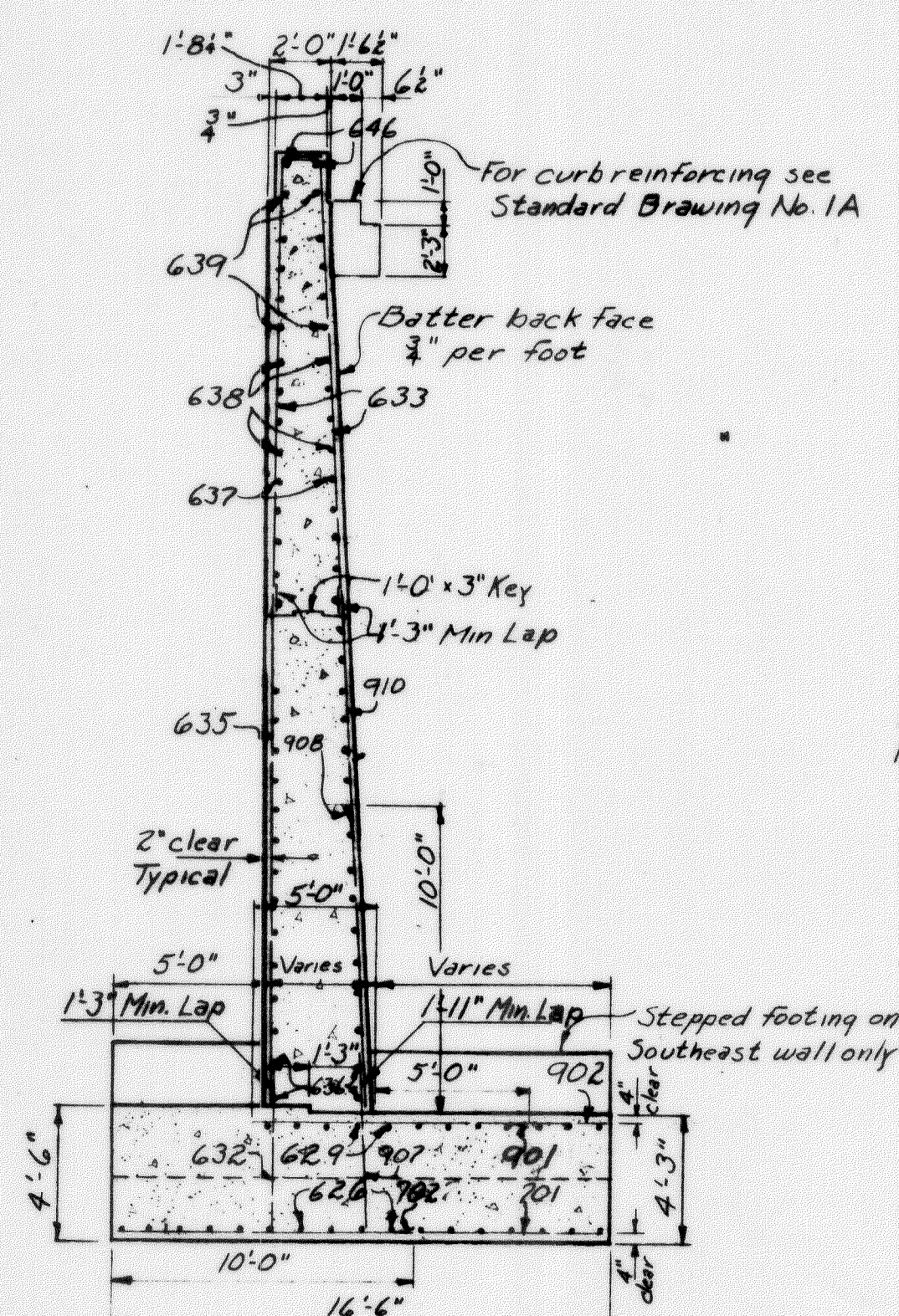
DRAWING 55.01.15

MADE	BY	DATE	NO.	REVISION	BY	DATE
	T.K.C.	2-9-54	3	As-Built	MBH	1-20-54
TRACED	R.P.	3-17-54	2	Added Drainage Troughs & Down Drains	HBN	4-6-54
CHECKED	H.J.G.	3-18-54	1	Added Note to Riprap Detail	GPD	3-23-54

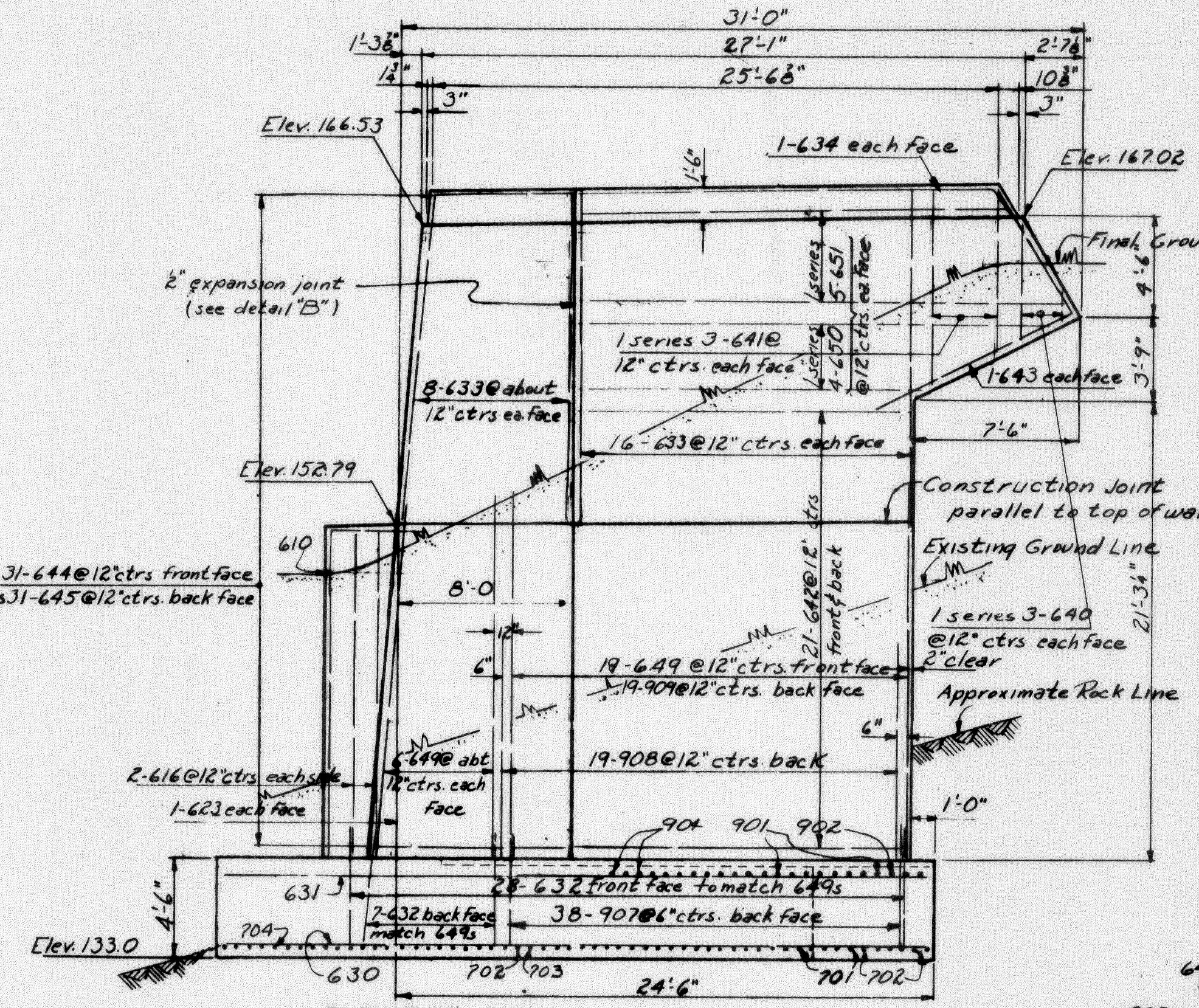
MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
 SECTION 2— PORTLAND TO AUGUSTA
 STRUCTURE NO. 55 TURNPIKE OVER
ANDROSCOGGIN RIVER
 STA 3989 + 37.17
GENERAL PLAN AND ELEVATION
 HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS
 NEW YORK KANSAS CITY
 SCALE: 1" = 50'-0"
 CONTRACT NO. _____
 SHEET NO. 236 OF 382



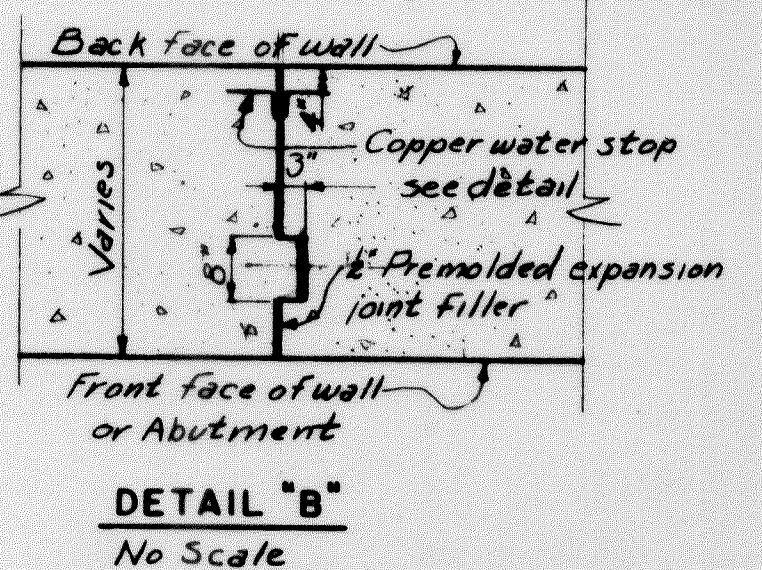
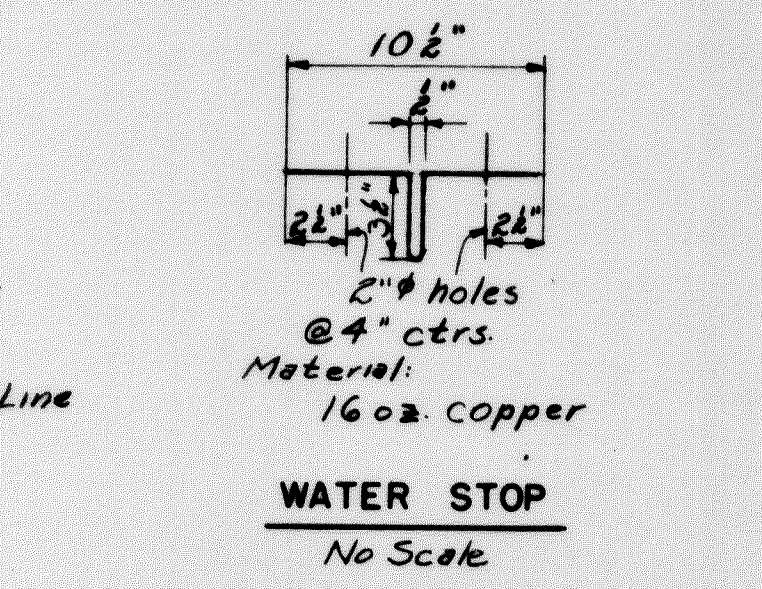
ELEVATION B-B
Scale: 3/8"=1'-0"



SECTION D-D
Scale: 3/8"=1'-0"

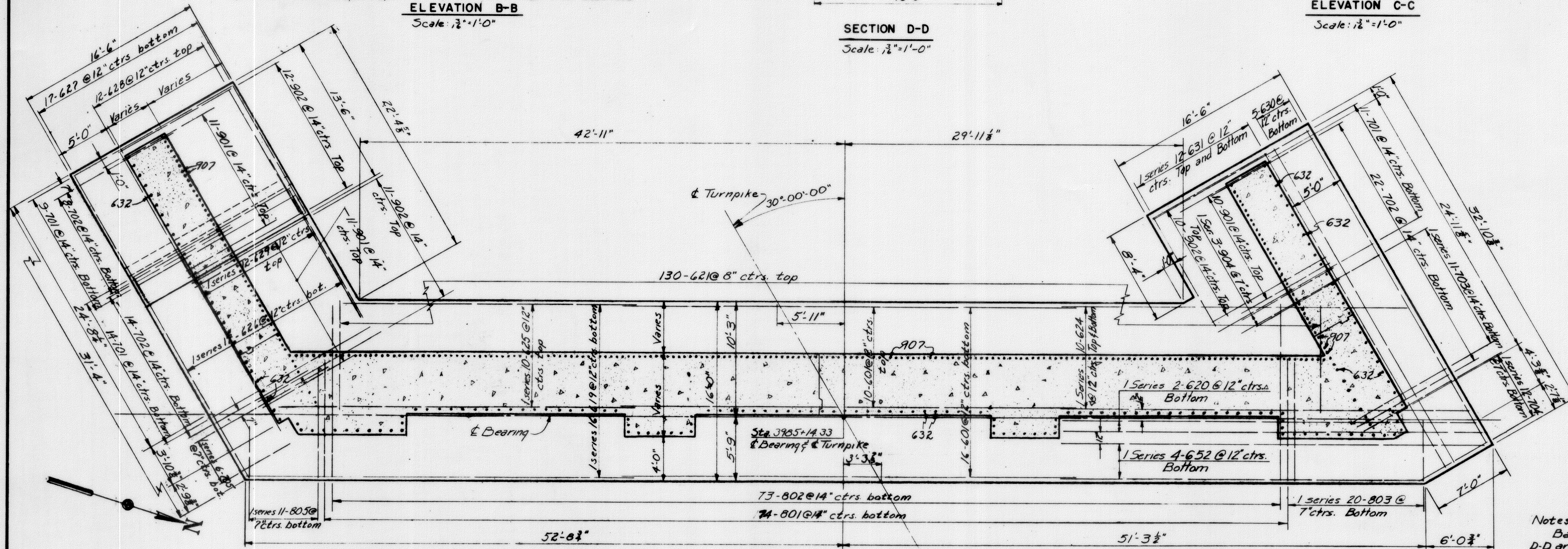


ELEVATION C-C
Scale: 3/8"=1'-0"



SECTION F-F
Scale: 4"=1'-0"

SECTION G-G
Scale: 4"=1'-0"



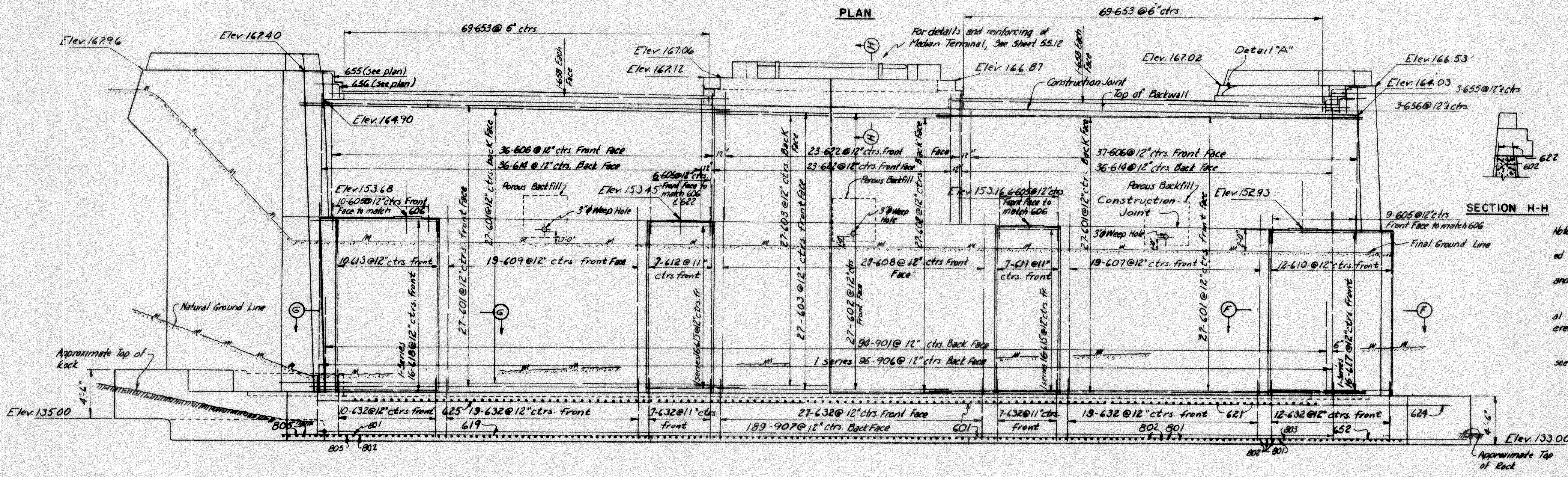
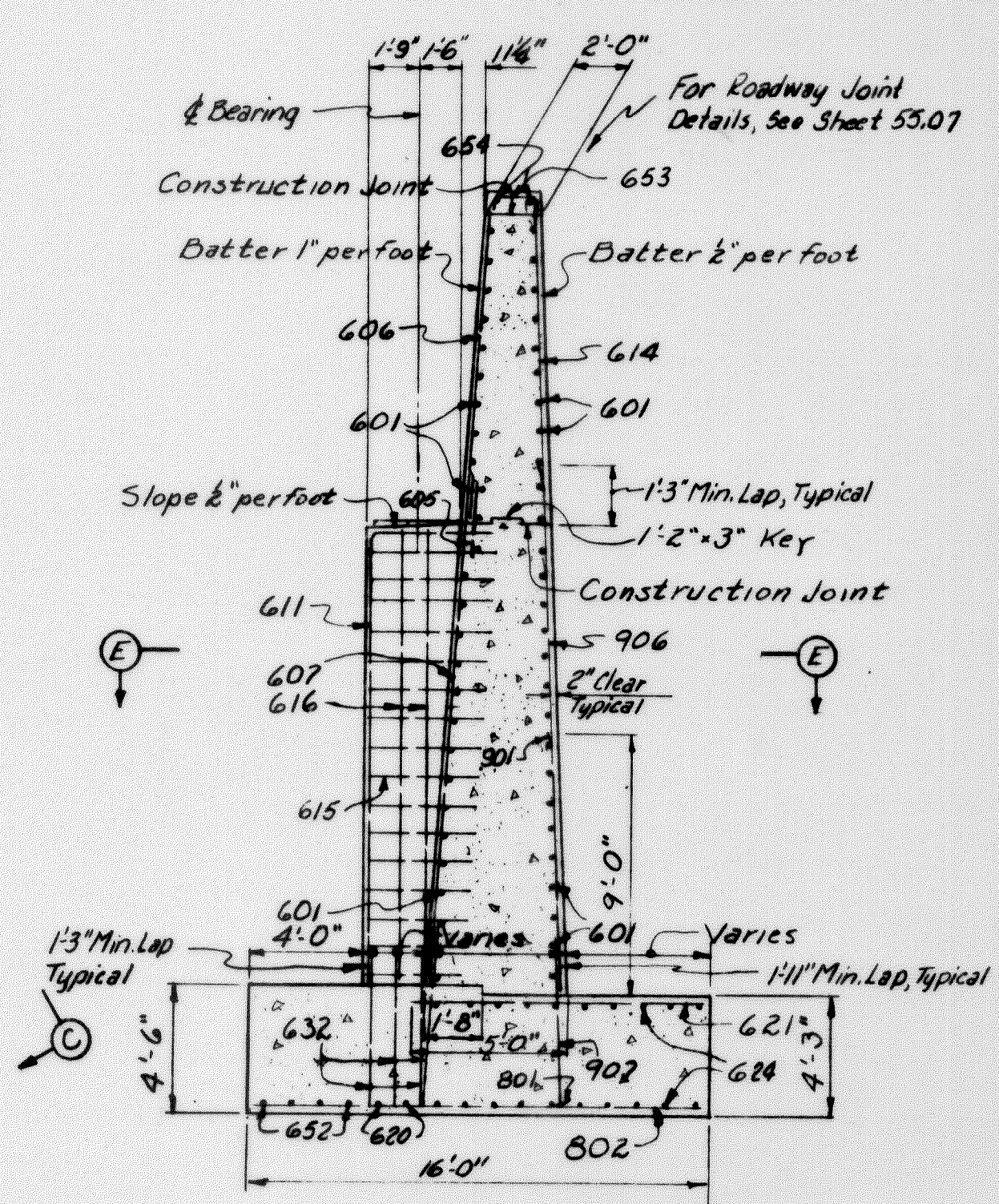
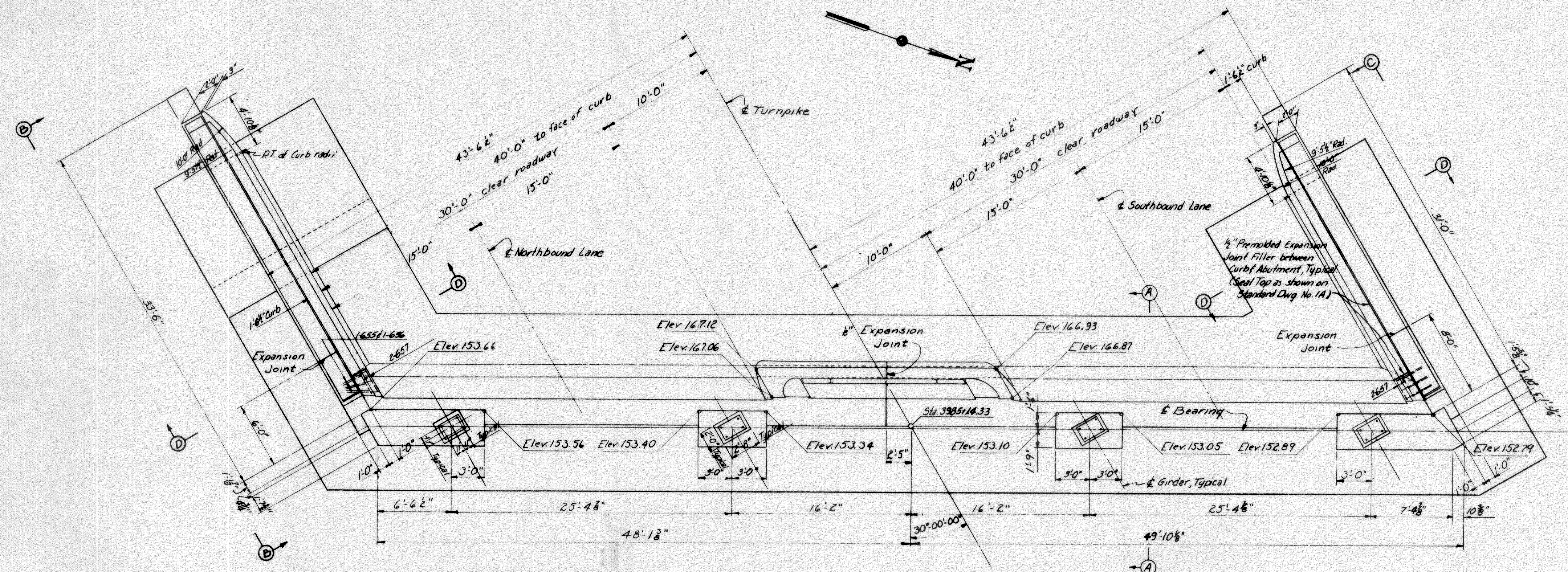
FOOTING PLAN
Scale: 3/8"=1'-0"

Notes:
 Bars numbered in section D-D are in East wingwall, bars in West wingwall are placed similar.
 For required form markings & curb reinforcing, see Standard Drawing No. 1A.

DRAWING 55.09.15

BY	DATE		
MADE	R.S.G.	2-18-54	
TRACED			
CHECKED	H.J.G.	3-16-54	As-Built HBH 12054
IN CHARGE OF	I.D.S.K.		

MAINE TURNPIKE AUTHORITY
SECTION 2— PORTLAND TO AUGUSTA
 STRUCTURE NO. 55 TURNPIKE OVER
 ANDROSCOGGIN RIVER
 STA. 3989+37.17
 ABUTMENT NO. 1
 HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS
 NEW YORK KANSAS CITY
 SCALE: As Shown
 CONTRACT NO. _____
 SHEET NO. 244 OF 252



SECTION H-H

SECTION E-E

Notes:

- Footings are to be carried a minimum of 6" into solid undisturbed rock.
- Curbs along wingwalls are not in this contract and will be placed by others.
- Concrete above top of backwall between wings and Median Terminal shall be placed after the roadway expansion joint has been erected in accordance with the specifications.
- For required form markings, and for curb reinforcing, see Standard Drawing No. 1A.
- For sections not shown see Sheet 55.08.
- Dampproof Abutment in accordance with the specifications.

For details of Porous Backfill see Sheet 55.11.

REINFORCING BAR LEGEND
(Typical for all reinforcing steel)

First Digit (1st two digits when four digits are used): Bar Size
Last Two Digits: Bar Number
Examples: 605 - #6 bar, fifth bar used
1106 - #11 bar, sixth bar used

DRAWING 55.08.15

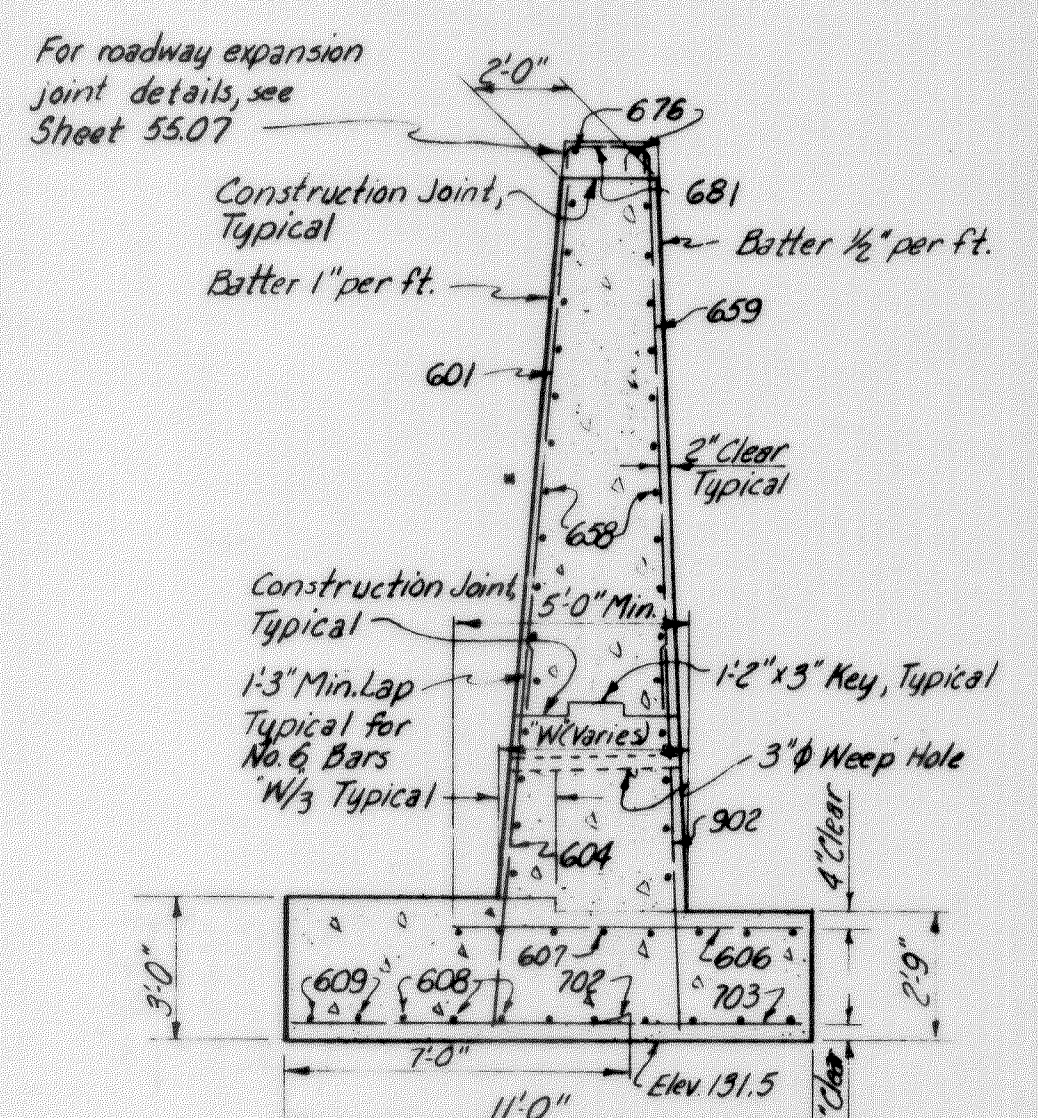
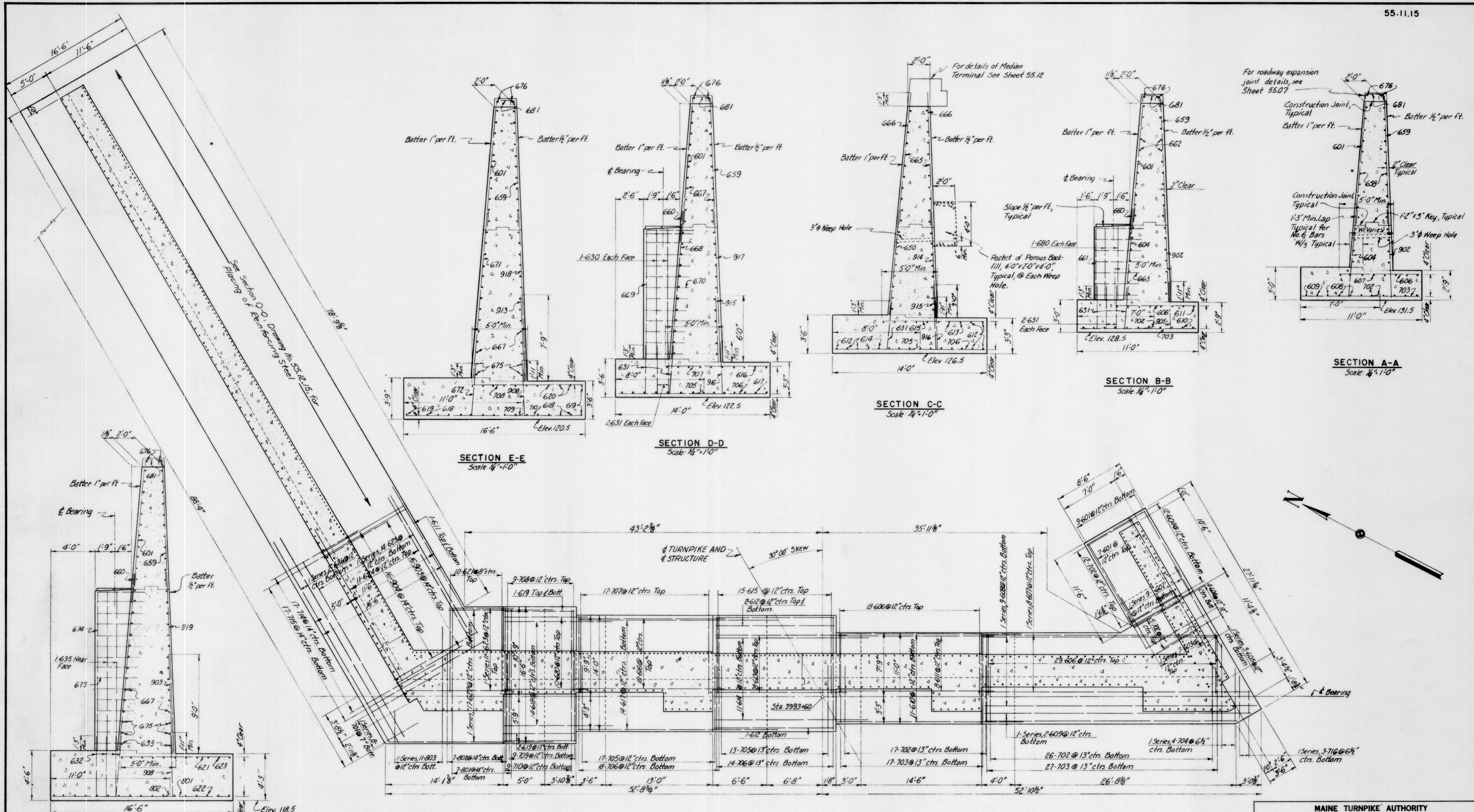
BY	DATE	REVISION	BY	DATE
RSC	2-15-54			
H.J.G.	3-16-54	1	Note Change	GRD 3-23-54
I.D.J.K.		No.	REVISION	BY DATE

MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
SECTION 2— PORTLAND TO AUGUSTA

STRUCTURE NO. 55 TURNPIKE OVER
ANDROSCOGGIN RIVER
STA. 3989+37.17
ABUTMENT NO. 1

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS
NEW YORK KANSAS CITY

SCALE: 3/4" = 1'-0"
CONTRACT NO. _____
SHEET NO. 243 OF 302



SECTION B-B
Scale: 1/4"=1'-0"

SECTION C-C
Scale: 1/4"=1'-0"

SECTION D-D
Scale: 1/4"=1'-0"

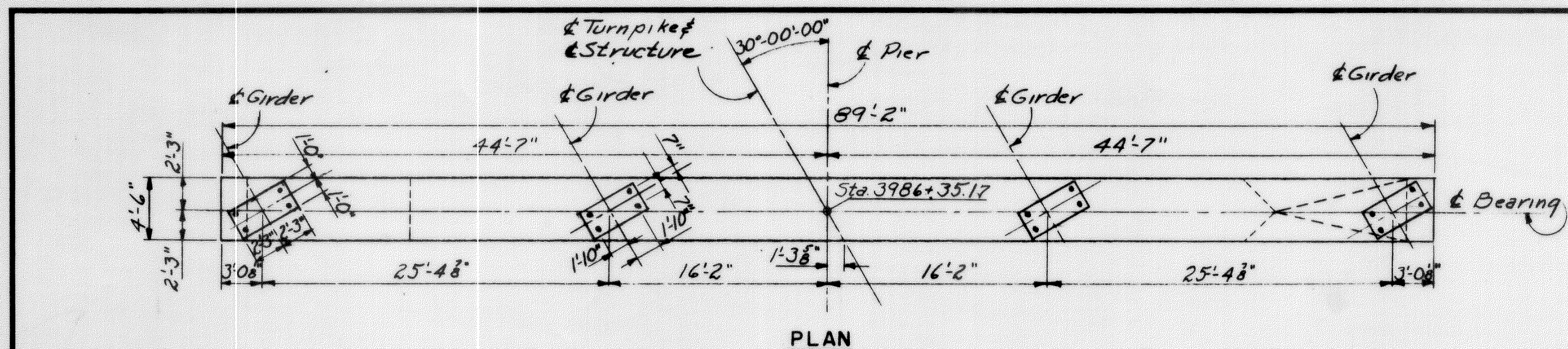
SECTION E-E
Scale: 1/4"=1'-0"

SECTION F-F
Scale: 1/4"=1'-0"

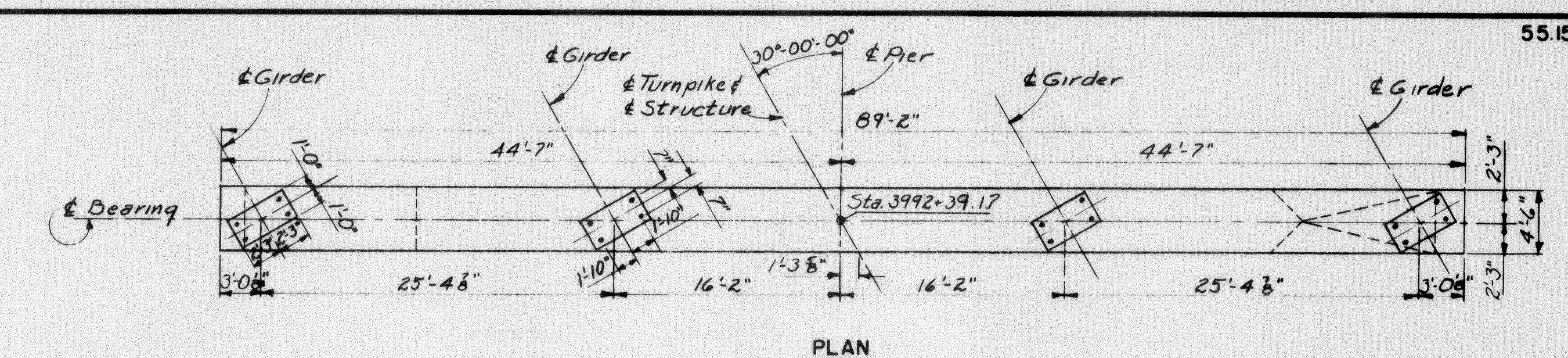
FOOTING PLAN
Scale: 3/8"=1'-0"

MAINE TURNPIKE AUTHORITY	
MAINE TURNPIKE	
SECTION 2— PORTLAND TO AUGUSTA	
STRUCTURE NO. 55	TURNPIKE OVER
ANDROSCOGGIN RIVER	
STA. 3989 + 37.17	
ABUTMENT NO. 2	
HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS	SCALE: As Noted CONTRACT NO.
NEW YORK KANSAS CITY	SHEET NO. 246 OF 382

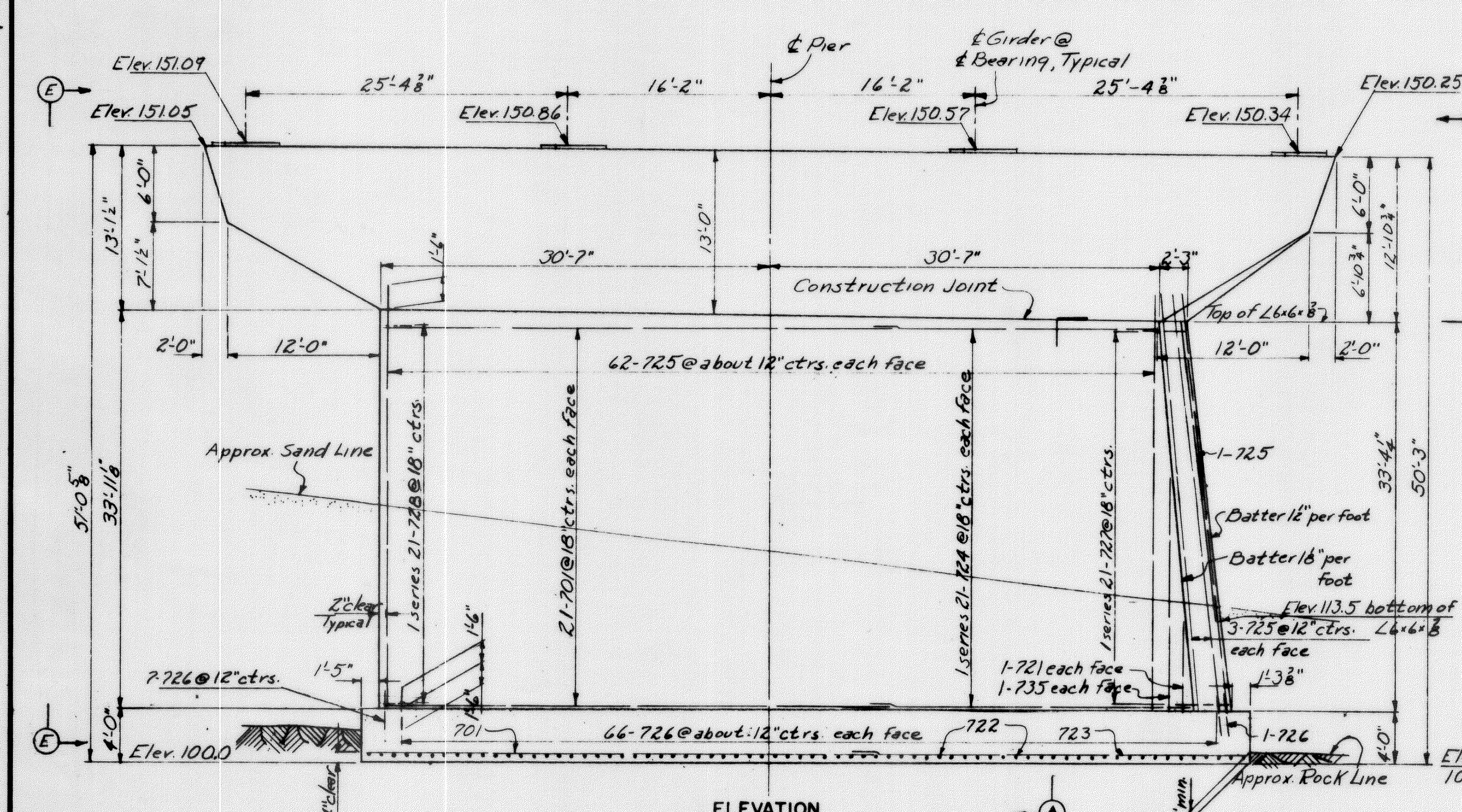
DRAWING NO. 55.11.15			
MADE	BY	DATE	
TRACED	H.J.G.	3-4-54	
CHECKED	R.S.G.	3-18-54	As Built
IN CHARGE OF	IDSM	No.	REVISION
			BY DATE



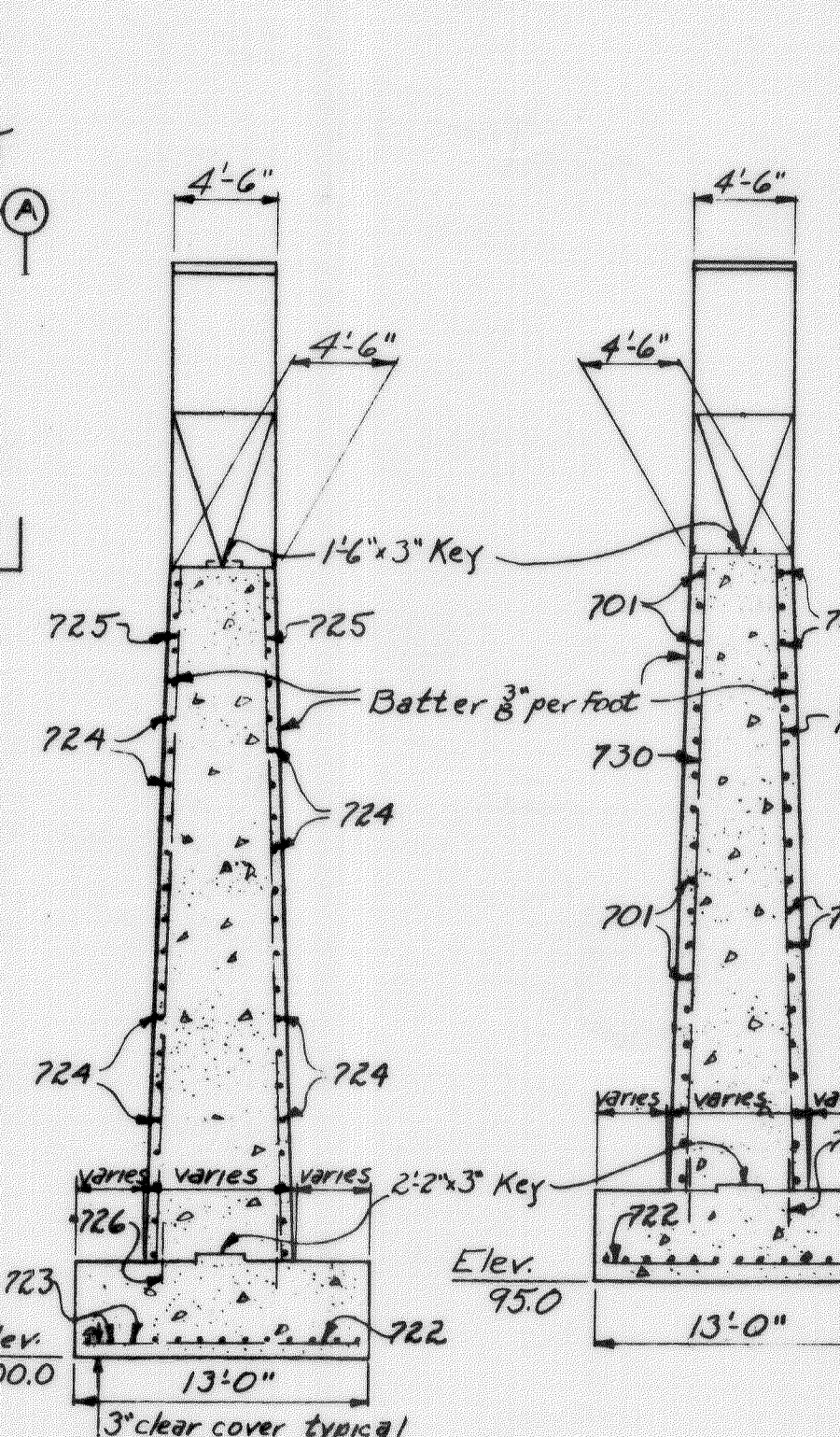
PLAN



PLAN

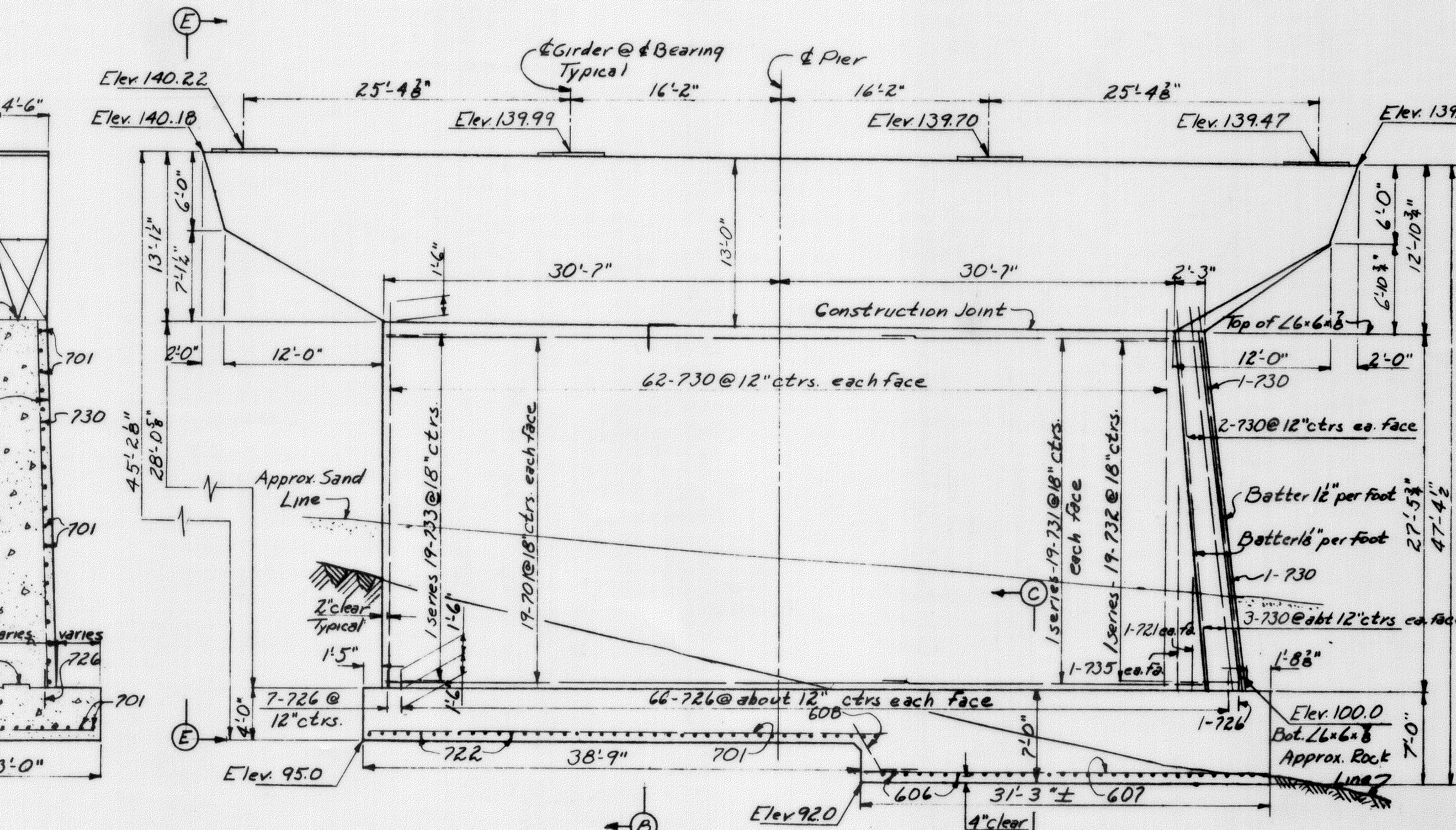


ELEVATION

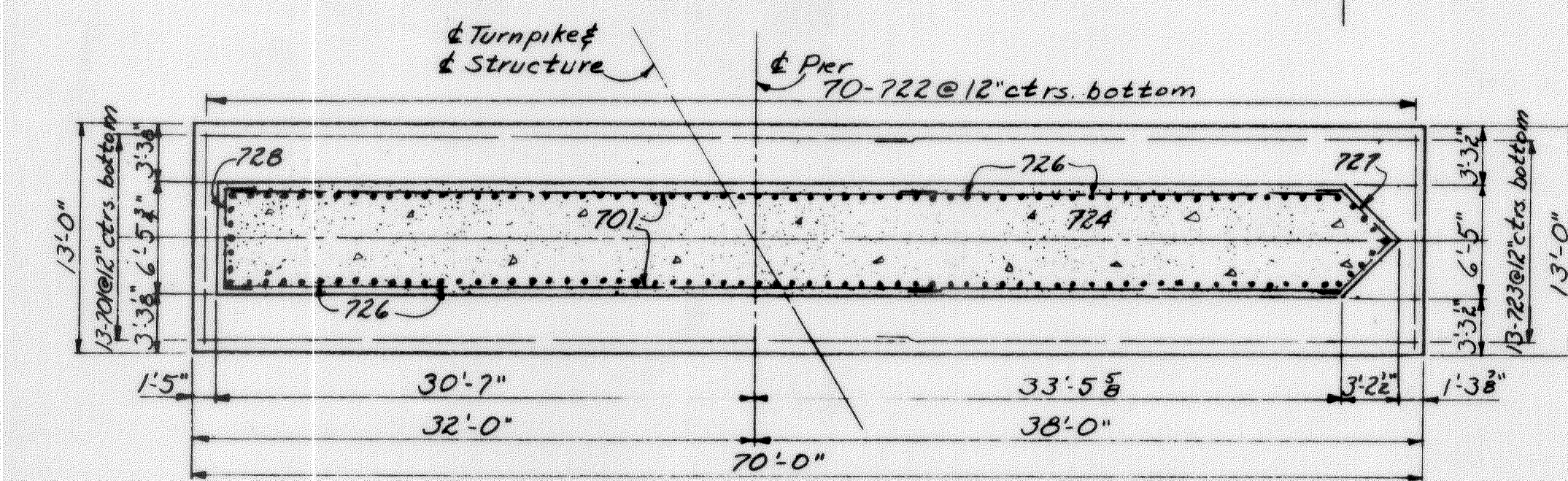


SECTION A-A

SECTION B-B

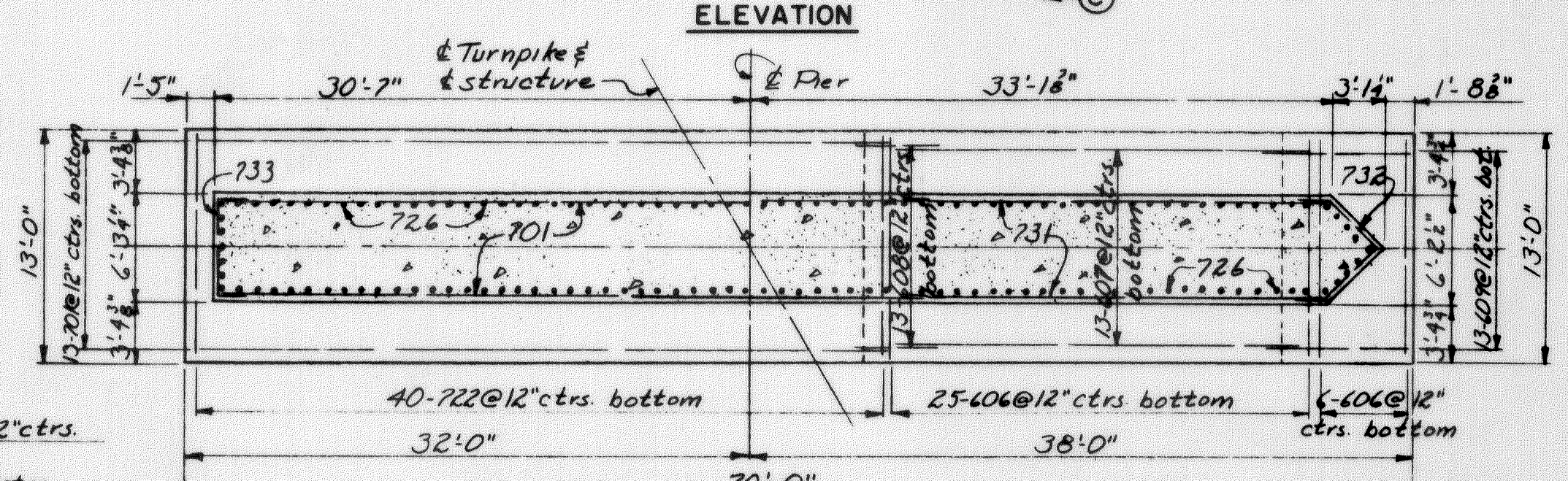


ELEVATION



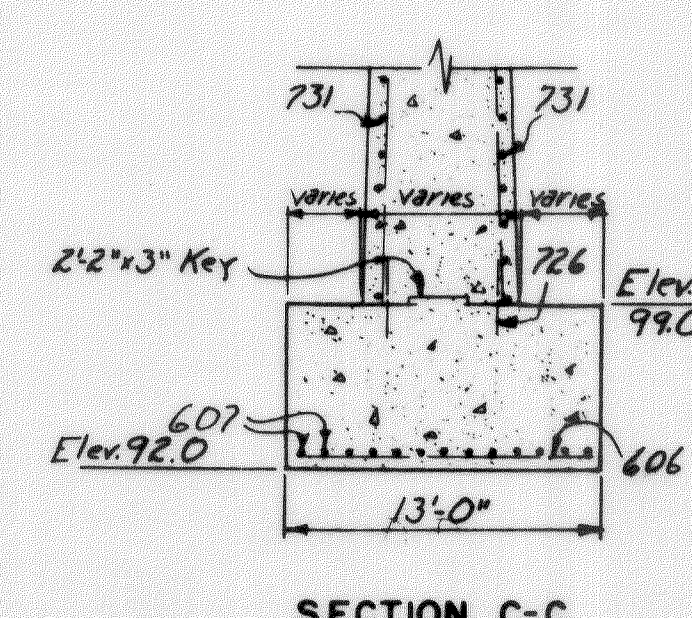
FOOTING PLAN

PIER 1

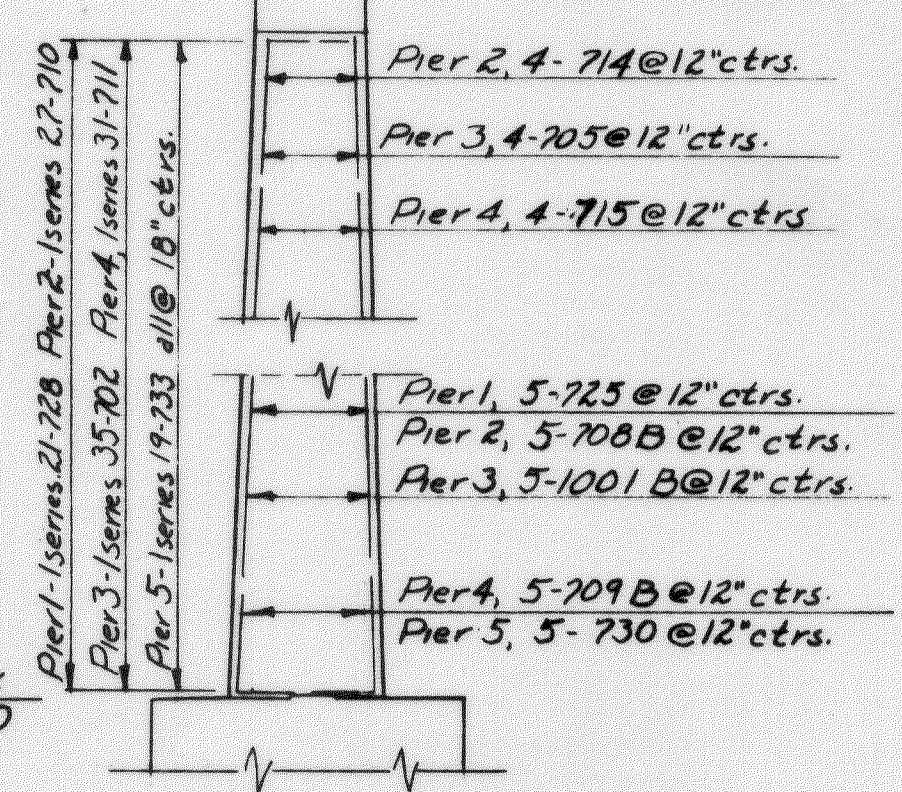


FOOTING PLAN

PIER 5



SECTION C-C



ELEVATION E-E

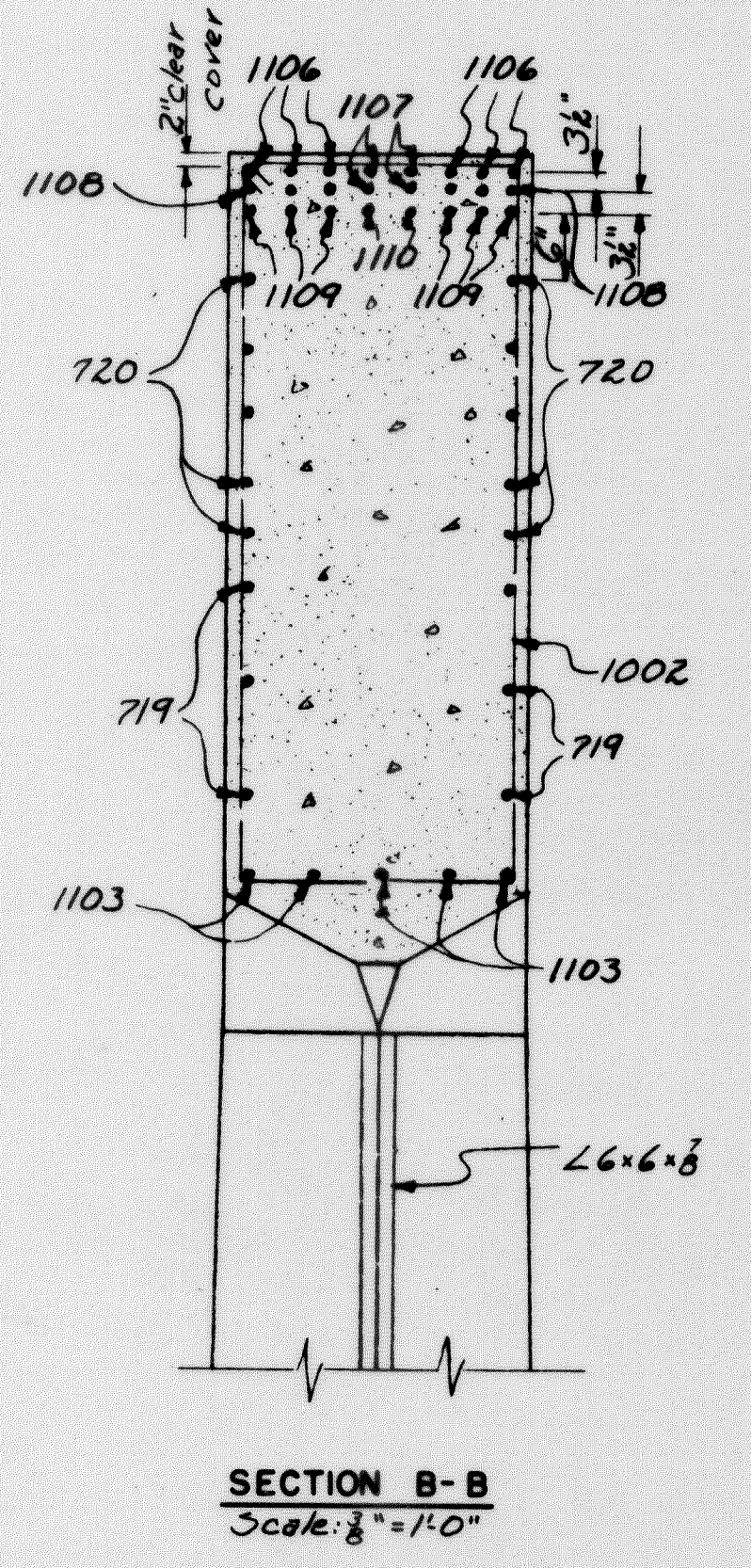
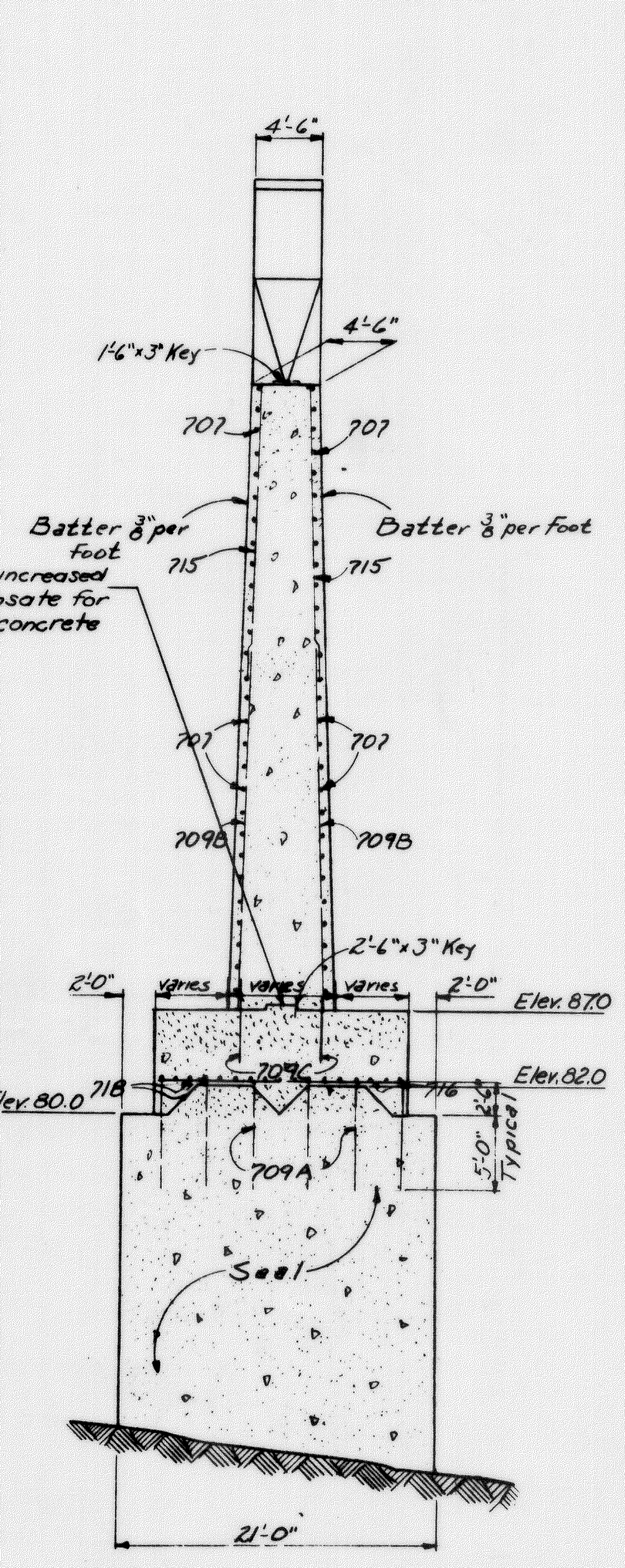
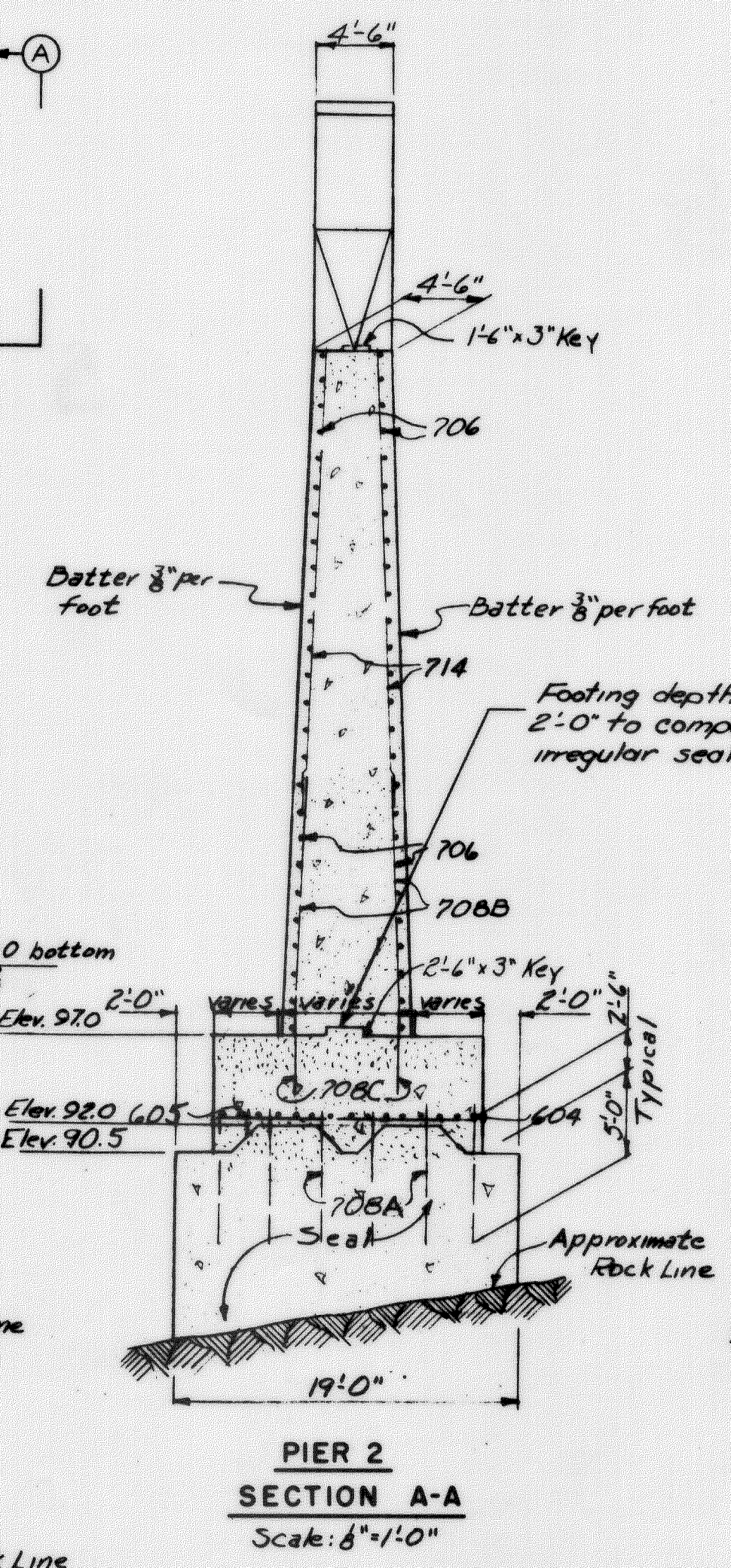
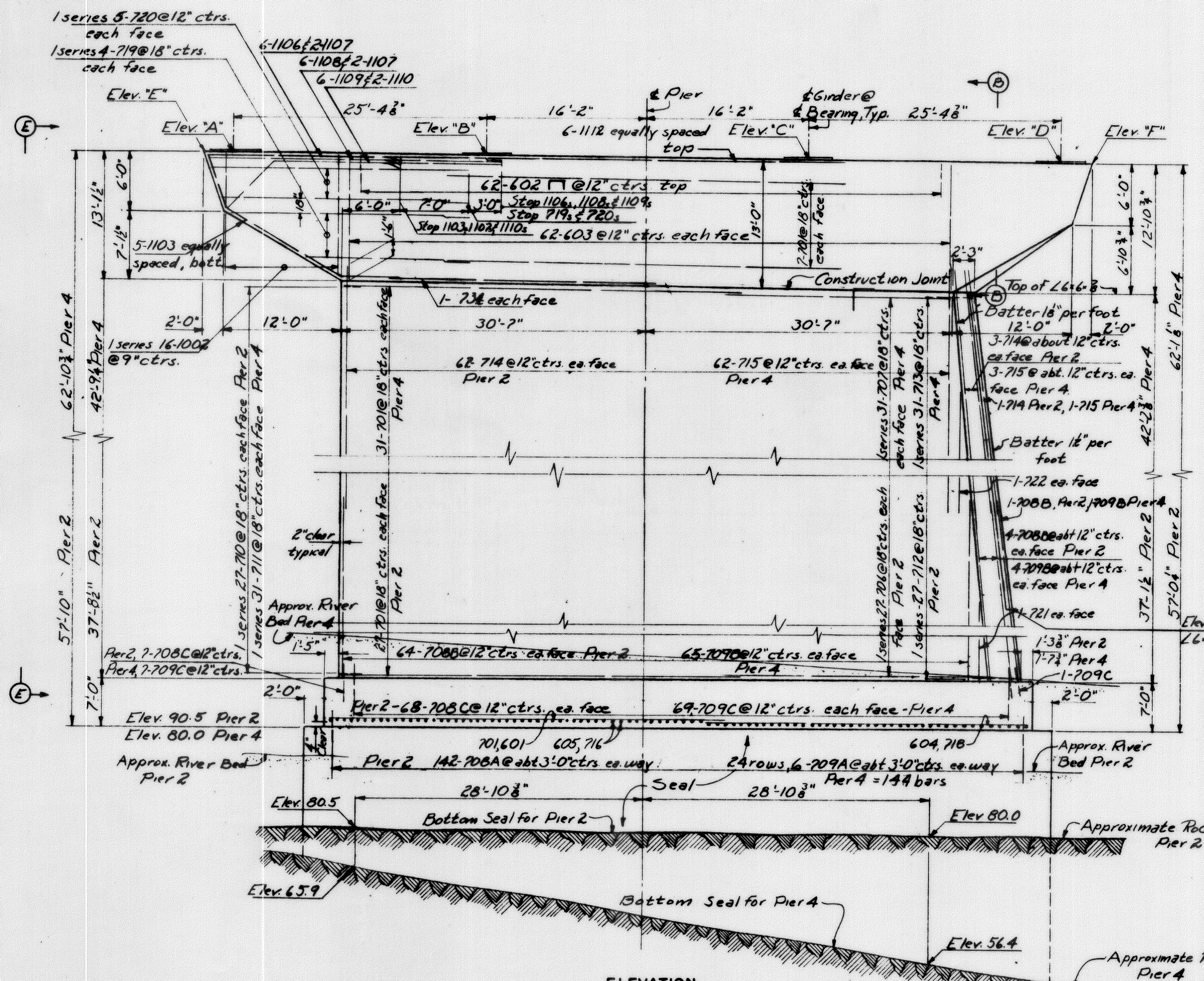
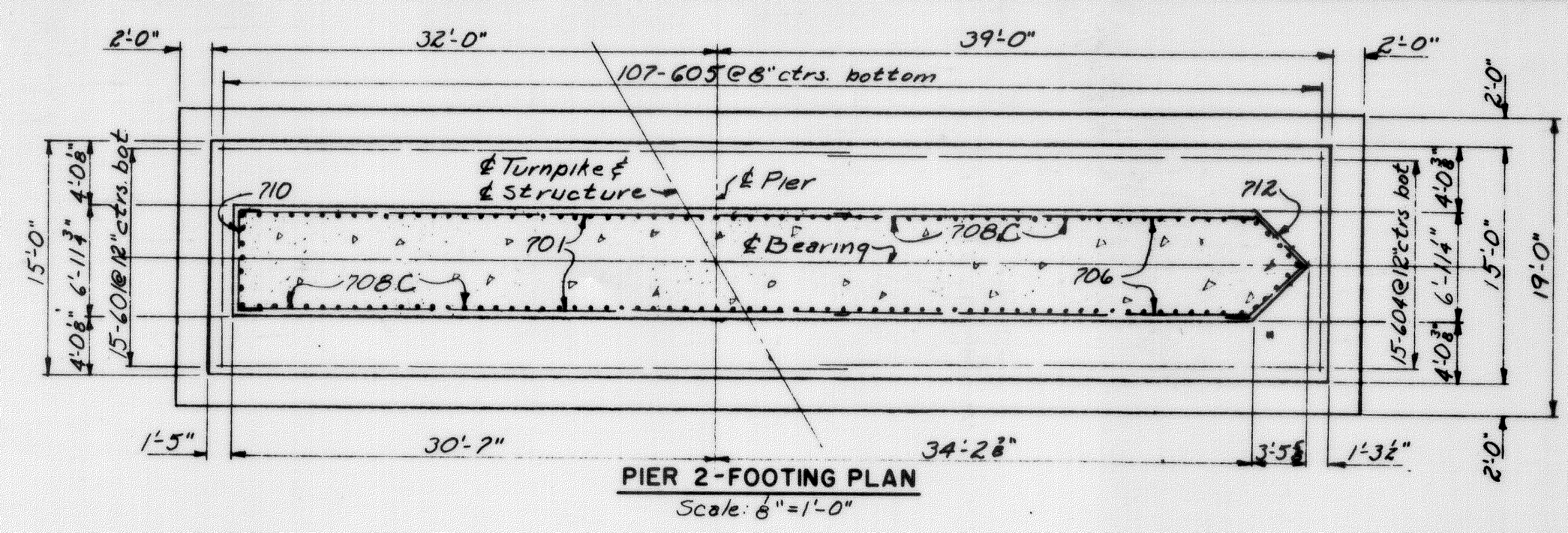
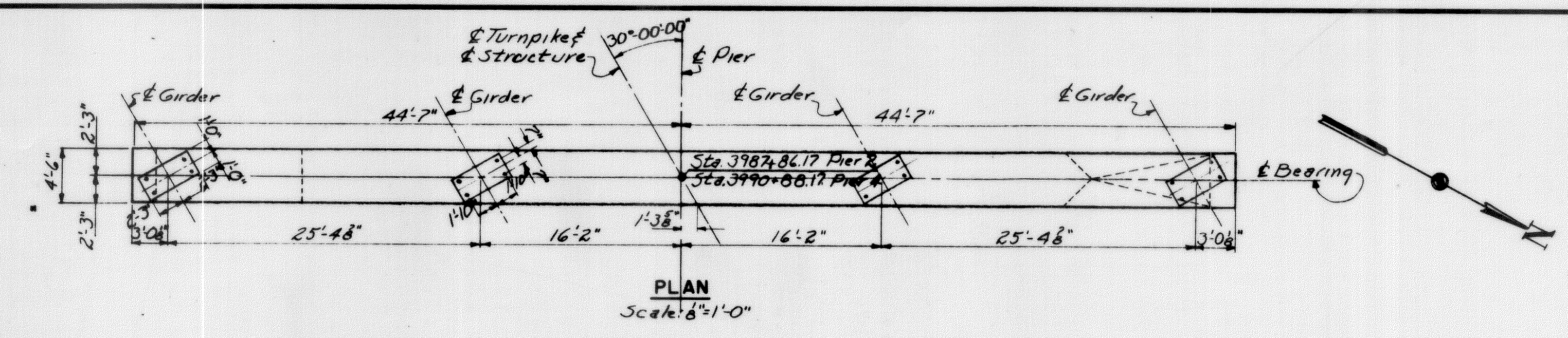
Notes:
 1. For reinforcing steel in pier caps see elevation of Piers 2&4 and section B-B on Sheet No. 55.14
 2. For nose angle anchoring details see Sheet No. 55.13.
 3. Forms for piers shall be of surfaced lumber, plywood, or lined with plywood or other approved lining, or may be approved metal forms.
 4. Nose angles are to be galvanized after strap anchors are attached.

REINFORCING BAR LEGEND
 (Typical for all reinforcing steel)
 First Digit (1st two digits when four digits are used): Bar Size
 Last Two Digits: Bar Number
 Examples: 605 - #6 bar, fifth bar used
 1106 - #11 bar, sixth bar used

DRAWING 55.15.15

BY	DATE	REVISION	BY	DATE
MADE	R.S.G. 3-7-54	3 As-Built	MBH	120-56
TRACED		2 Revised vertical cap dimensions	R.S.G.	6-14-59
CHECKED	DDG 3-18-54	1 Revised Pier Elevs	R.S.G.	6-4-59
IN CHARGE OF	I.D.S.K.	No.	REVISION	BY DATE

MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
SECTION 2 - PORTLAND TO AUGUSTA
 STRUCTURE NO. 55 TURNPIKE OVER
ANDROSCOGGIN RIVER
 STA. 3989 + 37.17
PIERS 1&5
 HOWARD, NEEDLES, TAMMEN & BERGENDOFF
 CONSULTING ENGINEERS
 NEW YORK KANSAS CITY
 SCALE: 3/4" = 1'-0"
 CONTRACT NO. _____
 SHEET NO. 250 OF 302



ELEVATIONS	
PIER 2	PIER 4
A 148.35	142.91
B 148.12	142.69
C 147.83	142.40
D 147.60	142.17
E 148.33	142.89
F 147.52	142.09

ELEVATION PIERS 2 & 4 Scale: 1/8"=1'-0"

PIER 2 SECTION A-A Scale: 1/8"=1'-0"

PIER 4 SECTION A-A Scale: 1/8"=1'-0"

- Notes:
- For nose angle anchoring detail see sheet No. 55.13
 - For method of dowelling footing to seal see note on sheet 55.13. Where bars 1001A, B, and C are called out, substitute bars 708 A, B, and C, for Pier 2, and bars 709 A, B, and C for Pier 4. Indesired lengths.
 - Reinforcing steel in piercaps is symmetrical about & of Pier.
 - For elevation E-E see sheet No. 55.15
 - Forms for piers shall be of surfaced lumber, plywood, or lined with plywood or other approved lining, or may be approved metal forms.
 - Angle to be galvanized after strap anchors are attached.

REINFORCING BAR LEGEND
(Typical for all reinforcing steel)

First Digit (1st two digits when four digits are used): Bar Size
Last Two Digits: Bar Number
Examples: 605 - #6 bar, fifth bar used
1106 - #11 bar, sixth bar used

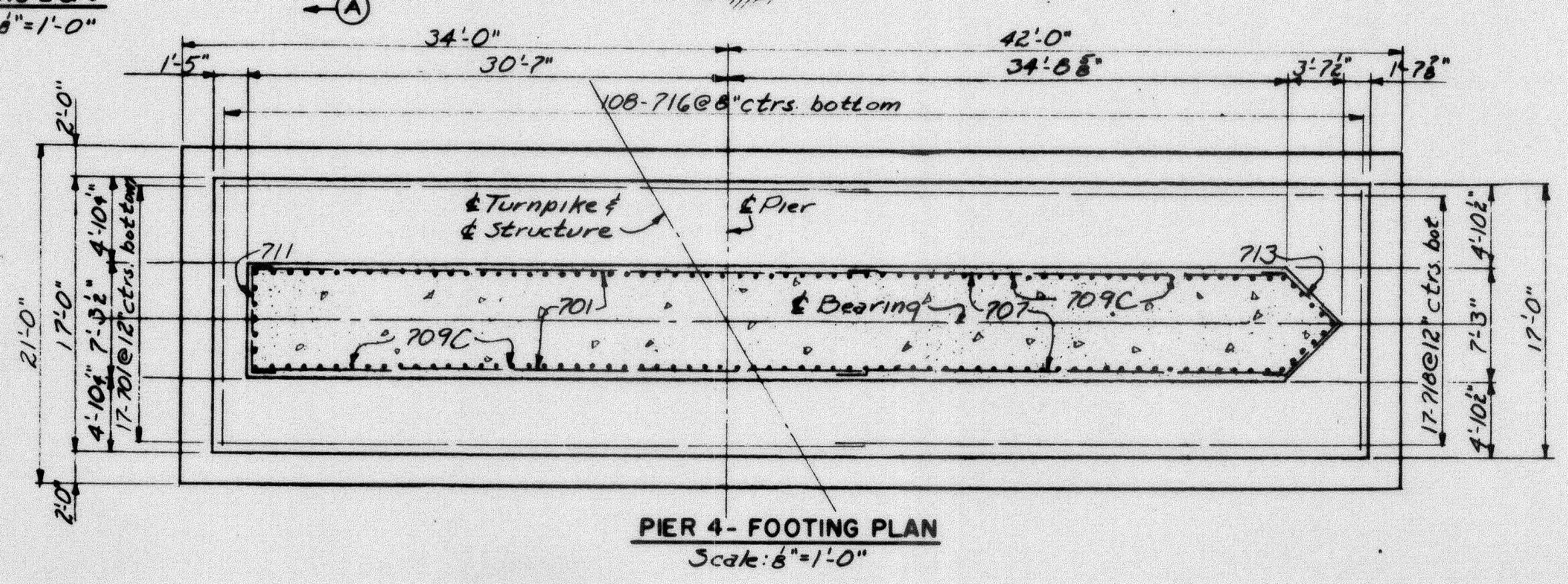
DRAWING 55.14.15				
MADE	BY	DATE	REVISION	BY
TRACED	R.S.G.	3-5-54	2 As-Built	HBM/2056
CHECKED	DDB	5-18-60	Revised vertical cap dimensions	R.S.G. 6-14-58
IN CHARGE OF	L.D.S.K.	No.	REVISION	BY
				DATE

MAINE TURNPIKE AUTHORITY
MAINE TURNPIKE
SECTION 2 - PORTLAND TO AUGUSTA

STRUCTURE NO. 88 TURNPIKE OVER
ANDROSCOGGIN RIVER
STA. 3989+ 37.17
PIERS 2 AND 4

HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS
NEW YORK KANSAS CITY

SCALE: As Shown
CONTRACT NO.
SHEET NO. 249 OF 382



PIER 4-FOOTING PLAN Scale: 1/8"=1'-0"

