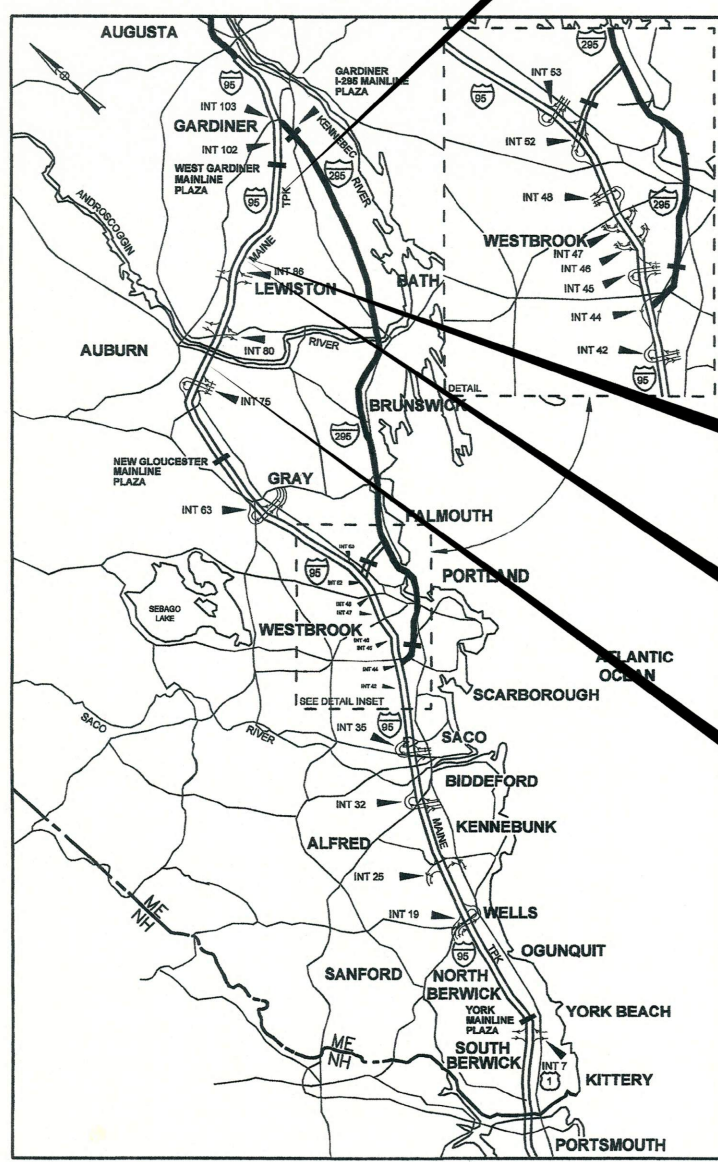


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 Division: HIGHWAY
 Filename: \\20\BRIDGE\MSTA\001_L Title.dgn



LOCATION MAP

CONTRACT 2018.10
 BRIDGE REPAIRS,
 CENTER ROAD
 UNDERPASS MILE 91.9

CONTRACT 2018.10
 BRIDGE REPAIRS,
 FISHER FARM ROAD
 UNDERPASS MILE 87.5

CONTRACT 2018.10
 CURTIS BROOK
 CULVERT REPAIRS
 MILE 87.49

CONTRACT 2018.10
 BRIDGE REPAIRS,
 DANVILLE CORNER ROAD
 UNDERPASS MILE 75.6



THE GOLD STAR
 MEMORIAL HIGHWAY

MAINE TURNPIKE AUTHORITY

DANIEL E. WATHEN, CHAIR
 ROBERT D. STONE, VICE CHAIR
 MICHAEL J. CIANCHETTE, MEMBER
 JOHN E. DORITY, MEMBER
 ANN R. ROBINSON, MEMBER
 THOMAS J. ZUKE, MEMBER
 KAREN S. DOYLE, MEMBER EX-OFFICIO
 S. PETER MILLS, EXECUTIVE DIRECTOR

INDEX OF SHEETS	
SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	ESTIMATED QUANTITIES
3-4	GENERAL NOTES
5	TRAFFIC CONTROL DETAILS
6	DETOUR PLAN MILE 75.8 DANVILLE CORNER ROAD
7	DETOUR PLAN MILE 87.5 FISHER FARM ROAD
8	DETOUR PLAN MILE 91.9 CENTER ROAD
9-10	MOT SIGN SUMMARY
11	FENCING PLAN MILE 87.5
12	CHAIN LINK FENCE DETAILS
13	LIMITS OF DISTURBANCE PLAN
14	TYPICAL DECK PLAN & SECTION
15	JOINT REPAIRS DANVILLE CORNER ROAD
16	JOINT REPAIRS FISHER FARM ROAD
17	JOINT REPAIRS CENTER ROAD
18-20	SUBSTRUCTURE REPAIRS MILE 75.8
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24-26	SUBSTRUCTURE REPAIRS MILE 91.9
27	CURTIS BROOK CULVERT REPAIRS
28	TYPICAL DETAILS

CONTRACT 2018.10 BRIDGE AND CULVERT REPAIRS 4 LOCATIONS DANVILLE CORNER ROAD UNDERPASS (MILE 75.8) CURTIS BROOK CULVERT (MILE 87.49) FISHER FARM ROAD UNDERPASS (MILE 87.5) CENTER ROAD UNDERPASS (MILE 91.9)

CONTRACT 2018.10

APPROVED: MAINE TURNPIKE AUTHORITY

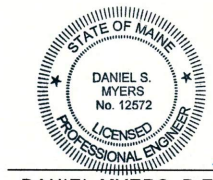
PETER S. MERFELD, P.E.
 CHIEF OPERATIONS OFFICER

12/14/17
 DATE

STEPHEN R. TARTRE, P.E.
 DIRECTOR OF ENGINEERING AND BUILDING MAINTENANCE

12/14/17
 DATE

TYLIN INTERNATIONAL




DANIEL MYERS, P.E.
 CONSULTANT PROJECT MANAGER

12/14/17
 DATE

Date: 12/27/2017

ITEM NO.	ITEM DESCRIPTION	UNIT	DANVILLE CORNER RD UNDERPASS (MILE 75.8)	CURTIS BROOK CULVERT REPAIRS (MILE 87.49)	FISHER FARM RD UNDERPASS (MILE 87.5)	CENTER ROAD UNDERPASS (MILE 91.9)	TOTAL
202.127	REMOVING OF EXISTING BITUMINOUS PAVEMENT (2075 SY)	LS	0.28		0.37	0.35	1
202.191	REMOVING EXISTING DRAIN TROUGHS	LS	0.25		0.50	0.25	1
202.202	REMOVING PAVEMENT SURFACE	SY	410		425	455	1290
203.24	COMMON BORROW	CY		5			5
403.208	HOT MIX ASPHALT, 12.5MM NOMINAL MAXIMUM SIZE	TON	82		98	98	278
403.213	HOT MIX ASPHALT, 12.5MM NOMINAL MAXIMUM SIZE (BASE AND INTERMEDIATE COURSE)	TON	48		63	60	171
409.15	BITUMINOUS TACK COAT - APPLIED	GAL	63		78	77	218
507.095	ALUMINUM BRIDGE RAILING - SPLICE MODIFICATION	EA	10		16	12	38
508.14	HIGH PERFORMANCE WATERPROOFING MEMBRANE (2075 SY)	LS	0.28		0.37	0.35	1
511.07	COFFERDAM	LS		1			1
515.201	PIGMENTED PROTECTIVE COATING FOR CONCRETE SURFACES	SY	320		340	355	1015
515.202	CLEAR PROTECTIVE COATING FOR CONCRETE SURFACES	SY	470		655	595	1720
518.10	ABUTMENT REPAIRS	SF	15	10	25	20	70
518.20	PIER REPAIRS	SF	80		100	80	260
518.39	GRANITE CURB JOINT MORTAR AND BEDDING MORTAR REPAIR	LF	120			150	270
518.40	EPOXY INJECTION CRACK REPAIR	LF		40	20	5	65
518.43	PARAPET JOINT REPAIR	LF	400		500	410	1310
518.75	FASCIA AND OVERHANG REPAIRS	SF	45		55	5	105
518.80	PARTIAL DEPTH CONCRETE DECK REPAIRS	SF	160		210	200	570
518.81	FULL DEPTH CONCRETE DECK REPAIRS	SF	10		10	10	30
520.2211	EXPANSION DEVICE MODIFICATIONS (DANVILLE CORNER ROAD)	EA	2				2
520.2211	EXPANSION DEVICE MODIFICATIONS (FISHER FARM ROAD)	EA			2		2
520.2211	EXPANSION DEVICE MODIFICATIONS (CENTER ROAD)	EA				2	2
526.306	TEMPORARY CONCRETE BARRIER, TYPE 1 - SUPPLIED BY AUTHORITY	LS	0.33		0.33	0.33	1
527.341	WORK ZONE CRASH CUSHIONS - TL-3	UNIT	2		2	2	6
603.91	PRESSURE TREATED WOOD DRAIN TROUGH	LF			70		70
607.17	CHAIN LINK FENCE - 6 FOOT	LF			355		355
607.171	CHAIN LINK FENCE - 6 FOOT PVC PRIVACY FENCE	LF			1750		1750
607.23	CHAIN LINK FENCE GATE	EA			2		2
607.34	BRACING ASSEMBLY CHAIN LINK FENCE	EA			20		20
610.08	PLAIN RIPRAP	CY		8			8
615.07	LOAM	CY	10		25	25	60
618.14	SEEDING METHOD NUMBER 2	UNIT	1		2	2	5
619.1201	MULCH - PLAN QUANTITY	UNIT	1		2	2	5
619.1202	TEMPORARY MULCH	LS	0.2		0.4	0.4	1
619.1401	EROSION CONTROL MIX	CY	20		15	15	50
620.58	EROSION CONTROL GEOTEXTILE	SY		24			24
627.733	4" WHITE OR YELLOW PAINTED PAVEMENT MARKING LINE	LF	350		440	1200	1990
639.19	FIELD OFFICE, TYPE B	EA	0.33		0.33	0.33	1
643.712	FLASHING BEACON - SOLAR POWERED	EA	1				1
652.30	FLASHING ARROW BOARD	EA	2		2	2	6
652.312	TYPE III BARRICADES	EA	4		4	4	12
652.331	DRUM	LS	0.33		0.33	0.33	1
652.35	CONSTRUCTION SIGNS	SF	1850		1500	1500	4850
652.361	MAINTENANCE OF TRAFFIC CONTROL DEVICES	LS	0.33		0.33	0.33	1
652.41	PORTABLE-CHANGEABLE MESSAGE SIGN	EA	2		2	2	6
652.45	TRUCK MOUNTED ATTENUATOR	CD	14		14	14	42
652.451	AUTOMATED TRAILER MOUNTED SPEED LIMIT SIGN	CD	14		14	14	42
652.46	TEMPORARY PORTABLE RUMBLE STRIPS	UNIT	42		42	42	126
656.632	30 INCH TEMPORARY SILT FENCE	LF	100	25	100	100	325
659.10	MOBILIZATION	LS	0.29	0.02	0.40	0.29	1

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Scale: <p style="text-align: center;">NOT TO SCALE</p>	Designed by: <p style="text-align: center;">TY-LIN INTERNATIONAL</p>	T.Y. Lin International 12 Northbrook Drive Building A, Suite One Falmouth, Maine 04105 TEL: (207) 781-4721 FAX: (207) 781-4753		<p style="font-size: 1.2em; font-weight: bold;">THE GOLD STAR MEMORIAL HIGHWAY</p>	BRIDGE AND CULVERT REPAIRS 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9) QUANTITIES																												
<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>No.</th> <th>Revision</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>		No.	Revision	By	Date													CONSULTANT PROJECT MANAGER: Norman L. Baker <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>By</th> <th>Date</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr> <td>Designed DSM</td> <td>11/2017</td> <td>Checked BLT</td> <td>11/2017</td> </tr> <tr> <td>Drawn DSM</td> <td>11/2017</td> <td>In Charge of DSM</td> <td>11/2017</td> </tr> </tbody> </table>		By	Date	By	Date	Designed DSM	11/2017	Checked BLT	11/2017	Drawn DSM	11/2017	In Charge of DSM	11/2017	MTA PROJECT MANAGER: R. NORWOOD	SHEET NUMBER: QT-01 CONTRACT: 2018.10 2 OF 28
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Drawn DSM	11/2017	In Charge of DSM	11/2017																														

Date: 12/22/2017

Filename: ... \BRIDGE\WSTA\003_Cen_Notes.dgn

SPECIFICATIONS:

DESIGN

AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS 7TH EDITION WITH INTERIMS THROUGH 2016.

CONSTRUCTION

STATE OF MAINE, DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS NOVEMBER, 2014

STATE OF MAINE DEPARTMENT OF TRANSPORTATION STANDARD DETAILS, NOVEMBER 2014, WITH LATEST REVISIONS

AASHTO LRFD BRIDGE CONSTRUCTION SPECIFICATIONS, THIRD EDITION WITH 2016 INTERIMS

MATERIALS:

CONCRETE

ALL PATCHING AND REPAIR AREAS SHALL BE CLASS AAA-MODIFIED.

REINFORCING STEEL

AASHTO M31, GRADE 60 EPOXY-COATED.

BASIC DESIGN STRESSES:

CONCRETE

CLASS AAA - MODIFIED, $f'_c = 4,500$ P.S.I.

REINFORCING STEEL

$f_y = 60,000$ P.S.I.

GENERAL NOTES:

1. THE CONTRACTOR IS REQUIRED TO CALL DIG SAFE AT 1-888-344-7233 AT LEAST 72 HOURS PRIOR TO THE START OF THE WORK.
2. ALL DETAILS SHALL BE IN CONFORMANCE WITH MAINE DEPARTMENT OF TRANSPORTATION (MDOT) STANDARD DETAILS, HIGHWAYS AND BRIDGES, LATEST REVISION AND MDOT BEST MANAGEMENT PRACTICES FOR EROSION AND SEDIMENT CONTROL, LATEST REVISION UNLESS OTHERWISE INCLUDED IN THESE PLANS.
3. IN AREAS OF PROPOSED PIPE ENTRY GATES AND CHAIN LINK FENCE, EXISTING CHAIN LINK OR WIRE FENCE SHALL BE REMOVED, REMOVING AND DISPOSING OF EXISTING FENCE SHALL BE INCIDENTAL TO PIPE ENTRY AND CHAIN LINK BID ITEMS. PROPOSED CHAIN LINK FENCE AND PIPE ENTRY GATE SHALL BE INSTALLED CONTINUOUSLY WITH NO GAPS. CHAIN LINK FENCE MAY NEED TO BE CUT OR ADDITIONAL POSTS, BRACING ASSEMBLIES OR FABRIC MAY NEED TO BE INSTALLED.
4. CONNECTION FOR PROPOSED FENCE TO EXISTING FENCE SHALL BE INCIDENTAL TO THE CONTRACT INCLUDING ANY REQUIRED ADDITIONAL BRACING FOR THE EXISTING FENCE.
5. NO SEPARATE PAYMENT FOR SUPERINTENDENT OR FOREMAN WILL BE MADE FOR THE SUPERVISION OF EQUIPMENT BEING PAID FOR UNDER THE EQUIPMENT RENTAL ITEMS.
6. THE PROPOSED ELEVATIONS ARE BASED ON THE NAVD 88 DATUM. THE AS-BUILT PLANS ARE BASED ON NGVD 29 DATUM.
7. FOR ADDITIONAL DETAILS REFERENCED OR NOT SHOWN IN THESE DRAWINGS, SEE THE STATE OF MAINE, DEPARTMENT OF TRANSPORTATION STANDARD DETAILS, HIGHWAYS AND BRIDGES, NOVEMBER 2014 WITH UPDATES.
8. COPIES OF THE AS-BUILT PLANS ARE ON FILE AT THE MAINE TURNPIKE AUTHORITY. A PORTION OF THESE PLANS ARE INCLUDED IN THIS CONTRACT FOR THE CONTRACTOR'S CONVENIENCE. THE COMPLETENESS AND ACCURACY OF THESE PLANS IS NOT GUARANTEED.
9. REINFORCING STEEL SHALL HAVE A CLEAR COVER OF 2" MIN., UNLESS OTHERWISE NOTED.
10. CHAMFER ALL EXPOSED CONCRETE EDGES $\frac{3}{4}$ " UNLESS OTHERWISE NOTED.
11. ALL PROPOSED CONCRETE CURB FACES AND INSIDE AND TOP FACES OF THE ENDPOSTS SHALL RECEIVE A RUBBED FINISH PRIOR TO THE APPLICATION OF THE CLEAR PROTECTIVE COATING FOR CONCRETE SURFACE.
12. SHIELDING REQUIRED DURING CONSTRUCTION SHALL NOT PROJECT BELOW THE BOTTOM FLANGES OF GIRDERS.
13. ALL PAINT ON EXISTING STEEL, DAMAGED BY CONTRACTOR'S OPERATIONS, SHALL BE REPAIRED TO THE SATISFACTION OF THE RESIDENT AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL NOTE THAT THE EXISTING BRIDGE STRUCTURES CONTAIN LEAD BASED PAINT. THE CONTRACTOR SHALL INSTITUTE EVERY PRECAUTION WHEN WORKING WITH MATERIALS COATED WITH LEAD PAINT.
14. WHERE DRILLING AND ANCHORING OF REINFORCING STEEL IS REQUIRED, THE CONTRACTOR SHALL USE A MATERIAL LISTED ON THE MAINEDOT PREQUALIFIED LIST OF CHEMICAL ANCHORING MATERIALS. THE DEPTH OF EMBEDMENT SHALL BE SUFFICIENT TO DEVELOP 125% OF THE YIELD STRENGTH OF THE BAR, BUT SHALL BE NO LESS THAN THE MINIMUM DEPTH OF EMBEDMENT WHEN SPECIFIED. WHERE MINIMUM EMBEDMENT DEPTH HAVE NOT BEEN SPECIFIED, BAR LENGTHS HAVE BEEN DEVELOPED BASED ON AN ASSUMED EMBEDMENT DEPTH OF 9" FOR #5 BARS AND 11" FOR #6 BARS. THE CONTRACTOR SHALL VERIFY THE REQUIRED DEPTH OF EMBEDMENT AND ADJUST THE REQUIRED BAR LENGTHS AS REQUIRED.
15. PAYMENT FOR DRILLING AND ANCHORING OF REINFORCING STEEL IS INCIDENTAL TO ITEM 503.15, EPOXY-COATED REINFORCING STEEL, PLACING.

UTILITY:

1. THE CONTRACTOR SHALL NOTIFY THE RESIDENT 10 DAYS PRIOR TO CONSTRUCTION SO THE RESIDENT CAN ARRANGE FOR MAINE TURNPIKE UNDERGROUND UTILITY LOCATION. ALL PROPOSED SIGN LOCATIONS AND EXCAVATIONS LOCATIONS SHALL BE MARKED AT THE NOTIFICATION TIME. EXCAVATING WILL NOT BE PERMITTED UNTIL THE AUTHORITY HAS LOCATED AND MARKED ITS UNDERGROUND UTILITIES, OR NOTIFIED THE RESIDENT THAT THERE ARE NO UNDERGROUND UTILITIES IN THE MARKED AREAS.
2. THE AUTHORITY AS PROGRAMMED TWO FIELD VISITS FOR MAINE TURNPIKE UTILITY COORDINATION ON THIS PROJECT. SHOULD THE CONTRACTOR NEED ADDITIONAL SIGN LOCATIONS AND/OR ADDITIONAL EXCAVATION LOCATIONS MARKED, OR SHOULD THE CONTRACTOR FAIL TO MAINTAIN THE AUTHORITY'S PREVIOUSLY ESTABLISHED DIG SAFE MARKS, THE AUTHORITY SHALL DEDUCT THE ADDED MARKINGS COSTS FROM THE CONTRACTOR'S PAYMENTS.

EARTHWORK & EROSION CONTROL:

1. WASTE MATERIALS SHALL BE DISPOSED OF OFF THE PROJECT SITE, IN ACCORDANCE WITH CHAPTER 404, DEPARTMENT OF ENVIRONMENTAL PROTECTION SOLID WASTE MANAGEMENT RULES.
2. EXCAVATIONS ACCOMPLISHED AS PART OF THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH OSHA SUBPART P OF 29 CFR PART 1926.650-652 (CONSTRUCTION STANDARDS FOR EXCAVATION).
3. THE ANTICIPATED EROSION CONTROL DEVICES FOR THE CONTRACT LIMIT OF DISTURBANCE ARE SHOWN ON THE PLANS. THE CONTRACTOR SHALL PROPOSE ACTUAL TYPE AND LOCATON OF DEVICES FOR APPROVAL BY THE RESIDENT. ADDITIONAL MEASURES MAY BE PROPOSED BY THE CONTRACTOR DUE TO SITE OR WEATHER CONDITIONS. THE RESIDENT MAY DIRECT THE CONTRACTOR TO IMPLEMENT ADDITIONAL MEASURES. ANY ADDITIONAL MEASURES APPROVED BY THE RESIDENT WILL BE MEASURED FOR PAYMENT.
4. 4" LOAM HAS BEEN ESTIMATED FOR 100% OF THE DISTURBED SLOPE AREAS UNLESS OTHERWISE SPECIFIED ON THE PLANS. ACTUAL PLACEMENT OF THE LOAM SHALL BE AS DESIGNATED BY THE RESIDENT.
5. ALL SLOPES SHALL BE SEEDED WITH SEEDING METHOD NO. 2 UNLESS OTHERWISE DIRECTED BY THE RESIDENT.
6. MULCH SHALL BE APPLIED IN AREAS SEEDED EXCEPT WHERE EROSION CONTROL BLANKET IS APPLIED.

CONCRETE REPAIR PROCEDURAL NOTES:

1. CONCRETE REPAIRS, INCLUDING THE ASSOCIATED REMOVALS, WILL BE PAID UNDER 518 ITEMS.
2. ANY REINFORCING STEEL REQUIRED TO REPLACE DETERIORATED EXISTING REINFORCEMENT SHALL BE EPOXY COATED AND WILL BE PAID FOR UNDER ITEM 503.14, "EPOXY-COATED REINFORCING STEEL, FABRICATED AND DELIVERED" AND ITEM 503.15, "EPOXY-COATED REINFORCING STEEL, PLACING."

REMOVAL PROCEDURE:

1. CONTRACTOR SHALL REMOVE TECTYL COATING WHERE PRESENT PRIOR TO WORK. PAYMENT SHALL BE INCIDENTAL TO THE SPECIFIED 518 ITEMS.
2. PRIOR TO THE START OF THE CONCRETE REMOVAL, THE RESIDENT AND THE CONTRACTOR SHALL SOUND ALL EXPOSED SURFACES OF CONCRETE AND AGREE ON THE REMOVAL LIMITS. SHOULD THE REMOVAL AREA LIMITS APPEAR TO CHANGE DURING THE DEMOLITION PROCESS, THE CONTRACTOR SHALL NOTIFY THE RESIDENT. THE RESIDENT AND CONTRACTOR SHALL AGREE ON THE REVISED PAY LIMITS PRIOR TO THE CONTRACTOR CONTINUING THE REMOVAL.
3. PERFORM 1 INCH DEEP SAWCUTS ALONG LIMITS OF REMOVAL.
4. CHIP CONCRETE TO DEPTH REQUIRED PER SPECIFICATIONS.

CONCRETE SURFACE PATCH/REPAIR PROCEDURE:

1. PREPARE AND PATCH REPAIR AREAS WITH CLASS AAA - MODIFIED CONCRETE. SEE SPECIFICATIONS FOR MATERIAL, PREPARATION, PLACEMENT, AND CURING REQUIREMENTS.

GENERAL FINISHING:

1. ALL EXPOSED CONCRETE SURFACES SHALL BE COATED WITH A PROTECTIVE COATING IN ACCORDANCE WITH THE SPECIAL PROVISIONS AFTER PATCHING IS COMPLETE AND PATCH MATERIALS HAVE CURED. PIGMENTED PROTECTIVE COATING SHALL BE APPLIED TO ALL ABUTMENT FACES, ALL WINGWALL FACES, AND ALL END POST OUTSIDE FACES. CLEAR PROTECTIVE COATING SHALL BE APPLIED TO ALL CONCRETE CURB FACES, ALL FASCIAS AND FASCIA OVERHANGS, AND ALL END POST INSIDE AND TOP FACES.

EPOXY INJECTION CRACK REPAIR:

1. PREPARE CONCRETE SURFACE AND CRACK AREA BY CLEANING SUBSTRATE WITH WIRE BRUSH TO REMOVE LAITANCE AND CONTAMINANTS. BLAST CLEAN THE CRACK AND CONCRETE SURFACE; DO NOT USE WATER.
2. SET INJECTION PORTS ALONG THE LENGTH OF THE CRACK USING A HIGH-MODULUS EPOXY ADHESIVE. ONCE SET, SEAL PORTS AND CRACK WITH SAME EPOXY ADHESIVE.
3. WHEN THE EPOXY ADHESIVE SEAL HAS CURED, INJECT A HIGH-MODULUS, LOW-VISCOSITY EPOXY SUCH AS SIKADUR 35, SIKADUR 55, OR APPROVED EQUAL.


LIST OF ABBREVIATIONS

ABUT.	ABUTMENT
ADDL.	ADDITIONAL
ASCG.	AGGREGATE SUBBASE COURSE GRAVEL
ALT.	ALTERNATE
APPROX.	APPROXIMATE
BOT.	BOTTOM
BRG.	BEARING
CL.	CLEAR
CL.	CENTERLINE
CONC.	CONCRETE
CONSTR.	CONSTRUCTION
DEMO.	DEMOLITION
DIA.	DIAMETER
DIAPH.	DIAPHRAGM
EA.	EACH
EB	EASTBOUND
E.F.	EACH FACE
EL.	ELEVATION
EQ.	EQUAL
EXIST.	EXISTING
EXP.	EXPANSION
F.F.	FAR FACE
JT.	JOINT
MAX.	MAXIMUM
MIN.	MINIMUM
MTA	MAINE TURNPIKE AUTHORITY
NB	NORTHBOUND
N.F.	NEAR FACE
N.T.S.	NOT TO SCALE
O.H.W.	ORDINARY HIGH WATER
PED.	PEDESTAL
PGL	PROFILE GRADE LINE
PL	PLATE
PROP.	PROPOSED
RDWY.	ROADWAY
SHLD.	SHOULDER
SB	SOUTHBOUND
SP.	SPACES
STA.	STATION
T.&B.	TOP & BOTTOM
TPKE.	TURNPIKE
TYP.	TYPICAL
U.O.N.	UNLESS OTHERWISE NOTED
VERT.	VERTICAL
WB	WESTBOUND
W.P.	WORKING POINT

Scale: NOT TO SCALE			
No.	Revision	By	Date

Designed by: TYLIN INTERNATIONAL			
CONSULTANT PROJECT MANAGER: Norman L. Baker			
	By	Date	
	DSM	10/2017	
	By	Date	
	NLB	10/2017	
	By	Date	
	DSM	10/2017	
	In Charge of		
	DSM	11/2017	

T.Y. Lin International
12 Northbrook Drive
Building A, Suite One
Falmouth, Maine 04105
TEL: (207) 781-4721
FAX: (207) 781-4753



**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)

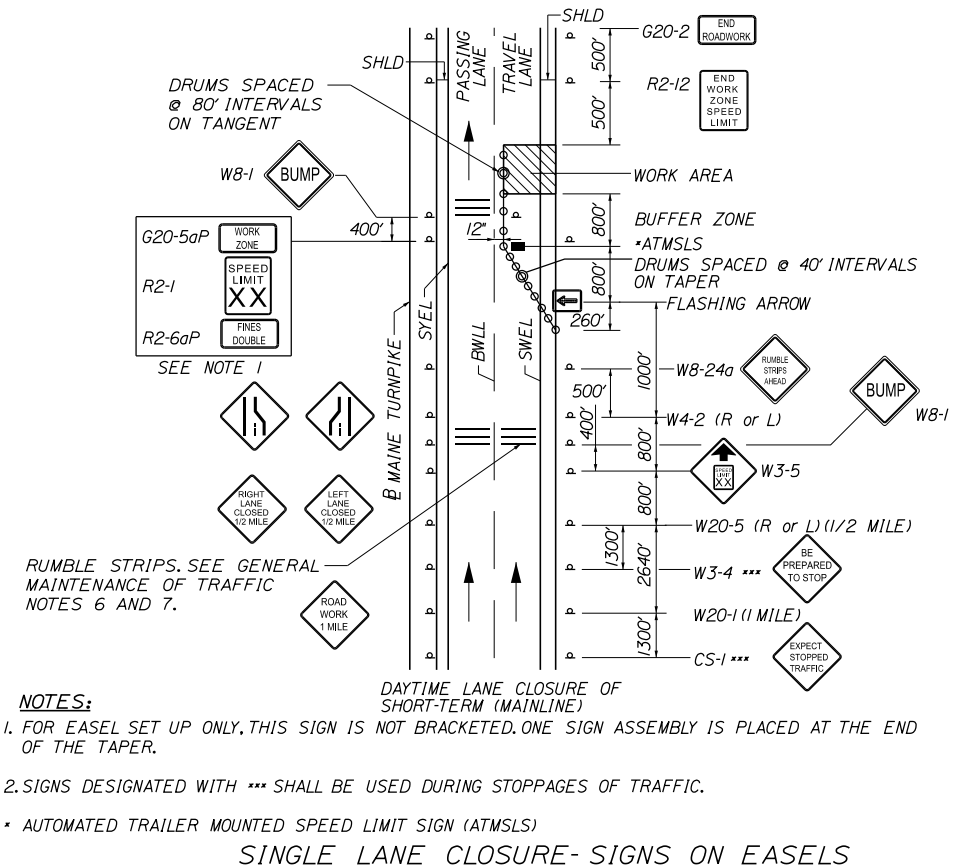
GENERAL NOTES

SHEET NUMBER: GN-01
3 OF 28

CONTRACT: 2018.10

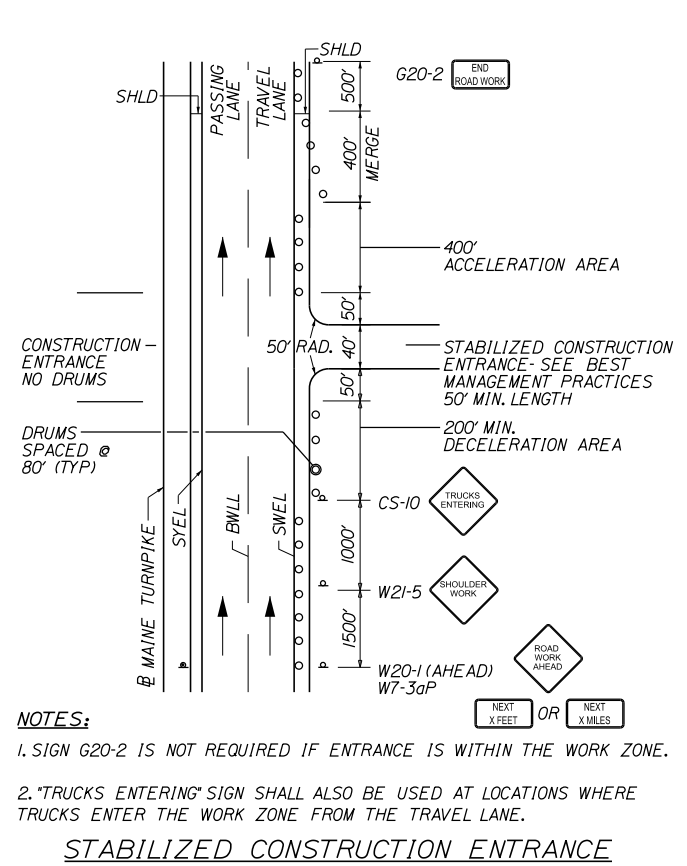
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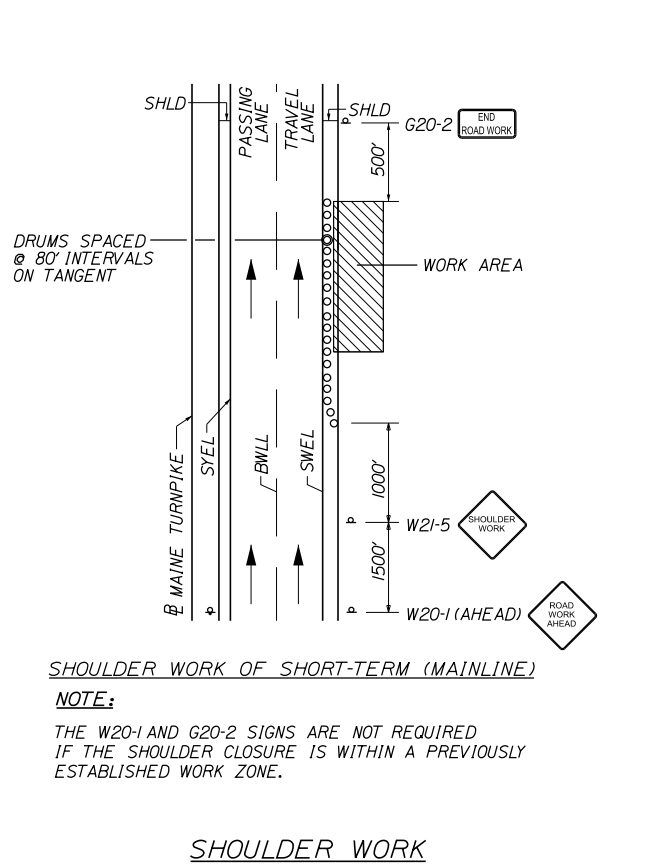
NOTES:
 1. FOR EASEL SET UP ONLY, THIS SIGN IS NOT BRACKETED. ONE SIGN ASSEMBLY IS PLACED AT THE END OF THE TAPER.
 2. SIGNS DESIGNATED WITH *** SHALL BE USED DURING STOPPAGES OF TRAFFIC.
 * AUTOMATED TRAILER MOUNTED SPEED LIMIT SIGN (ATMSLS)

SINGLE LANE CLOSURE- SIGNS ON EASELS



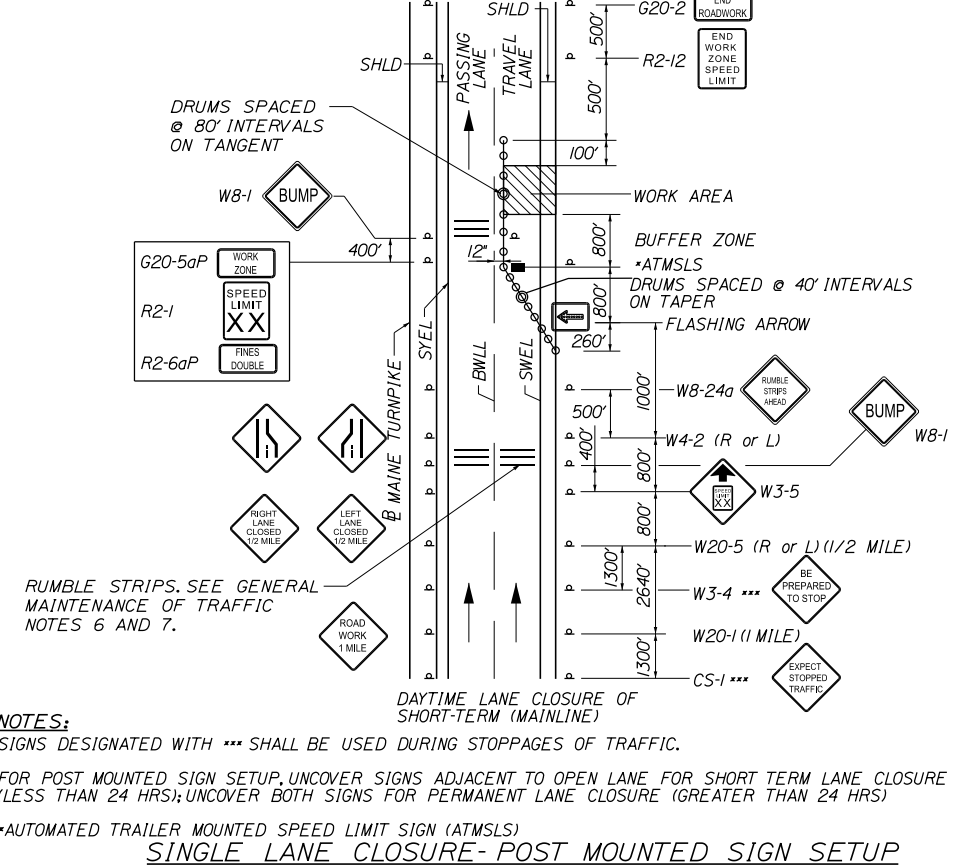
NOTES:
 1. SIGN G20-2 IS NOT REQUIRED IF ENTRANCE IS WITHIN THE WORK ZONE.
 2. "TRUCKS ENTERING" SIGN SHALL ALSO BE USED AT LOCATIONS WHERE TRUCKS ENTER THE WORK ZONE FROM THE TRAVEL LANE.

STABILIZED CONSTRUCTION ENTRANCE



NOTE:
 THE W20-1 AND G20-2 SIGNS ARE NOT REQUIRED IF THE SHOULDER CLOSURE IS WITHIN A PREVIOUSLY ESTABLISHED WORK ZONE.

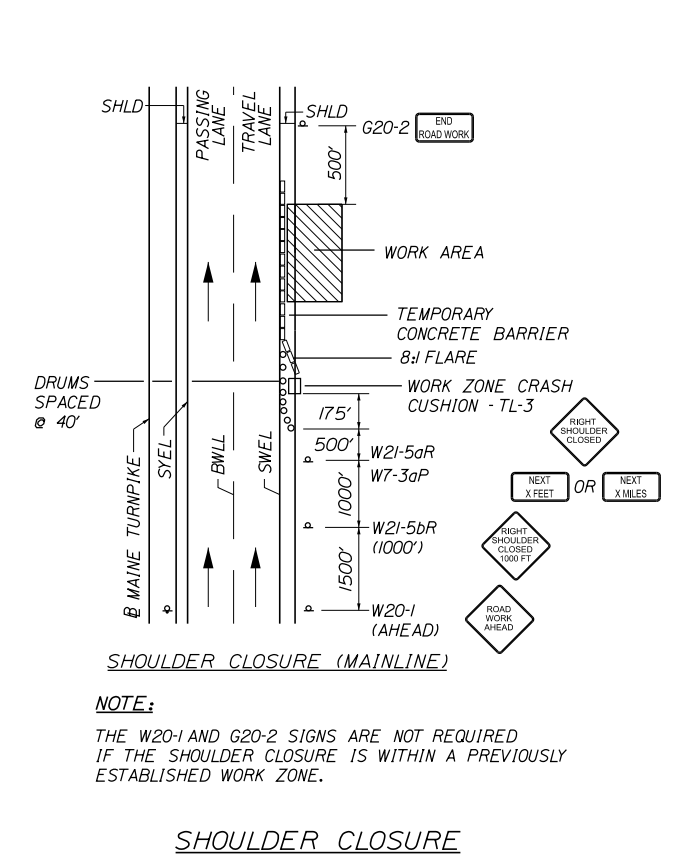
SHOULDER WORK



NOTES:
 SIGNS DESIGNATED WITH *** SHALL BE USED DURING STOPPAGES OF TRAFFIC.
 FOR POST MOUNTED SIGN SETUP, UNCOVER SIGNS ADJACENT TO OPEN LANE FOR SHORT TERM LANE CLOSURE (LESS THAN 24 HRS); UNCOVER BOTH SIGNS FOR PERMANENT LANE CLOSURE (GREATER THAN 24 HRS)

*AUTOMATED TRAILER MOUNTED SPEED LIMIT SIGN (ATMSLS)

SINGLE LANE CLOSURE- POST MOUNTED SIGN SETUP



NOTE:
 THE W20-1 AND G20-2 SIGNS ARE NOT REQUIRED IF THE SHOULDER CLOSURE IS WITHIN A PREVIOUSLY ESTABLISHED WORK ZONE.

SHOULDER CLOSURE

- GENERAL MAINTENANCE OF TRAFFIC NOTES:**
- ALL PAVEMENT STRIPING & SIGNING SHALL BE IN ACCORDANCE WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", U.S.D.O.T., F.H.W.A., LATEST EDITION. NOTE THERE IS A 2009 EDITION OF MUTCD.
 - THESE PLANS SHOW THE GENERAL CONDITION FOR TURNPIKE MAINLINE TRAFFIC CONTROL DURING CONSTRUCTION. SLIGHT MODIFICATIONS IN CONSTRUCTION PROCEDURE MAY OCCUR AND MAY REQUIRE SOME MINOR ADJUSTMENTS TO BE MADE IN THE FIELD. ALL PROCEDURES MUST BE APPROVED BY THE RESIDENT.
 - THE CONTRACTOR SHALL REMOVE ALL PAVEMENT MARKINGS THAT CONFLICT WITH PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH THE SPECIFICATIONS AND MUTCD. PAYMENT SHALL BE MADE UNDER ITEM 627.77 - REMOVING PAVEMENT MARKINGS.
 - EXPOSED BARRIER ENDS SHALL BE PROTECTED BY A WORK ZONE CRASH CUSHION. PAYMENT WILL BE UNDER ITEM 527.341 - WORK ZONE CRASH CUSHION - TL-3.
 - NO SIGNS SHALL BE PLACED ON 4' SHOULDERS WITH GUARDRAIL SIGNS REQUIRED AT THESE LOCATIONS SHALL BE PLACED ON TALLER EASELS ON THE MEDIAN SIDE OF THE GUARDRAIL.
 - IF RUMBLE STRIPS ARE USED THEY SHALL BE PLACED IN ONE OF THE FOLLOWING CONFIGURATIONS:
 - ADJACENT TO THE WORK ZONE (1 UNIT)
 - UPSTREAM FROM THE TAPER FOR THE WORKZONE (2 UNITS)
 - BOTH ADJACENT TO THE WORKZONE AND PRIOR TO THE TAPER (3 UNITS)
 W8-1 SIGNS SHALL BE PLACED ADJACENT TO THE FIRST RUMBLE STRIP AT ANY LOCATION. ONLY ONE SET OF W8-24a SIGNS ARE REQUIRED FOR ANY OF THE ABOVE CONFIGURATIONS.
 - RUMBLE STRIPS MAY BE PLACED UPSTREAM OF THE TAPER BETWEEN THE W3-5 SIGNS AND THE W4-2 SIGNS. IF RUMBLE STRIPS ARE INSTALLED PRIOR TO TAPER, W8-1 SIGNS SHALL BE PLACED ADJACENT TO THE FIRST STRIP AND THE W8-24a SIGNS SHALL BE MOVED TO 400' AFTER THE W20-5 SIGNS.

ABBREVIATIONS FOR ALL M.O.T. PLANS:
 BWLL = BROKEN WHITE LANE LINE
 SWEL = SOLID WHITE EDGE LINE
 SYEL = SOLID YELLOW EDGE LINE
 TBWL = TEMPORARY BROKEN WHITE LANE LINE
 TSWEL = TEMPORARY SOLID WHITE EDGE LINE
 TSYEL = TEMPORARY SOLID YELLOW EDGE LINE

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

By	Date	By	Date
Designed	JRH 8/2017	Checked	KSD 9/2017
Drawn	JRH 8/2017	In Charge of	DSM 11/2017

T.Y. Lin International
 12 Northbrook Drive
 Building A, Suite One
 Falmouth, Maine 04105
 TEL: (207) 781-4721
 FAX: (207) 781-4753

MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

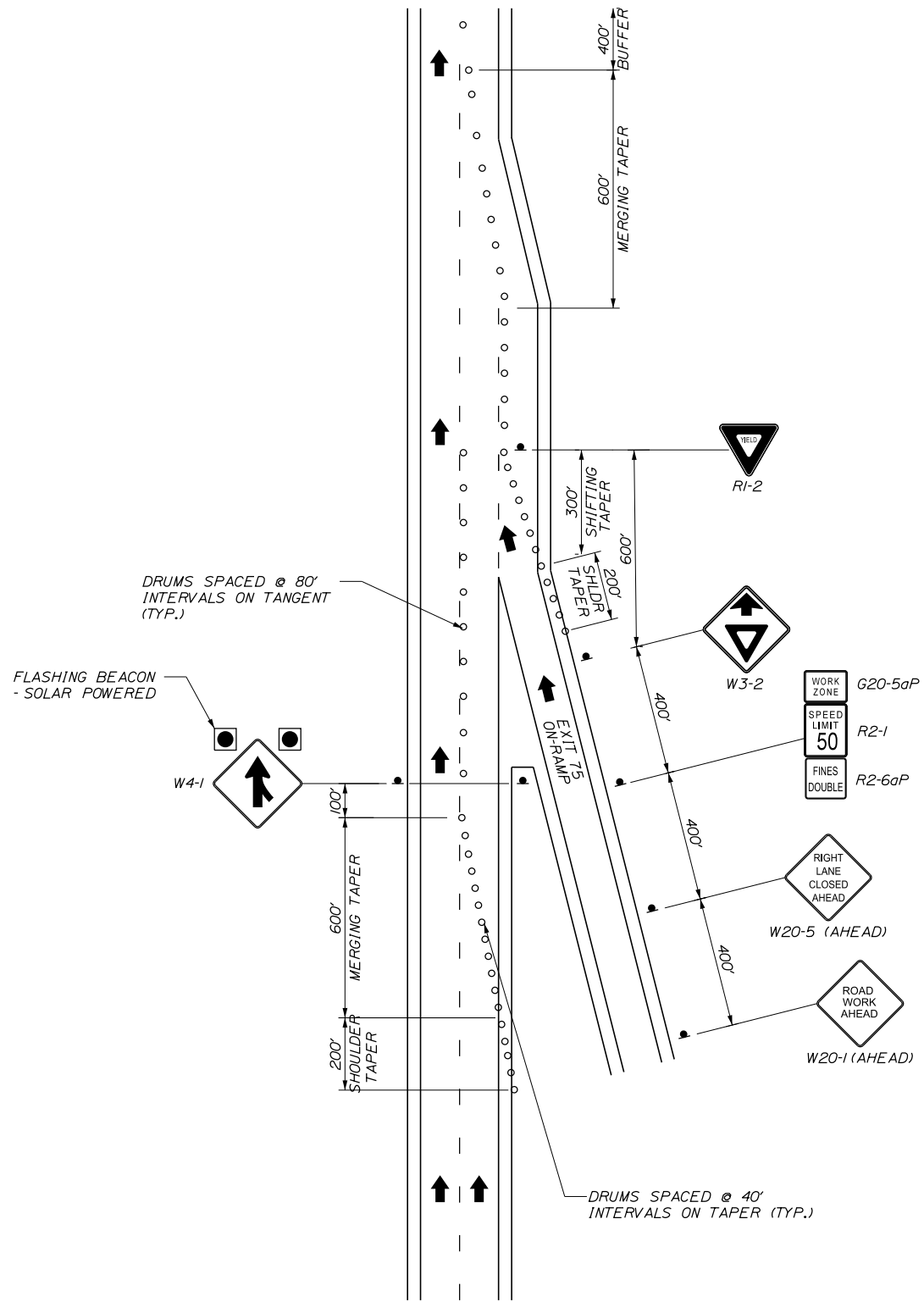
BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
 TRAFFIC CONTROL DETAILS
 SHEET 1 OF 2

SHEET NUMBER: MT-01
 4 OF 28

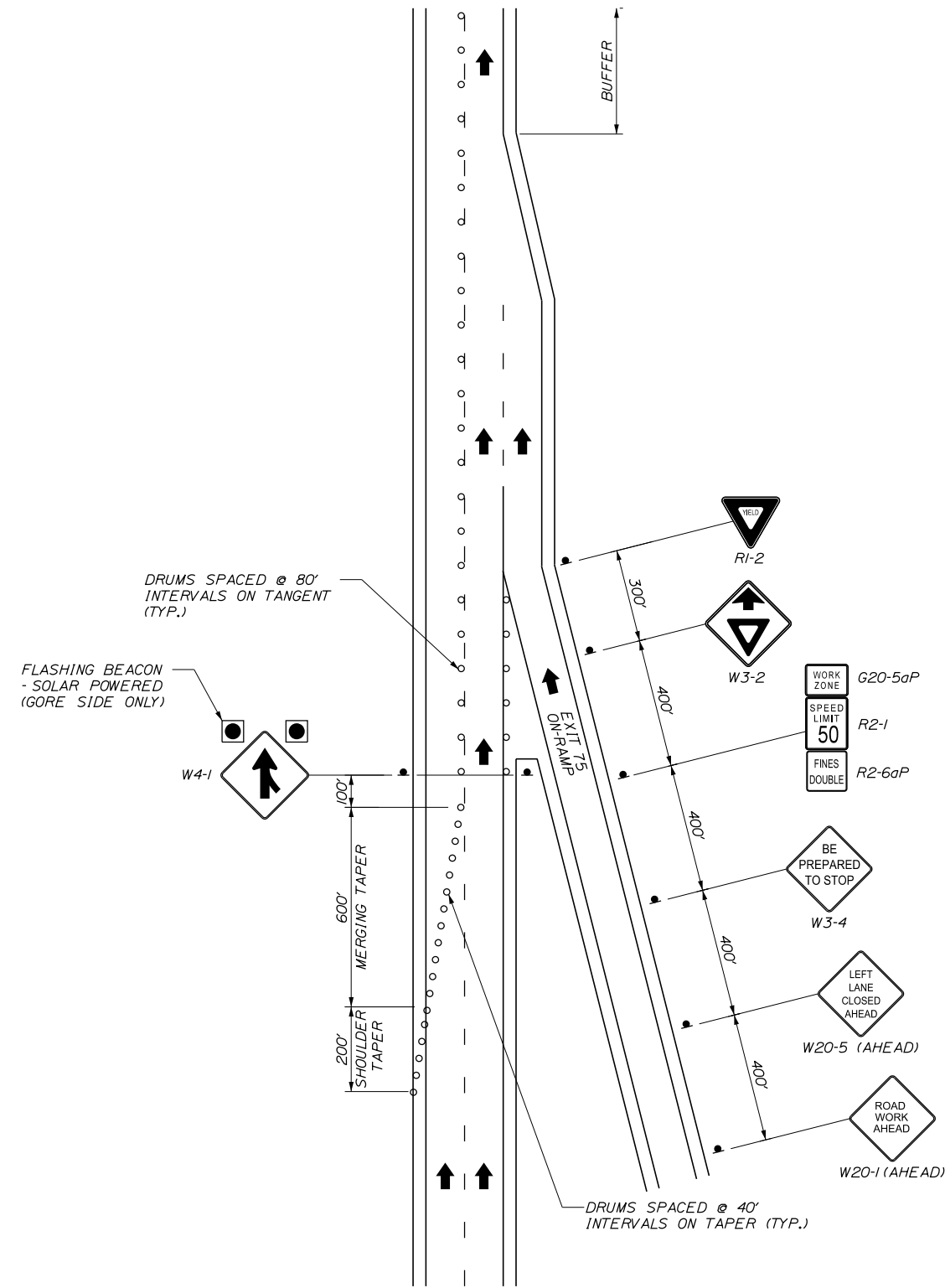
CONTRACT: 2018.10

Date: 12/22/2017

Filename: ...XXX_Traffic_Control_Details_2.dgn



DANVILLE CORNER ROAD MM 75.8
NORTHBOUND PHASE I - DRIVING LANE REPAIR



DANVILLE CORNER ROAD MM 75.8
NORTHBOUND PHASE II - PASSING LANE REPAIR


NOTES:

1. ALL MOT DEVICES AND LOCATIONS NOT NOTED ON THIS SHEET SHALL BE AS NOTED ON "TRAFFIC CONTROL DETAILS SHEET 1 OF 2".
2. FOR SIGN DETAILS SEE "MAINTENANCE OF TRAFFIC SIGN SUMMARY".
3. ALL STOP AND YIELD SIGNS SHALL BE MOUNTED 5 FEET ABOVE THE PAVEMENT.
4. OTHER TRAFFIC CONTROL DETAILS ARE AVAILBLE AT WWW.MAINTURNPIKE.COM. SEE THE RESIDENT BEFORE USING.

Scale:			
NOT TO SCALE			
No.	Revision	By	Date

Designed by:			
TYLIN INTERNATIONAL			
CONSULTANT PROJECT MANAGER: Norman L. Baker			
	By	Date	
Designed	JRH	9/2017	Checked KSD 9/2017
Drawn	SAM	9/2017	In Charge of DSM 11/2017

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**THE GOLD STAR
MEMORIAL HIGHWAY**

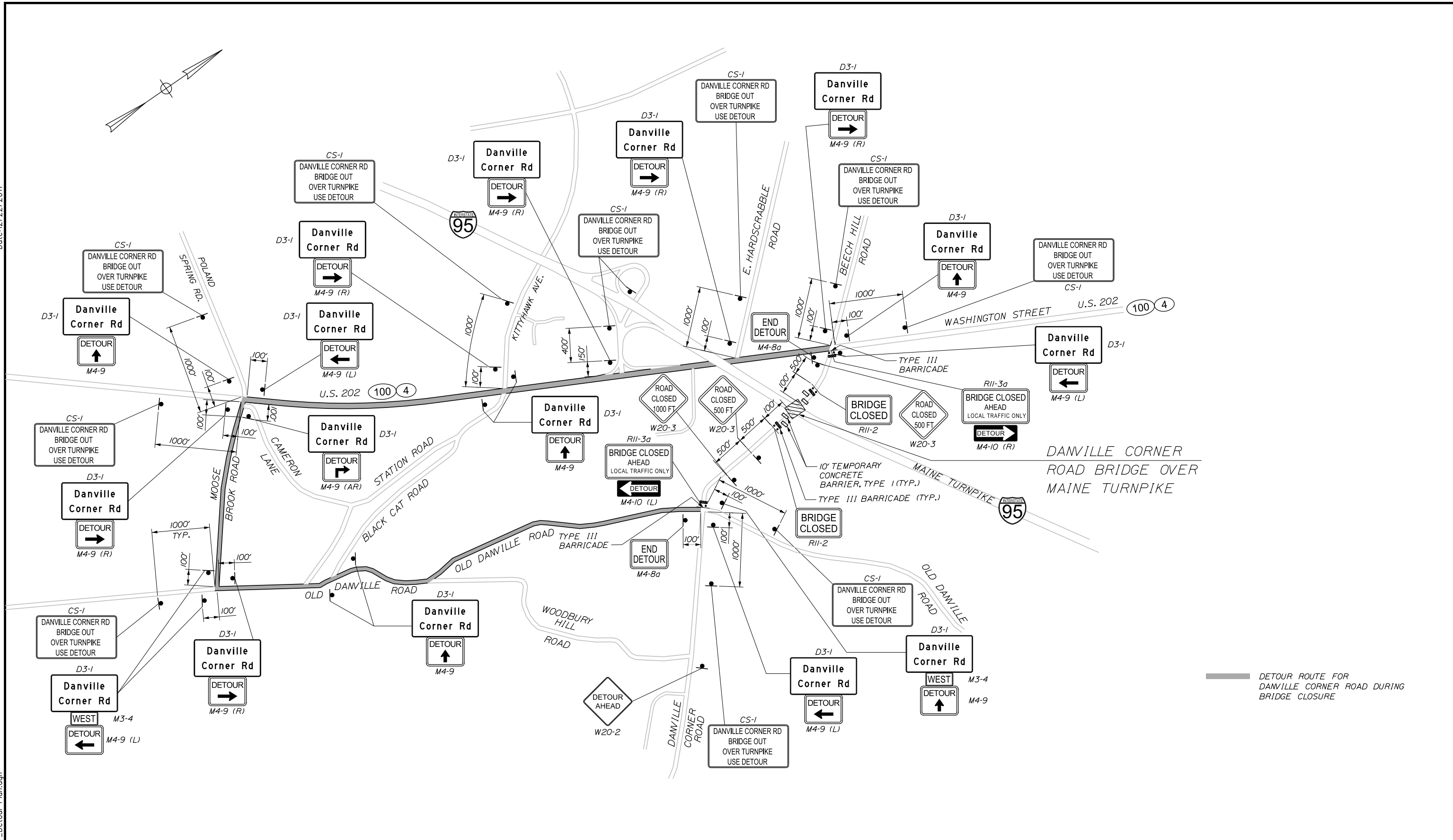
MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
TRAFFIC CONTROL DETAILS
SHEET 2 OF 2

SHEET NUMBER: MT-02
CONTRACT: 2018.10
5 OF 28

Date: 12/22/2017

Filename: ...XXX_75-8_MOT_Detour Plan.dgn



Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

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CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date	
Designed	JRH	8/2017	Checked	KSD	9/2017
Drawn	JTR	8/2017	In Charge of	DSM	11/2017

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 Building A, Suite One
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THE GOLD STAR MEMORIAL HIGHWAY

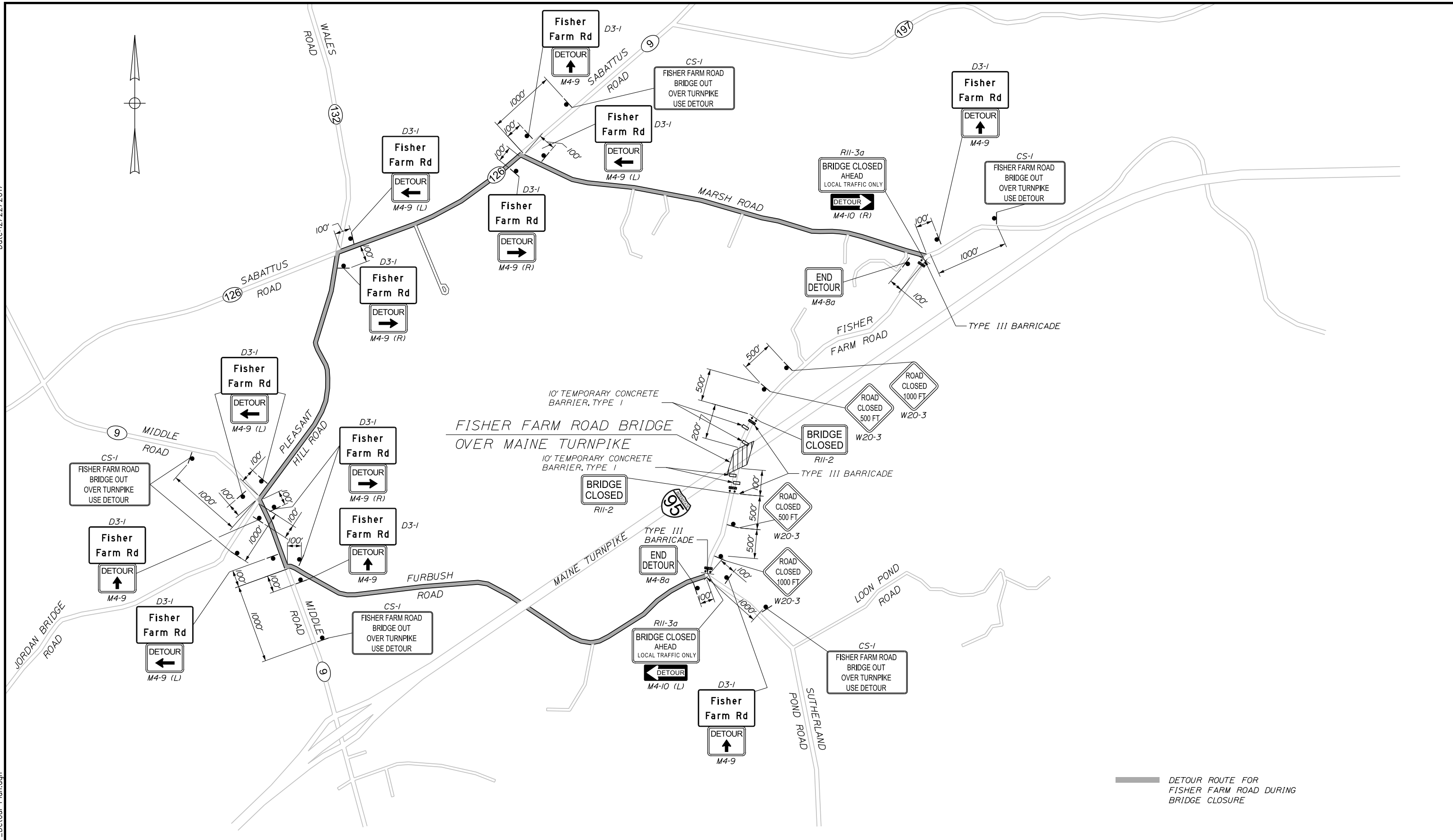
MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
 DETOUR PLAN MILE 75.8
 DANVILLE CORNER ROAD

SHEET NUMBER: MT-03
 CONTRACT: 2018.10
 6 OF 28

Date: 12/22/2017

Filename: ...XXX_87-5_MOT_Detour Plan.dgn



Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

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CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date	
Designed	JRH	8/2017	Checked	KSD	9/2017
Drawn	JTR	8/2017	In Charge of	DSM	11/2017

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THE GOLD STAR MEMORIAL HIGHWAY

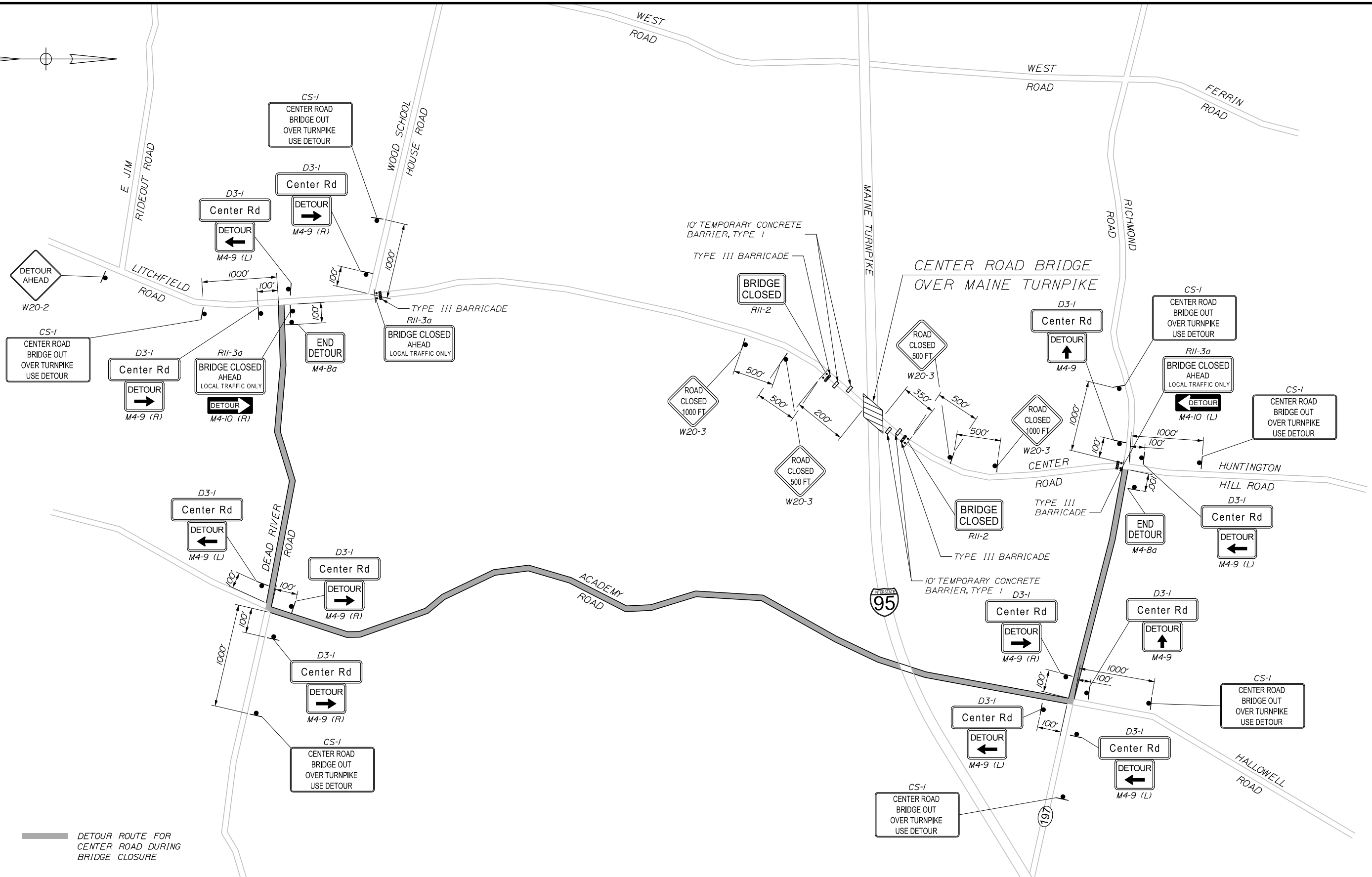
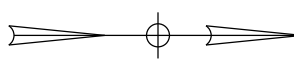
MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
 DETOUR PLAN MILE 87.5
 FISHER FARM ROAD

SHEET NUMBER: MT-04
 CONTRACT: 2018.10
 7 OF 28

Date: 12/22/2017

Filename: ...XXX_91-9_M01_Detour Plan.dgn



— DETOUR ROUTE FOR CENTER ROAD DURING BRIDGE CLOSURE

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

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CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date	
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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
 DETOUR PLAN MILE 91.9
 CENTER ROAD

SHEET NUMBER: MT-05
 CONTRACT: 2018.10
 8 OF 28

Date: 12/22/2017

Filename: ...XXX_MOT_Sign_Summary_01.dgn

IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET	NOTES
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER			
CS-1 (DANVILLE CORNER RD) (FISHER FARM ROAD) (CENTER ROAD)	66"	36"		5C	4"		11				16.50 (181.5)	
	60"	36"		5C	4"		6	ORANGE	BLACK		15.00 (90)	
	60"	36"		5C	4"		7				15.00 (105)	
CS-3	48"	48"		6"	4"		12	ORANGE	BLACK		16.00 (192)	
CS-10	48"	48"		7"	6"		6	ORANGE	BLACK		16.00 (96)	
D3-1 (DANVILLE CORNER RD)	48"	24"		TEXT DIMENSIONS SHALL CONFORM TO "STANDARD HIGHWAY SIGNS" - 2009 & 2012 SUPPLEMENT			19	ORANGE	BLACK		8.00 (152)	
D3-1 (FISHER FARM RD)	40"	24"					15	ORANGE	BLACK		6.67 (100)	
D3-1 (CENTER RD)	48"	12"					12				4.00 (48)	
G20-2	48"	24"					12	ORANGE	BLACK		8.00 (96)	
G20-5aP	48"	24"					13	ORANGE	BLACK		8.00 (104)	
M3-4	24"	12"					2	ORANGE	BLACK		2.00 (4)	
M4-8a	24"	18"					6	ORANGE	BLACK		3.00 (18)	
M4-9 (THRU) (LEFT) (RIGHT)	30"	24"					14	ORANGE	BLACK		5.00 (70)	
							15				5.00 (75)	
M4-9 (AR)	30"	24"					1	ORANGE	BLACK		5.00 (5)	
M4-10 (LEFT) (RIGHT)	48"	18"					3	ORANGE	BLACK		6.00 (18)	

IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET	NOTES
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER			
R1-2	60"	60"		TEXT DIMENSIONS SHALL CONFORM TO "STANDARD HIGHWAY SIGNS" - 2009 & 2012 SUPPLEMENT			1	WHITE	RED		10.80 (10.8)	
R2-1	48"	60"					13	WHITE	BLACK		20.00 (260)	
R2-6aP	48"	24"					13	WHITE	BLACK		8.00 (104)	
R2-12	36"	54"					12	WHITE	BLACK		13.5 (162)	
R11-2	48"	30"					6	WHITE	BLACK		10.00 (60)	
R11-3a	60"	30"					7	WHITE	BLACK		12.50 (87.5)	
W3-2	48"	48"					1	ORANGE	BLACK		16.00 (16)	
W3-4	48"	48"					13	ORANGE	BLACK		16.00 (208)	
W3-5	48"	48"					12	ORANGE	BLACK		16.00 (192)	

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

By	Date	By	Date
Designed	JRH 9/2017	Checked	KSD 9/2017
Drawn	JRH 9/2017	In Charge of	DSM 11/2017

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 12 Northbrook Drive
 Building A, Suite One
 Falmouth, Maine 04105
 TEL: (207) 781-4721
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MAINE TURNPIKE


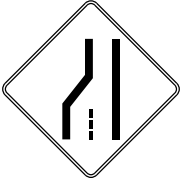






THE GOLD STAR MEMORIAL HIGHWAY



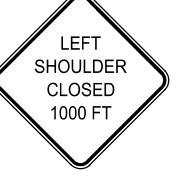


MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
 MAINTENANCE OF TRAFFIC
 SIGN SUMMARY 1 OF 2


SHEET NUMBER: MT-06
 CONTRACT: 2018.10
 9 OF 28

Date: 12/22/2017

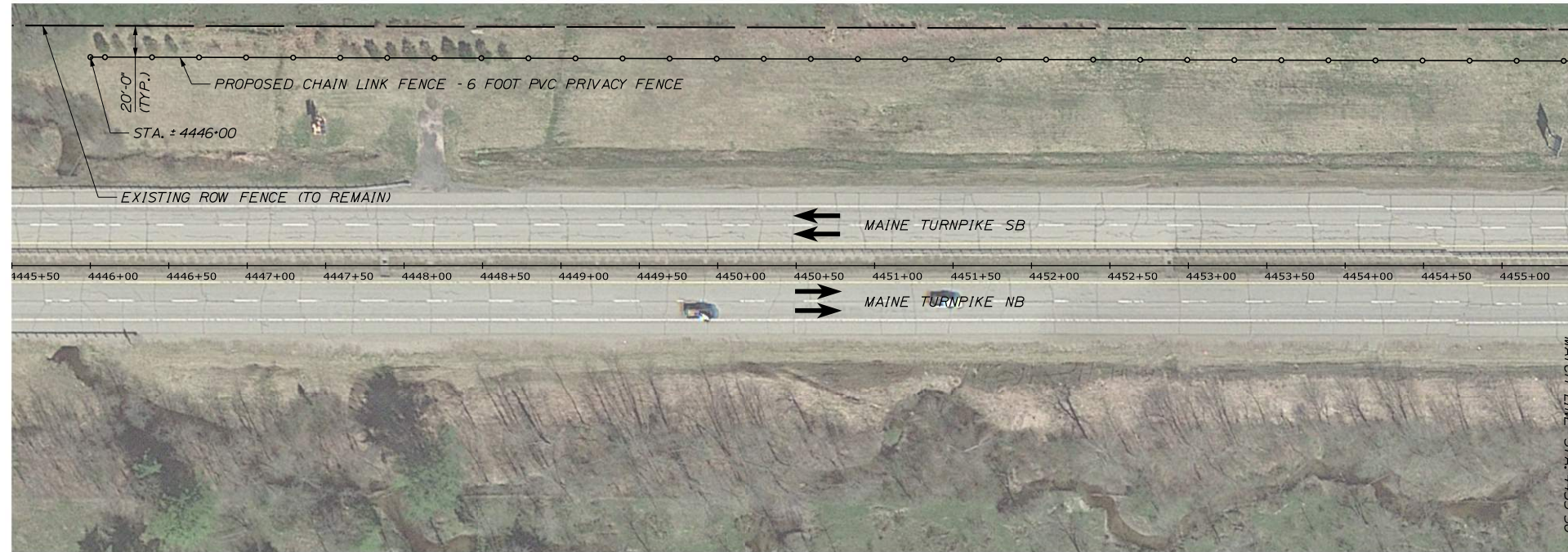
IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET	NOTES
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER			
W4-1 (RIGHT)	48"	48"		TEXT DIMENSIONS SHALL CONFORM TO "STANDARD HIGHWAY SIGNS" - 2009 & 2012 SUPPLEMENT			2	ORANGE	BLACK		16.00 (32)	
W4-2 (LEFT) (RIGHT)	48"	48"					12 12	ORANGE	BLACK		16.00 (192) (192)	
W7-3aP	36"	30"					12	ORANGE	BLACK		7.50 (90)	
W20-1 (1 MILE) (AHEAD)	48"	48"					12 13	ORANGE	BLACK		16.00 (192) (208)	
W20-2	36"	36"					2	ORANGE	BLACK		9.00 (18)	
W20-3 (1000') (500')	36"	36"					5 6	ORANGE	BLACK		9.00 (45) (54)	
W20-5 (LEFT) (RIGHT)	48"	48"					12 12	ORANGE	BLACK		16.00 (192) (192)	
W20-5 (LEFT) (RIGHT)	48"	48"					1 1	ORANGE	BLACK		16.00 (16) (16)	

IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		BORDER RADIUS	AREA IN SQUARE FEET	NOTES
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND BORDER			
W21-5	48"	48"		TEXT DIMENSIONS SHALL CONFORM TO "STANDARD HIGHWAY SIGNS" - 2009 & 2012 SUPPLEMENT			6	ORANGE	BLACK		16.00 (96)	
W21-5a (LEFT) (RIGHT)	48"	48"					6 6	ORANGE	BLACK		16.00 (96) (96)	
W21-5b (LEFT) (RIGHT)	48"	48"					6 6	ORANGE	BLACK		16.00 (96) (96)	
W8-1 (BUMP)	48"	48"					12	ORANGE	BLACK		16.00 (192)	
W8-24A (RUMBLE STRIPS AHEAD)	48"	48"					6	ORANGE	BLACK		16.00 (96)	

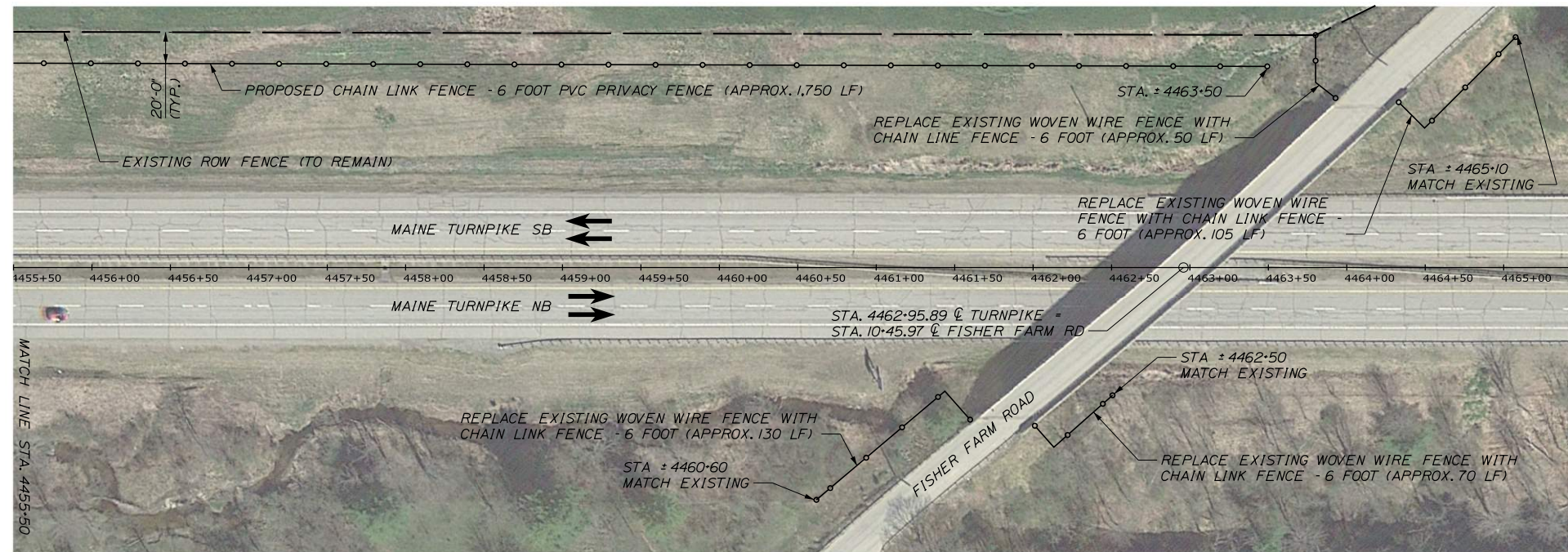
Filename: ...XXX_MOT_Sign_Summary_01.dgn

Scale: NOT TO SCALE	Designed by: TYLIN INTERNATIONAL	T.Y. Lin International 12 Northbrook Drive Building A, Suite One Falmouth, Maine 04105 TEL: (207) 781-4721 FAX: (207) 781-4753	 THE GOLD STAR MEMORIAL HIGHWAY	BRIDGE AND CULVERT REPAIRS 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9) MAINTENANCE OF TRAFFIC SIGN SUMMARY 2 OF 2																															
<table border="1" style="width: 100%;"> <thead> <tr> <th>No.</th> <th>Revision</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td><td> </td></tr> </tbody> </table>	No.	Revision	By	Date									<table border="1" style="width: 100%;"> <thead> <tr> <th colspan="4">CONSULTANT PROJECT MANAGER: Norman L. Baker</th> </tr> <tr> <th>By</th> <th>Date</th> <th>By</th> <th>Date</th> </tr> </thead> <tbody> <tr> <td>Designed</td> <td>JRH 9/2017</td> <td>Checked</td> <td>KSD 9/2017</td> </tr> <tr> <td>Drawn</td> <td>JRH 9/2017</td> <td>In Charge of</td> <td>DSM 11/2017</td> </tr> </tbody> </table>	CONSULTANT PROJECT MANAGER: Norman L. Baker				By	Date	By	Date	Designed	JRH 9/2017	Checked	KSD 9/2017	Drawn	JRH 9/2017	In Charge of	DSM 11/2017	<table border="1" style="width: 100%;"> <tr> <td colspan="2">MTA PROJECT MANAGER: R. NORWOOD</td> </tr> </table>	MTA PROJECT MANAGER: R. NORWOOD		<table border="1" style="width: 100%;"> <tr> <td>CONTRACT: 2018.10</td> <td style="text-align: right;">SHEET NUMBER: MT-07 10 OF 28</td> </tr> </table>	CONTRACT: 2018.10	SHEET NUMBER: MT-07 10 OF 28
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CONTRACT: 2018.10	SHEET NUMBER: MT-07 10 OF 28																																		

Date: 12/22/2017



PLAN VIEW

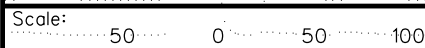


PLAN VIEW

NOTES:

1. NO ADDITIONAL PAYMENT WILL BE MADE FOR LONGER POSTS NECESSITATED BY LARGE GRADE DIFFERENTIAL.
2. CHAIN LINK FENCE SHALL BE INSTALLED WITH BARBS DOWN.
3. PVC PRIVACY SLATS SHALL BE INSTALLED AND SHALL CONFORM TO THE REQUIREMENTS OF SPECIAL PROVISION SECTION 607 FENCES OF THE PROJECT SPECIFICATIONS.
4. ONE (1) 3'-0" SINGLE GATE SHALL BE INSTALLED IN PROPOSED CHAIN LINK - 6 FOOT, ADJACENT TO EACH ABUTMENT (2 GATES TOTAL). FINAL GATE LAYOUT SHALL BE DETERMINED BY RESIDENT.
5. EXISTING AND PROPOSED FENCING SHOWN IS APPROXIMATE ONLY. FINAL LIMITS AND LAYOUT FOR ALL REMOVAL AND INSTALLATION SHALL BE DETERMINED BY RESIDENT.
6. REMOVAL AND DISPOSAL OF EXISTING WOVEN WIRE FENCING BEING REPLACED WITH CHAIN LINK FENCING SHALL BE INCIDENTAL TO ITEM 607.17, CHAIN LINK FENCE - 6 FOOT.

Filename: ... \MSTA\XXX-87-5_ROW_Fence.dgn



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TY-LIN INTERNATIONAL

T.Y. Lin International
 12 Northbrook Drive
 Building A, Suite One
 Falmouth, Maine 04105
 TEL: (207) 781-4721
 FAX: (207) 781-4753



**THE GOLD STAR
 MEMORIAL HIGHWAY**

BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)

FENCING PLAN MILE 87.5

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	BLT	11/2017	Checked	DSM	9/2017
Drawn	BLT	11/2017	In Charge of	DSM	11/2017

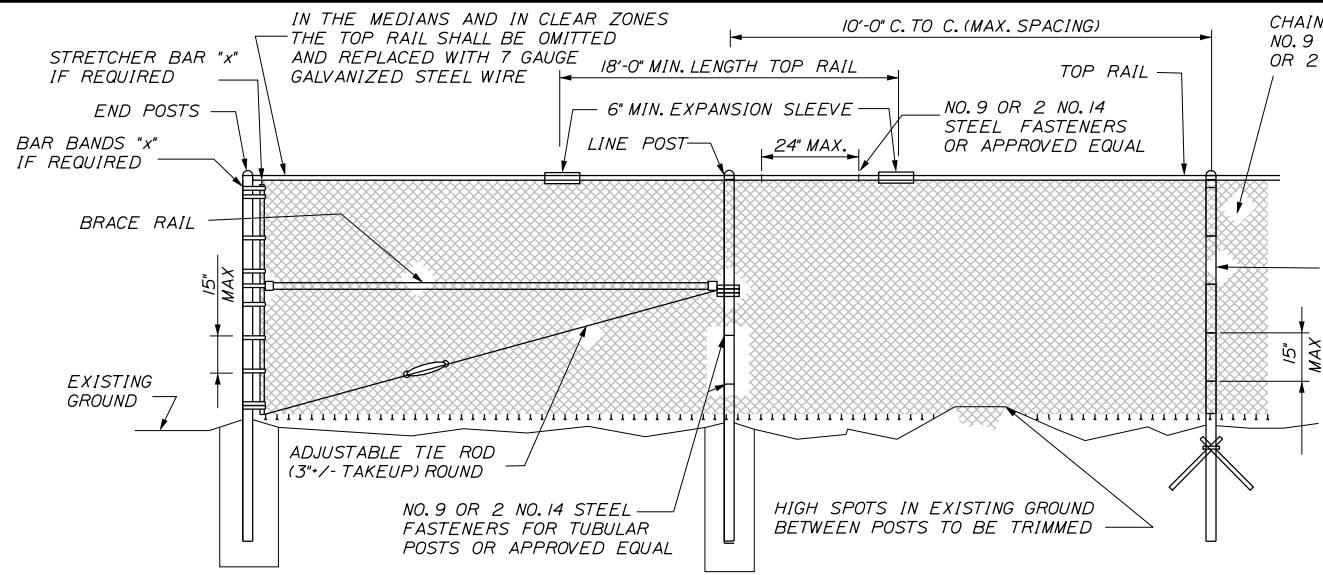
MTA PROJECT MANAGER: R. NORWOOD

CONTRACT: 2018.10

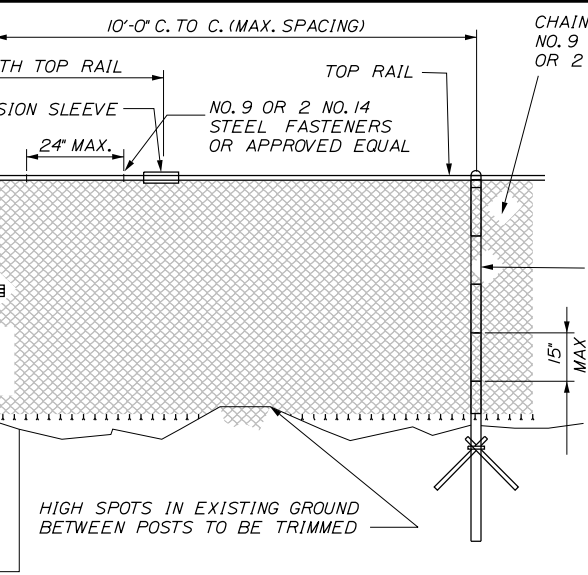
SHEET NUMBER: F-01

11 OF 28

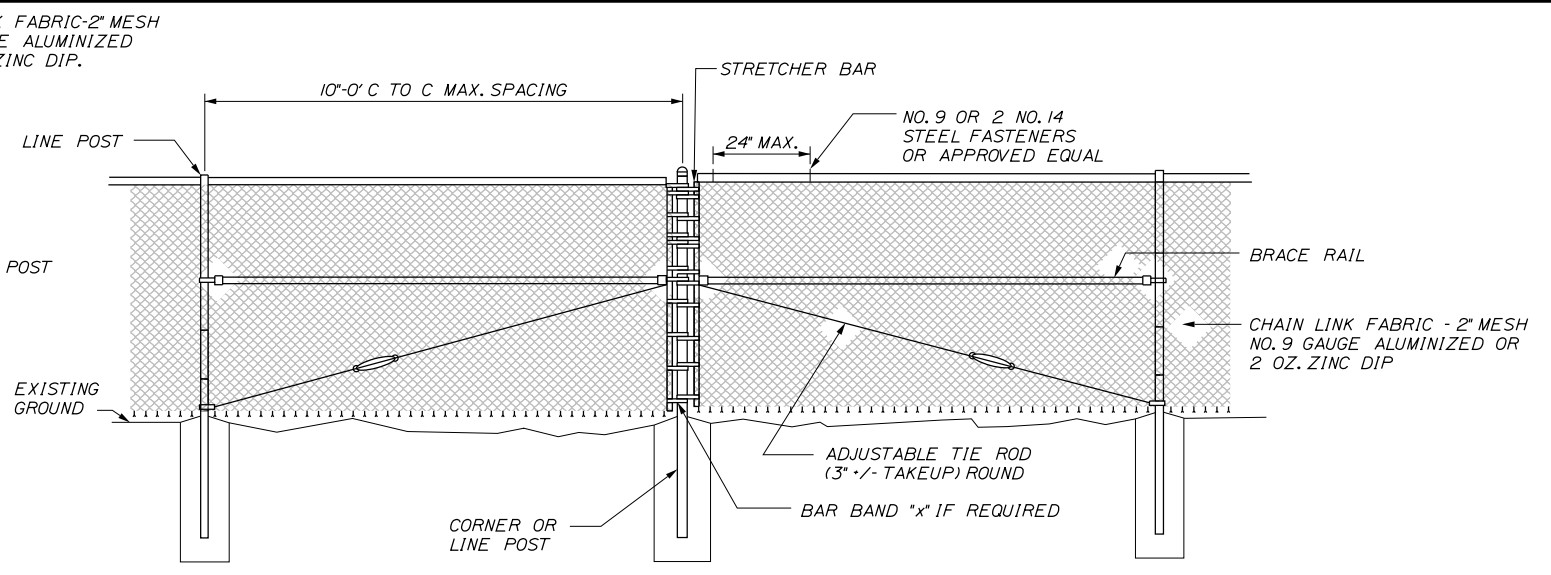
Date: 12/22/2017



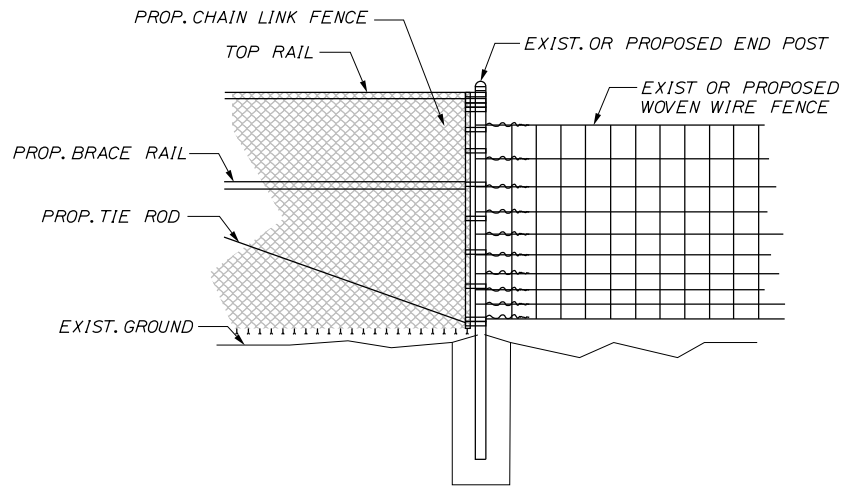
TYPICAL TYPE I END PANEL
N.T.S.



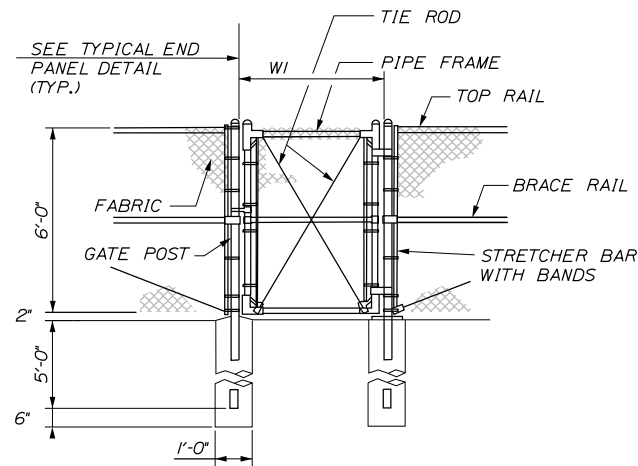
TYPICAL INTERMEDIATE PANEL
N.T.S.



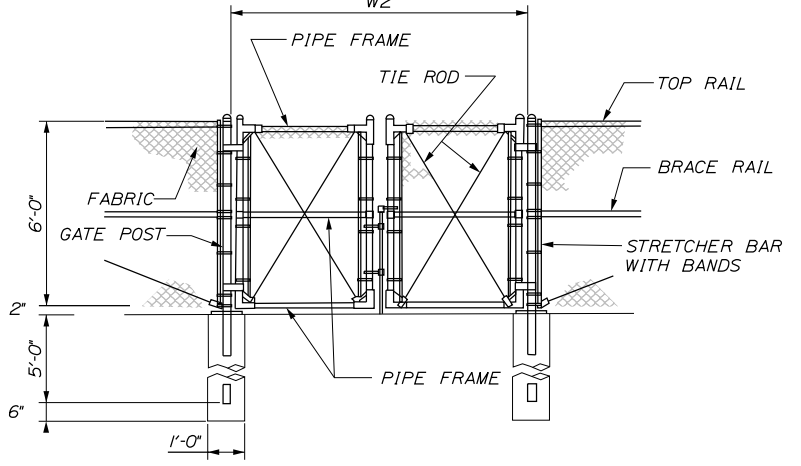
TYPICAL TYPE II BRACE PANEL
N.T.S.



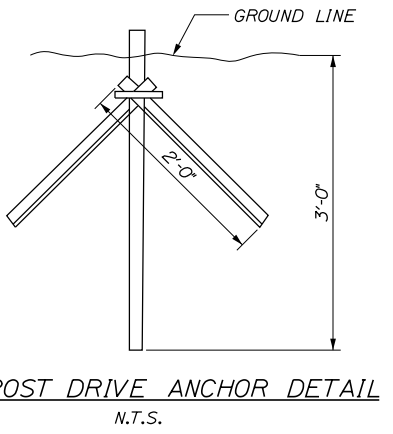
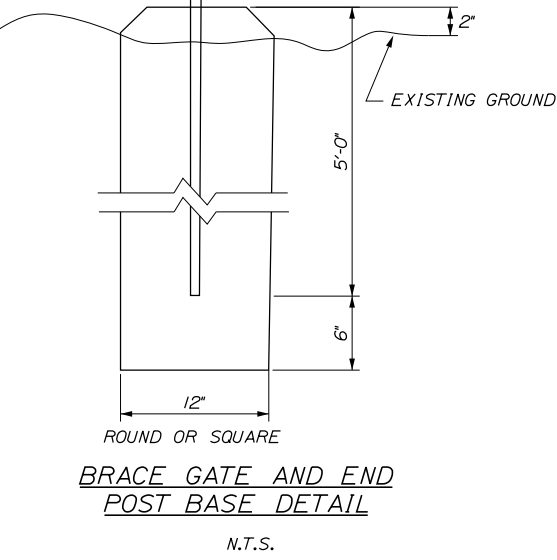
METHOD OF ATTACHING CHAIN LINK FENCE TO WOVEN WIRE FENCE
N.T.S.



SINGLE GATE



DOUBLE GATE



BENDING MOMENTS BASED ON GRADE 1 (SCHEDULE 40 STEEL)

CHAIN LINK FENCE	SHAPE	NOMINAL SIZE (INCHES)	BENDING MOMENT*
			(LBS.-IN.)
END & CORNER POSTS	Ø	2 I.D.	14,025
LINE POSTS	Ø	1 1/2 I.D.	8,150
TOP & BRACE RAILS	Ø	1 1/4 I.D.	5,875

* MATERIAL FOR GRADE 2 END, CORNER & LINE POSTS AND TOP & BRACE RAILS MUST MEET OR EXCEED BENDING MOMENTS FOR GRADE 1 STEEL AS NOTED ABOVE.

GATE WIDTH		GATE POST O.D.
W1	W2	
TO 6'	TO 12'	3"
6' TO 12'	12' TO 24'	4"
12' TO 18'	24' TO 36'	6"

- NOTES:**
- BRACE PANELS SHALL BE INSTALLED WHERE THE CHANGE IN GRADE BETWEEN ANY THREE POSTS EXCEEDS 15 PERCENT.
 - NO ADDITIONAL PAYMENT WILL BE MADE FOR LONGER POSTS NECESSITATED BY LARGE GRADE DIFFERENTIAL.
 - TYPE I BRACING WILL BE USED AT FENCE ENDS. TYPE II BRACING WILL BE USED AT CORNER POSTS. PAYMENT SHALL BE MADE UNDER ITEM 607.34, "BRACING ASSEMBLY CHAIN LINK FENCE"
 - WHEN LEDGE IS ENCOUNTERED, STEEL POSTS SHALL BE SET AND GROUTED 12" DEEP UNLESS THE POSTS PENETRATE THE GROUND TO THE DEPTH INDICATED ON THE DRAWINGS.
 - CONCRETE FOR POST FOUNDATION SHALL BE CLASS B.
 - BRACE, GATE AND END POSTS SHALL BE SET IN CONCRETE.
 - CHAIN LINK FENCE SHALL BE INSTALLED WITH BARBS DOWN.
 - ALL COMPONENTS OF CHAIN LINK FENCE SHALL BE IN ACCORDANCE WITH AASHTO M181.
 - PVC PRIVACY SLATS SHALL BE INSTALLED AS NOTED ON THE PLANS AND SHALL CONFORM TO THE REQUIREMENTS OF SPECIAL PROVISION SECTION 607 FENCES OF THE PROJECT SPECIFICATIONS.

Filename: ... \MSTA\XXX_Chain_Link.dgn

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

By	Date	By	Date
Designed DSM	11/2017	Checked JRH	11/2017
Drawn PWM	11/2017	In Charge of DSM	11/2017

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Falmouth, Maine 04105
TEL: (207) 781-4721
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MAINE TURNPIKE

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

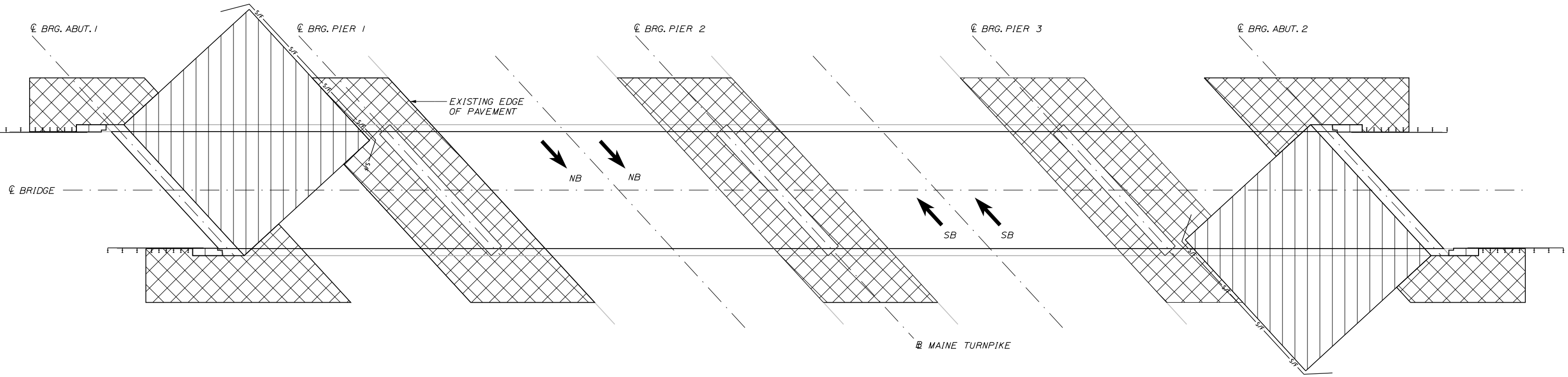
BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)

CHAIN LINK FENCE DETAILS

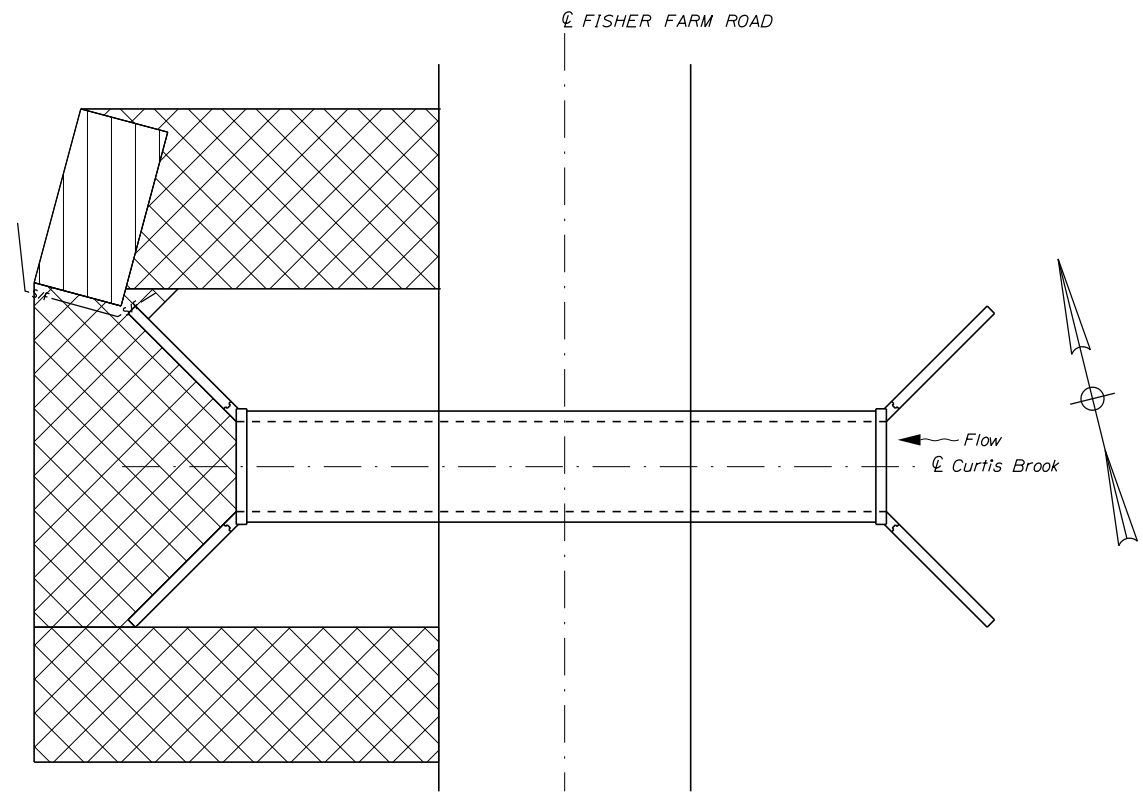
SHEET NUMBER: F-02
CONTRACT: 2018.10
12 OF 28

Date: 12/22/2017

Filename: ... \004_Limits_of_Disturbance.dgn



LIMIT OF DISTURBANCE PLAN (TYPICAL UNDERPASS)
NOT TO SCALE

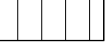



LIMIT OF DISTURBANCE PLAN CURTIS BROOK CULVERT
NOT TO SCALE

AREAS:

LOCATION	CONTRACT LIMIT OF DISTURBANCE	CONTRACTOR'S LIMIT OF DISTURBANCE AREA FOR ACCESS AND STORAGE
DANVILLE CORNER ROAD UNDERPASS (MILE 75.8)	0.05 ACRES	0.10 ACRES
CURTIS BROOK CULVERT (MILE 87.49)	0.004 ACRES	0.05 ACRES
FISHER FARM ROAD UNDERPASS (MILE 87.5)	0.06 ACRES	0.10 ACRES
CENTER ROAD UNDERPASS (MILE 91.9)	0.06 ACRES	0.10 ACRES

LEGEND:

-  CONTRACT LIMIT OF DISTURBANCE
-  CONTRACTOR'S LIMIT OF DISTURBANCE AREA FOR ACCESS AND STORAGE

NOTES:

1. EROSION AND SEDIMENT CONTROL DEVICES REQUIRED FOR THE CONTRACTOR'S ACCESS LOCATIONS AND STORAGE AREAS WILL NOT BE MEASURED FOR PAYMENT.
2. THE CONTRACTOR WILL BE ALLOWED TO STORE MATERIAL AND EQUIPMENT ON GRAVEL AND PAVEMENT WITHIN THE CLOSED PORTIONS OF THE MAINLINE TURNPIKE AND DANVILLE CORNER, FISHER FARM, AND CENTER ROADS. THESE AREAS ARE EITHER INCLUDED IN THE LIMITS OF DISTURBANCE OR HAVE BEEN PREVIOUSLY SURFACED AND DO NOT ADD TO THE LIMITS OF DISTURBANCE.
3. CONTRACTOR SHALL TAKE PRECAUTIONS TO AVOID IMPACTING OR DISTURBING ROADWAY DITCHES.

Scale: NOT TO SCALE

No.	Revision	By	Date


Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date		By	Date
Designed	DSM	9/2017	Checked	NLB	9/2017
Drawn	SAM	9/2017	In Charge of	DSM	11/2017

T.Y. Lin International
12 Northbrook Drive
Building A, Suite One
Falmouth, Maine 04105
TEL: (207) 781-4721
FAX: (207) 781-4753



THE GOLD STAR MEMORIAL HIGHWAY

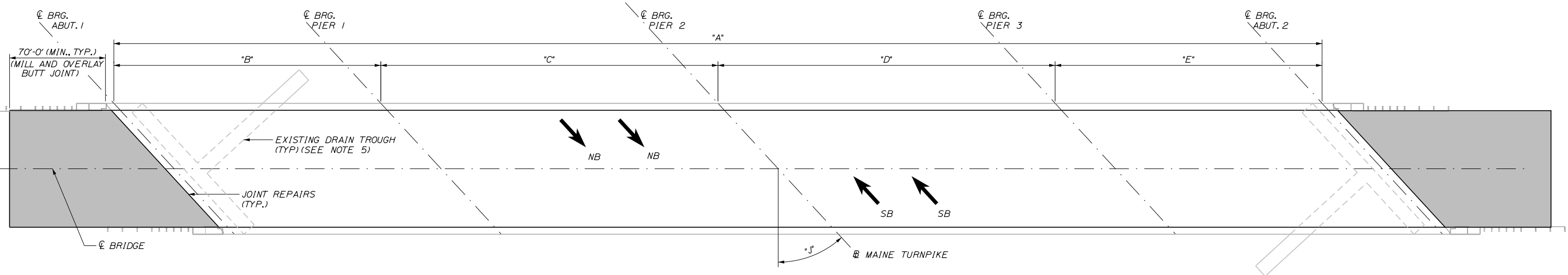
MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)

LIMITS OF DISTURBANCE

SHEET NUMBER: LD-01
CONTRACT: 2018.10
13 OF 28

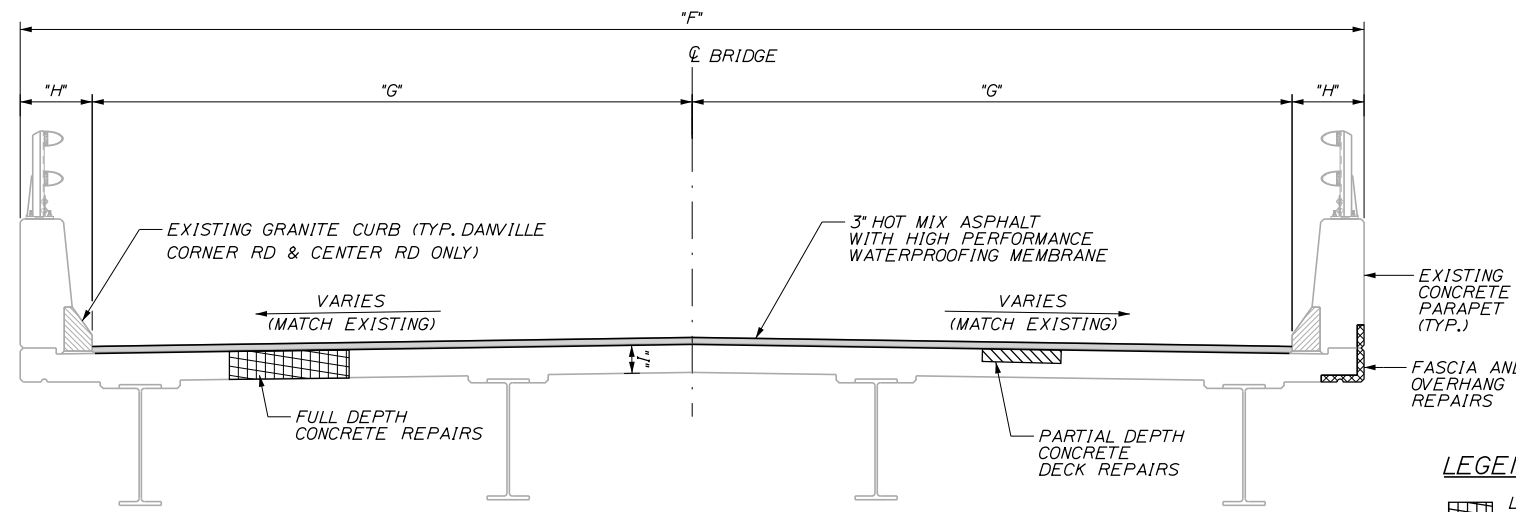
Date: 12/22/2017



DECK PLAN
NOT TO SCALE

BRIDGE	"A"	"B"	"C"	"D"	"E"	"F"	"G"	"H"	"I"	"J"
DANVILLE CORNER ROAD UNDERPASS (MILE 75.8)	207'-0"	44'-0"	54'-0"	59'-0"	50'-0"	28'-0"	12'-6"	1'-6"	8"	13'24'30"
FISHER FARM ROAD UNDERPASS (MILE 87.5)	294'-6"	60'-6"	76'-0"	85'-0"	73'-0"	26'-4"	11'-6"	1'-8"	7 1/2"	47'58'00"
CENTER ROAD UNDERPASS (MILE 91.9)	258'-0"	57'-0"	72'-0"	72'-0"	57'-0"	28'-0"	12'-6"	1'-6"	7"	42'35'00"

	FULL DEPTH CONCRETE DECK REPAIRS (S.F.)	PARTIAL DEPTH CONCRETE DECK REPAIRS (S.F.)	FASCIA AND OVERHANG REPAIRS (S.F.)
DANVILLE CORNER ROAD UNDERPASS (MILE 75.8)	10	160	45
FISHER FARM ROAD UNDERPASS (MILE 87.5)	10	210	55
CENTER ROAD UNDERPASS (MILE 91.9)	10	200	5



TRANSVERSE SECTION
NOT TO SCALE

LEGEND:

- LIMITS OF FULL DEPTH CONCRETE REPAIR (REPAIR AREAS TO BE FIELD VERIFIED)
- LIMITS OF PARTIAL DEPTH CONCRETE DECK REPAIRS (REPAIR AREAS TO BE FIELD VERIFIED)
- LIMITS OF FASCIA AND OVERHANG REPAIRS (REPAIR AREAS TO BE FIELD VERIFIED)

NOTES:

1. CONTRACTOR SHALL REPAIR AREAS OF CONCRETE DECK DETERIORATION AS IDENTIFIED BY THE RESIDENT. RESIDENT SHALL IDENTIFY AND MARK AREAS FOR DECK REPAIR AFTER THE WEARING SURFACE AND MEMBRANE ARE REMOVED. CONTRACTOR SHALL ALSO REPAIR DETERIORATED AREAS OF FASCIA, FASCIA OVERHANGS, AND PARAPET JOINTS. COORDINATE WORK WITH THE RESIDENT. PAYMENT FOR REPAIR WORK SHALL BE UNDER THE 518 PAY ITEMS.
2. CONTRACTOR SHALL REPAIR AREAS OF DETERIORATED GRANITE CURB BEDDING AND JOINT MORTAR ON THE DECK AS REQUIRED. LOCATIONS OF GRANITE CURB BEDDING AND JOINT MORTAR SHALL BE FIELD DETERMINED BY THE RESIDENT. PAYMENT SHALL BE UNDER THE 518 PAY ITEMS.
3. ALL PVC DECK WEEPS WHICH DRAIN ONTO PIERS, ABUTMENTS OR STRUCTURAL STEEL SHALL BE EXTENDED AND DIVERTED AWAY FROM BRIDGE COMPONENTS BELOW DECK, AND SHALL EXTEND 1 FT BELOW BOTTOM FLANGE, AS DIRECTED BY THE RESIDENT. PAYMENT SHALL BE BY THE VARIOUS LABOR AND EQUIPMENT ITEMS.
4. REMOVAL OF EXISTING GRANITE CURB BEDDING AND JOINT MORTAR, AS SPECIFIED BY THE RESIDENT, SHALL BE INCIDENTAL TO ITEM 202.2021, "REMOVING PAVEMENT SURFACE - BRIDGE DECK".
5. FOR WORK REQUIRED AT DRAIN TROUGHS, SEE SHEET "TYPICAL DETAILS".
6. A BEAD OF HOT-APPLIED RUBBERIZED ASPHALT SEALER SHALL BE APPLIED ALONG CURB LINES AFTER INSTALLATION OF BASE COURSE. PAYMENT SHALL NOT BE MADE DIRECTLY BUT SHALL BE CONSIDERED INCIDENTAL TO RELATED CONTRACT ITEMS.

Filename: ...XXX_Typical_Deck_Plan_Section.dgn

Scale: AS NOTED			
No.	Revision	By	Date

Designed by:
TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

By	Date	By	Date
Designed DSM	9/2017	Checked NLB	9/2017
Drawn SAM	9/2017	In Charge of DSM	11/2017

T.Y. Lin International
12 Northbrook Drive
Building A, Suite One
Falmouth, Maine 04105
TEL: (207) 781-4721
FAX: (207) 781-4753

THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

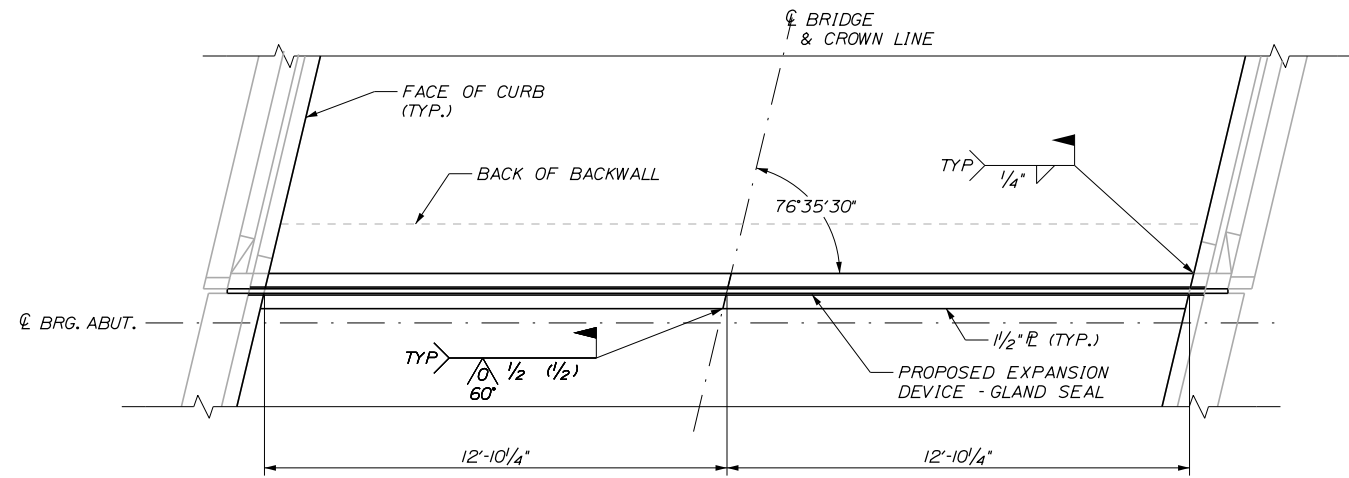
BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)

TYPICAL UNDERPASS DECK PLAN & SECTION

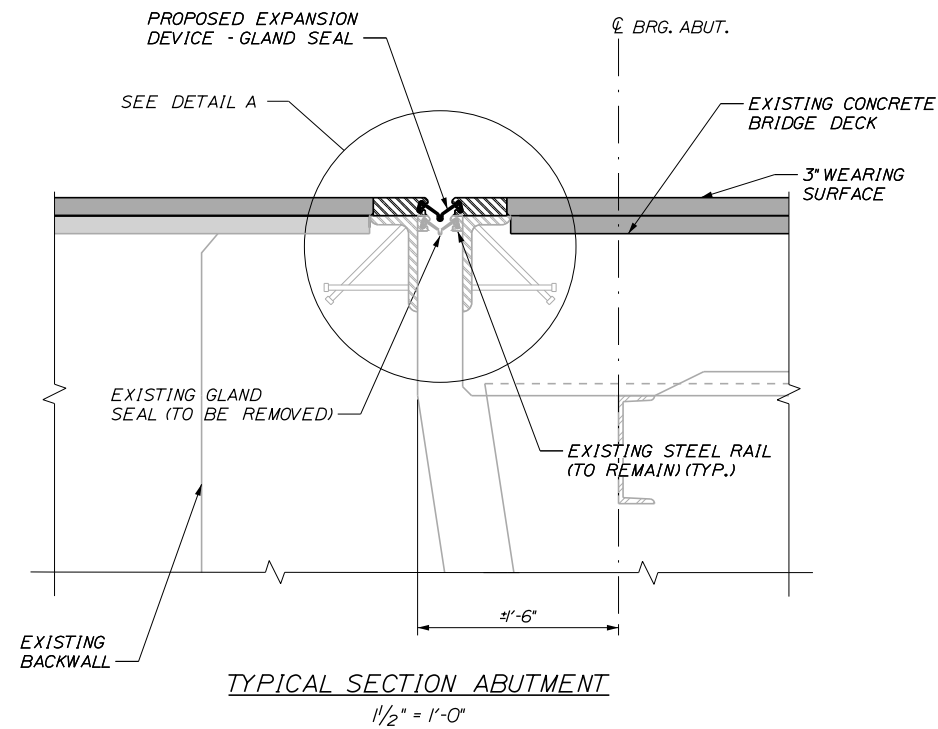
SHEET NUMBER: S-01
CONTRACT: 2018.10
14 OF 28

Date: 12/22/2017

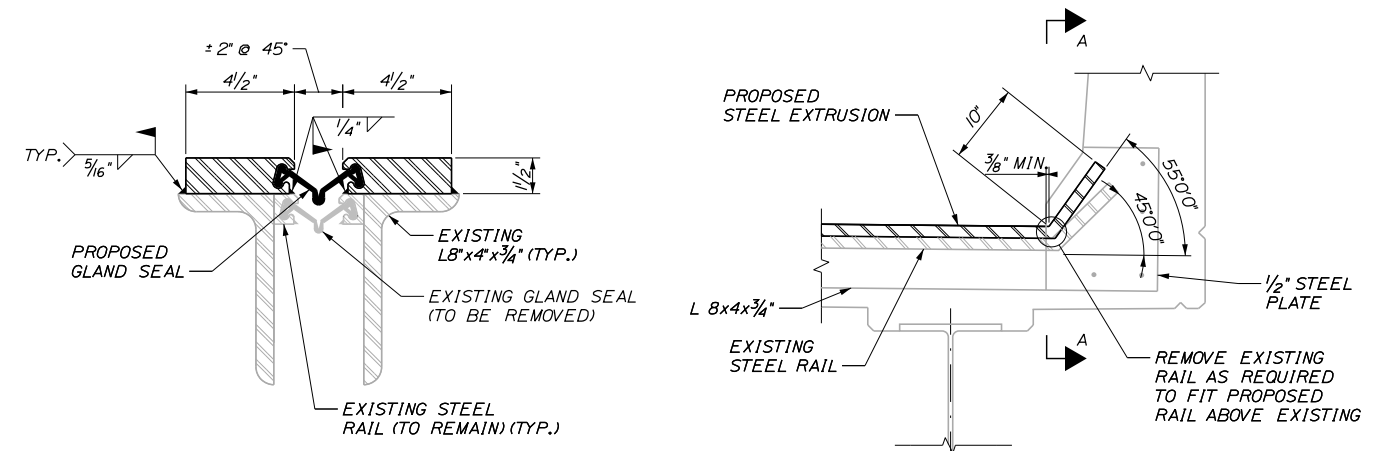
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ABUTMENT PLAN - DANVILLE CORNER ROAD
SCALE: 3/8" = 1'-0"

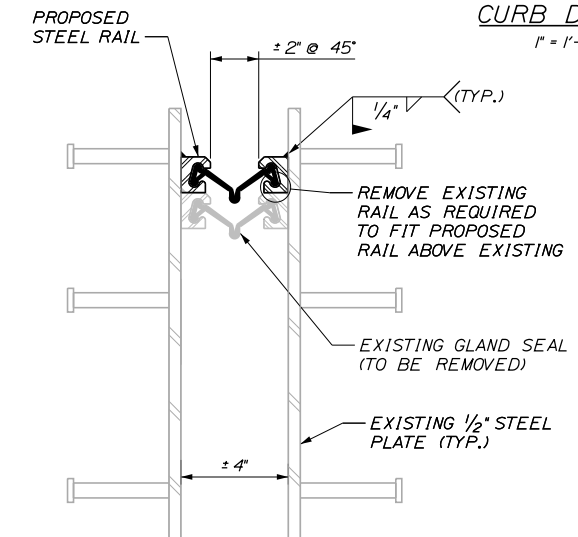


TYPICAL SECTION ABUTMENT
1/2" = 1'-0"



DETAIL A
3" = 1'-0"

CURB DETAIL
1" = 1'-0"



SECTION A-A
3" = 1'-0"
(CONCRETE NOT SHOWN)

EXPANSION JOINT NOTES:

1. ALL STEEL COMPONENTS SHALL BE AASHTO M270 GRADE 36, UNLESS OTHERWISE NOTED.
2. CONTRACTOR SHALL FIELD MEASURE EXISTING CURB PLATE AND JOINT DIMENSIONS PRIOR TO DEVELOPMENT OF SHOP DRAWINGS.
3. EXPANSION JOINT MODIFICATIONS AT EACH ABUTMENT SHALL BE PAID UNDER ITEM 520.22II, EXPANSION DEVICE MODIFICATIONS.

Scale:				AS NOTED			
No.	Revision	By	Date				

Designed by:					
TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	9/2017	Checked	NLB	9/2017
Drawn	PEM	9/2017	In Charge of	DSM	11/2017

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12 Northbrook Drive
Building A, Suite One
Falmouth, Maine 04105
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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
JOINT REPAIRS
DANVILLE CORNER ROAD

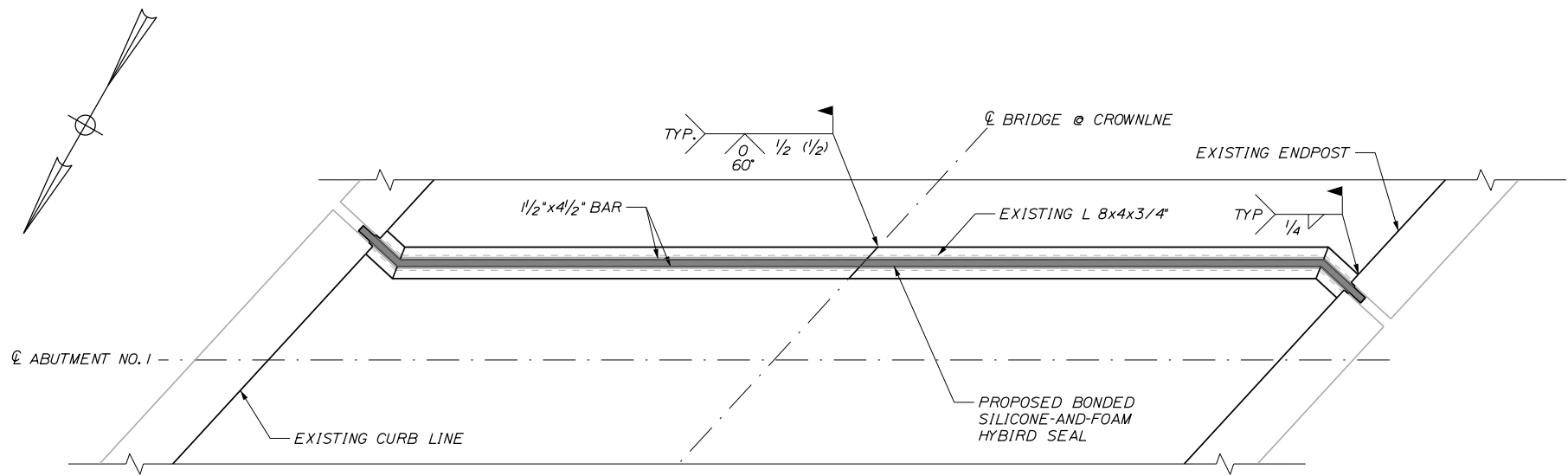
CONTRACT: 2018.10

SHEET NUMBER: S-02

15 OF 28

Date: 12/22/2017

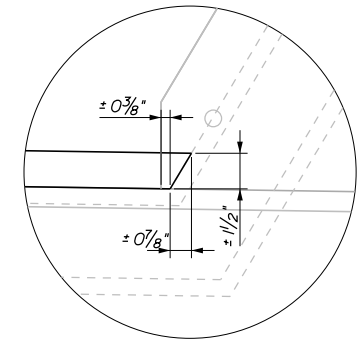
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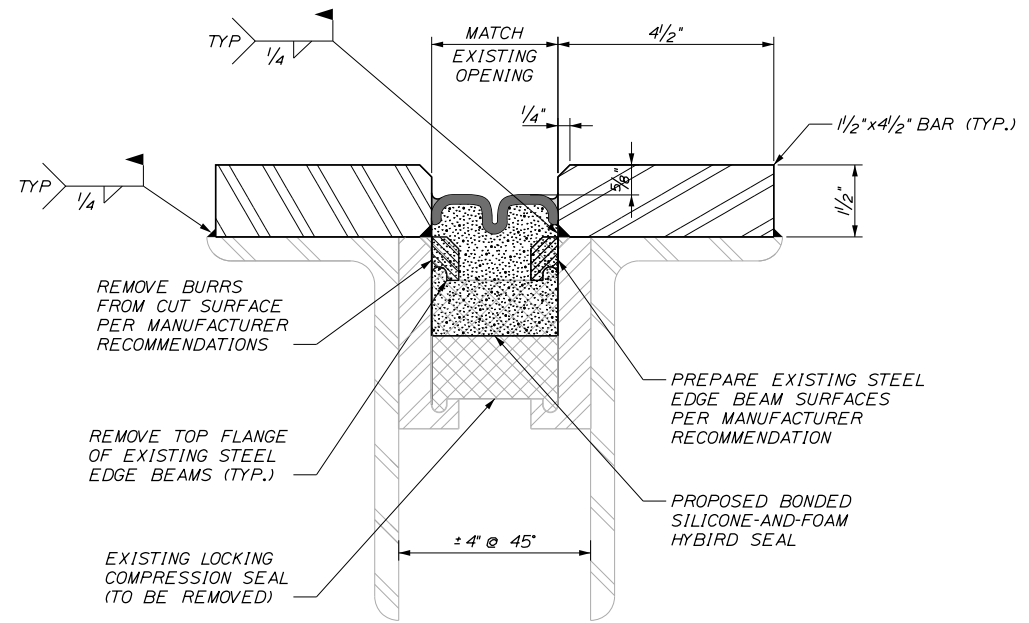
PLAN
SCALE: 3/8" = 1'-0"
(ABUTMENT 1 SHOWN, ABUTMENT 2 SIMILAR)

JOINT REPAIR NOTES:

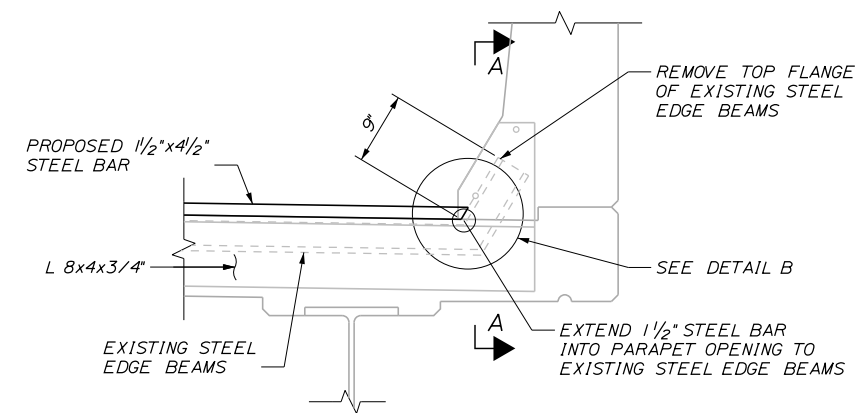
1. ALL STEEL COMPONENTS SHALL BE AASHTO M270 GRADE 36, UNLESS OTHERWISE NOTED.
2. CONTRACTOR SHALL FIELD MEASURE EXISTING CURB PLATE AND JOINT DIMENSIONS PRIOR TO DEVELOPMENT OF SHOP DRAWINGS.
3. EXPANSION JOINT MODIFICATIONS AT EACH ABUTMENT SHALL BE PAID UNDER ITEM 520.22II, EXPANSION DEVICE MODIFICATIONS.
4. CONTRACTOR SHALL REPLACE EXISTING NEOPRENE COMPRESSION SEALS WITH NEW BONDED SILICONE-AND-FOAM HYBRID SEALS.
5. THE BONDED SILICONE-AND-FOAM HYBRID SEALS TO BE FURNISHED SHALL HAVE A MINIMUM MOVEMENT RANGE OF 1/2 INCHES.



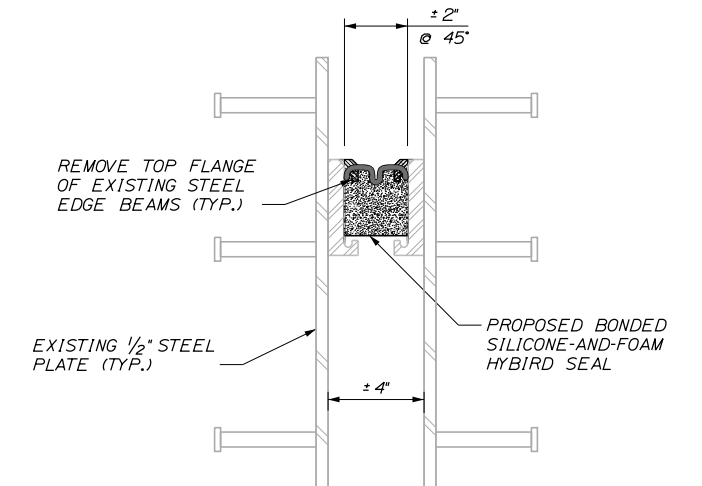
DETAIL B



EDGE BEAM MODIFICATIONS AND PROPOSED SEAL TYPICAL SECTION
SCALE: 6" = 1'-0"



CURB DETAIL
SCALE: 1" = 1'-0"




SECTION A-A
SCALE: 3" = 1'-0"
(CONCRETE NOT SHOWN)

Scale: AS NOTED			
No.	Revision	By	Date

Designed by: TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	9/2017	Checked	NLB	9/2017
Drawn	SAM	9/2017	In Charge of	DSM	11/2017

T.Y. Lin International
12 Northbrook Drive
Building A, Suite One
Falmouth, Maine 04105
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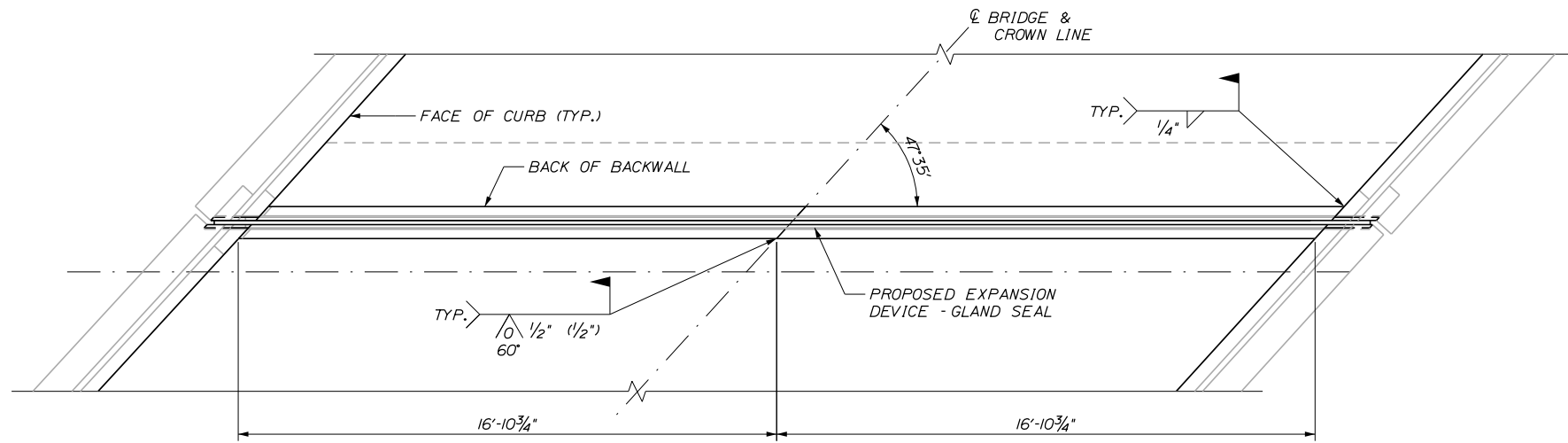
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
JOINT REPAIRS
FISHER FARM ROAD

SHEET NUMBER: S-03
CONTRACT: 2018.10
16 OF 28

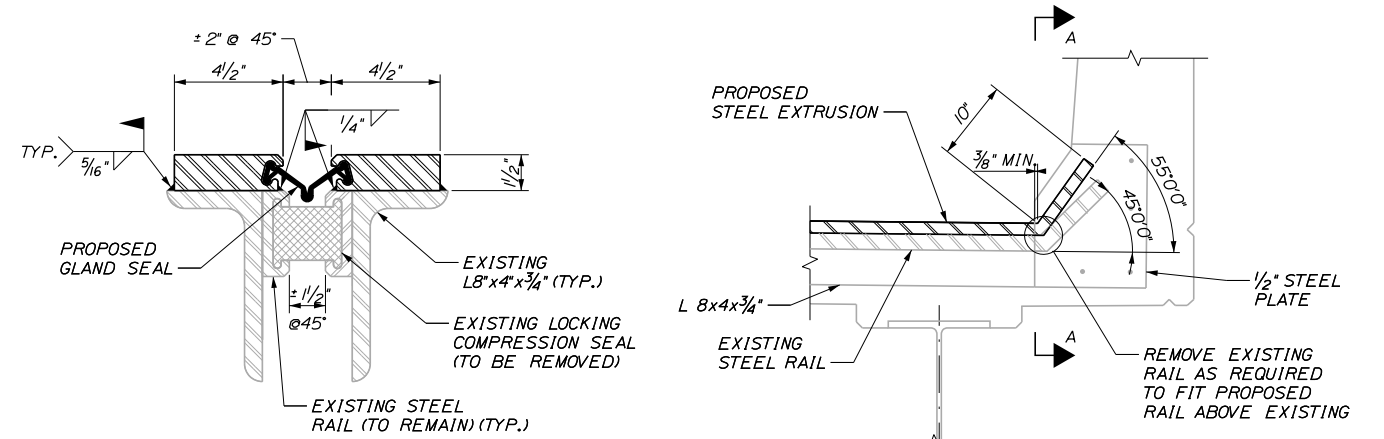
Date: 12/22/2017



ABUTMENT PLAN - CENTER ROAD
SCALE: 3/8" = 1'-0"

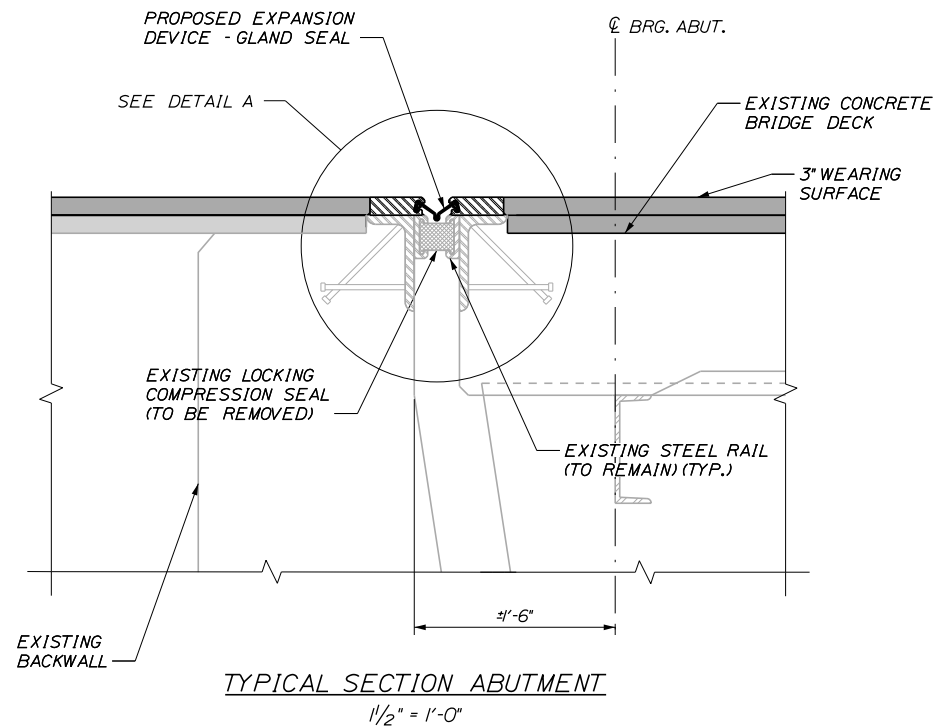
EXPANSION JOINT NOTES:

1. ALL STEEL COMPONENTS SHALL BE AASHTO M270 GRADE 36, UNLESS OTHERWISE NOTED.
2. CONTRACTOR SHALL FIELD MEASURE EXISTING CURB PLATE AND JOINT DIMENSIONS PRIOR TO DEVELOPMENT OF SHOP DRAWINGS.
3. EXPANSION JOINT MODIFICATIONS AT EACH ABUTMENT SHALL BE PAID UNDER ITEM 520.22II, EXPANSION DEVICE MODIFICATIONS.

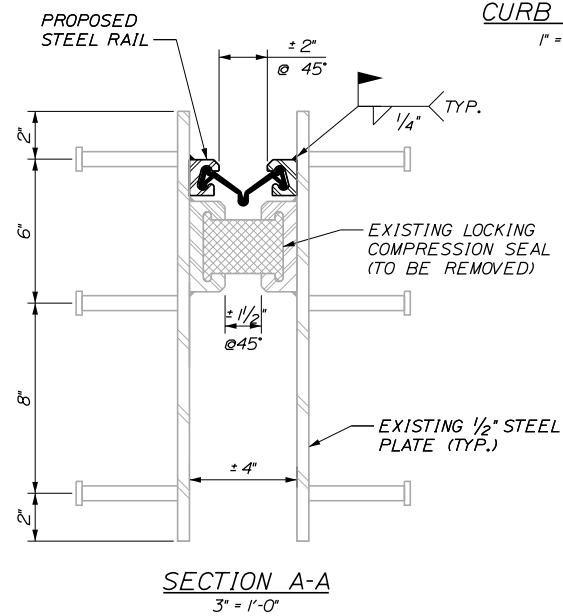


DETAIL A
3" = 1'-0"

CURB DETAIL
1" = 1'-0"



TYPICAL SECTION ABUTMENT
1/2" = 1'-0"



SECTION A-A
3" = 1'-0"

Scale: AS NOTED			
No.	Revision	By	Date

Designed by: TYLIN INTERNATIONAL					
CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	9/2017	Checked	NLB	9/2017
Drawn	SAM	9/2017	In Charge of	DSM	11/2017

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12 Northbrook Drive
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Falmouth, Maine 04105
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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
JOINT REPAIRS
CENTER ROAD

CONTRACT: 2018.10

SHEET NUMBER: S-04

17 OF 28

Filename: ...XXX_Joint_Repair_Center.dgn

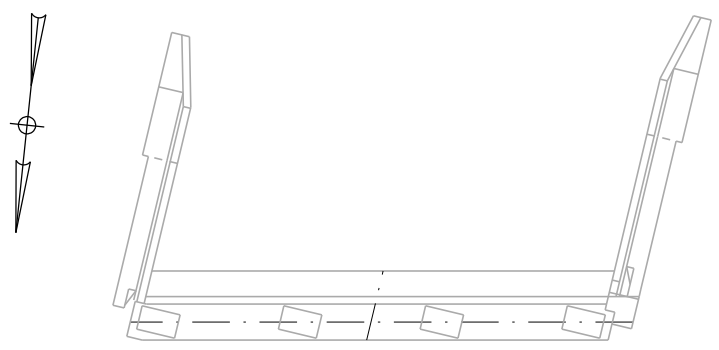
Date: 12/22/2017

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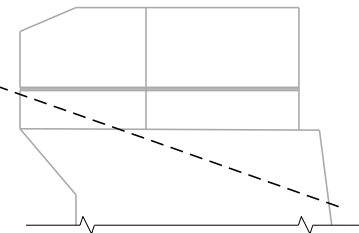
DANVILLE CORNER ROAD (MILE 75.8) ABUTMENTS ESTIMATED QUANTITY OF REPAIRS			
	ABUT. NO. 1	ABUT. NO. 2	TOTAL
ABUTMENT REPAIRS	0 S.F.	25 S.F.	25 S.F.

NOTE:
1. FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

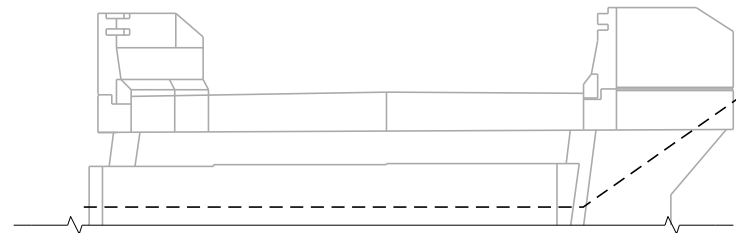
- LEGEND:**
- LIMIT OF CONCRETE SURFACE REPAIRS
 - LIMIT OF EPOXY INJECTED CRACK REPAIR
 - EXISTING CONCRETE
 - EXISTING AND PROPOSED GROUND



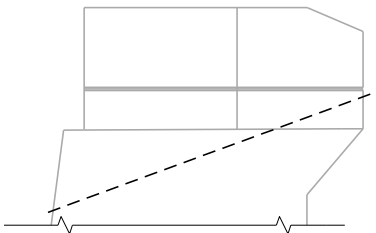
ABUTMENT NO. 1 PLAN



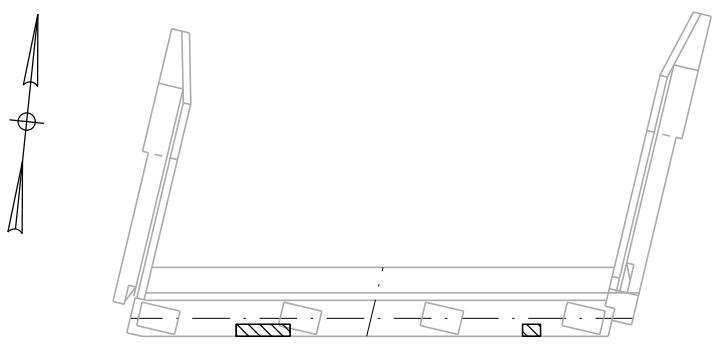
LEFT WING ELEVATION



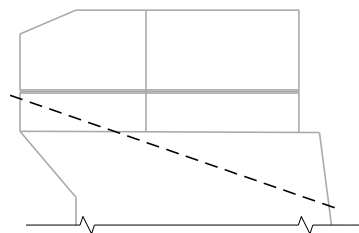
ABUTMENT NO. 1 ELEVATION



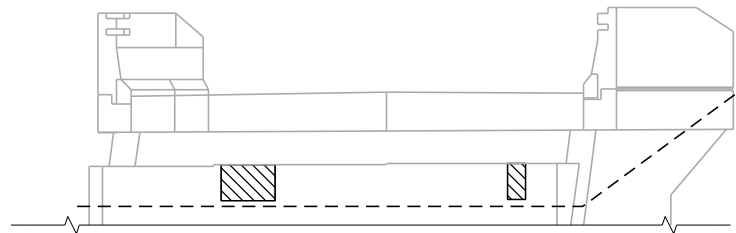
RIGHT WING ELEVATION



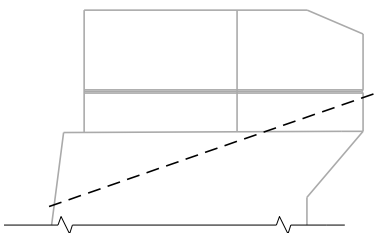
ABUTMENT NO. 2 PLAN



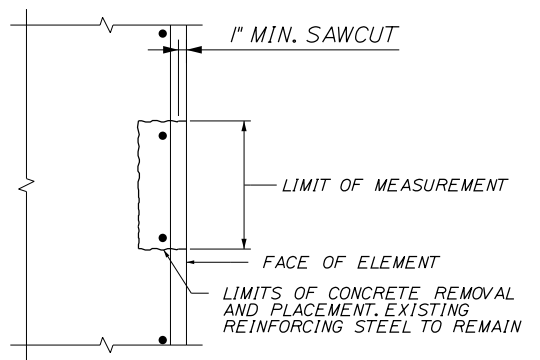
LEFT WING ELEVATION



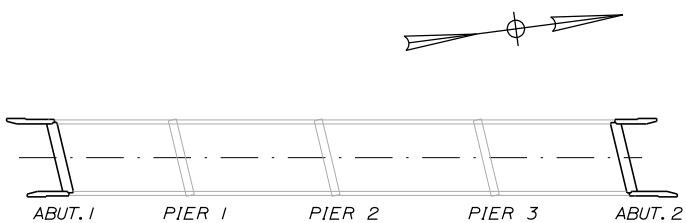
ABUTMENT NO. 2 ELEVATION



RIGHT WING ELEVATION



CONCRETE REPAIR DETAIL



LOCATION PLAN

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date	
Designed	DSM	8/2017	Checked	BLT	9/2017
Drawn	SAM	8/2017	In Charge of	DSM	11/2017

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 12 Northbrook Drive
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 Falmouth, Maine 04105
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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
 SUBSTRUCTURE REPAIRS MILE 75.8
 SHEET 1 OF 3





SHEET NUMBER: S-05
 CONTRACT: 2018.10
 18 OF 28

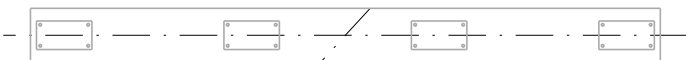
DANVILLE CORNER ROAD (MILE 75.8) PIERS ESTIMATED QUANTITY OF REPAIRS				
	PIER NO. 1	PIER NO. 2	PIER NO. 3	TOTAL
PIER REPAIRS	0 S.F.	25 S.F.	55 S.F.	80 S.F.

NOTES:

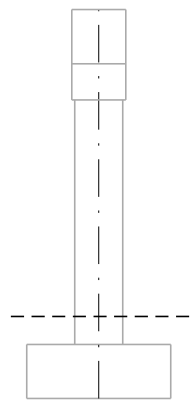
- FOR CONCRETE REPAIR DETAIL SEE SHEET 'SUBSTRUCTURE REPAIRS MILE 75.8 SHEET 1 OF 3'.
- FOR PROCEDURAL NOTES SEE SHEET 'GENERAL NOTES'

LEGEND:

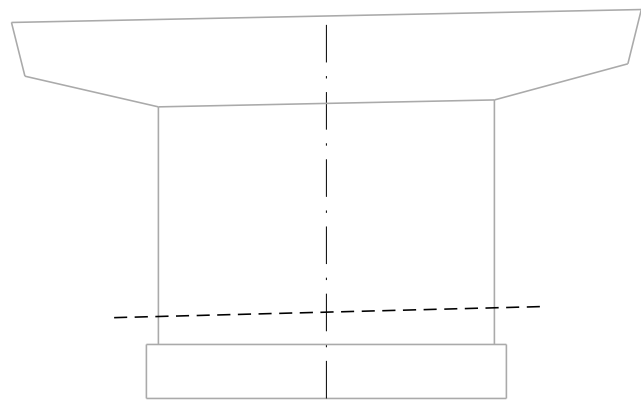
-  LIMIT OF CONCRETE SURFACE REPAIRS
-  LIMIT OF EPOXY INJECTED CRACK REPAIR
-  EXISTING CONCRETE
-  EXISTING AND PROPOSED GROUND



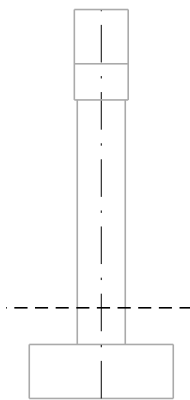
PLAN



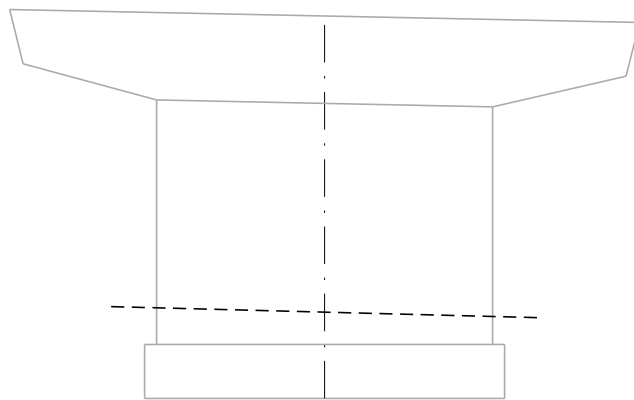
ELEVATION D



ELEVATION C



ELEVATION B

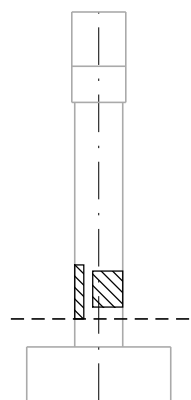


ELEVATION A

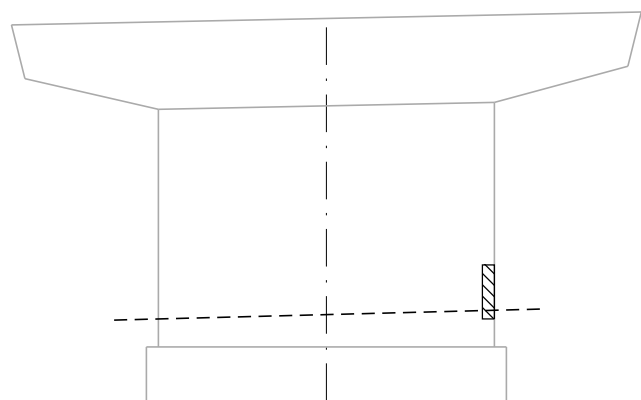
PIER 1



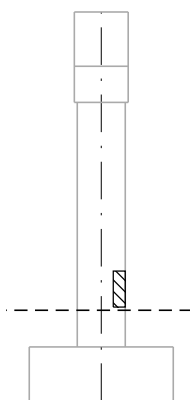
PLAN



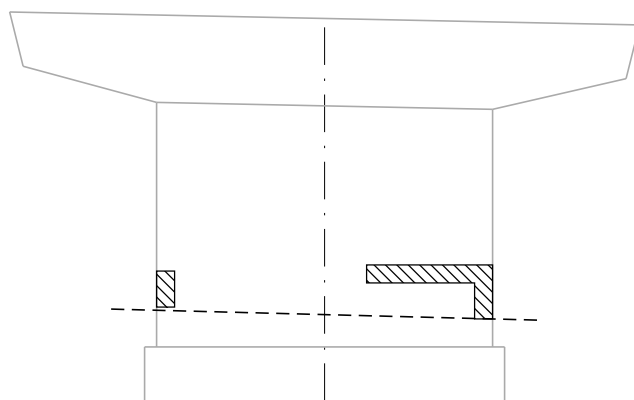
ELEVATION D



ELEVATION C

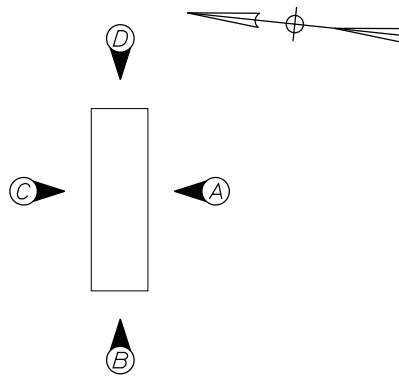


ELEVATION B

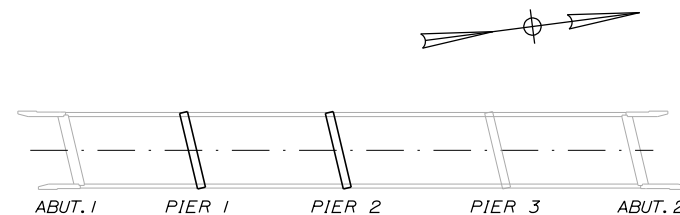


ELEVATION A

PIER 2



PIER ELEVATIONS



LOCATION PLAN

Scale: NOT TO SCALE

No.	Revision	By	Date


Designed by:

TY-LIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date	
Designed	DSM	8/2017	Checked	BLT	8/2017
Drawn	SAM	8/2017	In Charge of	DSM	11/2017

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
 SUBSTRUCTURE REPAIRS MILE 75.8
 SHEET 2 OF 3

SHEET NUMBER: S-06
 CONTRACT: 2018.10
 19 OF 28

Date: 12/22/2017

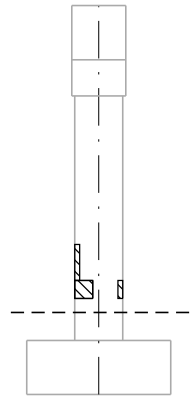
Filename: ...XXX_75-8_Sub_Repairs_2.dgn

Date: 12/22/2017

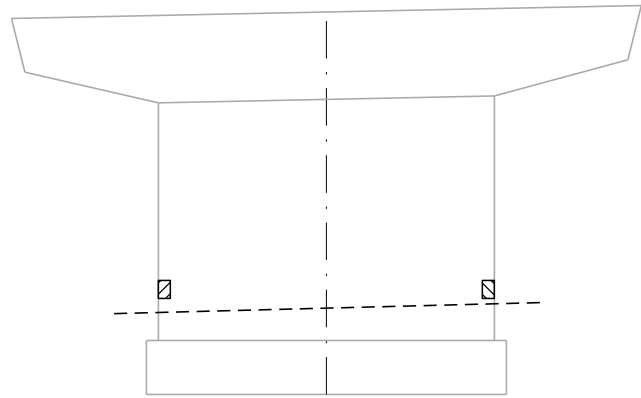
Filename: ...XXX_75-8_Sub_Repairs_3.dgn



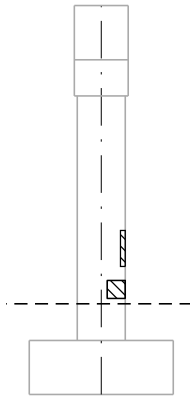
PLAN



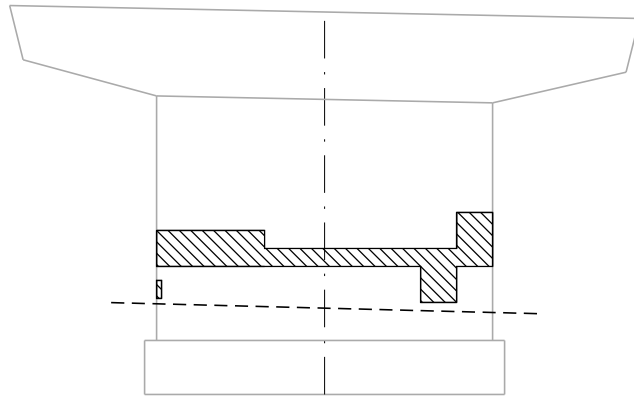
ELEVATION D



ELEVATION C



ELEVATION B

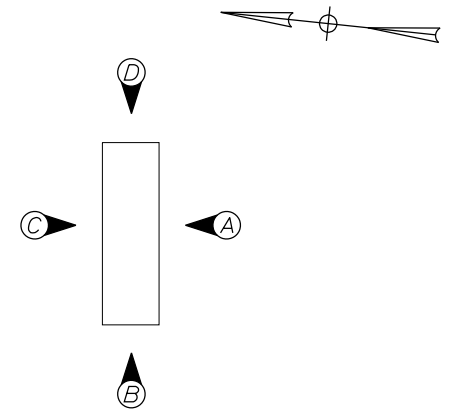


ELEVATION A

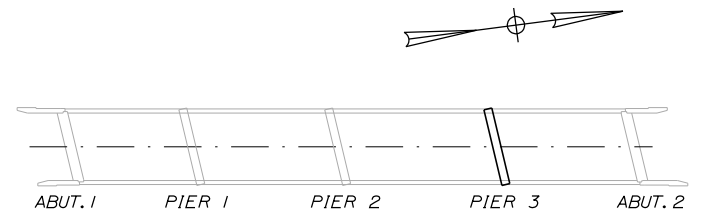
PIER 3

- NOTES:**
- FOR CONCRETE REPAIR DETAIL SEE SHEET "SUBSTRUCTURE REPAIRS MILE 75.8 SHEET 1 OF 3".
 - FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

- LEGEND:**
- LIMIT OF CONCRETE SURFACE REPAIRS
 - LIMIT OF EPOXY INJECTED CRACK REPAIR
 - EXISTING CONCRETE
 - EXISTING AND PROPOSED GROUND



PIER ELEVATIONS



LOCATION PLAN

Scale: NOT TO SCALE

No.	Revision	By	Date


Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date		By	Date
Designed	DSM	8/2017	Checked	BLT	8/2017
Drawn	SAM	8/2017	In Charge of	DSM	11/2017

T.Y. Lin International
 12 Northbrook Drive
 Building A, Suite One
 Falmouth, Maine 04105
 TEL: (207) 781-4721
 FAX: (207) 781-4753



THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
 SUBSTRUCTURE REPAIRS MILE 75.8
 SHEET 3 OF 3





CONTRACT: 2018.10 SHEET NUMBER: S-07 20 OF 28

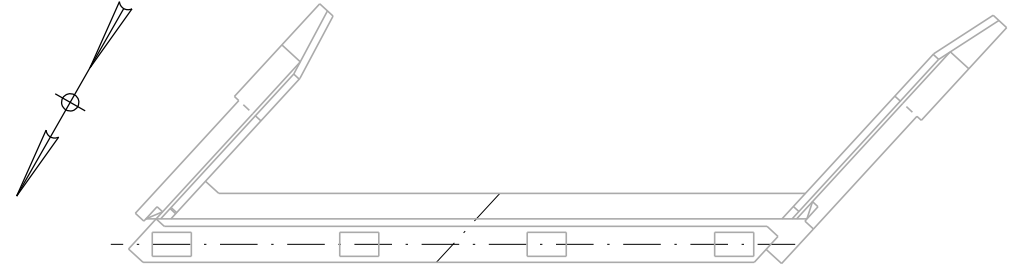
Date: 12/22/2017

Filename: ...XXX_87-5_Sub_Repairs_1.dgn

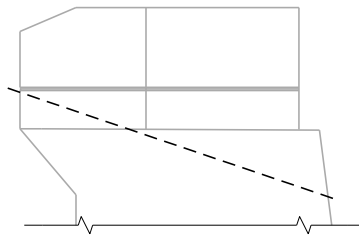
FISHER FARM ROAD (MILE 87.5) ABUTMENTS ESTIMATED QUANTITY OF REPAIRS			
	ABUT. NO. 1	ABUT. NO. 2	TOTAL
ABUTMENT REPAIRS	0 S.F.	25 S.F.	25 S.F.
EPOXY INJECTED CRACK REPAIRS	0 L.F.	10 L.F.	10 L.F.

NOTES:
 1. FOR CONCRETE REPAIR DETAIL SEE SHEET "SUBSTRUCTURE REPAIRS MILE 87.5 SHEET 1 OF 3".
 2. FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

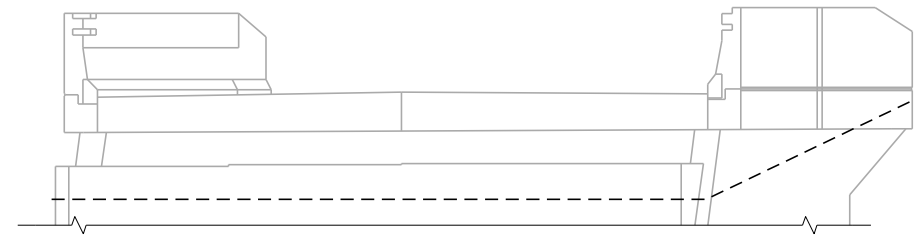
LEGEND:
 LIMIT OF CONCRETE SURFACE REPAIRS
 LIMIT OF EPOXY INJECTED CRACK REPAIR
 EXISTING CONCRETE
 EXISTING AND PROPOSED GROUND



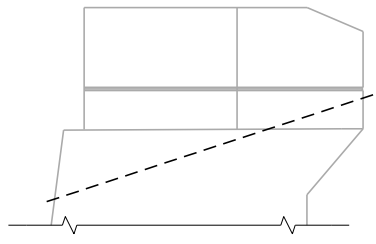
ABUTMENT NO. 1 PLAN



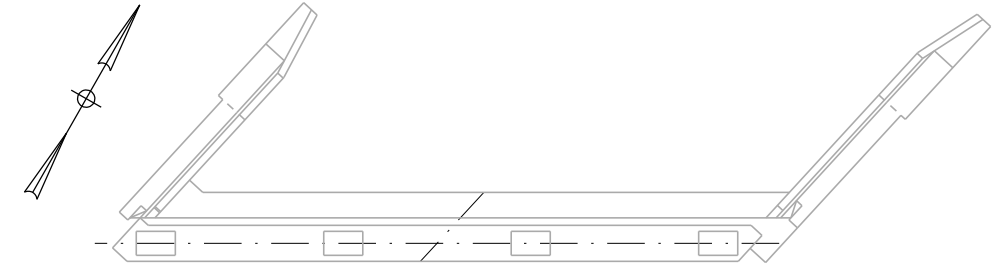
LEFT WING ELEVATION



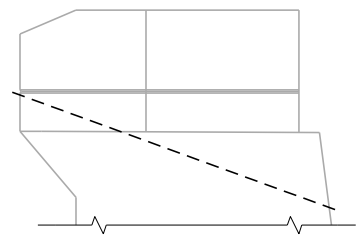
ABUTMENT NO. 1 ELEVATION



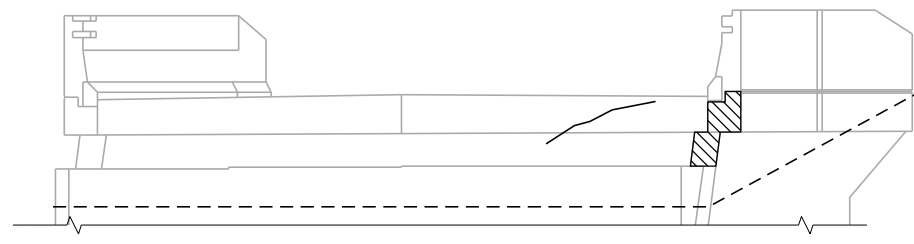
RIGHT WING ELEVATION



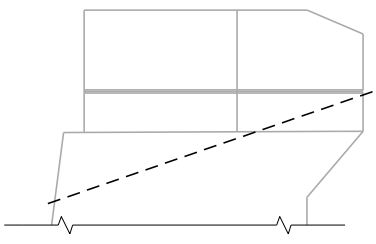
ABUTMENT NO. 2 PLAN



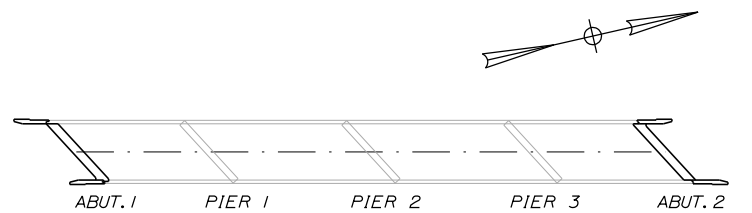
LEFT WING ELEVATION



ABUTMENT NO. 2 ELEVATION



RIGHT WING ELEVATION



LOCATION PLAN


Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:
TYLIN INTERNATIONAL
 CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date
Designed	DSM	8/2017	Checked	BLT 9/2017
Drawn	SAM	8/2017	In Charge of	DSM 11/2017

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 12 Northbrook Drive
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THE GOLD STAR MEMORIAL HIGHWAY
 MTA PROJECT MANAGER: R. NORWOOD





BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
 SUBSTRUCTURE REPAIRS MILE 87.5
 SHEET 1 OF 3
 SHEET NUMBER: S-08
 CONTRACT: 2018.10
 21 OF 28

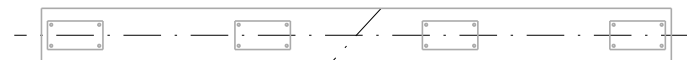
FISHER FARM ROAD (MILE 75.8) PIERS ESTIMATED QUANTITY OF REPAIRS				
	PIER NO. 1	PIER NO. 2	PIER NO. 3	TOTAL
PIER REPAIRS	85 S.F.	15 S.F.	0 S.F.	100 S.F.

NOTES:

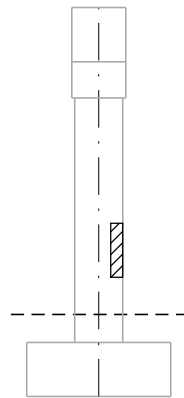
- FOR CONCRETE REPAIR DETAIL SEE SHEET "SUBSTRUCTURE REPAIRS MILE 87.5 SHEET 1 OF 3".
- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

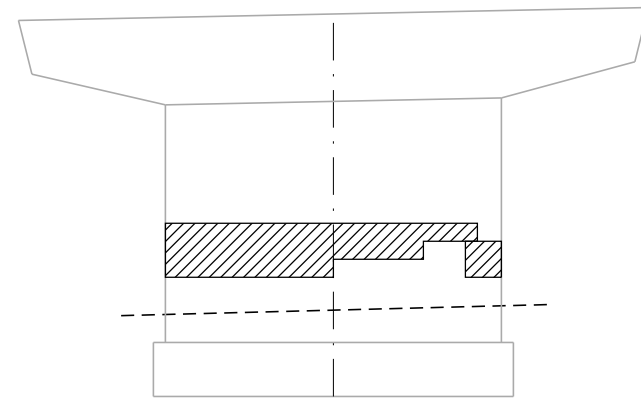
-  LIMIT OF CONCRETE SURFACE REPAIRS
-  LIMIT OF EPOXY INJECTED CRACK REPAIR
-  EXISTING CONCRETE
-  EXISTING AND PROPOSED GROUND



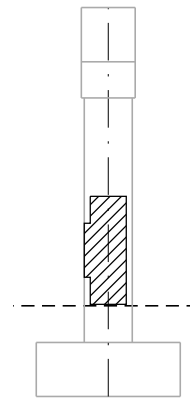
PLAN



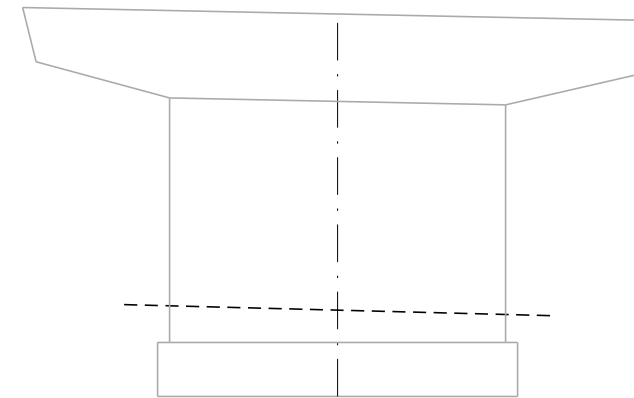
ELEVATION D



ELEVATION C

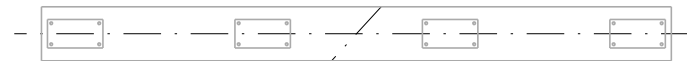


ELEVATION B

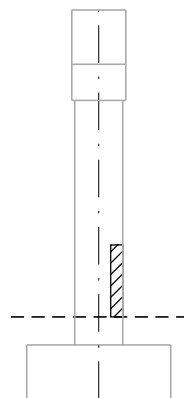


ELEVATION A

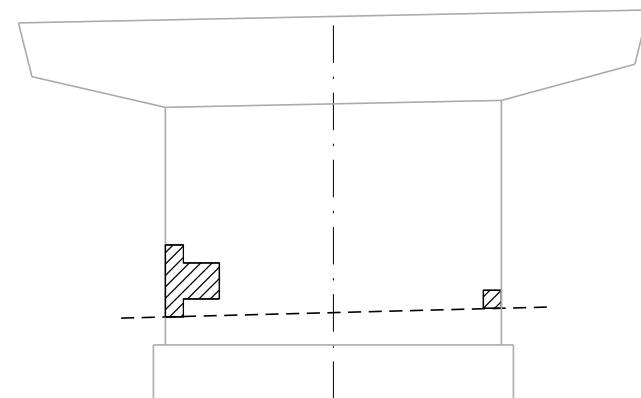
PIER 1



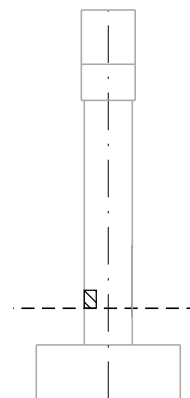
PLAN



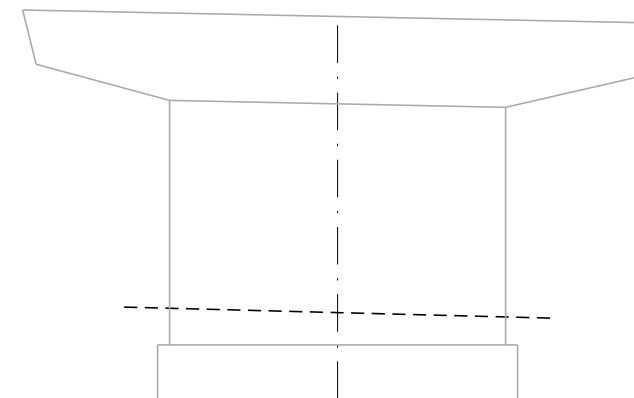
ELEVATION D



ELEVATION C

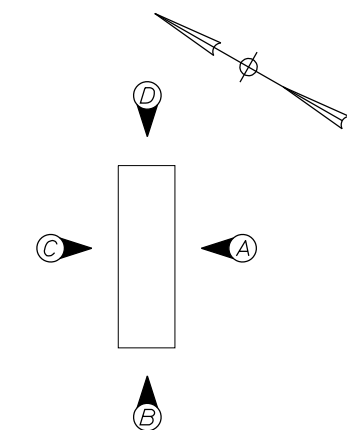


ELEVATION B

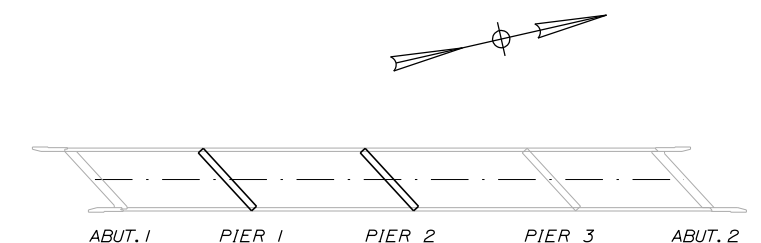


ELEVATION A

PIER 2



PIER ELEVATIONS



LOCATION PLAN

Scale: NOT TO SCALE

No.	Revision	By	Date

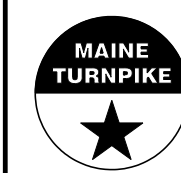
Designed by:

TY-LIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date	
Designed	DSM	8/2017	Checked	BLT	8/2017
Drawn	SAM	8/2017	In Charge of	DSM	11/2017

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**THE GOLD STAR
 MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
 SUBSTRUCTURE REPAIRS MILE 87.5
 SHEET 2 OF 3

CONTRACT: 2018.10 SHEET NUMBER: S-09 22 OF 28

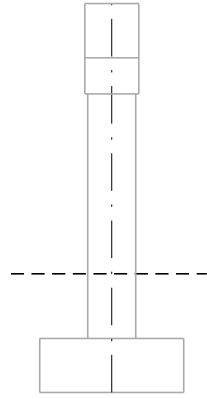
Date: 12/22/2017

Filename: ...XXX_87-5_Sub_Repairs_2.dgn

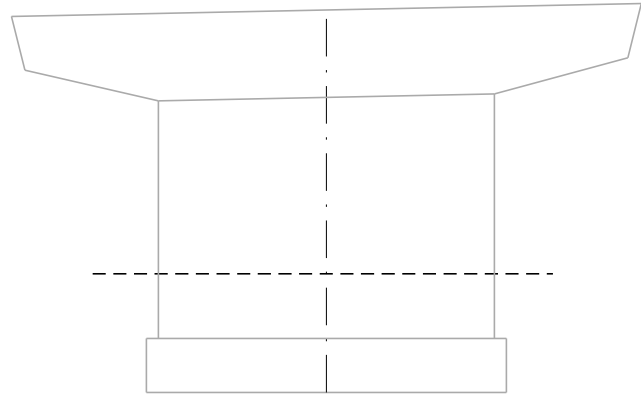
Date: 12/22/2017



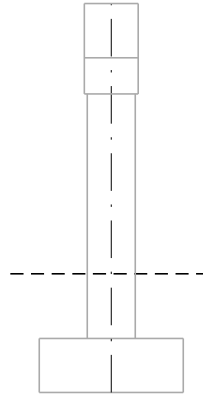
PLAN



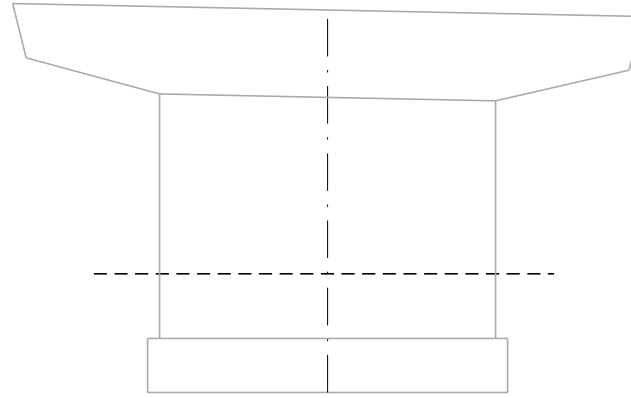
ELEVATION D



ELEVATION C



ELEVATION B



ELEVATION A

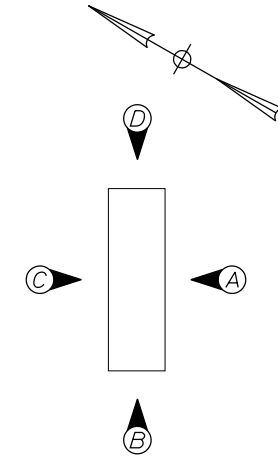
PIER 3

NOTES:

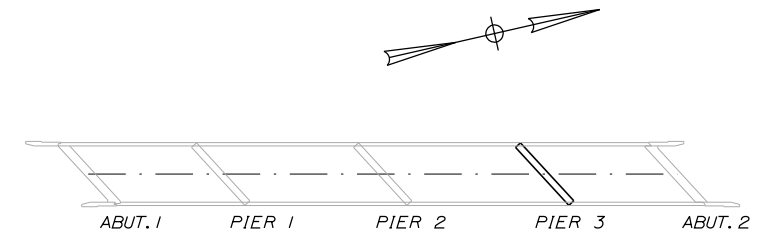
1. FOR CONCRETE REPAIR DETAIL SEE SHEET "SUBSTRUCTURE REPAIRS MILE 87.5 SHEET 1 OF 3".
2. FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

- LIMIT OF CONCRETE SURFACE REPAIRS
- LIMIT OF EPOXY INJECTED CRACK REPAIR
- EXISTING CONCRETE
- EXISTING AND PROPOSED GROUND



PIER ELEVATIONS



LOCATION PLAN

Scale: NOT TO SCALE

Designed by:

TYLIN INTERNATIONAL

T.Y. Lin International
 12 Northbrook Drive
 Building A, Suite One
 Falmouth, Maine 04105
 TEL: (207) 781-4721
 FAX: (207) 781-4753



**THE GOLD STAR
 MEMORIAL HIGHWAY**

BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
 SUBSTRUCTURE REPAIRS MILE 87.5
 SHEET 3 OF 3

No.	Revision	By	Date

CONSULTANT PROJECT MANAGER: Norman L. Baker					
	By	Date		By	Date
Designed	DSM	8/2017	Checked	BLT	8/2017
Drawn	SAM	8/2017	In Charge of	DSM	11/2017

MTA PROJECT MANAGER: R. NORWOOD

CONTRACT: 2018.10

SHEET NUMBER: S-10

23 OF 28

Filename: ...XXX_87-5_Sub_Repairs_3.dgn

Date: 12/22/2017





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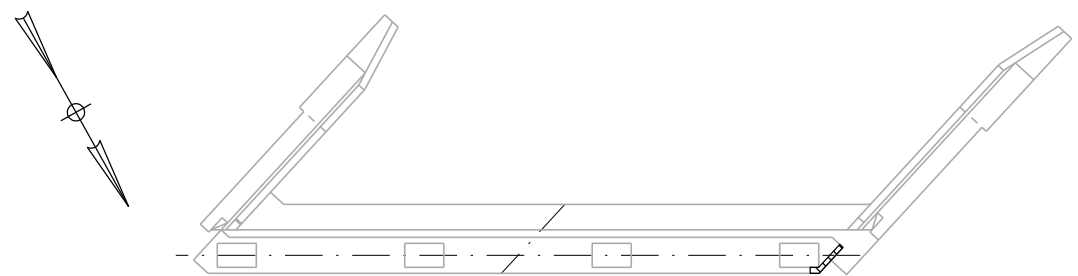
CENTER ROAD (MILE 91.9) ABUTMENTS ESTIMATED QUANTITY OF REPAIRS			
	ABUT. NO. 1	ABUT. NO. 2	TOTAL
ABUTMENT REPAIRS	5 S.F.	15 S.F.	20 S.F.
EPOXY INJECTED CRACK REPAIRS	0 L.F.	5 L.F.	5 L.F.

NOTES:

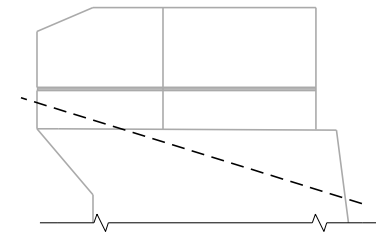
- FOR CONCRETE REPAIR DETAIL SEE SHEET "SUBSTRUCTURE REPAIRS MILE 91.9 SHEET 1 OF 3".
- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

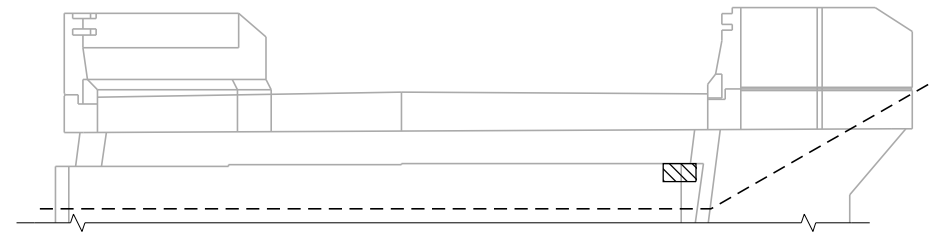
-  LIMIT OF CONCRETE SURFACE REPAIRS
-  LIMIT OF EPOXY INJECTED CRACK REPAIR
-  EXISTING CONCRETE
-  EXISTING AND PROPOSED GROUND



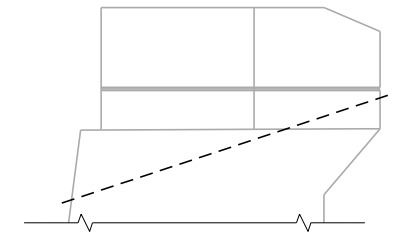
ABUTMENT NO. 1 PLAN



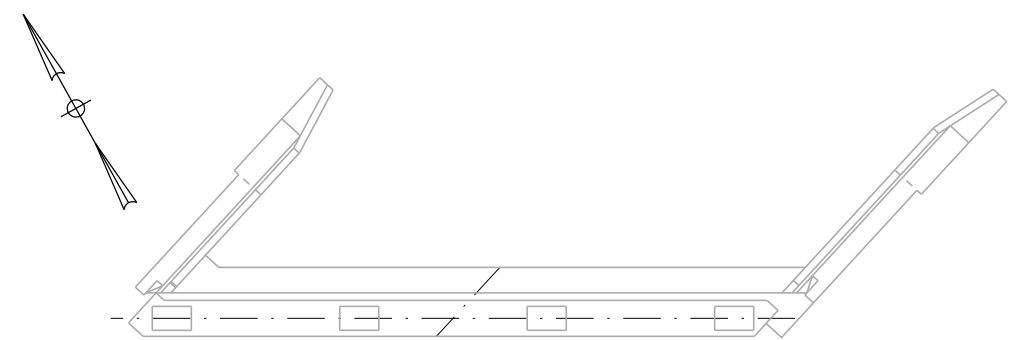
LEFT WING ELEVATION



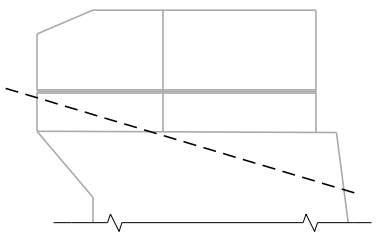
ABUTMENTS NO. 1 ELEVATION



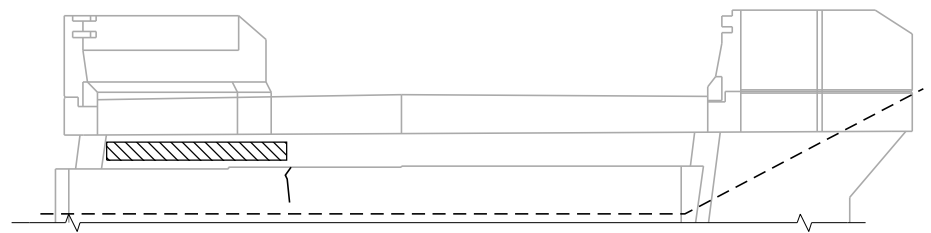
RIGHT WING ELEVATION



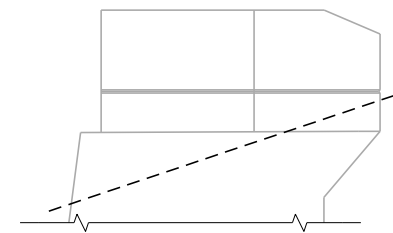
ABUTMENT NO. 2 PLAN



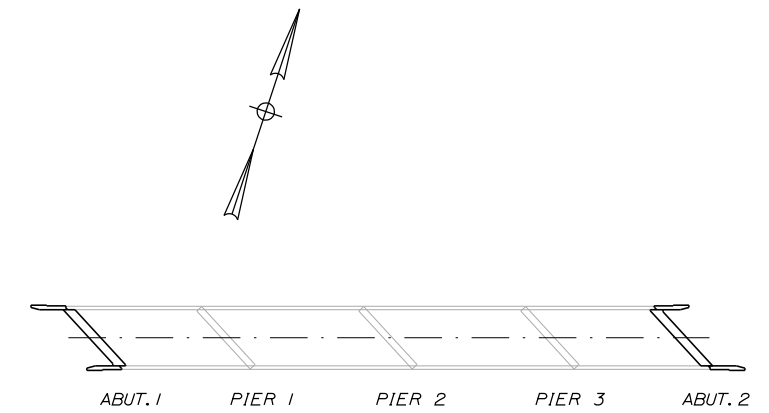
LEFT WING ELEVATION



ABUTMENTS NO. 2 ELEVATION



RIGHT WING ELEVATION



LOCATION PLAN

Scale: NOT TO SCALE

No.	Revision	By	Date


Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date	
Designed	DSM	8/2017	Checked	BLT	9/2017
Drawn	SAM	8/2017	In Charge of	DSM	11/2017

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**THE GOLD STAR
MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: R. NORWOOD

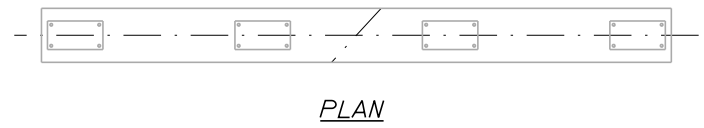
BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
 SUBSTRUCTURE REPAIRS MILE 91.9
 SHEET 1 OF 3

SHEET NUMBER: S-11
 CONTRACT: 2018.10
 24 OF 28

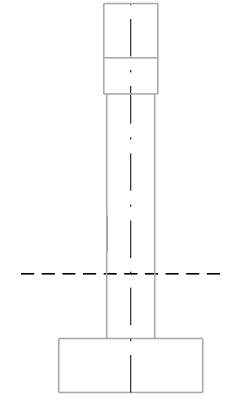
CENTER ROAD (MILE 91.9) PIERS ESTIMATED QUANTITY OF REPAIRS				
	PIER NO. 1	PIER NO. 2	PIER NO. 3	TOTAL
PIER REPAIRS	50 S.F.	10 S.F.	20 S.F.	80 S.F.

- NOTES:**
- FOR CONCRETE REPAIR DETAIL SEE SHEET "SUBSTRUCTURE REPAIRS MILE 91.9 SHEET 1 OF 3".
 - FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

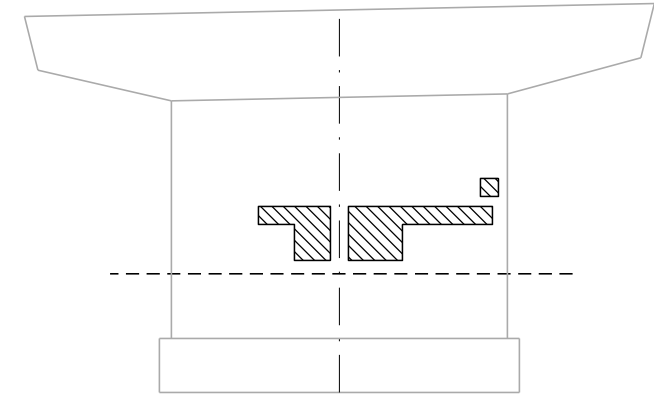
- LEGEND:**
- LIMIT OF CONCRETE SURFACE REPAIRS
 - LIMIT OF EPOXY INJECTED CRACK REPAIR
 - EXISTING CONCRETE
 - EXISTING AND PROPOSED GROUND



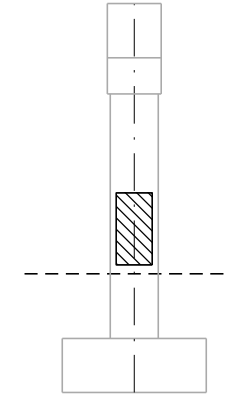
PLAN



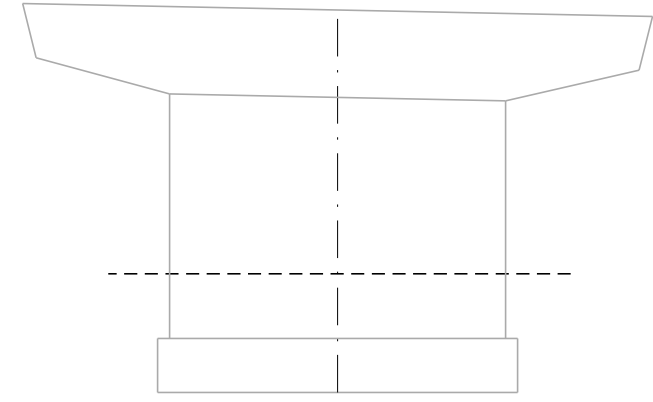
ELEVATION D



ELEVATION C

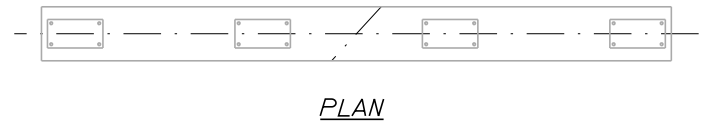


ELEVATION B

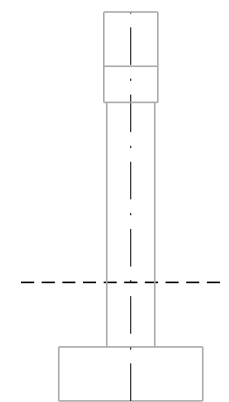


ELEVATION A

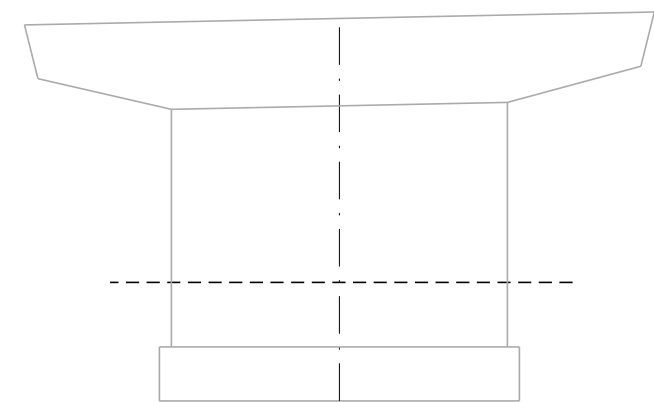
PIER 1



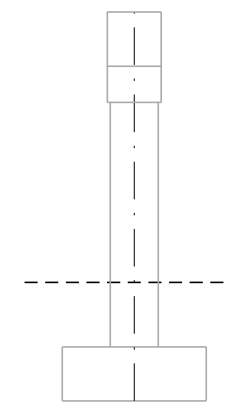
PLAN



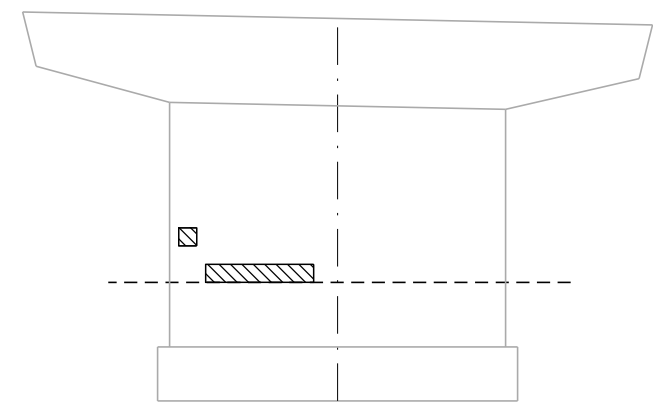
ELEVATION D



ELEVATION C

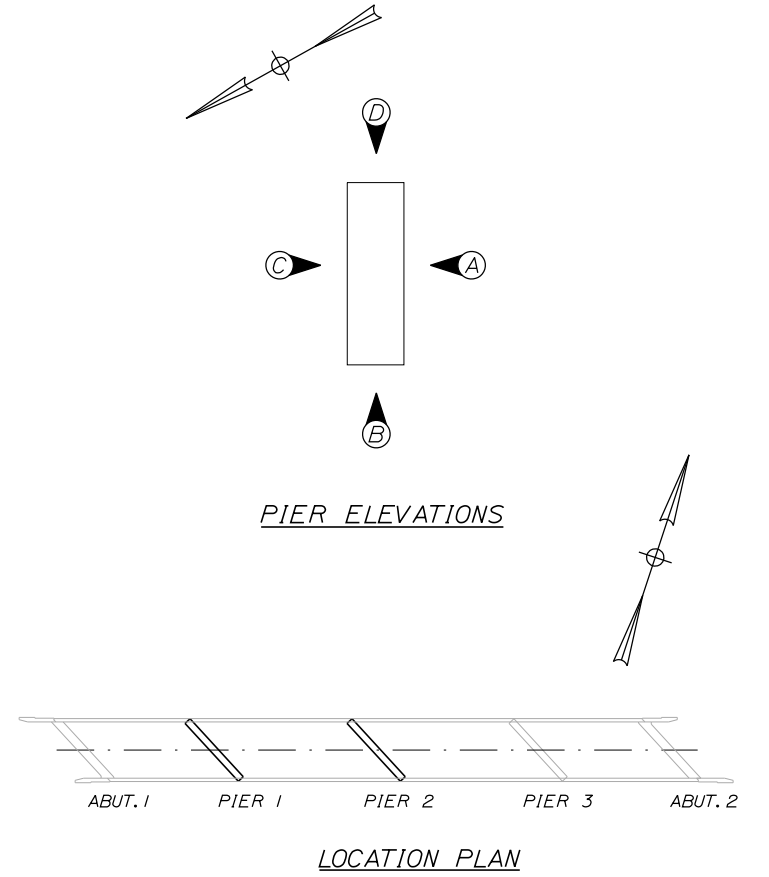


ELEVATION B



ELEVATION A

PIER 2



PIER ELEVATIONS

LOCATION PLAN

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

TY-LIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date	
Designed	DSM	8/2017	Checked	BLT	8/2017
Drawn	SAM	8/2017	In Charge of	DSM	11/2017

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THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
 SUBSTRUCTURE REPAIRS MILE 91.9
 SHEET 2 OF 3

SHEET NUMBER: S-12
 CONTRACT: 2018.10
 25 OF 28

Date: 12/22/2017

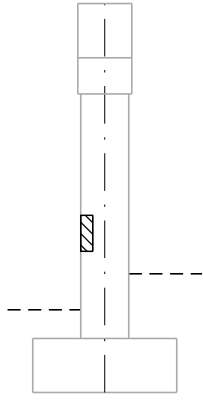
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Date: 12/22/2017

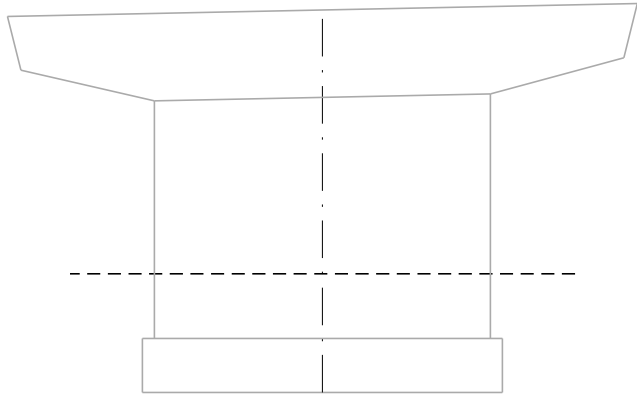
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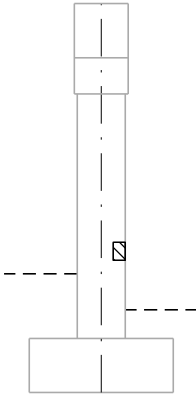
PLAN



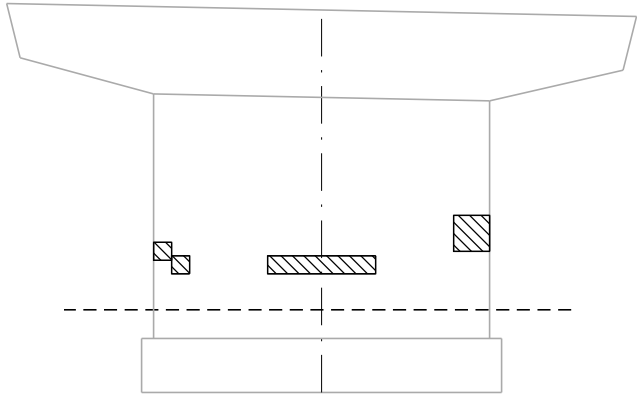
ELEVATION D



ELEVATION C



ELEVATION B



ELEVATION A

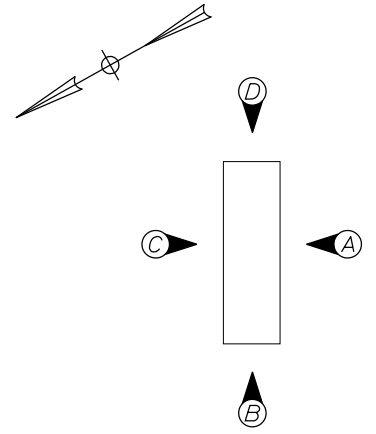
PIER 3

NOTES:

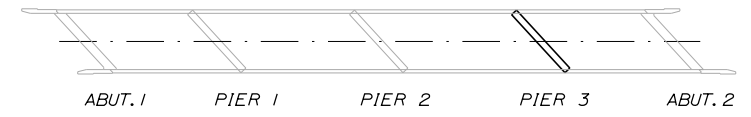
1. FOR CONCRETE REPAIR DETAIL SEE SHEET "SUBSTRUCTURE REPAIRS MILE 91.9 SHEET 1 OF 3".
2. FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"

LEGEND:

- LIMIT OF CONCRETE SURFACE REPAIRS
- LIMIT OF EPOXY INJECTED CRACK REPAIR
- EXISTING CONCRETE
- EXISTING AND PROPOSED GROUND



PIER ELEVATIONS



LOCATION PLAN

Scale: NOT TO SCALE

No.	Revision	By	Date


Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date		By	Date
Designed	DSM	8/2017	Checked	BLT	8/2017
Drawn	SAM	8/2017	In Charge of	DSM	11/2017

T.Y. Lin International
 12 Northbrook Drive
 Building A, Suite One
 Falmouth, Maine 04105
 TEL: (207) 781-4721
 FAX: (207) 781-4753



**THE GOLD STAR
 MEMORIAL HIGHWAY**

MTA PROJECT MANAGER: R. NORWOOD

BRIDGE AND CULVERT REPAIRS
 4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
 SUBSTRUCTURE REPAIRS MILE 91.9
 SHEET 3 OF 3

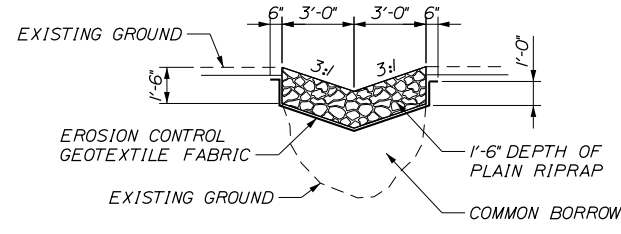
CONTRACT: 2018.10

SHEET NUMBER: S-13

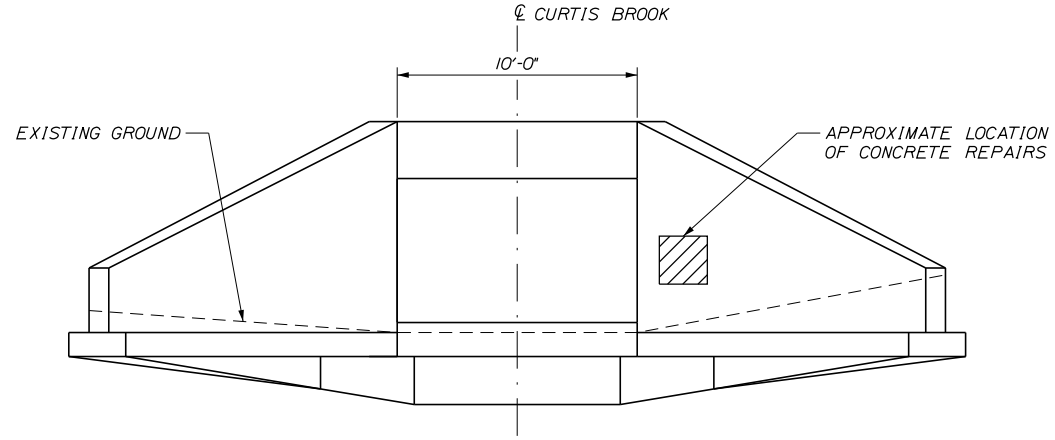
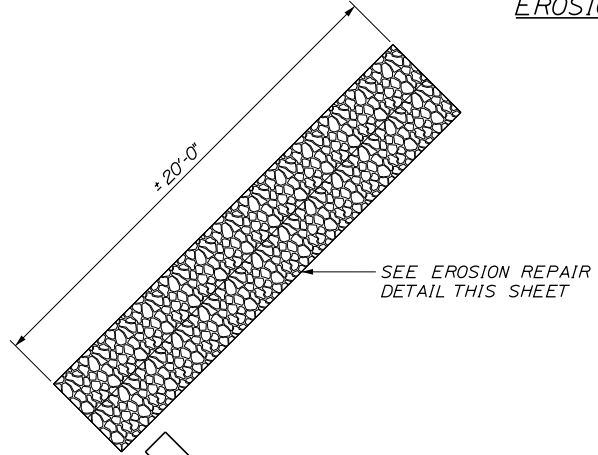
26 OF 28

Date: 12/22/2017

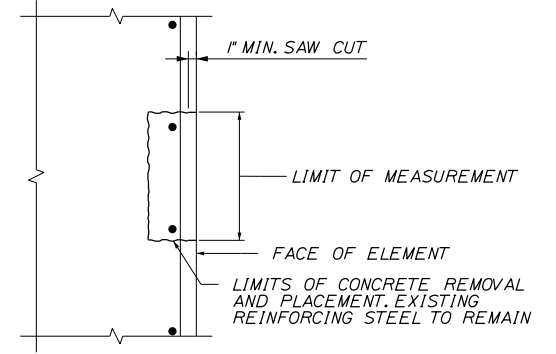
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EROSION REPAIR DETAIL
SCALE: 1/4" = 1'-0"

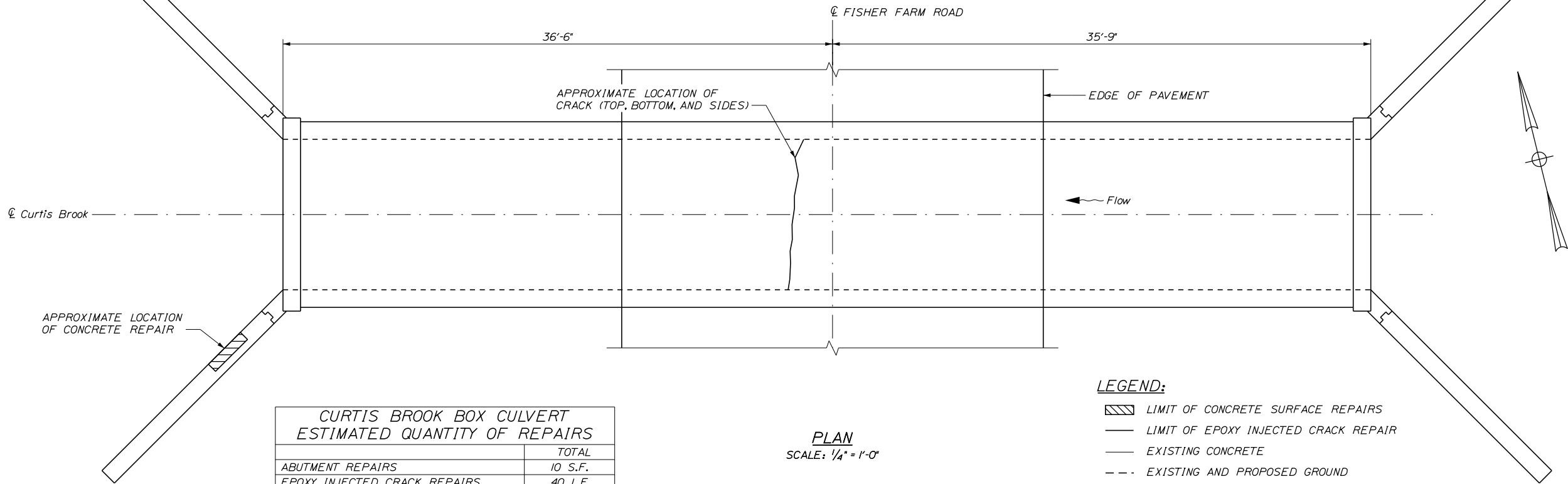


DOWNSTREAM END VIEW
SCALE: 1/4" = 1'-0"



CONCRETE REPAIR DETAIL

- NOTES:**
- FOR PROCEDURAL NOTES SEE SHEET "GENERAL NOTES"
 - COFFERDAMS INTERNAL TO THE CULVERT WILL BE ALLOWED TO DIRECT FLOW AROUND CRACK AREA BEING REPAIRED. COFFERDAMS SHALL BE CONSTRUCTED ONLY OUT OF WOOD. SAND BAGS OR SIMILAR WILL NOT BE PERMITTED.



CURTIS BROOK BOX CULVERT ESTIMATED QUANTITY OF REPAIRS

	TOTAL
ABUTMENT REPAIRS	10 S.F.
EPOXY INJECTED CRACK REPAIRS	40 L.F.

PLAN
SCALE: 1/4" = 1'-0"

- LEGEND:**
- LIMIT OF CONCRETE SURFACE REPAIRS
 - LIMIT OF EPOXY INJECTED CRACK REPAIR
 - EXISTING CONCRETE
 - EXISTING AND PROPOSED GROUND

Scale: AS NOTED

No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

	By	Date	By	Date	
Designed	DSM	9/2017	Checked	NLB	9/2017
Drawn	SAM	9/2017	In Charge of	DSM	11/2017

T.Y. Lin International
12 Northbrook Drive
Building A, Suite One
Falmouth, Maine 04105
TEL: (207) 781-4721
FAX: (207) 781-4753

MAINE TURNPIKE

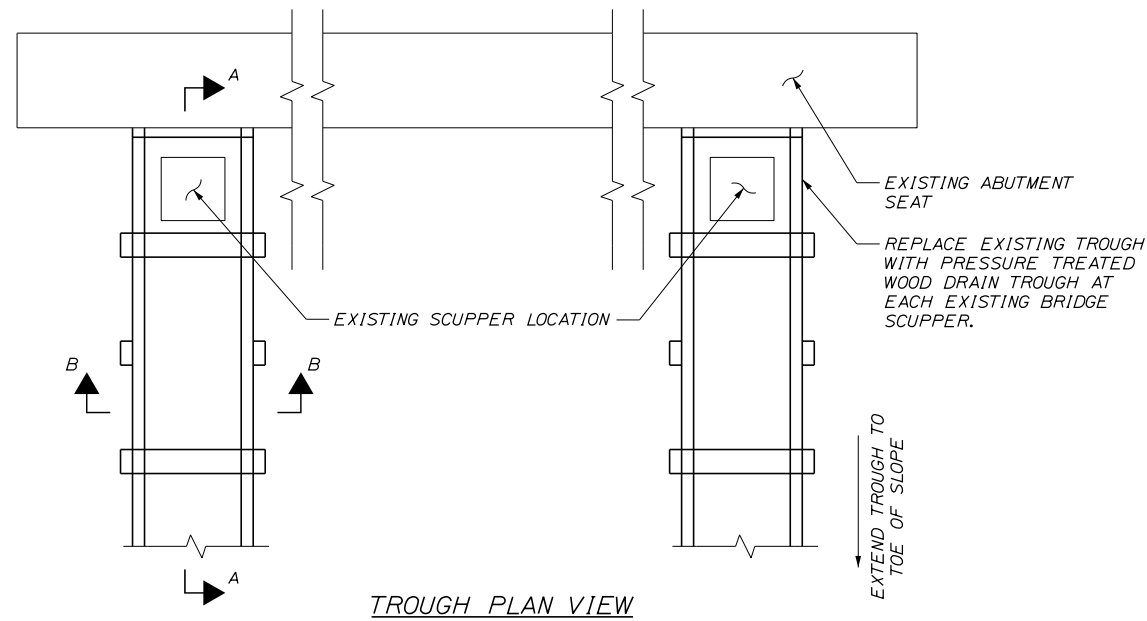
THE GOLD STAR MEMORIAL HIGHWAY

MTA PROJECT MANAGER: R. NORWOOD

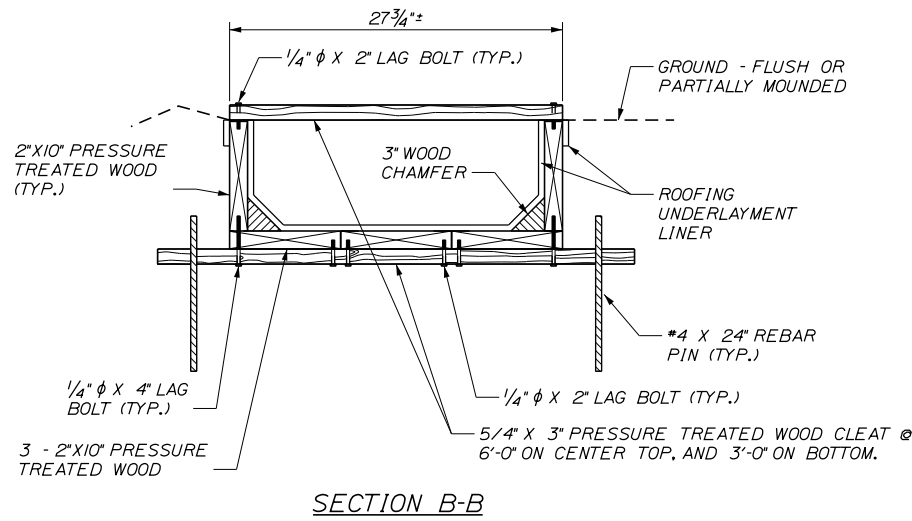
BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)
CURTIS BROOK CULVERT REPAIRS
PLAN AND DETAILS

SHEET NUMBER: S-14
CONTRACT: 2018.10
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Date: 12/22/2017



TROUGH PLAN VIEW

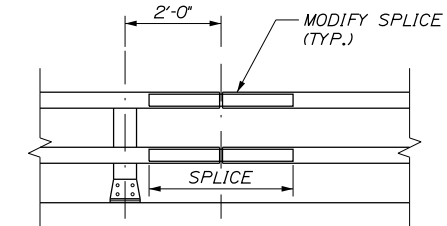


SECTION B-B

TROUGH NOTES:

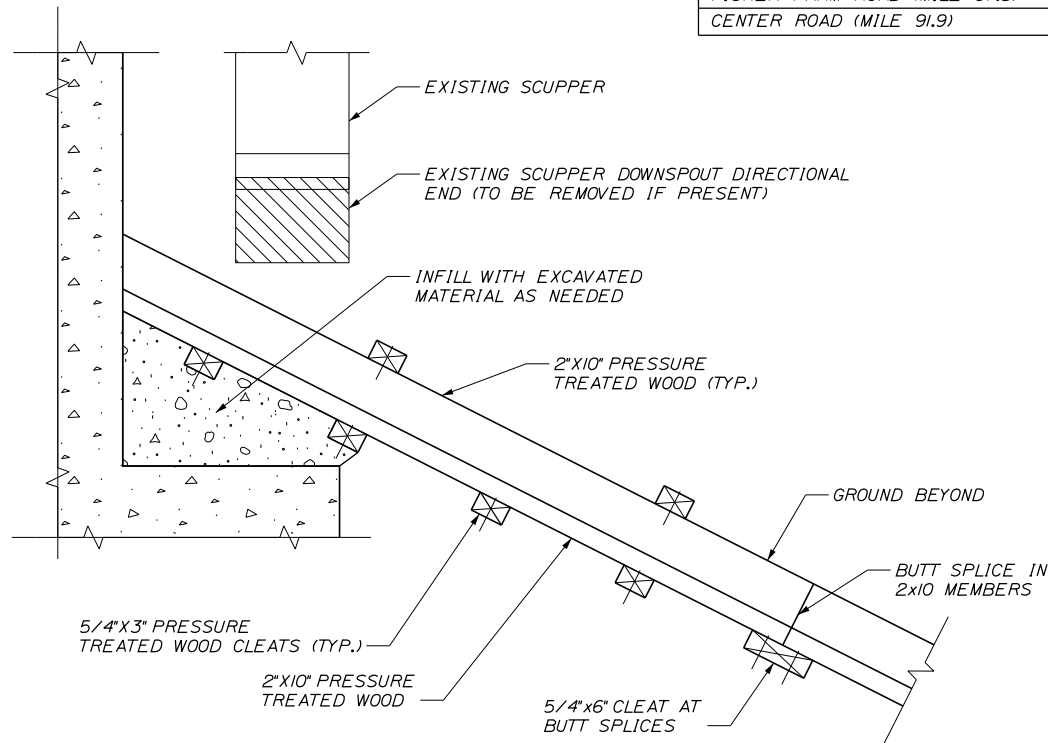
- WOOD DRAIN TROUGH AND ROOFING UNDERLAYMENT LINER SHALL BE PAID UNDER ITEM 603.91, "PRESSURE TREATED WOOD DRAIN TROUGH."
- REMOVAL SHALL BE PAID UNDER ITEM 202.191, "REMOVING EXISTING DRAIN TROUGHS".
- MECHANICALLY ATTACH WOOD DOWNSPOUT TO ABUTMENT WITH TWO 3/8" φ X 6" BOLTS INTO DROP IN ANCHORS.
- BUTT SPLICE 2"x10" PRESSURE TREATED WOOD MEMBERS AT CLEATS (PROVIDE 5/4"x6" CLEATS AT BUTT SPLICES); OR USE 2"x10"x16" PRESSURE TREATED WOOD SPLICE BOARDS. SPIKE WITH 10-P GALVANIZED NAILS OR SCREWS.
- INSTALL ROOFING UNDERLAYMENT (ICE AND WATER SHIELD) LINER AND SECURE WITH GALVANIZED ROOFING NAILS PRIOR TO INSTALLING TOP CLEATS.
- AT LOCATIONS SPECIFIED, NO REPLACEMENT TROUGH SHALL BE INSTALLED. REGRADE EXISTING SLOPE AS DIRECTED BY THE RESIDENT. PAYMENT SHALL BE INCIDENTAL TO ITEM 202.191, REMOVING EXISTING DRAIN TROUGHS.
- AT LOCATIONS SPECIFIED, THE EXISTING PRECAST CONCRETE DRAIN TROUGH SHALL REMAIN. RESEAL JOINTS AS DIRECTED BY THE RESIDENT. PAYMENT SHALL BE INCIDENTAL TO THE CONTRACT.
- DISTURBED SLOPES BENEATH THE BRIDGE THAT ARE UNLIKELY TO GROW SIGNIFICANT VEGETATION DUE TO SHADE SHALL BE COVERED WITH EROSION CONTROL MIX AS DIRECTED BY THE RESIDENT.

LOCATION OF DRAIN TROUGH WORK TABLE		
LOCATION	ABUTMENT 1	ABUTMENT 2
DANVILLE CORNER ROAD (MILE 75.8)	REMOVE & REGRADE SLOPE (NOTE 6)	REPAIR EXISTING (NOTE 7)
FISHER FARM ROAD (MILE 87.5)	REMOVE & REPLACE (NOTES 1-5)	REMOVE & REGRADE SLOPE (NOTE 6)
CENTER ROAD (MILE 91.9)	REMOVE & REGRADE SLOPE (NOTE 6)	REPAIR EXISTING (NOTE 7)

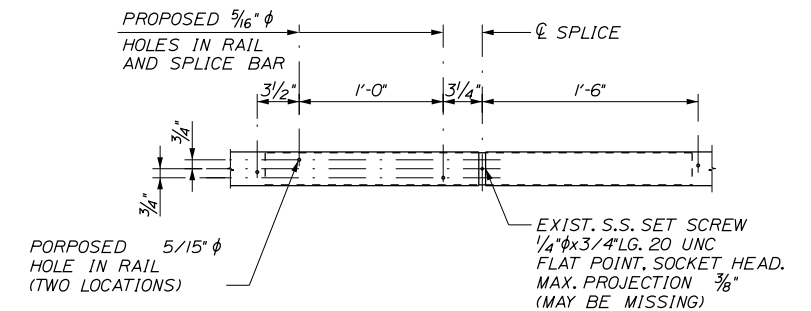


SPlice MODIFICATION RAILING - ELEVATION

SCALE: 1/2" = 1'-0"
(TWO BAR RAILING SHOWN,
THREE BAR RAILING SIMILAR)



SECTION A-A



SPlice MODIFICATION DETAIL

SCALE: 1/2" = 1'-0"
(ELEVATION VIEW FROM BACK SIDE OF RAIL)

Scale: NOT TO SCALE

No.	Revision	By	Date

Designed by:

TYLIN INTERNATIONAL

CONSULTANT PROJECT MANAGER: Norman L. Baker

By	Date	By	Date
Designed DSM	9/2017	Checked NLB	9/2017
Drawn SAM	9/2017	In Charge of DSM	11/2017

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**THE GOLD STAR
MEMORIAL HIGHWAY**

BRIDGE AND CULVERT REPAIRS
4 LOCATIONS (MILE 75.8, 87.49, 87.5, & 91.9)

TYPICAL DETAILS

CONTRACT: 2018.10

SHEET NUMBER: S-15

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Filename: ... \MSTA\XXX_Typical_Details.dgn